

CHALLENGER COMMUNITIES, LLC
BENT GRASS RESIDENTIAL FILING NO. 2
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD - FALCON, CO
CONSTRUCTION DOCUMENTS

PROJECT CONTACTS

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EL PASO COUNTY, PLANNING & COMMUNITY DEVELOPMENT
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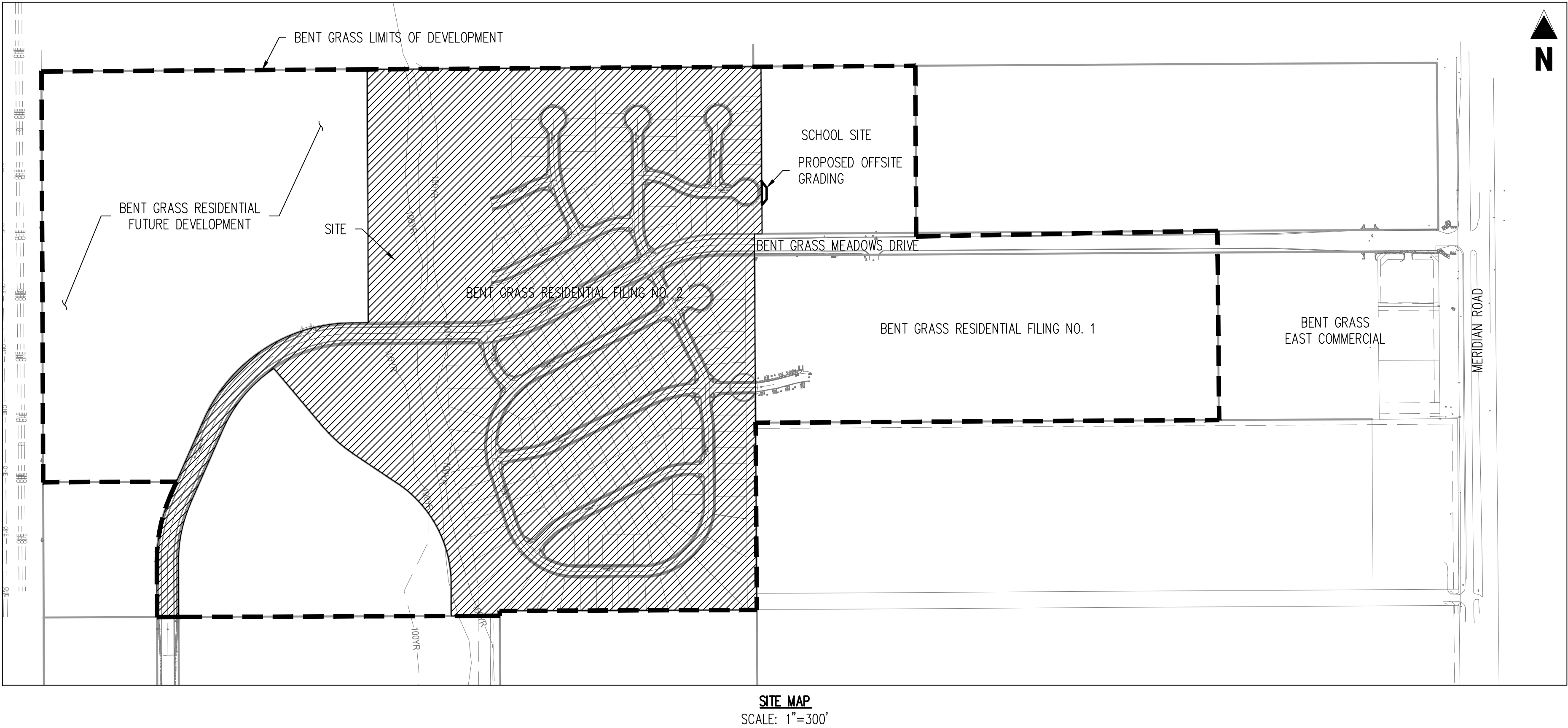
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WATER/WASTEWATER
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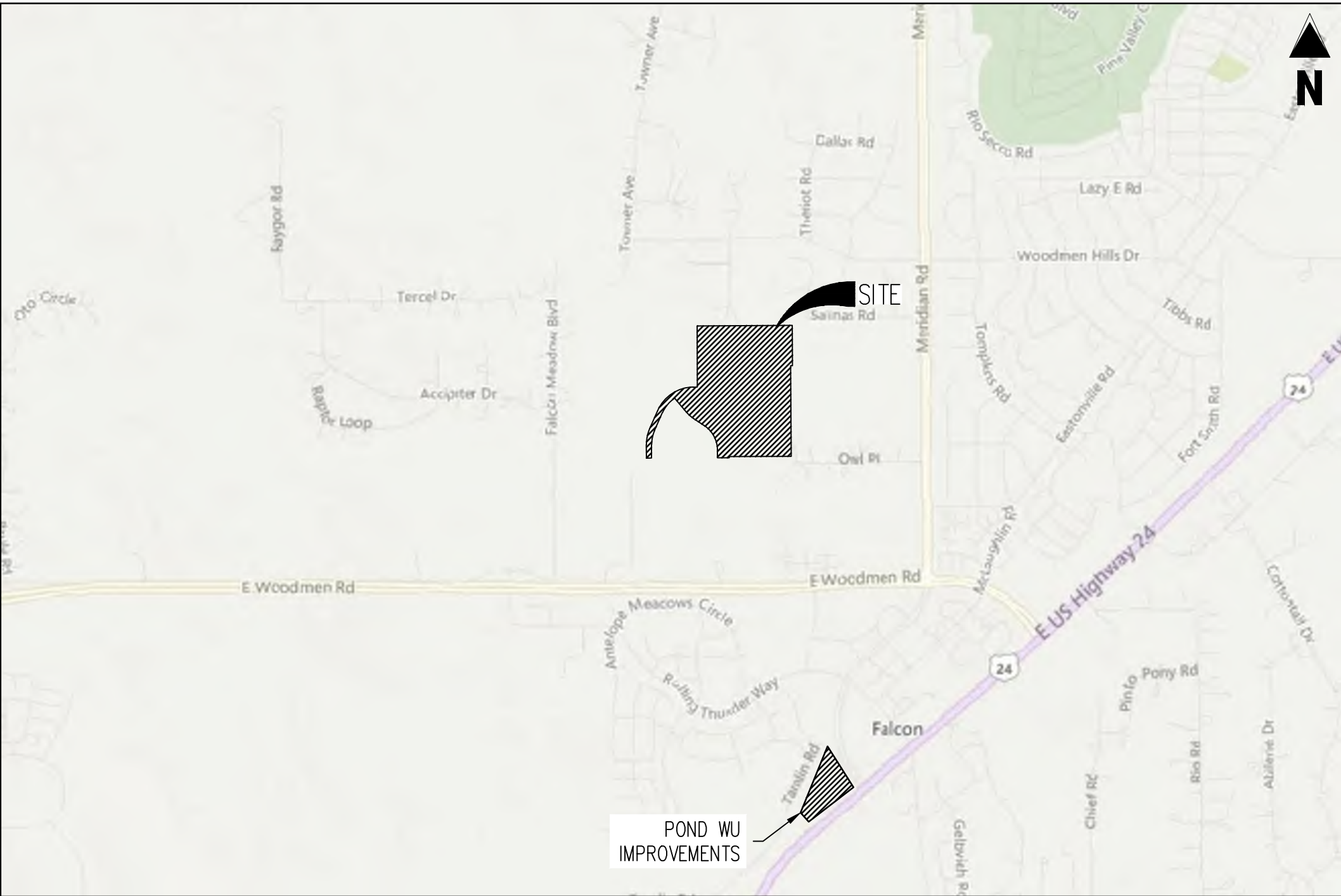
GAS
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FIRE DISTRICT
FALCON FIRE PROTECTION DISTRICT
7030 OLD MERIDIAN ROAD
PETTIN, COLORADO 80831
TEL: (719) 495-4050
EMAIL: FALCONFIRE@FALCONFIREPD.ORG



SITE MAP
SCALE: 1"=300'



VICINITY MAP
1"=2,000'

Engineering Review
03/13/2020 9:50:56 AM
dsdrice
JeffRice@elpasoco.com
(719) 520-7877
EPC Planning & Community
Development Department

ENGINEER'S STATEMENT

THESE DETAILED PLANS AND SPECIFICATIONS WERE PREPARED UNDER MY DIRECTION AND SUPERVISION. SAID PLANS AND SPECIFICATIONS HAVE BEEN PREPARED ACCORDING TO THE CRITERIA ESTABLISHED BY THE COUNTY FOR DETAILED ROADWAY, DRAINAGE, GRADING AND EROSION CONTROL PLANS AND SPECIFICATIONS, AND SAID PLANS AND SPECIFICATIONS ARE IN CONFORMITY WITH APPLICABLE MASTER DRAINAGE PLANS AND MASTER TRANSPORTATION PLANS. SAID PLANS AND SPECIFICATIONS MEET THE PURPOSES FOR WHICH THE PARTICULAR ROADWAY AND DRAINAGE FACILITIES ARE DESIGNED AND ARE CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. I ACCEPT RESPONSIBILITY FOR ANY LIABILITY CAUSED BY ANY NEGLIGENT ACTS, ERRORS OR OMISSIONS ON MY PART IN PREPARATION OF THESE DETAILED PLANS AND SPECIFICATIONS.

Donald G. Derrick
DONALD G. DERRICK, COLORADO P.E. NO. 0051622
2/10/20
DATE

OWNER'S STATEMENT

I, THE OWNER/DEVELOPER HAVE READ AND WILL COMPLY WITH THE REQUIREMENTS OF THE GRADING AND EROSION CONTROL PLAN AND ALL OF THE REQUIREMENT SPECIFIED IN THESE DETAILED PLANS AND SPECIFICATIONS.

Lee Eisenheim
LEE EISENHEIM, CHALLENGER HOMES, INC.
2-10-20
DATE

EL PASO COUNTY

COUNTY PLAN REVIEW IS PROVIDED ONLY FOR GENERAL CONFORMANCE WITH COUNTY DESIGN CRITERIA. THE COUNTY IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND/OR ELEVATIONS WHICH SHALL BE CONFIRMED AT THE JOB SITE. THE COUNTY THROUGH THE APPROVAL OF THIS DOCUMENT ASSUMES NO RESPONSIBILITY FOR COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT. FILED IN ACCORDANCE WITH THE REQUIREMENTS OF THE EL PASO COUNTY LAND DEVELOPMENT CODE, DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2, AND ENGINEERING CRITERIA MANUAL AS AMENDED.

IN ACCORDANCE WITH EDCM SECTION 1.12, THESE CONSTRUCTION DOCUMENTS WILL BE VALID FOR CONSTRUCTION FOR A PERIOD OF 2 YEARS FROM THE DATE SIGNED BY THE EL PASO COUNTY ENGINEER. IF CONSTRUCTION HAS NOT STARTED WITHIN THOSE 2 YEARS, THE PLANS WILL NEED TO BE RESUBMITTED FOR APPROVAL, INCLUDING PAYMENT OF REVIEW FEES AT THE PLANNING AND COMMUNITY DEVELOPMENT DIRECTOR'S DISCRETION.

JENNIFER IRVINE, P.E.
COUNTY ENGINEER / EDCM ADMINISTRATOR
DATE

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SHEET NUMBER	SHEET TITLE	SHEET DESCRIPTION
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2	GENERAL NOTES	C0.02
3	TYPICAL ROADWAY SECTIONS	C0.03
4	CONSTRUCTION PHASING PLAN	C0.04
5	OVERALL SITE PLAN	C1.00
6	AVENA ROAD STREET IMPROVEMENT PLAN	C2.01
7	BOSSETT DRIVE STREET IMPROVEMENT PLAN	C2.02
8	BENT GRASS MEADOWS DRIVE STREET IMPROVEMENT PLAN	C2.03
9	BENT GRASS MEADOWS DRIVE STREET IMPROVEMENT PLAN	C2.04
10	BENT GRASS MEADOWS DRIVE STREET IMPROVEMENT PLAN	C2.05
11	BARLEY COURT STREET IMPROVEMENT PLAN	C2.06
12	NEBRARA DRIVE & ANSLEY COURT STREET IMPROVEMENT PLAN	C2.07
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16	FEATHER REED DRIVE & CORDGRASS ROAD STREET IMPROVEMENT PLAN	C2.11
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18	THEDFORD COURT & SILKY THREAD ROAD STREET IMPROVEMENT PLAN	C2.13
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22	CROSS PAN & CURB RETURN PROFILES	C3.04
23	CROSS PAN & CURB RETURN PROFILES	C3.05
24	CROSS PAN & CURB RETURN PROFILES	C3.06
25	CROSS PAN & CURB RETURN PROFILES	C3.07
26	BENT GRASS MEADOWS DRIVE STORM PLAN & PROFILE	C4.01
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28	BERWYN DRIVE STORM PLAN & PROFILE	C4.03
29	STORM PLAN & PROFILE	C4.04
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34	BOX CULVERT DETAILS	C4.09
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36	POND DETAILS (NORTH)	C5.02
37	POND PLAN (SOUTH)	C5.03
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43	SITE DETAILS	C6.04

SEE BENT GRASS RESIDENTIAL FILING NO. 2 GEC PLANS FOR GRADING AND EROSION CONTROL PLANS
SEE BENT GRASS RESIDENTIAL FILING NO. 2 UTILITY CONSTRUCTION PLANS FOR WATER AND SANITARY IMPROVEMENTS

WOODMEN HILLS METROPOLITAN DISTRICT

THESE CONSTRUCTION DOCUMENTS HAVE BEEN REVIEWED AND APPROVED FOR SANITARY SEWER, WATER MAIN AND ASSOCIATED UTILITY SERVICE CONSTRUCTION.

FOR AND BEHALF OF WOODMEN HILLS METROPOLITAN DISTRICT
DATE

FALCON FIRE PROTECTION DISTRICT

ALL FIRE HYDRANTS SHALL BE INSTALLED ACCORDING TO THE FALCON FIRE PROTECTION DISTRICT SPECIFICATIONS. THE NUMBER OF FIRE HYDRANTS AND HYDRANT LOCATIONS AS SHOWN ON THIS WATER INSTALLATION PLAN ARE CORRECT AND ADEQUATE TO SATISFY THE FIRE PROTECTION REQUIREMENTS AS SPECIFIED BY THE FALCON FIRE PROTECTION DISTRICT.

FOR AND BEHALF OF FALCON FIRE PROTECTION DISTRICT
DATE

LEGAL DESCRIPTION

BENT GRASS RESIDENTIAL FILING NO. 2, LOCATED IN THE NORTHWEST QUARTER AND SOUTHWEST QUARTER OF SECTION 1 T13S R65W OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF EL PASO, STATE OF COLORADO

BASIS OF BEARINGS

ALL BEARINGS ARE GRID BEARINGS OF THE COLORADO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE, NORTH AMERICAN DATUM 1983. THE BEARING OF THE LINE BETWEEN THE SOUTHWEST CORNER OF SECTION 1, T13S, R65W AND THE WEST QUARTER CORNER SECTION 1, T13S, R65W IS N007°13'46"W AND MONUMENTED AS SHOWN:

BENCHMARK

THE SOUTHWESTERLY CORNER OF LOT 1 WOODMEN HILLS FILING NO. 4, MONUMENTED BY A YELLOW PLASTIC SURVEYORS CAP ON A NO. 4 REBAR L5# 24954 ELEVATION = 6947.67

CAUTION - NOTICE TO CONTRACTOR

1. ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURFACE EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN APPROXIMATE LOCATION ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE FIELD LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION. REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.

2. WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOLING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.



Know what's below.
Call before you dig.

Galloway

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Colorado Springs, CO 80920
719.900.7220
GallowayUS.com



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CHALLENGER
HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

#	Date	Issue / Description	Init.
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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

COVER SHEET

C0.01
Sheet 1 of 43

1. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE AND LOCATION OF ALL UNDERGROUND UTILITIES ALONG THE ROUTE OF THE WORK. THE OMISSION FROM THE INCLUSION OF UTILITY LOCATIONS ON THE PLANS IS NOT TO BE CONSIDERED AS THE NONEXISTENCE OF OR A DEFINITE LOCATION OF EXISTING UNDERGROUND UTILITIES.
2. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT EXISTING UTILITIES FROM DAMAGE DUE TO THIS OPERATION. ANY DAMAGE TO THE UTILITIES WILL BE REPLACED AT THE CONTRACTORS EXPENSE AND ANY SERVICE DISRUPTION WILL BE SETTLED BY THE CONTRACTOR.
3. ADDITIONAL EROSION CONTROL STRUCTURES MAY BE REQUIRED AT THE TIME OF CONSTRUCTION.
4. ALL BACKFILL, SUB-BASE AND / OR BASE COURSE (CLASS 6) MATERIAL SHALL BE COMPACTED TO THE SOILS ENGINEERS RECOMMENDATIONS, AND APPROVED BY EL PASO COUNTY DEVELOPMENT SERVICES ENGINEERING DIVISION.
5. ALL STATIONING IS CENTERLINE UNLESS OTHERWISE INDICATED. ALL ELEVATIONS ARE FLOW LINE UNLESS OTHERWISE INDICATED.
6. ALL DISTURBED PAVEMENT EDGES SHALL BE CUT TO NEAT LINES. REPAIR SHALL CONFORM TO THE EPC EOM APPENDIX K - 1.2C.
7. ALL INTERSECTION ACCESS TO BE CONSTRUCTED WITH A 25 FOOT SIGHT VISIBILITY TRIANGLES AND THERE SHALL BE NO OBSTRUCTIONS GREATER THAN 18" IN THIS AREA.
8. ALL CULVERT AND STORM PIPES SHALL BE SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE (HDPE), OR REINFORCED CONCRETE PIPE (RCP), ALL CULVERTS SHALL BE PLACED COMPLETE WITH FLARED END SECTIONS. ADEQUACY OF MATERIAL THICKNESS FOR ANY CSP INSTALLED SHALL BE VERIFIED BY OWNERS GEOTECHNICAL ENGINEER TO SUPPORT MINIMUM 50 YEAR DESIGN LIFE. CULVERTS MUST CONFORM TO EPC EOM SECTION 3.32 - CULVERTS.
9. ASPHALT THICKNESS AND BASE COURSE THICKNESS (COMPACTED FOR ROADS SHALL BE PER DESIGN REPORT BY OWNERS GEOTECHNICAL ENGINEER. OWNERS GEOTECHNICAL ENGINEER TO BE ON SITE AT TIME OF ROAD CONSTRUCTION TO EVALUATE SOIL CONDITIONS AND DETERMINE IF ADDITIONAL MEASURES ARE NECESSARY TO ASSURE STABILITY OF THE NEW ROADS. PAVEMENT DESIGN SHALL BE APPROVED BY EL PASO COUNTY DEVELOPMENT SERVICES ENGINEERING DIVISION PRIOR TO CONSTRUCTION.
10. TYPE M RIP-RAP WITH 4" OF TYPE II GRANULAR BEDDING AND MIRAFI 180N OR EQUAL MAY BE SUBSTITUTED WHERE TYPE L RIP-RAP WITH MIRAFI FW 700 OR EQUAL IS SPECIFIED.
11. ALL MATERIALS AND INSTALLATION PROCEDURES SHALL BE IN COMPLIANCE WITH ANY AND ALL APPLICABLE EL PASO COUNTY STANDARDS AND WITH WOODMAN HILLS METRO DISTRICT CONSULTING ENGINEER APPROVAL.
12. ALL POTABLE WATER MAINS SHALL BE AWWA C900-SDR18 PVC WITH PUSH-ON SINGLE GASKET TYPE JOINTS AND SHALL MEET THE REQUIREMENTS OF ANSI / NSF 61.
13. ALL WATER MAIN FITTINGS SHALL BE MADE FROM GRAY-IRON OR DUCTILE IRON AND FURNISHED WITH MECHANICAL JOINT ENDS. ALL FITTINGS SHALL HAVE A PRESSURE RATING OF 250 PSI AND SHALL MEET THE REQUIREMENTS OF ANSI / NSF 61.
14. ALL WATER LINE BENDS, TEES, BLOW-OFFS AND PLUGS AT DEAD-END MAINS SHALL BE PROTECTED FROM THURST BY USING CONCRETE THRUST BLOCKS AND / OR RODDING AND RESTRAINED PIPE PER THE PAINT BRUSH HILLS METRO DISTRICT CONSULTING ENGINEER APPROVAL.
15. MAXIMUM DEFLECTION OF 8" OR 12" PVC WATER MAIN JOINTS IS 4 DEGREES. CORRESPONDING MINIMUM CURVE RADIUS IS 286'. ADDITIONAL 11.25" OR 22.5" BENDS MAY BE REQUIRED FOR PROPER ALIGNMENT.
16. CONTRACTOR IS RESPONSIBLE FOR PROVIDING DETAILED AS-BUILTS OF ALL WATER MAIN, STORM SEWER AND SANITARY. SEWER MAIN INSTALLATIONS, INCLUDING ACCURATE DISTANCES OF MAIN LINES, VALVES, FITTINGS, MANHOLES AND LOCATIONS OF WATER AND SEWER SERVICES.
17. SANITARY SEWER PIPE AND FITTINGS: PVC 4" - 8" ASTM D3034, TYPE FSM, SDR 35; PUSH-ON JOINTS AND MOLDED RUBBER GASKETS MAXIMUM HORIZONTAL DEFLECTIONS, AFTER INSTALLATION AND BACK FILLING SHALL NOT EXCEED 3% OF THE PIPE DIAMETER. (MINIMUM CURVE RADIUS IS 100' FOR 8" PVC SANITARY SEWER MAIN)

1. STORMWATER DISCHARGES FROM CONSTRUCTION SITES SHALL NOT CAUSE OR THREATEN TO CAUSE POLLUTION, CONTAMINATION, OR DEGRADATION OF STATE WATERS. ALL WORK AND EARTH DISTURBANCE SHALL BE DONE IN A MANNER THAT MINIMIZES POLLUTION OF ANY ON-SITE OR OFF-SITE WATERS, INCLUDING METANS.
2. NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT APPROVED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS TO REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING.
3. A SEPARATE STORMWATER MANAGEMENT PLAN (SMP) FOR THIS PROJECT SHALL BE COMPLETED AND AN EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQP) ISSUED PRIOR TO COMMENCING CONSTRUCTION. DURING CONSTRUCTION THE SMP IS THE RESPONSIBILITY OF THE DESIGNATED QUALIFIED STORMWATER MANAGER OR CERTIFIED EROSION CONTROL INSPECTOR AND SHALL BE LOCATED ON SITE. AT ALL TIMES DURING CONSTRUCTION AND SHALL BE KEPT UP TO DATE WITH WORK PROGRESS AND CHANGES IN THE FIELD.
4. ONCE THE ESQP IS APPROVED AND A NOTICE TO PROCEED HAS BEEN ISSUED, THE CONTRACTOR MAY INSTALL THE INITIAL STAGE EROSION AND SEDIMENT CONTROL MEASURES AS INDICATED ON THE APPROVED EROSION PRECONSTRUCTION MEETING BETWEEN THE CONTRACTOR, ENGINEER, AND EL PASO COUNTY WILL BE HELD PRIOR TO ANY CONSTRUCTION. IT IS THE RESPONSIBILITY OF THE APPLICANT TO COORDINATE THE MEETING TIME AND PLACE WITH COUNTY STAFF.
5. CONTROL MEASURES MUST BE INSTALLED PRIOR TO COMMENCEMENT OF ACTIVITIES THAT MAY CONTRIBUTE POLLUTANTS TO STORMWATER. TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES FOR ALL SLOPES, CHANNELS, DITCHES, OR ANY DISTURBED LAND AREA SHALL BE COMPLETED IMMEDIATELY UPON COMPLETION OF THE DISTURBANCE.
6. ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE MAINTAINED AND REMAINED IN EFFECTIVE OPERATING CONDITION UNTIL PERMANENT SOIL EROSION CONTROL MEASURES ARE IMPLEMENTED AND STABILIZATION IS ESTABLISHED. ALL PERSONS ENGAGED IN LAND DISTURBANCE ACTIVITIES SHALL ASSESS THE ADEQUACY OF CONTROL MEASURES AT THE SITE AND IDENTIFY IF CHANGES TO THOSE CONTROL MEASURES IS NEEDED TO ENSURE THE CONTINUED EFFECTIVE PERFORMANCE OF THE CONTROL MEASURES. ALL CHANGES TO TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES MUST BE INCORPORATED INTO THE STORMWATER MANAGEMENT PLAN PRIOR TO IMPLEMENTATION.
7. TEMPORARY STABILIZATION SHALL BE IMPLEMENTED ON DISTURBED AREAS AND STORMWATER WHERE GROUND DISTURBING CONSTRUCTION ACTIVITY HAS PERMANENTLY CEASED OR TEMPORARILY CEASED FOR LONGER THAN 14 DAYS. AN AREA THAT IS GOING TO REMAIN IN AN INTERIM STATE FOR MORE THAN 60 DAYS SHALL ALSO BE STABILIZED.
8. FINAL STABILIZATION MUST BE IMPLEMENTED AT ALL APPLICABLE CONSTRUCTION SITES. FINAL STABILIZATION IS ACHIEVED WHEN ALL GROUND DISTURBING ACTIVITIES ARE COMPLETE AND ALL DISTURBED AREAS EITHER HAVE A UNIFORM VEGETATIVE COVER WITH INDIVIDUAL PLANT DENSITY OF 70 PERCENT OF PRE-DISTURBANCE LEVELS ESTABLISHED OR EQUIVALENT PERMANENT ALTERNATIVE STABILIZATION METHOD IS IMPLEMENTED. ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE REMOVED UPON FINAL STABILIZATION AND BEFORE PERMIT CLOSURE.
9. ALL PERMANENT STORMWATER MANAGEMENT FACILITIES SHALL BE APPROVED AS DEFINED IN THE APPROVED PLANS. ANY PROPOSED CHANGES THAT EFFECT THE HYDROLOGY OR HYDRAULICS OF A PERMANENT STORMWATER MANAGEMENT STRUCTURES MUST BE INSTALLED BY THE EGM ADMINISTRATOR PRIOR TO IMPLEMENTATION.
10. ANY EARTH DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER SO AS TO EFFECTIVELY MINIMIZE ACCELERATED SOIL EROSION AND RESULTING SEDIMENTATION. ALL DISTURBANCES SHALL BE DESIGNED, CONSTRUCTED, AND COMPLETED SO THAT THE EXPOSED AREA OF ANY DISTURBED LAND SHALL BE LIMITED TO THE SHORTEST PRACTICAL PERIOD OF TIME. PRE-EXISTING VEGETATION SHALL BE PROTECTED AND MAINTAINED WITHIN 50 HORIZONTAL FEET OF A WATERS OF THE STATE, UNLESS INFEASIBLE.
11. COMPACTION OF SOIL MUST BE PREVENTED IN AREAS DESIGNATED FOR INFILTRATION CONTROL MEASURES OR WHERE FINAL STABILIZATION WILL BE ACHIEVED BY VEGETATIVE COVER. AREAS DESIGNATED FOR INFILTRATION CONTROL SHALL ALSO BE PROTECTED FROM SEDIMENTATION DURING CONSTRUCTION UNTIL FINAL STABILIZATION IS ACHIEVED.
12. ANY TEMPORARY OR PERMANENT FACILITY DESIGNED AND CONSTRUCTED FOR THE CONVEYANCE OF STORMWATER DRAINAGE, THROUGH, OR FROM THE EARTH DISTURBANCE AREA SHALL BE A STABILIZED CONVEYANCE DESIGNED TO MINIMIZE EROSION AND THE DISCHARGE OF SEDIMENT OFF SITE.
13. CONCRETE WASH WATER SHALL BE CONTAINED AND DISPOSED OF IN ACCORDANCE WITH THE NO WASH WATER SHALL BE DISCHARGED TO OR ALLOWED TO RUNOFF TO STATE WATERS, INCLUDING ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR FACILITIES. CONCRETE WASHOUT SHALL NOT BE LOCATED IN AN AREA WHERE SHALLOW GROUNDWATER MAY BE PRESENT, OR WITHIN 50 FEET OF A SURFACE WATER BODY.
14. DEWATERING OPERATIONS: UNCONTAMINATED GROUND WATER MAY BE DISCHARGED ON SITE, BUT MAY NOT LEAVE THE SITE IN THE FORM OF SURFACE RUNOFF.
15. EROSION CONTROL BLANKETING IS TO BE USED ON SLOPES STEEPER THAN 3:1.
16. BUILDING, CONSTRUCTION, EXCAVATION, OR OTHER WASTE MATERIALS SHALL NOT BE TEMPORARILY PLACED OR STORED IN THE STREET, ALLEY, OR OTHER PUBLIC WAY, UNLESS IN ACCORDANCE WITH AN APPROVED TRAFFIC CONTROL PLAN. BMPs MAY BE REQUIRED BY EL PASO COUNTY ENGINEERING IF DEMAED NECESSITY, BASED ON SPECIFIC CONDITIONS AND CIRCUMSTANCES.
17. VEHICLE TRACKING OF SOLS AND CONSTRUCTION DEBRIS OFF-SITE SHALL BE MINIMIZED. MATERIALS TRACKED OFFSITE SHALL BE CLEANED UP AND PROPERLY DISPOSED OF IMMEDIATELY.
18. CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL WASTES FROM THE CONSTRUCTION SITE FOR DISPOSAL. IN ACCORDANCE WITH LOCAL AND STATE REGULATORY REQUIREMENTS. NO CONSTRUCTION DEBRIS, TREE SLASH, BUILDING MATERIAL WASTES OR UNUSUED BUILDING MATERIALS SHALL BE BURIED, DUMPED, OR DISCHARGED AT THE SITE.
19. THE OWNER, SITE DEVELOPER, CONTRACTOR, AND/OR THEIR AUTHORIZED AGENTS SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL CONSTRUCTION DEBRIS, DIRT, TRASH, ROCK, SEDIMENT, AND SAND THAT MAY ACCUMULATE IN THE STORED SEWER OR OTHER DRAINAGE CONVEYANCE SYSTEM AND STORMWATER APPURTENANCES AS A RESULT OF SITE DEVELOPMENT.
20. THE QUANTITY OF MATERIALS STORED ON THE PROJECT SITE SHALL BE LIMITED, AS MUCH AS PRACTICAL, TO THAT QUANTITY REQUIRED TO PERFORM THE WORK IN AN ORDERLY SEQUENCE. ALL MATERIALS STORED ON-SITE SHALL BE STORED IN A NEAT, ORDERLY MANNER, IN THEIR ORIGINAL CONTAINERS, WITH ORIGINAL MANUFACTURER'S LABELS.
21. NO CHEMICALS ARE TO BE USED BY THE CONTRACTOR, WHICH HAVE THE POTENTIAL TO BE RELEASED IN STORMWATER UNLESS PERMISSION FOR THE USE OF A SPECIFIC CHEMICAL IS GRANTED IN WRITING BY THE EGM ADMINISTRATOR. IN GRANTING THE USE OF SUCH CHEMICALS, SPECIAL CONDITIONS AND MONITORING MAY BE REQUIRED.
22. BULK STORAGE OF PETROLEUM PRODUCTS OR OTHER LIQUID CHEMICALS IN EXCESS OF 55 GALLONS SHALL HAVE ADEQUATE SECONDARY CONTAINMENT PROTECTION TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS, INCLUDING ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR FACILITIES.
23. NO PERSON SHALL CAUSE THE IMPEDIMENT OF STORMWATER FLOW IN THE FLOW LINE OF THE CURB AND GUTTER OR IN THE DITCH FLOW LINE.
24. INDIVIDUALS SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT"(TITLE 25, ARTICLE 8, CRS) AND THE CLEAN WATER ACT(33 USC 1344). IN ADDITION TO THE REQUIREMENTS INCLUDED IN THE DCM VOLUME II AND THE EGM APPENDIX II, ALL APPROPRIATE PERMITS MUST BE OBTAINED BY THE CONTRACTOR PRIOR TO CONSTRUCTION (NPDES, FLOODPLAIN, 404, FUGITIVE DUST, ETC.). IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, STATE, OR COUNTY AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
25. ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE AT APPROVED CONSTRUCTION ACCESS POINTS.
26. PRIOR TO ACTUAL CONSTRUCTION THE PERMITTEE SHALL VERIFY THE LOCATION OF EXISTING UTILITIES.
27. A WATER SOURCE SHALL BE AVAILABLE ON SITE DURING EROSION CONTROL OPERATIONS AND UTILIZED AS REQUIRED TO MINIMIZE DUST FROM EARTHWORK EQUIPMENT AND WIND.
28. THE SOLS REPORT FOR THIS SITE HAS BEEN PREPARED BY ROCKY MOUNTAIN GROUP AND SHALL BE CONSIDERED A PART OF THESE PLANS.
29. AT LEAST TEN (10) DAYS PRIOR TO THE ANTICIPATED START OF CONSTRUCTION, FOR PROJECTS THAT WILL DISTURB 1 ACRE OR MORE, THE OWNER OR OPERATOR OF CONSTRUCTION ACTIVITY SHALL SUBMIT A PERMIT APPLICATION FOR STORMWATER DISCHARGE TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT, WATER QUALITY DIVISION. THE APPLICATION CONTAINS CERTIFICATION OF COMPLETION OF A STORMWATER MANAGEMENT PLAN (SMP), OF WHICH THIS GRADING AND EROSION CONTROL PLAN MAY BE A PART. FOR INFORMATION OR APPLICATION MATERIALS CONTACT:

COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT
WATER QUALITY CONTROL DIVISION
WOOD - PERMITS
4300 CHERRY CREEK DRIVE SOUTH
DENVER, CO 80246-1530
ATTN: PERMITS UNIT

1. ALL DRAINAGE AND ROADWAY CONSTRUCTION SHALL MEET THE STANDARDS AND SPECIFICATIONS OF THE CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2, AND THE EL PASO COUNTY ENGINEERING CRITERIA MANUAL.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION AND FIELD NOTIFICATION OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, BEFORE BEGINNING CONSTRUCTION. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CALL 811 TO CONTACT THE UTILITY NOTIFICATION CENTER OF COLORADO (UNCC).
3. CONTRACTOR SHALL KEEP A COPY OF THESE APPROVED PLANS, THE GRADING AND EROSION CONTROL PLAN, THE STORMWATER MANAGEMENT PLAN (SWMP), THE SOILS AND GEOTECHNICAL REPORT, AND THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES, INCLUDING THE FOLLOWING:
 - A. EL PASO COUNTY ENGINEERING CRITERIA MANUAL (ECM)
 - B. CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2
 - C. COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
 - D. CDOT M & S STANDARDS
4. NOTWITHSTANDING ANYTHING DEPICED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS FROM REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING. ANY MODIFICATIONS NEEDED TO MEET CRITERIA AFTER-THE-FACT WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
5. IT IS THE DESIGN ENGINEER'S RESPONSIBILITY TO ACCURATELY SHOW EXISTING CONDITIONS, BOTH ONSITE AND OFFSITE, ON THE CONSTRUCTION PLANS. ANY MODIFICATIONS NECESSARY DUE TO CONFLICTS, OMISSIONS, OR CHANGED CONDITIONS WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
6. CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (PCD) - INSPECTIONS, PRIOR TO STARTING CONSTRUCTION.
7. IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE REQUIREMENTS OF ALL JURISDICTIONAL AGENCIES AND TO OBTAIN ALL REQUIRED PERMITS, INCLUDING BUT NOT LIMITED TO EL PASO COUNTY DRAINAGE CRITERIA MANUAL, COUNTY PERMIT (ESOP), REGIONAL BUILDING FLOODPLAIN DEVELOPMENT PERMIT, U.S. ARMY CORPS OF ENGINEERS-ISSUED 401 AND/OR 404 PERMITS, AND COUNTY AND STATE FUGITIVE DUST PERMITS.
8. CONTRACTOR SHALL NOT DEVIATE FROM THE PLANS WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE DESIGN ENGINEER AND PCD. CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY UPON DISCOVERY OF ANY ERRORS OR INCONSISTENCIES.
9. ALL STORM DRAIN PIPE SHALL BE CLASS II RCP UNLESS OTHERWISE NOTED AND APPROVED BY PCD.
10. CONTRACTOR SHALL COORDINATE GEOTECHNICAL TESTING PER ECM STANDARDS. PAVEMENT DESIGN SHALL BE APPROVED BY EL PASO COUNTY PCD PRIOR TO PLACEMENT OF CURB AND GUTTER AND PAVEMENT.
11. ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE AT APPROVED CONSTRUCTION ACCESS POINTS.
12. SIGHT VISIBILITY FRANGES AS IDENTIFIED IN THE PLANS SHALL BE PROVIDED AT ALL INTERSECTIONS. OBSTRUCTIONS GREATER THAN 18 INCHES ABOVE FLOWLINE ARE NOT ALLOWED WITHIN SIGHT TRIANGLES.
13. SIGNING AND STRIPING SHALL COMPLY WITH EL PASO COUNTY DOT AND MUTCD CRITERIA. [IF APPLICABLE, ADDITIONAL SIGNING AND STRIPING NOTES WILL BE PROVIDED.]
14. CONTRACTOR SHALL OBTAIN ANY PERMITS REQUIRED BY EL PASO COUNTY DOT, INCLUDING WORK WITHIN THE RIGHT-OF-WAY AND SPECIAL TRANSPORT PERMITS.
15. THE LIMITS OF CONSTRUCTION SHALL REMAIN WITHIN THE PROPERTY LINE UNLESS OTHERWISE NOTED. THE OWNER/DEVELOPER SHALL OBTAIN WRITTEN PERMISSION AND EASEMENTS, WHERE REQUIRED, FROM ADJOINING PROPERTY OWNER(S) PRIOR TO ANY OFF-SITE DISTURBANCE, GRADING, OR CONSTRUCTION.

1. ALL SIGNS AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2. REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY A METHOD THAT DOES NOT MATERIALLY DAMAGE THE PAVEMENT. THE PAVEMENT MARKINGS SHALL BE REMOVED TO THE EXTENT THAT THEY WILL NOT BE VISIBLE UNDER DAY OR NIGHT CONDITIONS. AT NO TIME WILL IT BE ACCEPTABLE TO PAINT OVER EXISTING PAVEMENT MARKINGS.
3. ANY DEVIATION FROM THE STRIPING AND SIGNING PLAN SHALL BE APPROVED BY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT.
4. ALL SIGNS SHOWN ON THE SIGNING AND STRIPING PLAN SHALL BE NEW SIGNS. EXISTING SIGNS MAY REMAIN OR BE REUSED IF THEY MEET CURRENT EL PASO COUNTY AND MUTCD STANDARDS.
5. STREET NAME AND REGULATORY STOP SIGNS SHALL BE ON THE SAME POST AT INTERSECTIONS.
6. ALL REMOVED SIGNS SHALL BE DISPOSED OF IN A PROPER MANNER BY THE CONTRACTOR.
7. ALL STREET NAME SIGNS SHALL HAVE 1" SERIES LETTERING, WITH LOCAL ROADWAY SIGNS BEING 4" UPPER-LOWER CASE LETTERING ON 8" BLANK AND NON-LOCAL ROADWAY SIGNS BEING 6" LETTERING, UPPER-LOWER CASE ON 12" BLANK, WITH A WHITE BORDER THAT IS NOT RECESSED. MULTI-LANE ROADWAYS WITH SPEED LIMITS OF 40 MPH OR HIGHER SHALL HAVE 8" UPPER-LOWER CASE LETTERING ON 18" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. THE WIDTH OF THE NON-RECESSED WHITE BORDERS SHALL MATCH PAGE 255 OF THE 2012 MUTCD "STANDARD HIGHWAY SIGNS"
8. ALL TRAFFIC SIGNS SHALL HAVE A MINIMUM HIGH INTENSITY PRISMATIC GLASS SHEETING.
9. ALL LOCAL RESIDENTIAL STREET SIGNS SHALL BE MOUNTED ON A 1.75"X 1.75" SQUARE TUBE SIGN POST AND STUB POST BASE. FOR OTHER APPLICATIONS, REFER TO THE CDOT STANDARD S-614-8 REGARDING USE OF THE P2 TUBULAR STEEL POST SUBBASE DESIGN.
10. ALL SIGNS SHALL BE SINGLE SHEET ALUMINUM WITH 0.100" MINIMUM THICKNESS.
11. ALL LIMIT LINES/STOP LINES, CROSSWALK LINES, PAVEMENT LEGENDS, AND ARROWS SHALL BE A MINIMUM 125 MIL THICKNESS PREFORMED THERMOPLASTIC PAVEMENT MARKINGS WITH TAPERED LEADING EDGES PER CDOT STANDARD S-627-1. WORD AND SYMBOL MARKINGS SHALL BE THE NARROW TYPE. STOP BARS SHALL BE 24" IN WIDTH. CROSSWALKS LINES SHALL BE 12" WIDE AND 8 LONG PER CDOT S-627-1.
12. ALL LONGITUDINAL STRIPING AS REQUIRED BY MINIMUM 15MIL THICKNESS EPOXY PAINT. ALL NON-LOCAL RESIDENTIAL ROADWAYS SHALL INCLUDE BOTH RIGHT AND LEFT EDGE LINE STRIPING AND ANY ADDITIONAL STRIPING AS REQUIRED BY CDOT S-627-1.
13. THE CONTRACTOR SHALL NOTIFY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (719) 520-6819 PRIOR TO AND UPON COMPLETION OF SIGNING AND STRIPING.
14. THE CONTRACTOR SHALL OBTAIN A WORK IN THE RIGHT OF WAY PERMIT FROM THE EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS (DPW) PRIOR TO ANY SIGNAGE OR STRIPING WORK WITHIN AN EXISTING EL PASO COUNTY ROADWAY.

1. ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURFACE EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN APPROXIMATE LOCATION ONLY. THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE FIELD LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION, REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.

2. WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POTHoling OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.

811
Call before you dig.

**Know what's below.
Call before you dig.**





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CHALLENGER HOMES

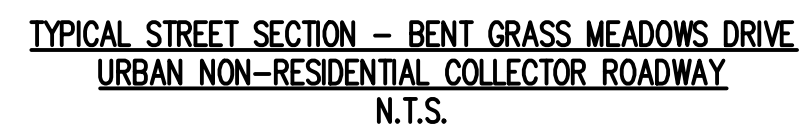
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

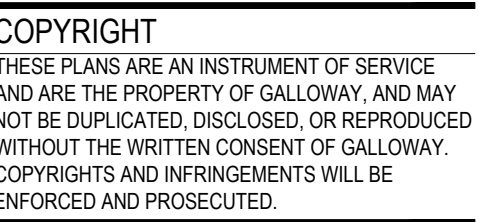
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

TYPICAL ROADWAY SECTIONS

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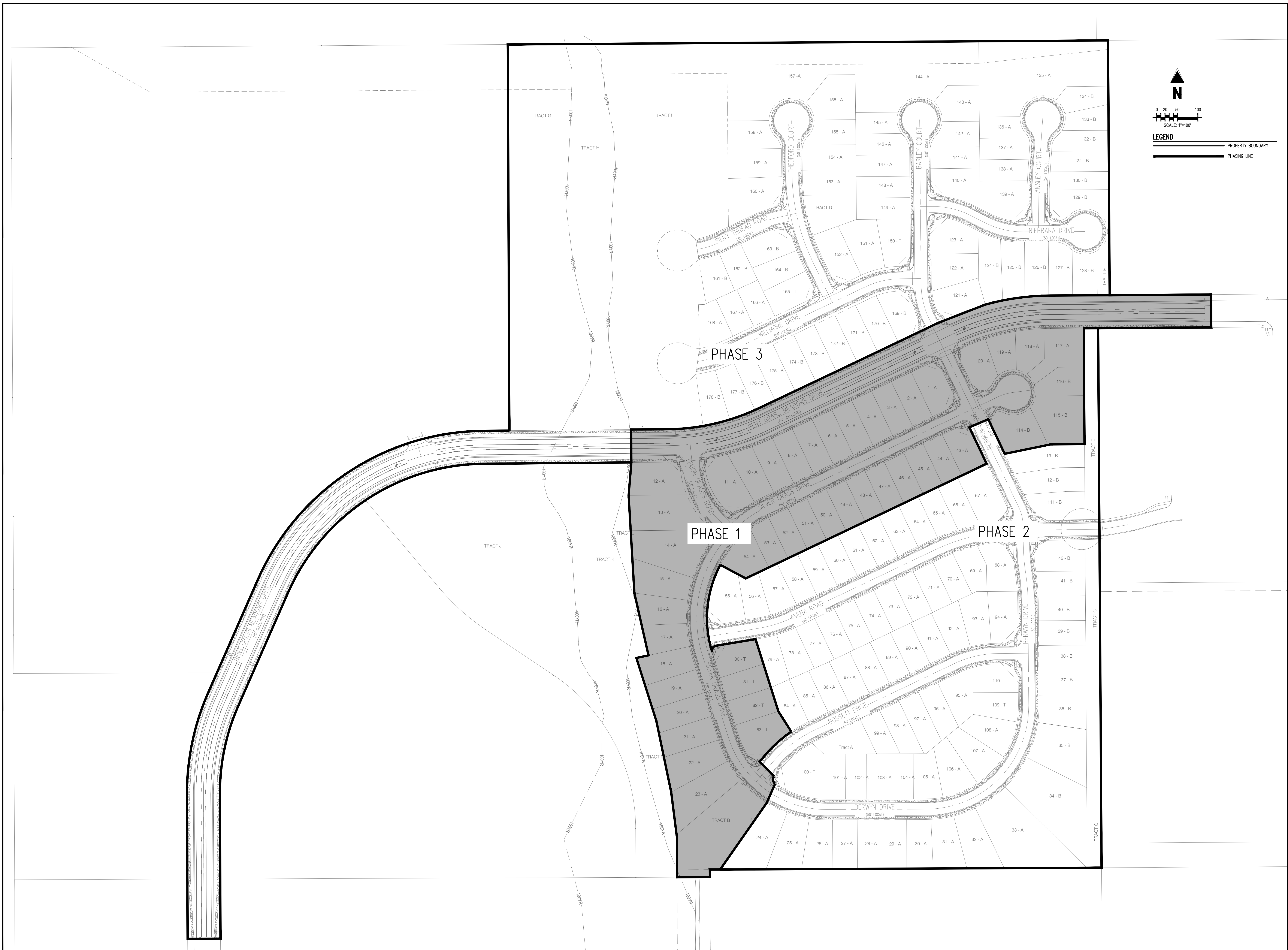


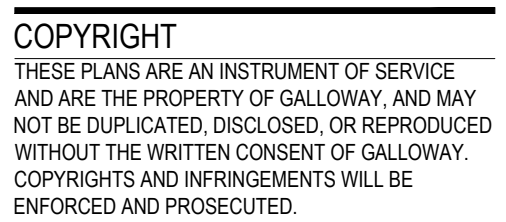
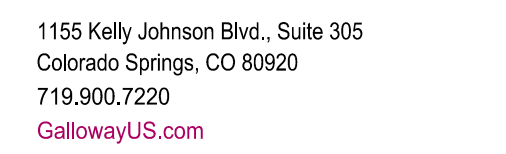
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

CONSTRUCTION PHASING PLAN

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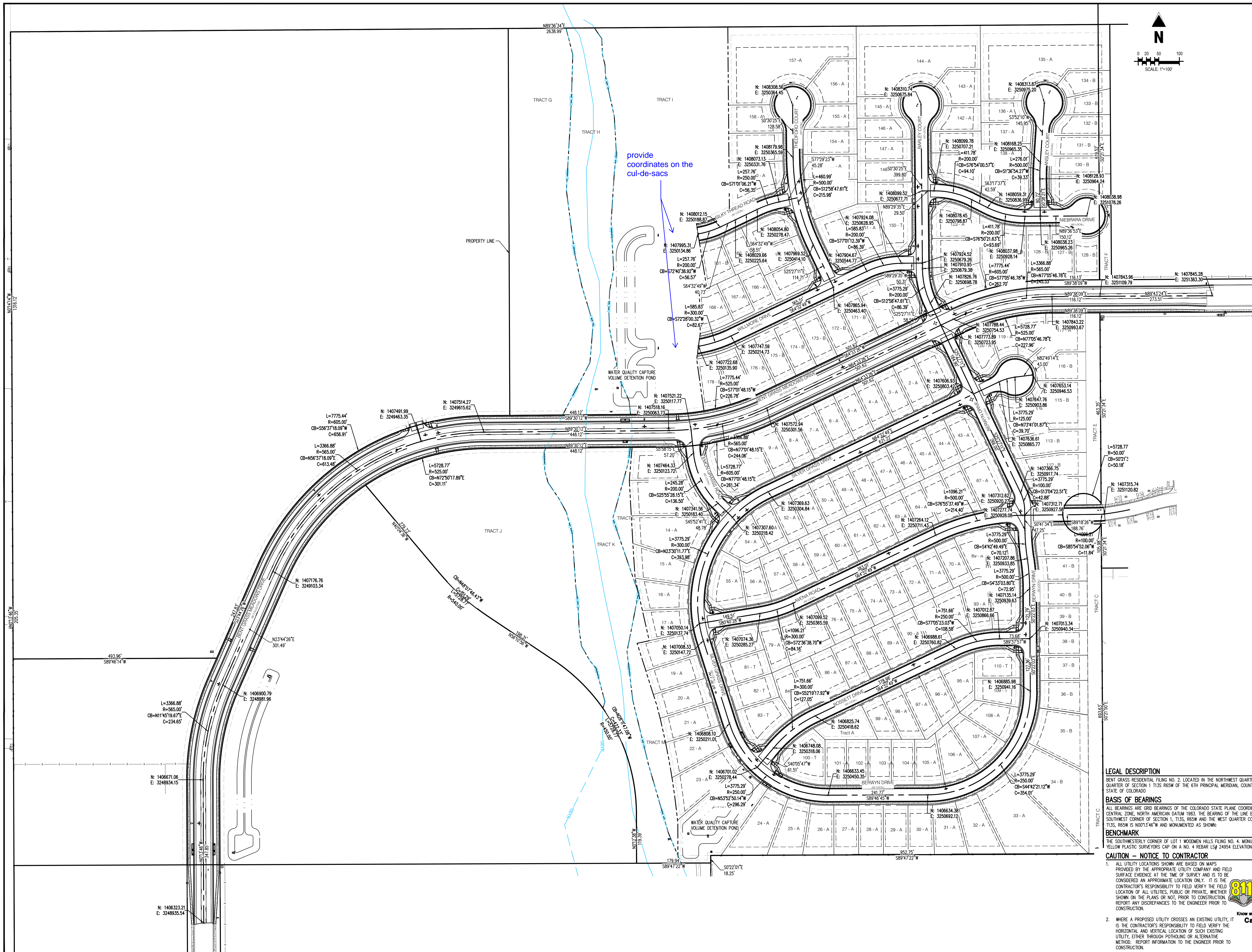


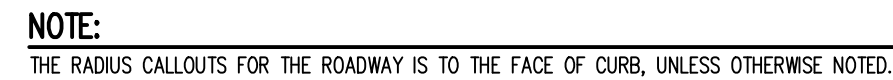
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC

BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

[illegible]

C1.00
Sheet 5 of 43





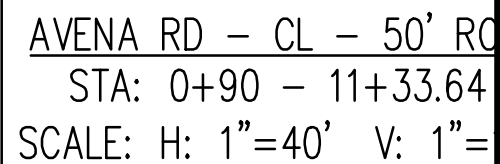
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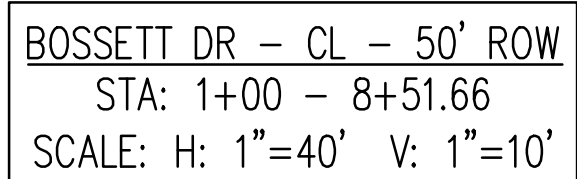
ROAD NAME

STREET NAME (D-3)

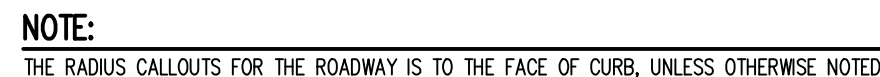
STOP

STOP SIGN (R1-1)

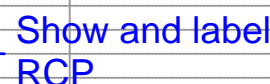


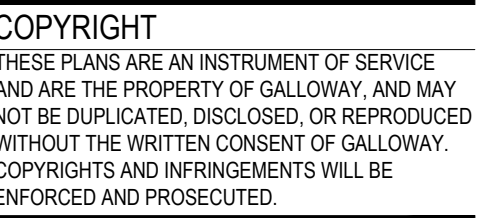


1. ALL SIGNS AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2. REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY A METHOD THAT DOES NOT MATERIALLY DAMAGE THE PAVEMENT. THE PAVEMENT MARKINGS SHALL BE REMOVED TO THE EXTENT THAT THEY WILL NOT BE VISIBLE UNDER DAY OR NIGHT CONDITIONS. AT NO TIME WILL IT BE ACCEPTABLE TO PAINT OVER EXISTING PAVEMENT MARKINGS.
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4. ALL SIGNS SHOWN ON THE SIGNING AND STRIPING PLAN SHALL BE NEW SIGNS. EXISTING SIGNS MAY REMAIN OR BE REUSED IF THEY MEET CURRENT EL PASO COUNTY AND MUTCD STANDARDS.
5. STREET NAME AND REGULATORY STOP SIGNS SHALL BE ON THE SAME POST AT INTERSECTIONS.
6. ALL REMOVED SIGNS SHALL BE DISPOSED OF IN A PROPER MANNER BY THE CONTRACTOR.
7. ALL STRIPES AND SIGNS SHALL HAVE "1" SERIES LETTERS WITH LOCAL ROADWAY SIGNS BEING "4" UPPER-LOWER CASE LETTERING ON 8" BLANK AND NON-LOCAL ROADWAY SIGNS BEING 6" LETTERING; UPPER-LOWER CASE ON 12" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. MULTI-LANE ROADWAYS WITH SPEED LIMITS OF 40 MPH OR HIGHER SHALL HAVE 6" UPPER-LOWER CASE LETTERING ON 18" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. THE WIDTH OF THE NON-RECESSED WHITE BORDERS SHALL MATCH PAGE 255 OF THE 2012 MUTCD "STANDARD HIGHWAY SIGNS".
8. ALL TRAFFIC SIGNS SHALL HAVE A MINIMUM HIGH INTENSITY PRISMATIC GRADE SHEETING.
9. ALL LOCAL RESIDENTIAL STREET SIGNS SHALL BE MOUNTED ON A 1.75" X 1.75" SQUARE TUBE SIGN POST AND STUB POST BASE. FOR OTHER APPLICATIONS, REFER TO THE CDOT STANDARD 6-14-4 REGARDING USE OF THE P72 TUBULAR STEEL POST SUPRASE DESIGN.
10. ALL SIGNS SHALL BE SINGLE SHEET ALUMINUM WITH A MINIMUM THICKNESS.
11. ALL LIMIT LINES/STOP LINES, CROSSWALK SIGNS, PAVEMENT LEGENDS, AND ARROWS SHALL BE A MINIMUM 1/8" MIN THICKNESS PRE-FORMED THERMOPLASTIC PAVEMENT MARKINGS WITH TAPERED LEADING EDGES PER DOT STANDARD 6-23.7-7. WORD AND SYMBOL MARKINGS SHALL BE THE NARROW TYPE. STOP BARS SHALL BE 24" IN WIDTH. CROSSWALK SIGNS SHALL BE 12" WIDE AND 8" LONG PER CDOT 6-27.1.
12. ALL LONGITUDINAL LINES SHALL BE A MINIMUM 1/8" MIN THICKNESS PER DOT. ALL NON-LOCAL RESIDENTIAL ROADWAYS SHALL INCLUDE BOTH RIGHT AND LEFT EDGE LINE STRIPING AND ANY ADDITIONAL STRIPING AS REQUIRED BY CDOT 6-27.1.
13. THE CONTRACTOR SHALL NOTIFY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (719) 520-6819 FOR NOTICE TO AND UPON COMPLETION OF SIGNING AND STRIPING.
14. THE CONTRACTOR SHALL OBTAIN A WORK IN PROGRESS PERMIT FROM THE EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS (DPW) PRIOR TO ANY SIGNAGE OR STRIPING WORK WITHIN AN EXISTING EL PASO COUNTY ROADWAY.



BENT GRASS MEADOWS DRIVE (PUBLIC)
(80' R.O.W.)
DESIGN SPEED - 40 MPH COLLECTOR

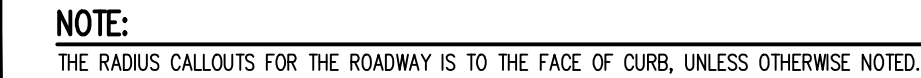
[illegible]BENT GRASS MEADOWS DRIVE
STREET IMPROVEMENT PLAN



Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

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1. ALL SIGNS AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
2. REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY A METHOD THAT DOES NOT MATERIALLY DAMAGE THE PAVEMENT. THE PAVEMENT MARKINGS SHALL BE REMOVED TO THE EXTENT THAT THEY WILL NOT BE VISIBLE UNDER DAY OR NIGHT CONDITIONS. AT NO TIME WILL IT BE ACCEPTABLE TO PAINT OVER EXISTING PAVEMENT MARKINGS.
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4. ALL SIGNS SHOWN ON THE SIGNING AND STRIPING PLAN SHALL BE NEW SIGNS. EXISTING SIGNS MAY REMAIN OR BE REUSED IF THEY MEET CURRENT EL PASO COUNTY AND MUTCD STANDARDS.
5. STREET NAME AND REGULATORY STOP SIGNS SHALL BE ON THE SAME POST AT INTERSECTIONS.
6. ALL REMOVED SIGNS SHALL BE DISPOSED OF IN A PROPER MANNER BY THE CONTRACTOR.
7. ALL STREET NAME SIGNS SHALL HAVE "D" SERIES LETTERS, WITH LOCAL ROADWAY SIGNS BEING 4" UPPER/LOWER CASE LETTERING ON 8" BLANK AND NON-LOCAL ROADWAY SIGNS BEING 6" LETTERING, UPPER/LOWER CASE ON 12" BLANK, WITH A WHITE BORDER THAT IS NOT RECESSED. MULTI-LANE ROADWAYS WITH SPEED LIMITS OF 40 MPH OR HIGHER SHALL HAVE 8" UPPER/LOWER CASE LETTERING ON 18" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. THE WIDTH OF THE NON-RECESSED WHITE BORDERS SHALL MATCH PAGE 255 OF THE 2012 MUTCD STANDARD HIGHWAY SIGNS.
8. ALL TRAFFIC SIGNS SHALL HAVE A MINIMUM HIGH INTENSITY PRISMATIC GRADE SHEETING.
9. ALL LOCAL RESIDENTIAL STREET SIGNS SHALL BE MOUNTED ON A 15 X 17 1/2 SQUARE TUBE SIGN POST AND STUB POST. FOR OTHER APPLICATIONS, REFER TO THE CDOT STANDARD S-614.8 REGARDING USE OF THE P2 TUBULAR STEEL POST SUBSAFE DESIGN.
10. ALL SIGNS SHALL BE SINGLE SHEET ALUMINUM WITH A 1/16" MINIMUM THICKNESS.
11. ALL LIMIT LINES/STOP SIGNS, CROSSWALK LINES, PAVEMENT, WORDS AND ARROWS SHALL BE A MINIMUM 1/2 IN. THICKNESS PREFORMED THERMOPLASTIC PAVEMENT MARKINGS WITH TAPERED LEADING EDGES. ALL PAVEMENT, STOP, SPEED LIMIT, WARNING, AND SYMBOL MARKINGS SHALL BE THE NARROW TYPE. STOP BARS SHALL BE 24" IN WIDTH. CROSSWALKS SHALL BE 12" WIDE AND 8" LONG PER CDOT S-627.1.
12. ALL LONGITUDINAL LINES SHALL BE A MINIMUM 1/16" MINIMUM THICKNESS EPoxy PUT. ALL NON-LOCAL RESIDENTIAL ROADWAYS SHALL INCLUDE BOTH RIGHT AND LEFT EDGE LINE STRIPING AND ANY ADDITIONAL STRIPING AS REQUIRED BY CDOT S-627.1.
13. ALL SIGNS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (719) 526-8919 (P) AND TO COMPLY WITH THE CURRENT SIGNING AND STRIPING.
14. THE CONTRACTOR SHALL OBTAIN A WORK IN THE RIGHT OF WAY PERMIT FROM THE EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS (DPW) PRIOR TO ANY SIGNAGE OR STRIPING WORK WITHIN AN EXISTING EL PASO COUNTY ROADWAY.



KEY NOTE: A

ROAD NAME

STREET NAME (D-3)

STOP

STOP SIGN (R1-1)

KEY NOTE: B



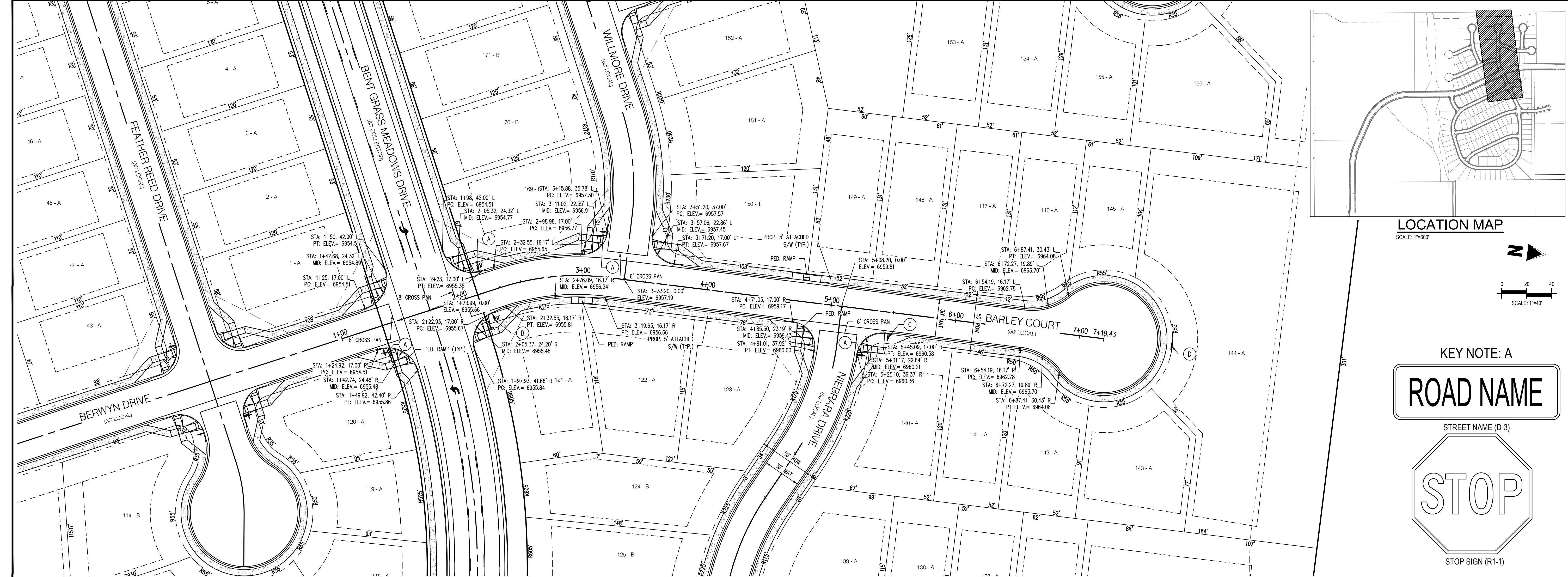
SPEED LIMIT SIGN (R2-1)

ALL 4", 8" SOLID OR SKIP PAVEMENT MARKING SHALL BE EPOXY.
SIGN AND POLES SHALL BE PER CDOT STANDARDS S-614-2,
S-614-3, AND S-614-8, LATEST REVISION.
ALL SIGNAGE INSTALLATION SHALL BE IN COMPLIANCE WITH THE
MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

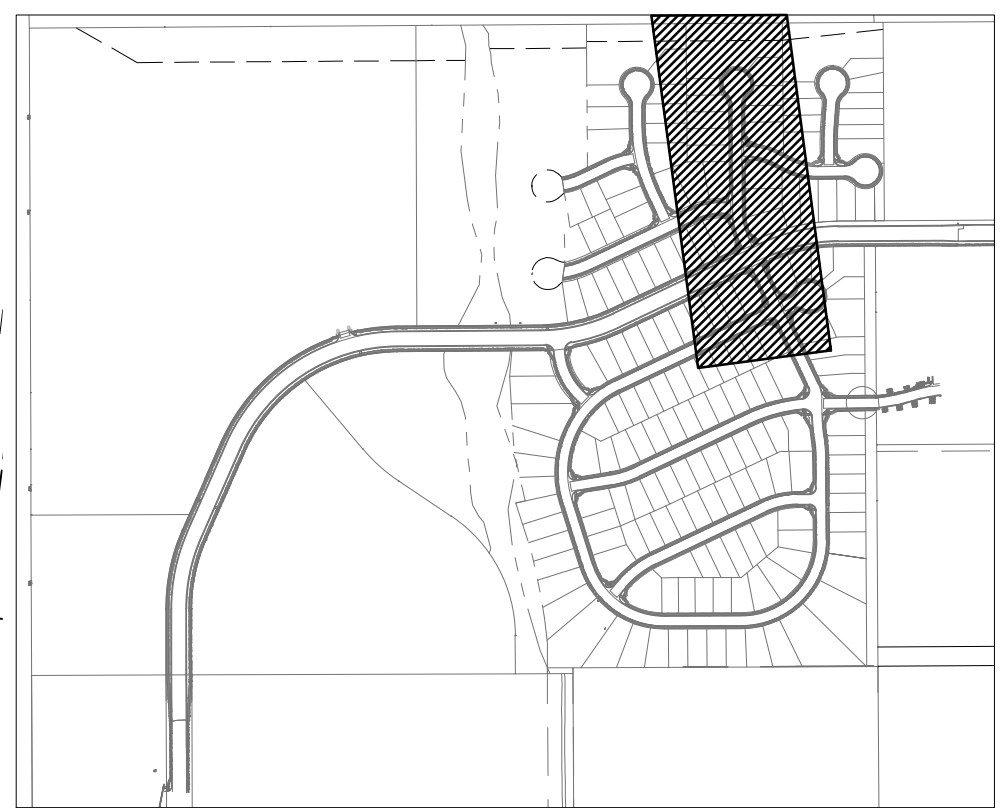
BENT GRASS MEADOWS DRIVE
STREET IMPROVEMENT PLAN

C2.05

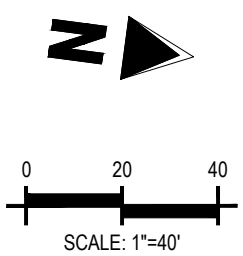
Sheet 10 of 43



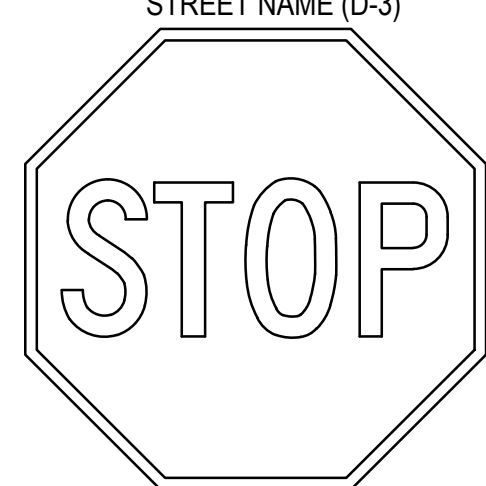
BARLEY COURT (PUBLIC)
(50' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL



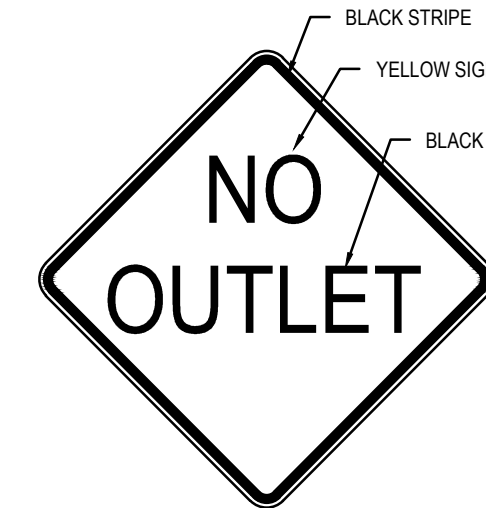
LOCATION MAP
SCALE: 1"=600'



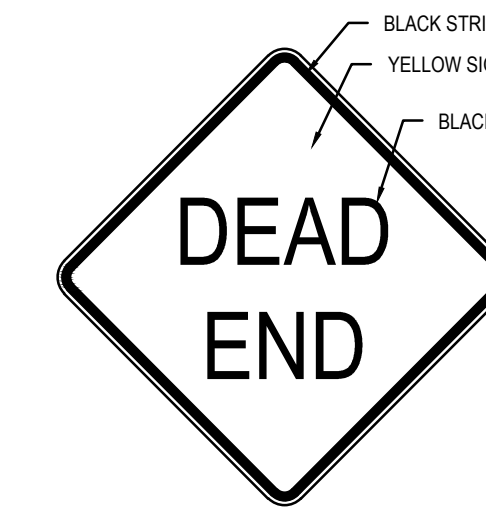
KEY NOTE: A
ROAD NAME
STREET NAME (D-3)



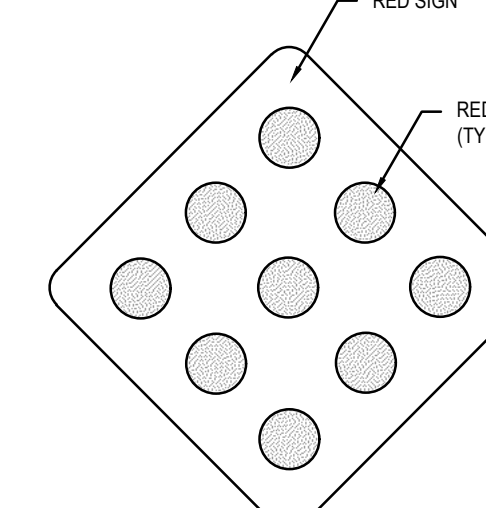
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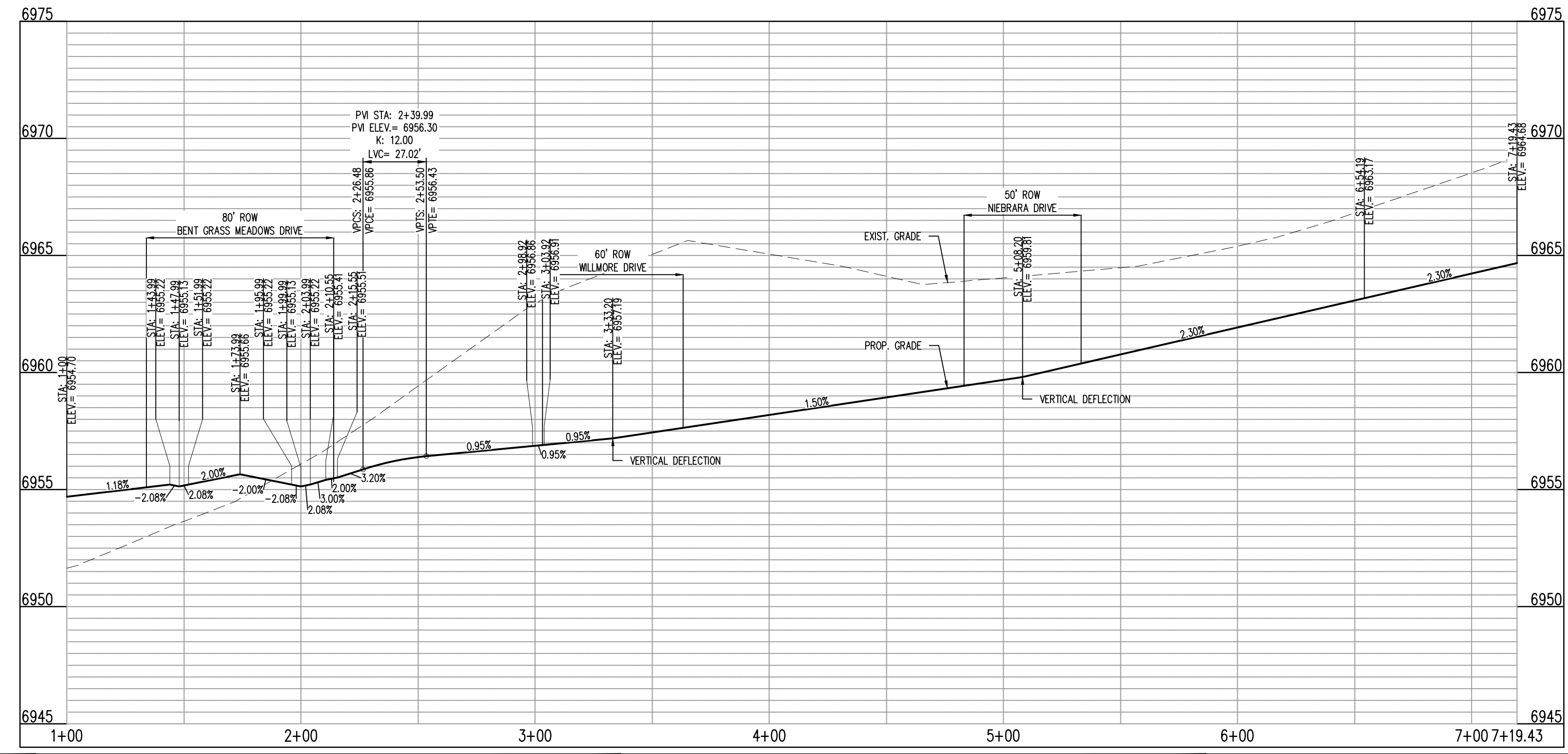
NO OUTLET SIGN (W14-2)
KEY NOTE: B



DEAD END SIGN (W14-1)
KEY NOTE: C



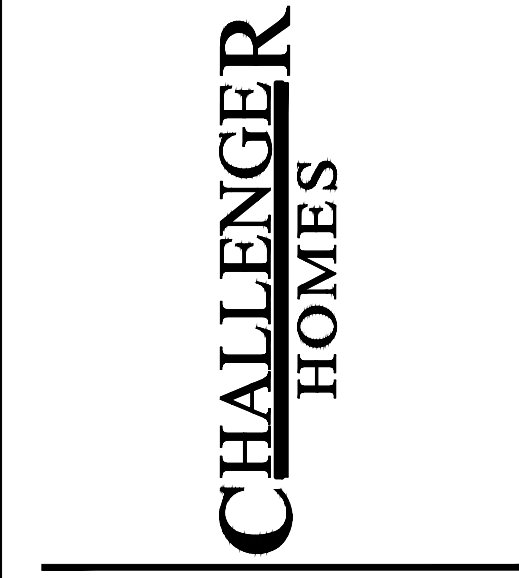
TYPE 4 OBJECT MARKER
KEY NOTE: D



BARLEY COURT - CL - 50' ROW
STA: 1+00 - 7+19.43
SCALE: H: 1"=40' V: 1"=10'



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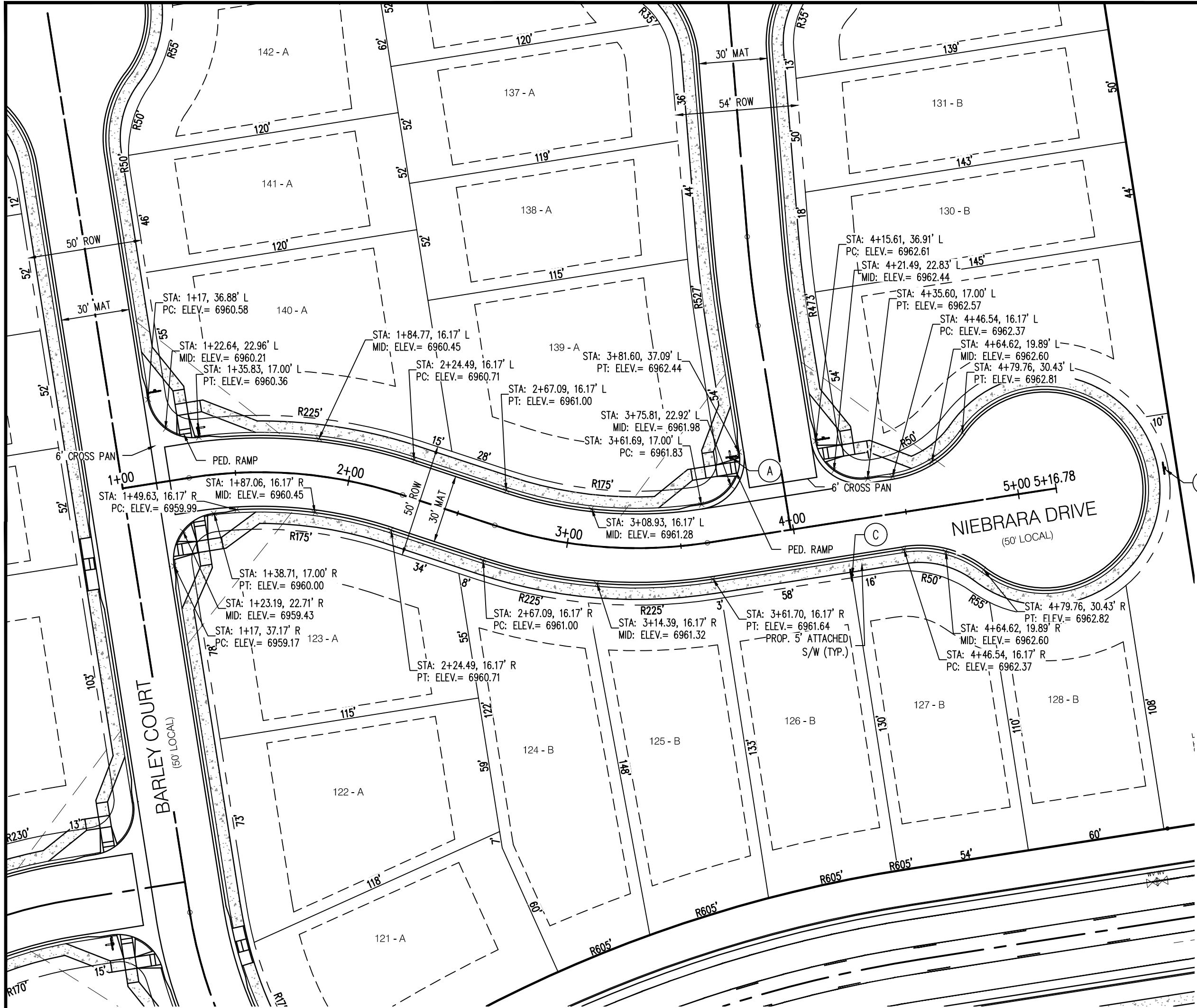


CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

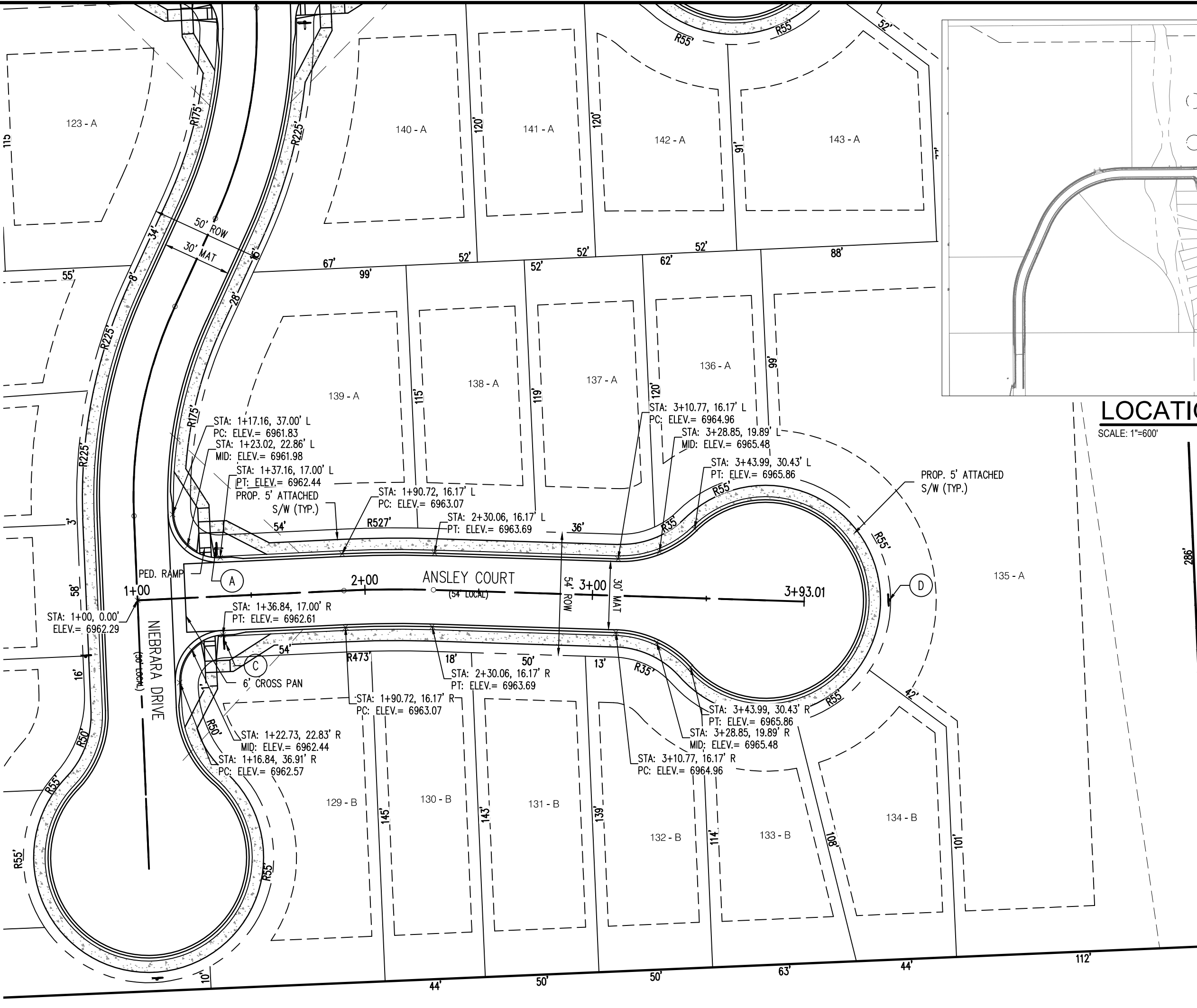
#	Date	Issue / Description	Init.
1			
2			
3			
4			
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7			
8			
9			
10			

Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

BARLEY COURT
STREET IMPROVEMENT PLAN



NIEBRARA DRIVE (PUBLIC)
(50' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL



ANSLEY COURT (PUBLIC)
(54' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL

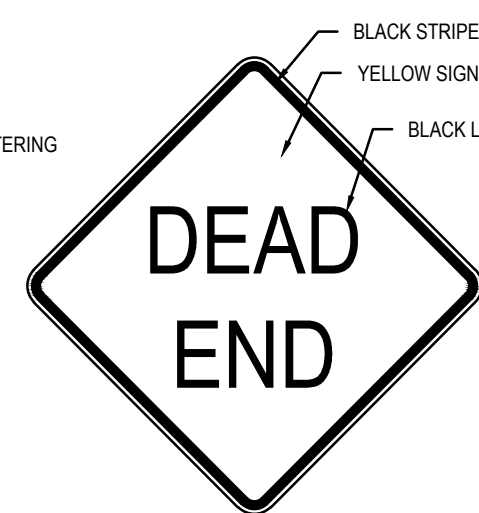
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ROAD NAME



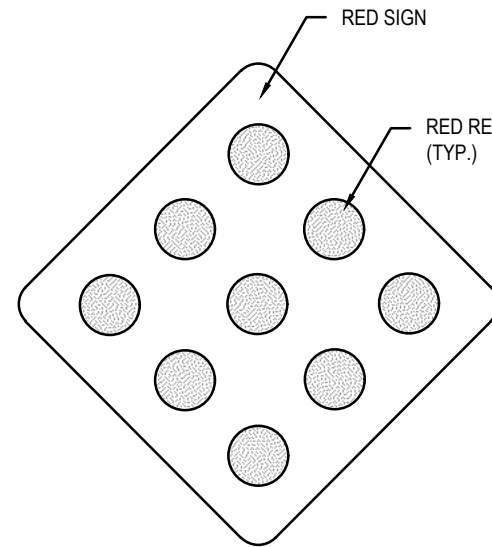
STOP SIGN (R1-1)



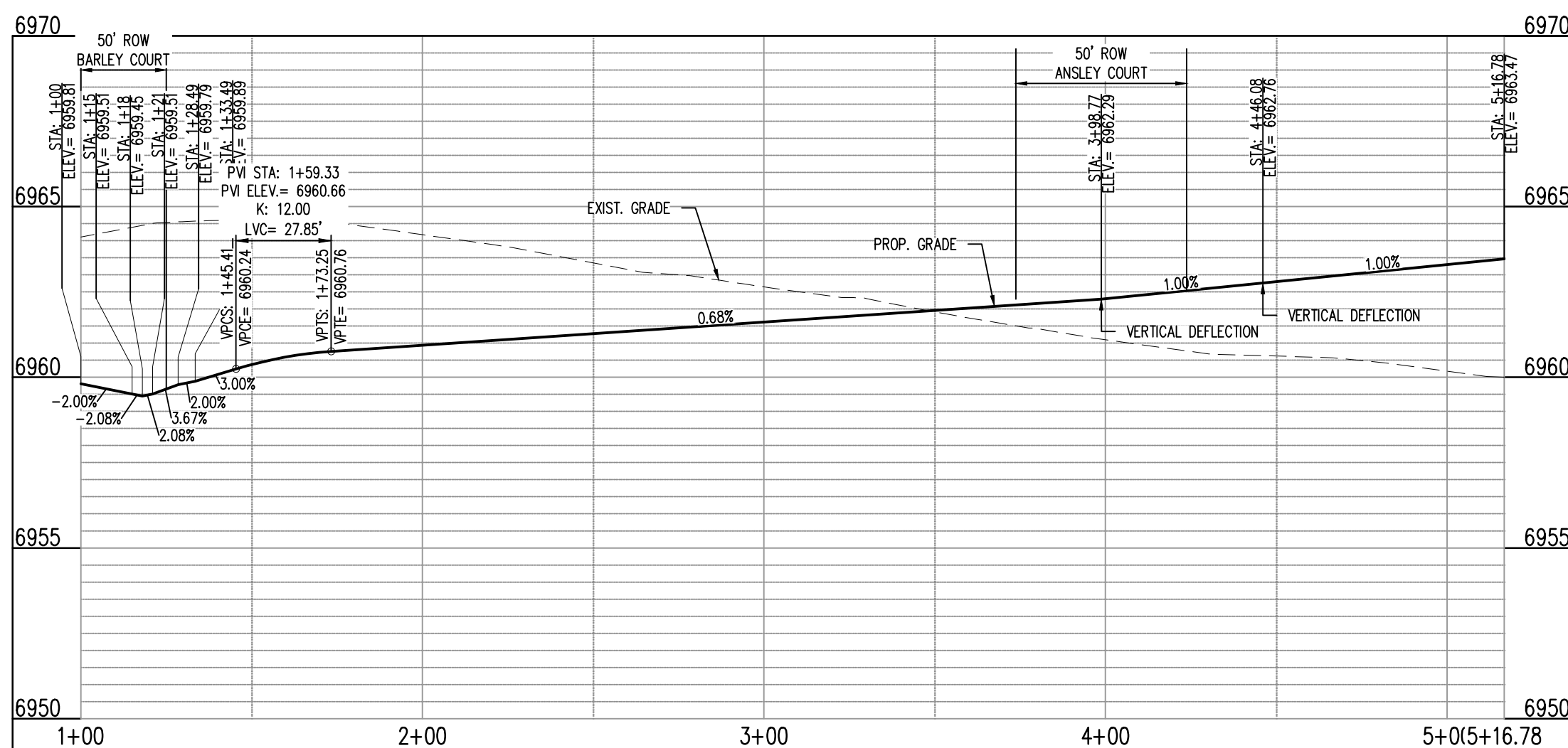
NO OUTLET SIGN (W14-2)
KEY NOTE: B



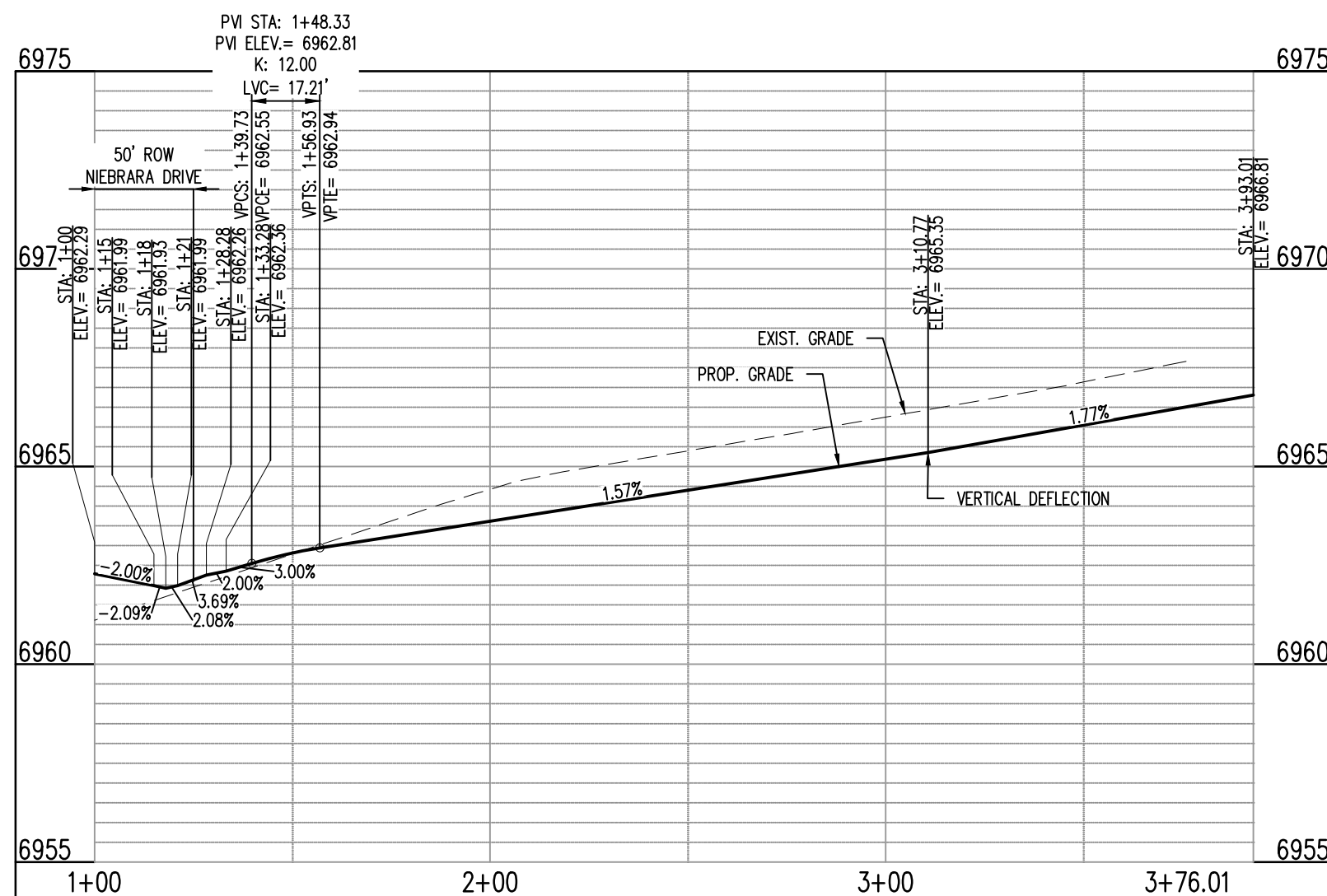
DEAD END SIGN (W14-1)
KEY NOTE: C



TYPE 4 OBJECT MARKER
KEY NOTE: D



NIEBRARA - CL - 50' ROW
STA: 1+00 - 5+16.78
SCALE: H: 1"=40' V: 1"=10'



ANSLEY CT - CL - 50' ROW
STA: 1+00 - 3+93.01
SCALE: H: 1"=40' V: 1"=10'



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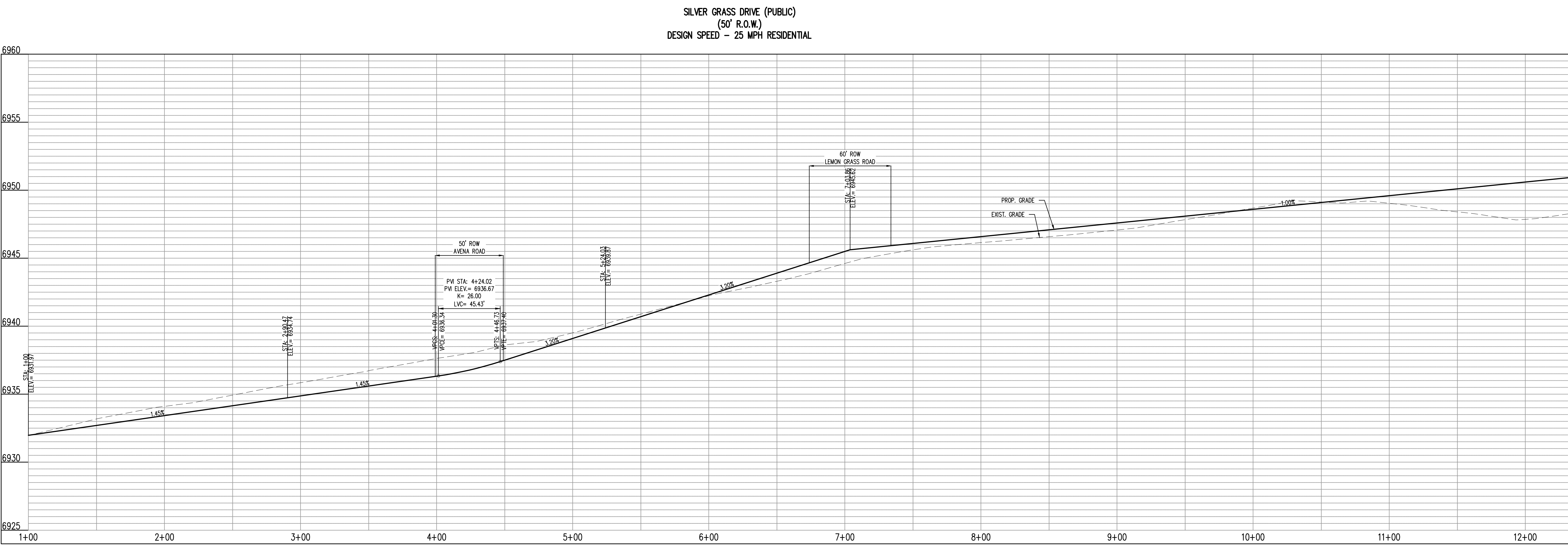
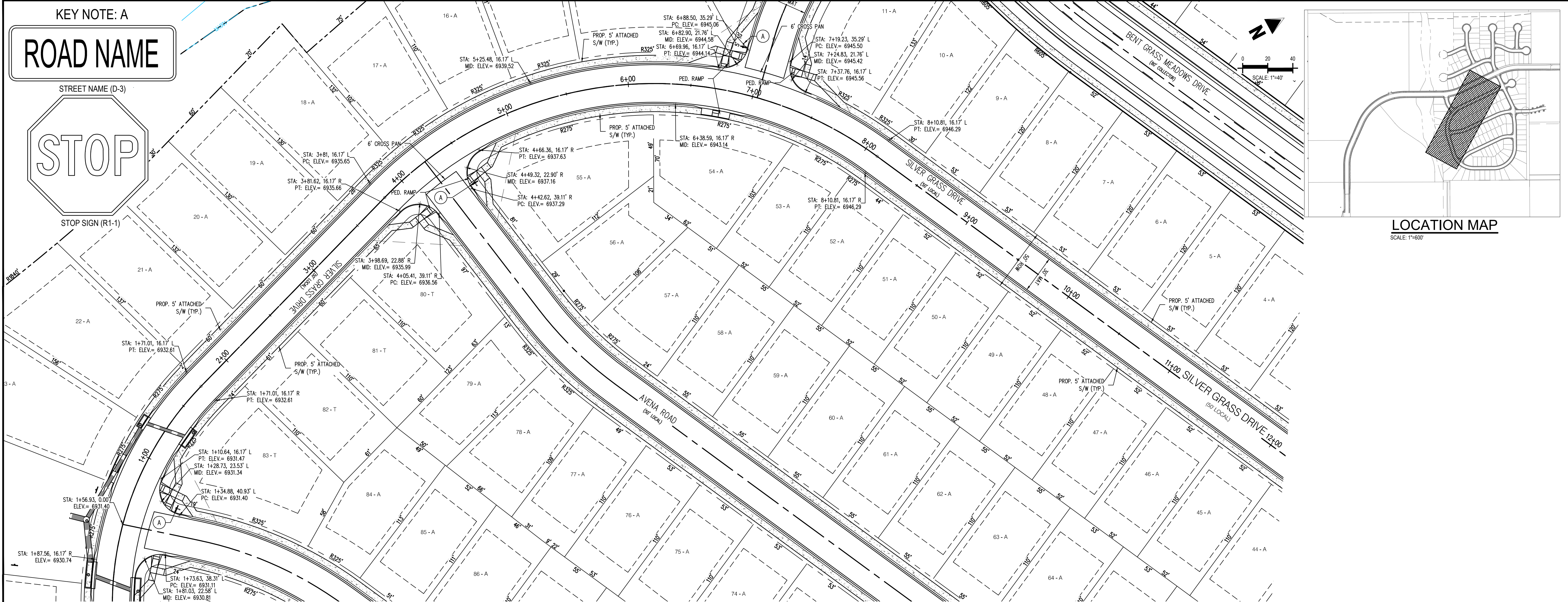
CHALLENGER
HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

#	Date	Issue / Description	Init.
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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

NIEBRARA DRIVE &
ANSLEY COURT
STREET IMPROVEMENT PLAN



Galloway

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Colorado Springs, CO 80920
719.900.7220
gallowayus.com

CHALLENGER HOMES

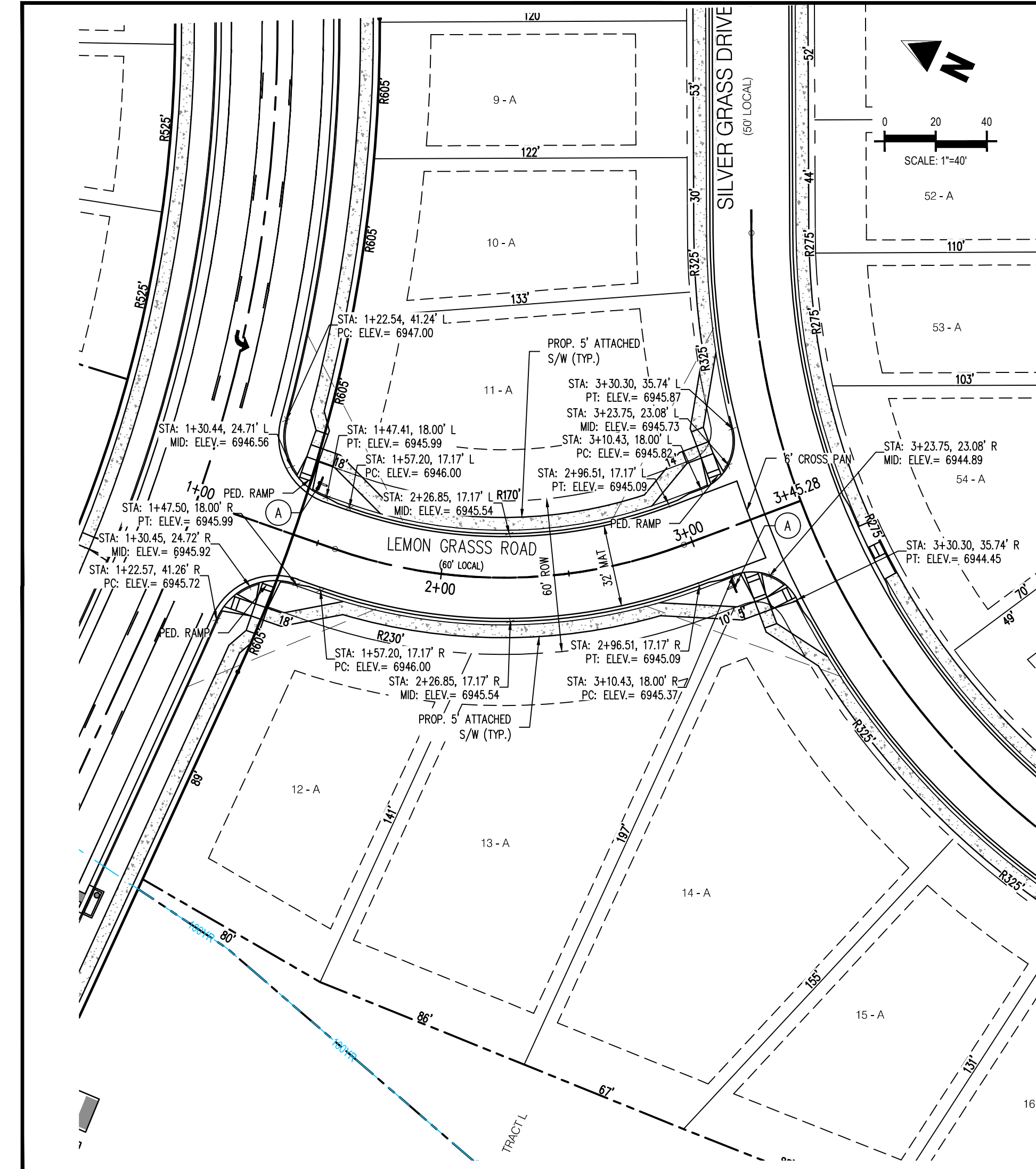
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

SILVER GRASS DRIVE
STREET IMPROVEMENT PLAN

C2.10
Sheet 15 of 43



LEMON GRASS ROAD (PUBLIC)
(60' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL

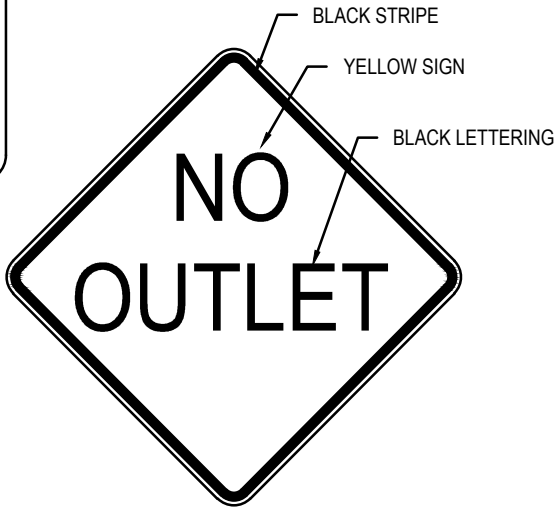
KEY NOTE: A



STREET NAME (D-3)

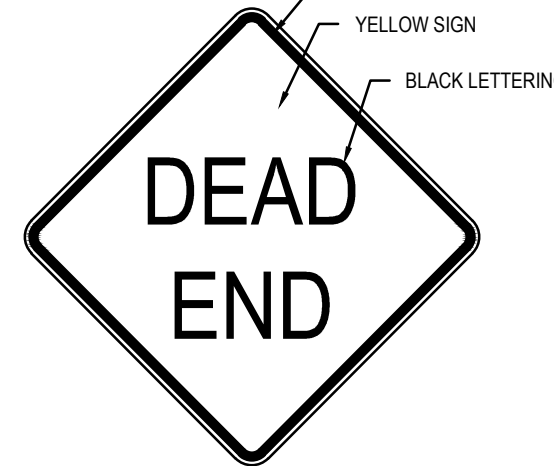


STOP SIGN (R1-1)



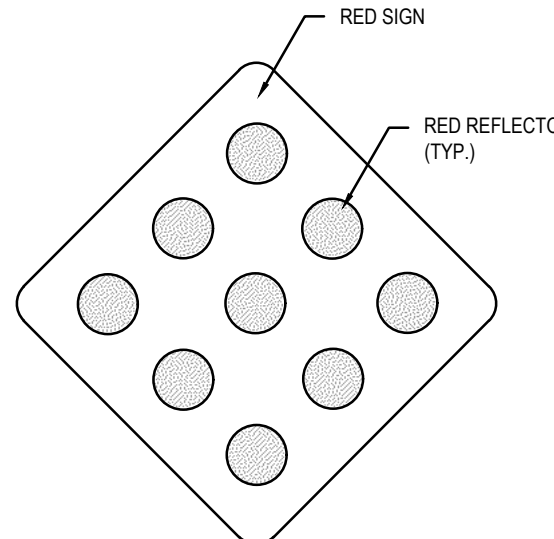
NO OUTLET SIGN (W14-2)

KEY NOTE: B



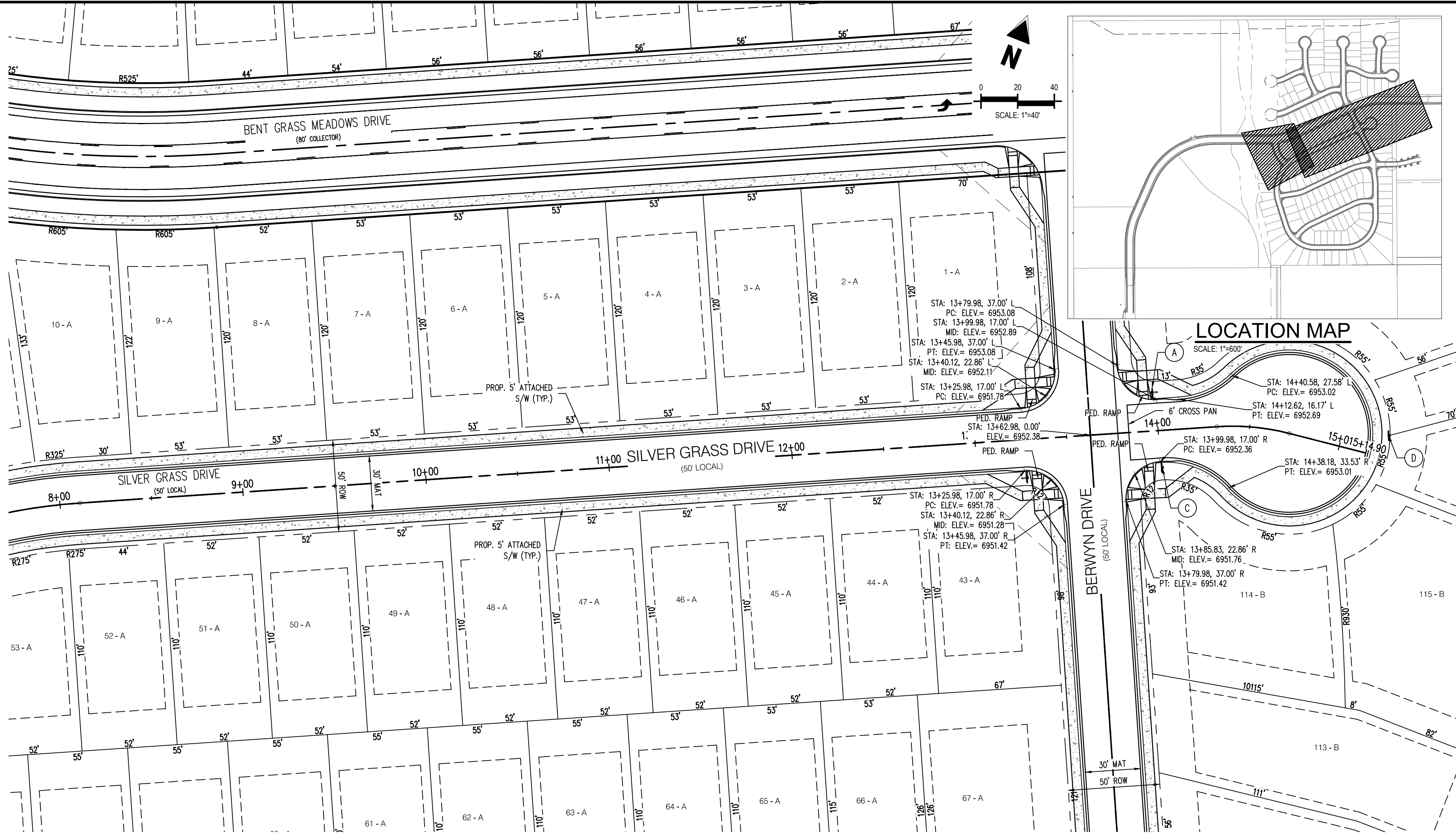
DEAD END SIGN (W14-1)

KEY NOTE: C

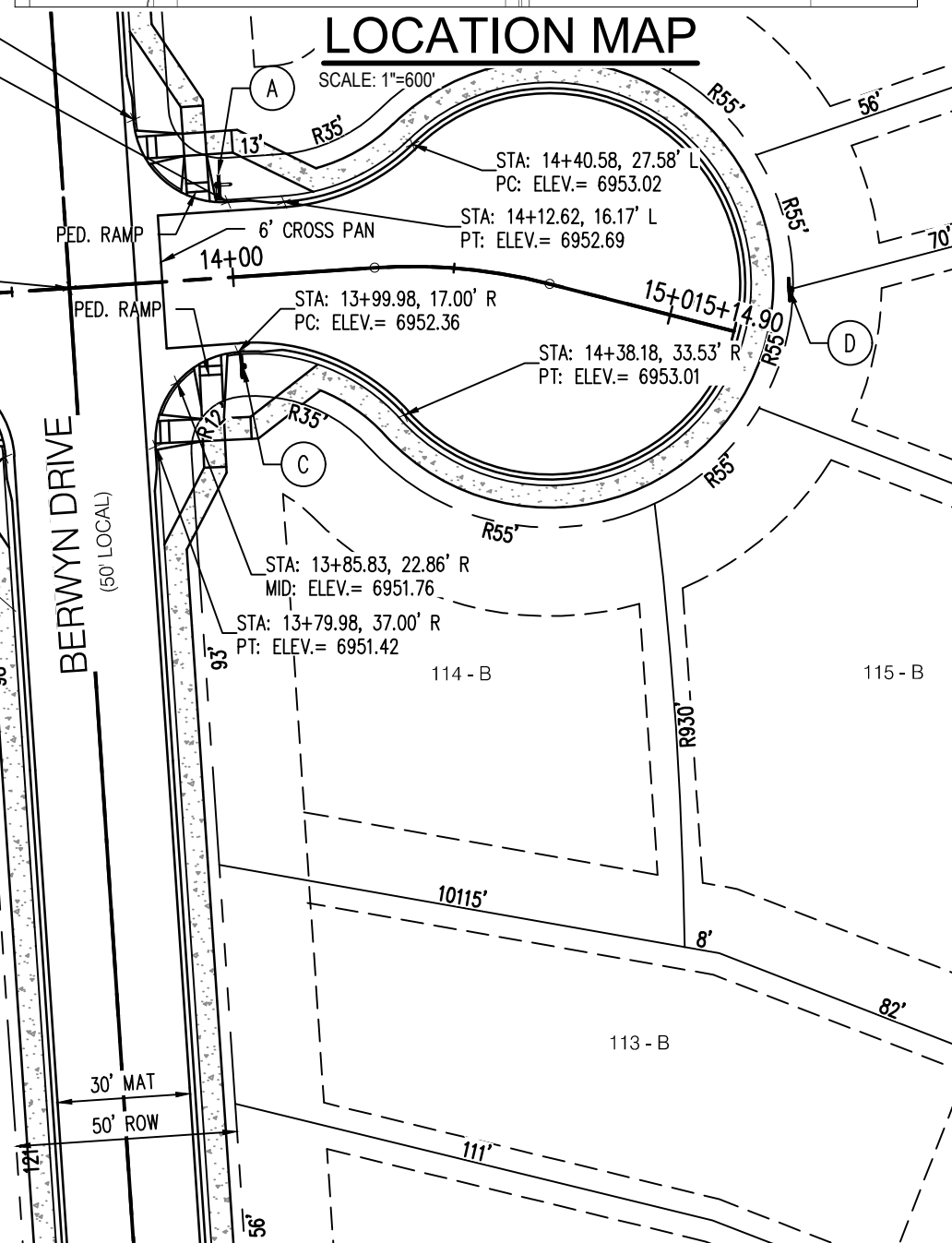
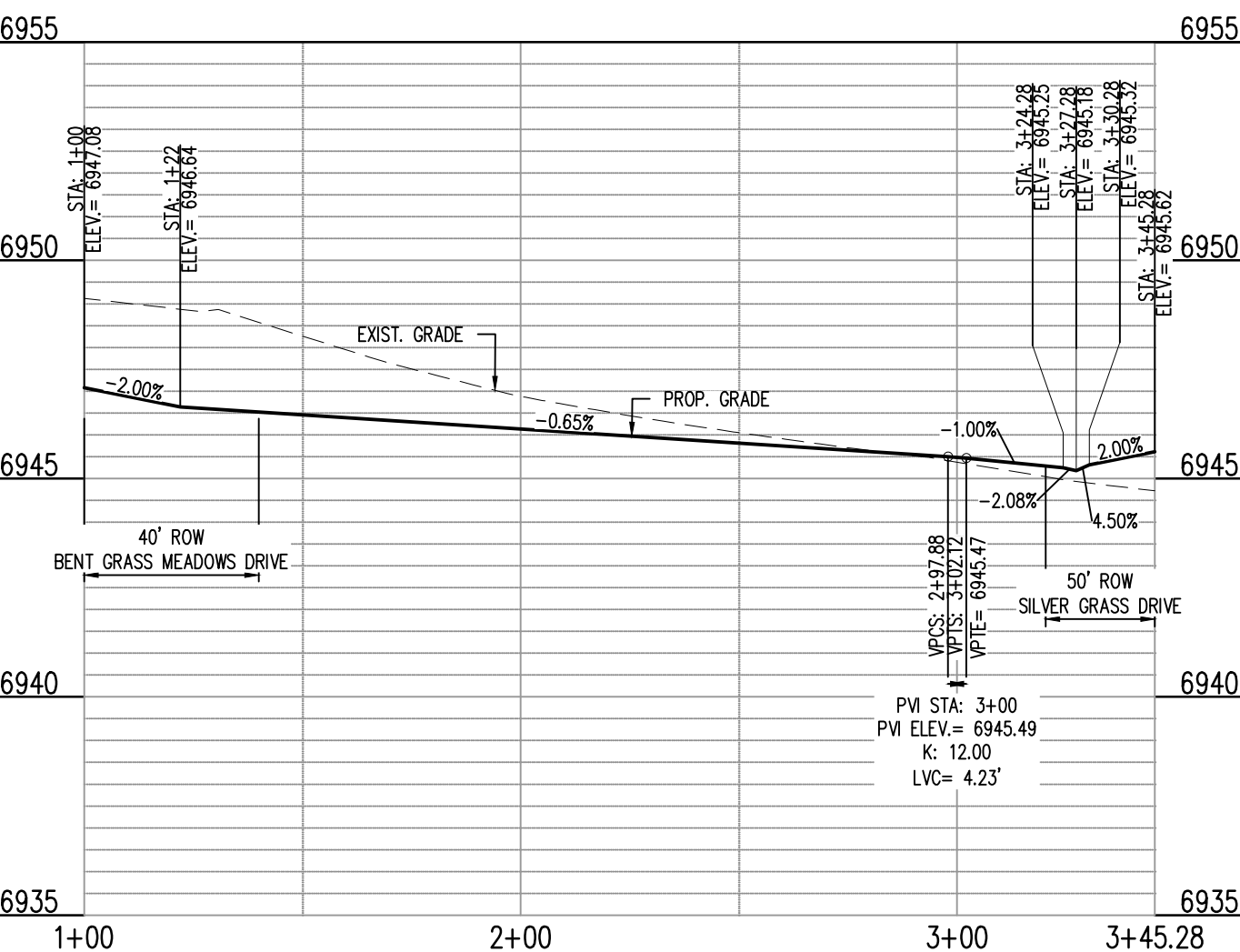
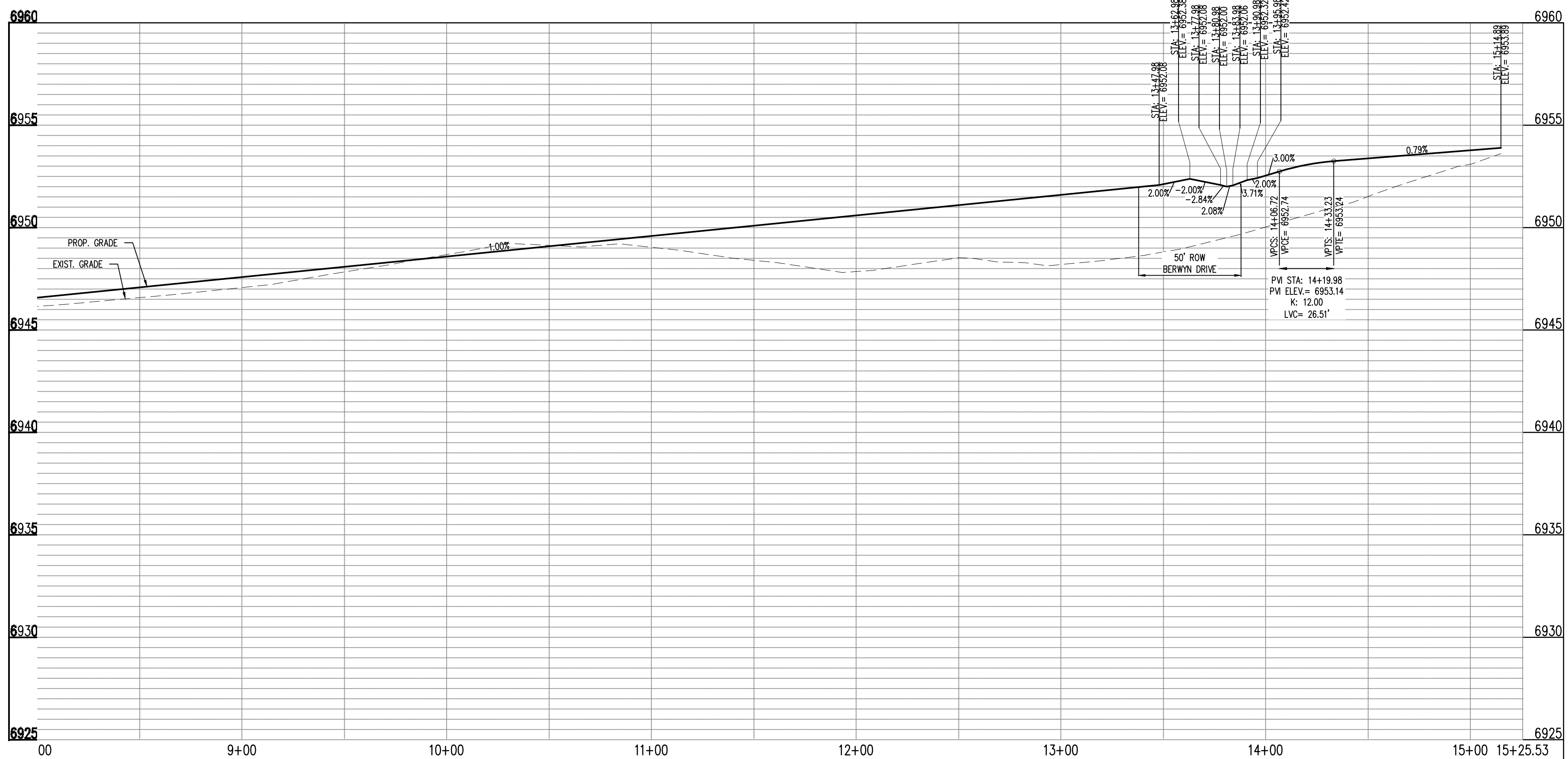


TYPE 4 OBJECT MARKER

KEY NOTE: D



SILVER GRASS ROAD (PUBLIC)
(50' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL



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HOMES**

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

SILVER GRASS DRIVE &
LEMON GRASS ROAD
STREET IMPROVEMENT PLAN

C2.11

Sheet 16 of 43



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CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

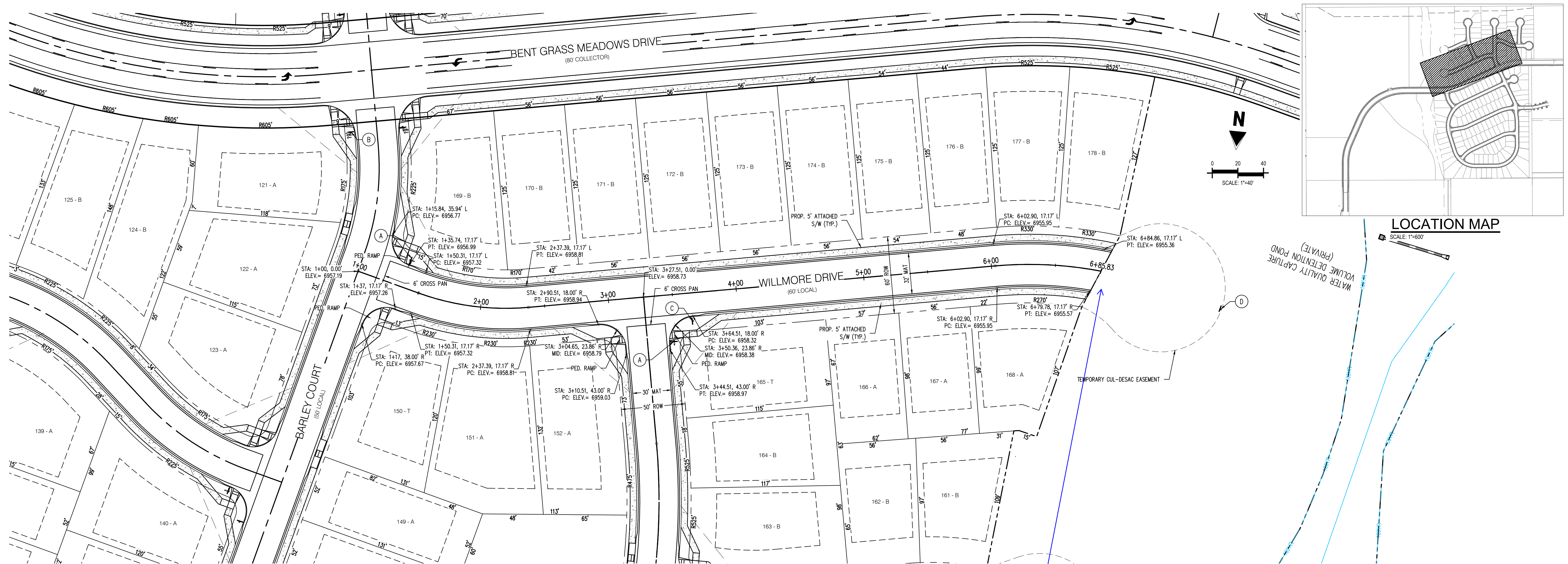
[illegible]

Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

WILLMORE DRIVE
STREET IMPROVEMENT PLAN

C2.12

Sheet 17 of 43

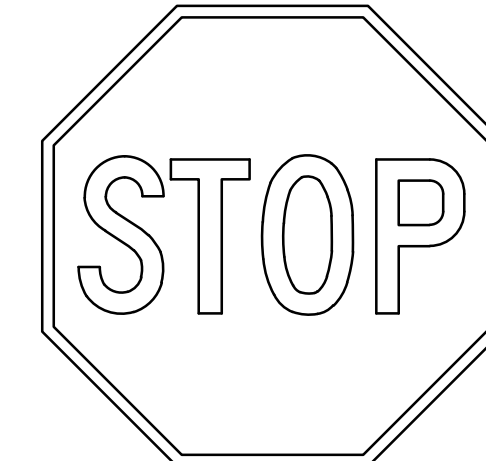


WILMORE DRIVE (PUBLIC)
(60' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL

Provide all
cul-de-sac and
ROW and profile
geometry. Show
pavement and
shoulder.

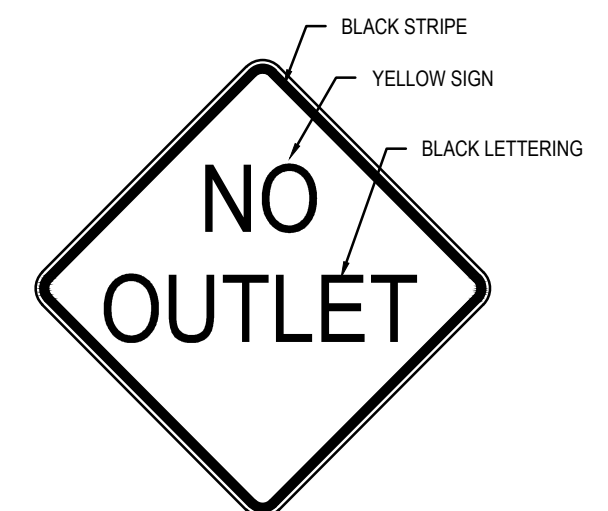


STREET NAME (D-3)



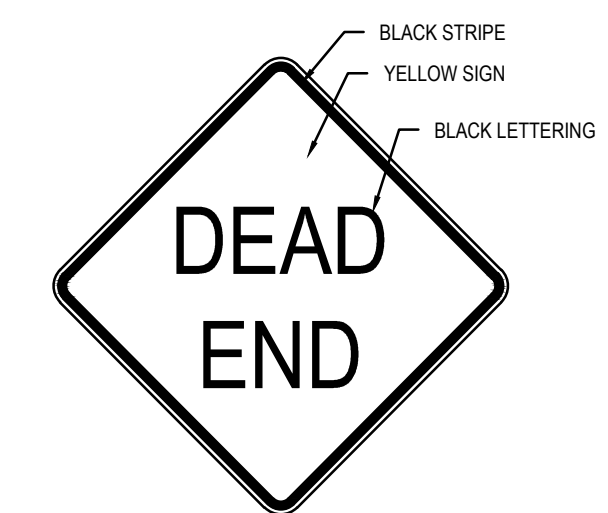
STOP SIGN (R1-1)

KEY NOTE: A



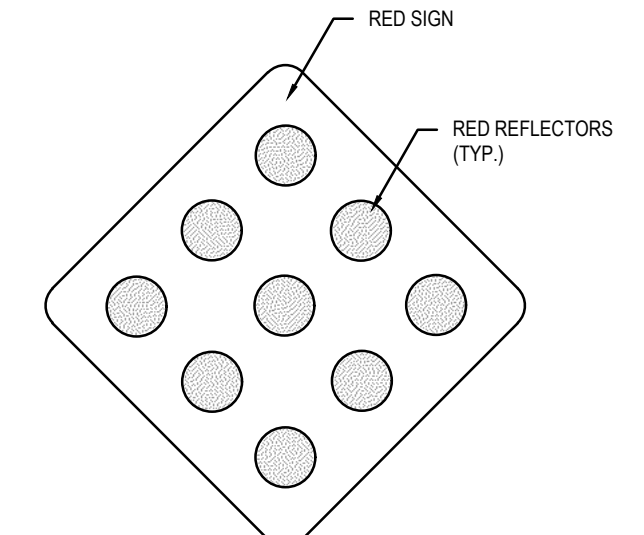
NO OUTLET SIGN (W14-2)

KEY NOTE: B



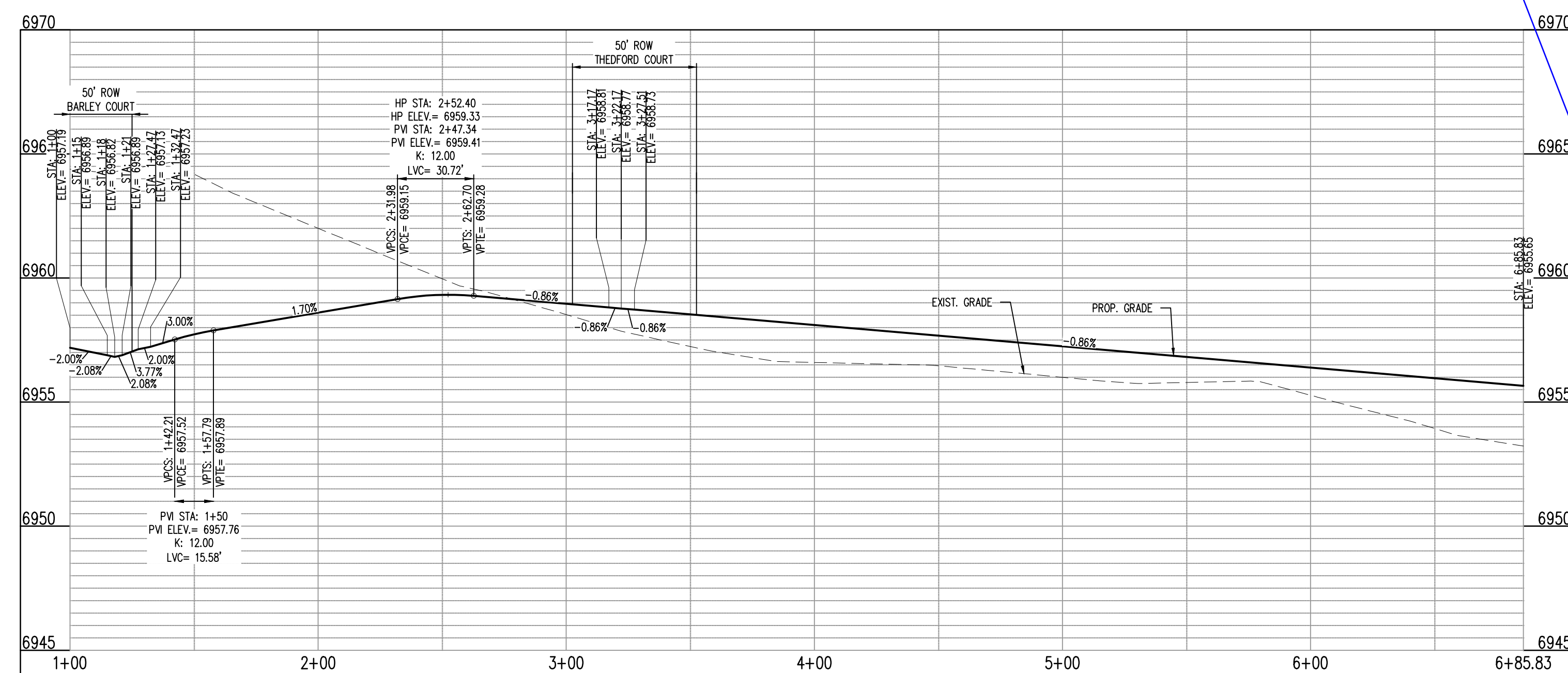
DEAD END SIGN (W14-1)

KEY NOTE: C

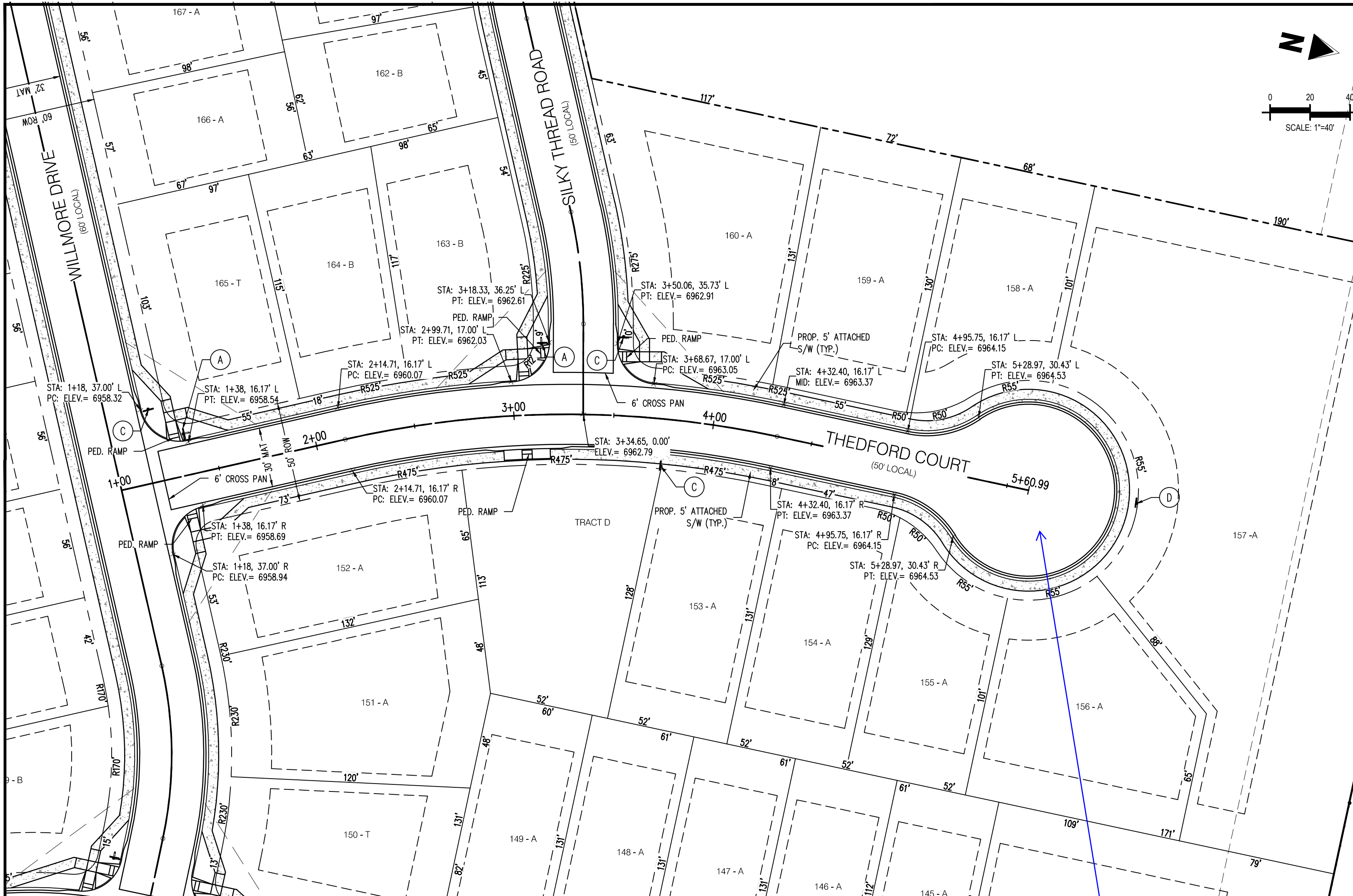


TYPE 4 OBJECT MARKER

KEY NOTE: D

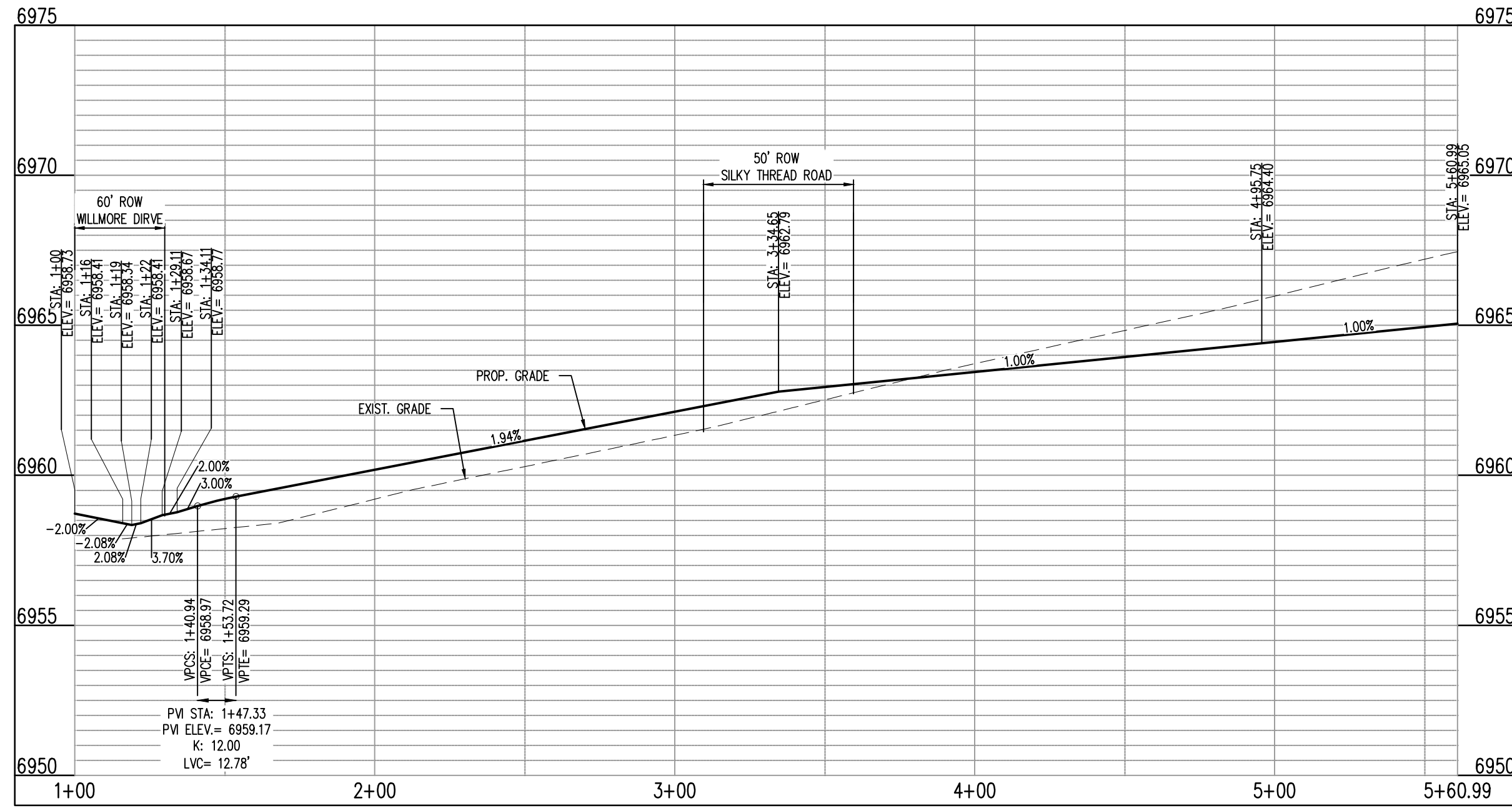


WILLMORE DR EAST - CL - 60' ROW
STA: 1+00 - 6+85.83
SCALE: H: 1"=40' V: 1"=10'

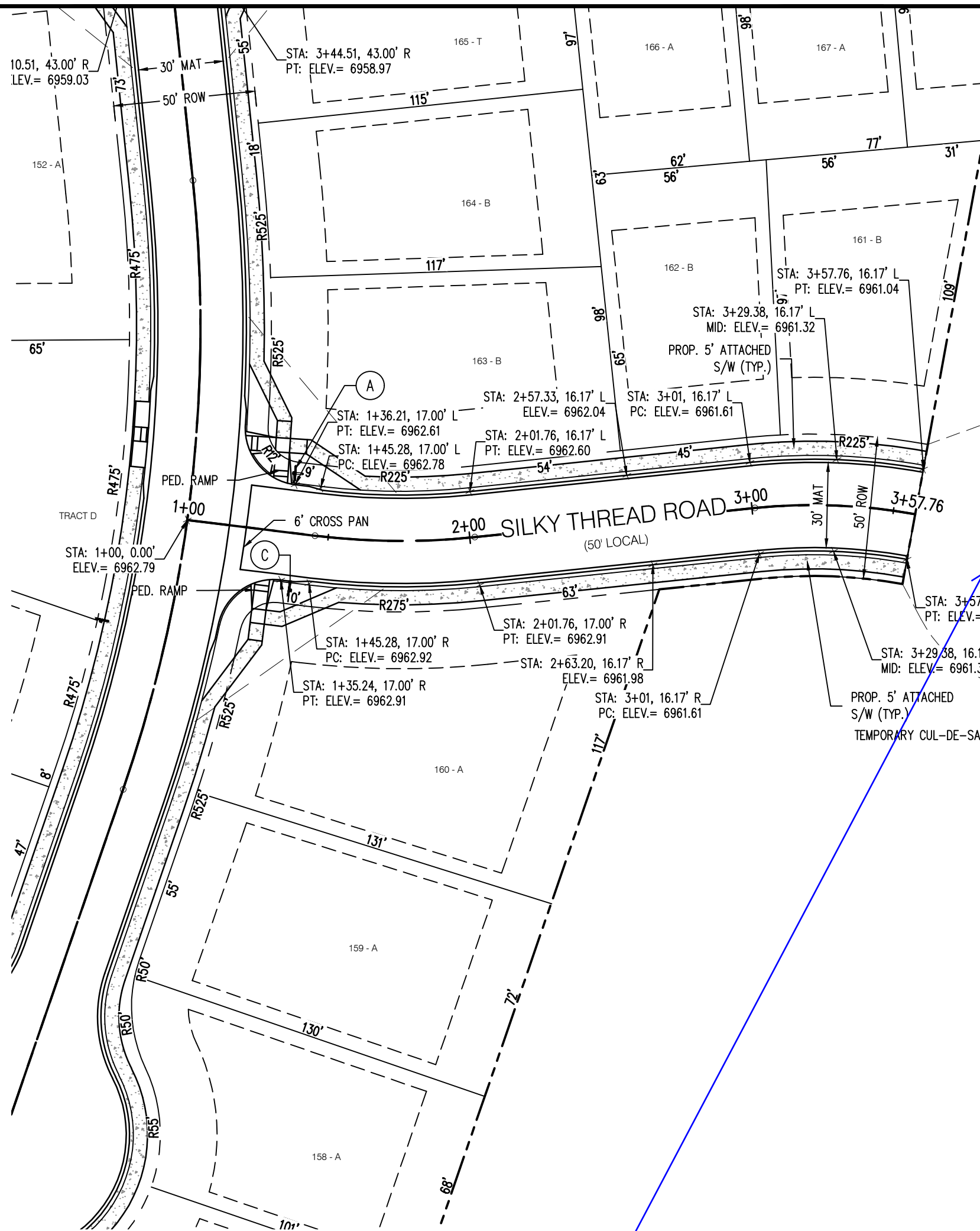


THEDFORD COURT (PUBLIC)
(50' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL

Label all slopes
and cross-slopes
(all cul-de-sacs)

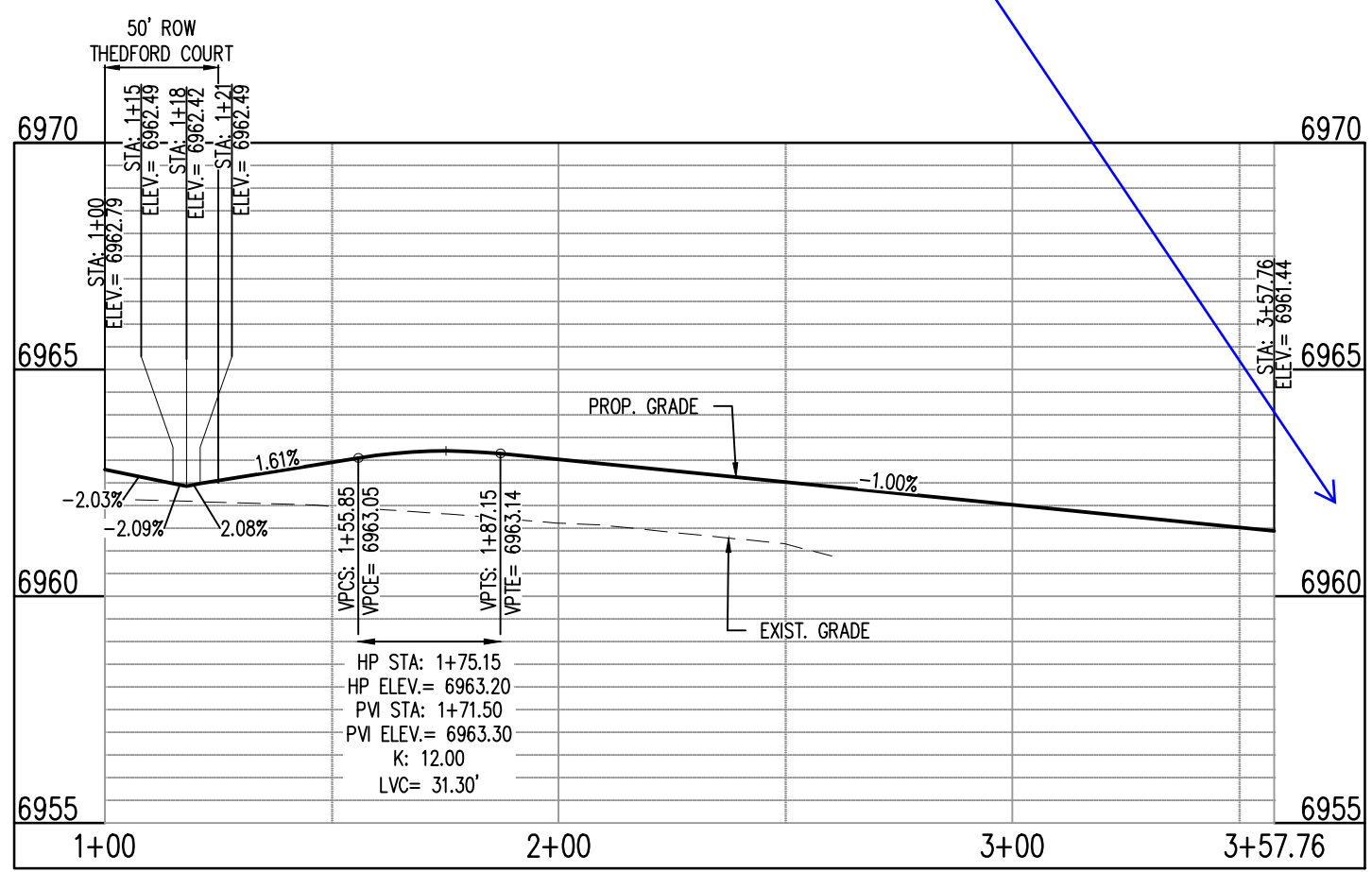


THEDFORD CT - CL - 50' ROW
STA: 1+00 - 5+60.99
SCALE: H: 1"=40' V: 1"=10'

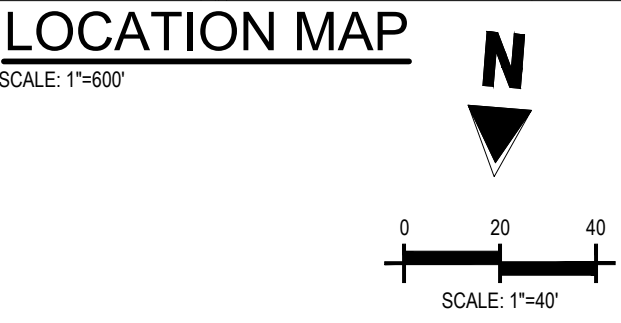
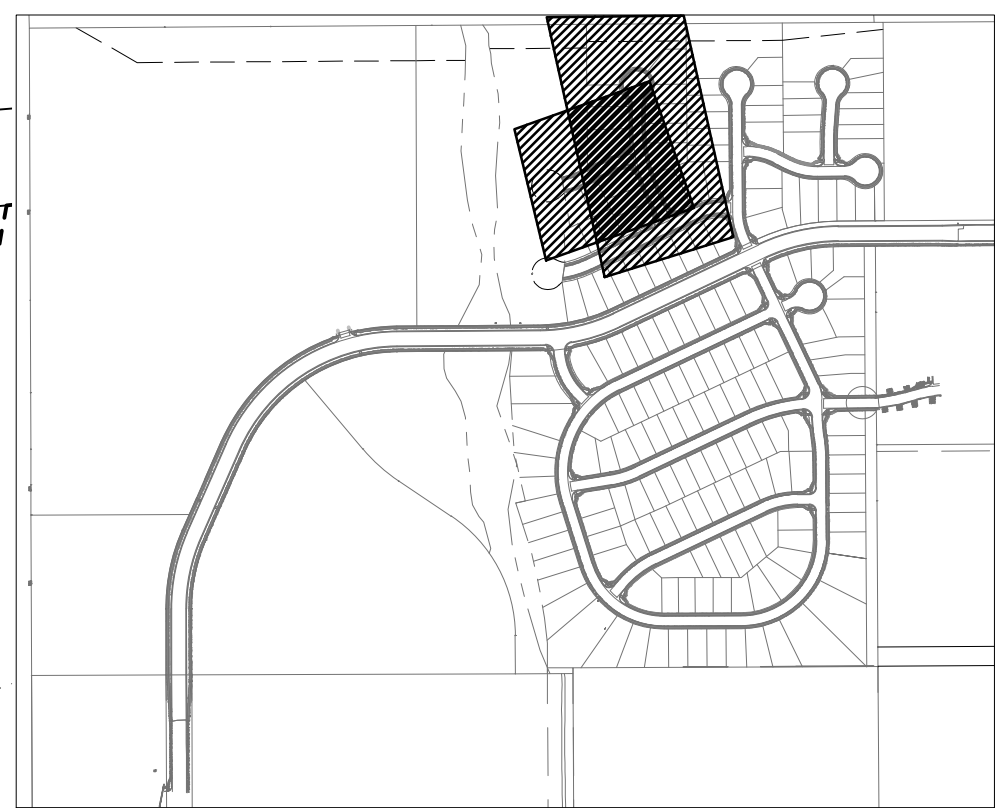


SILKY THREAD ROAD (PUBLIC)
(50' R.O.W.)
DESIGN SPEED - 25 MPH RESIDENTIAL

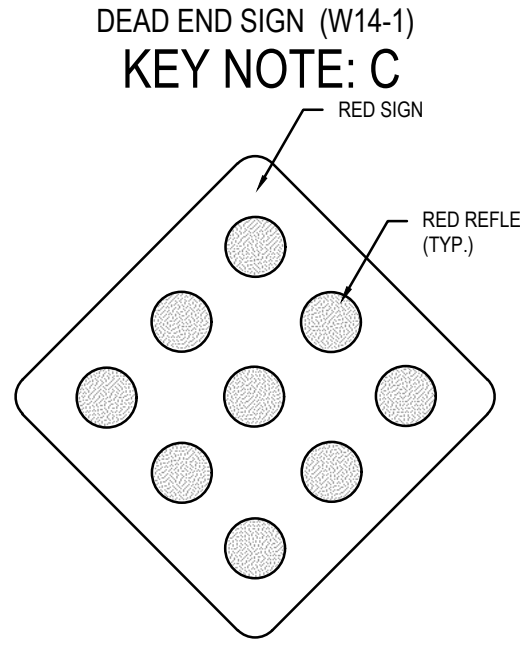
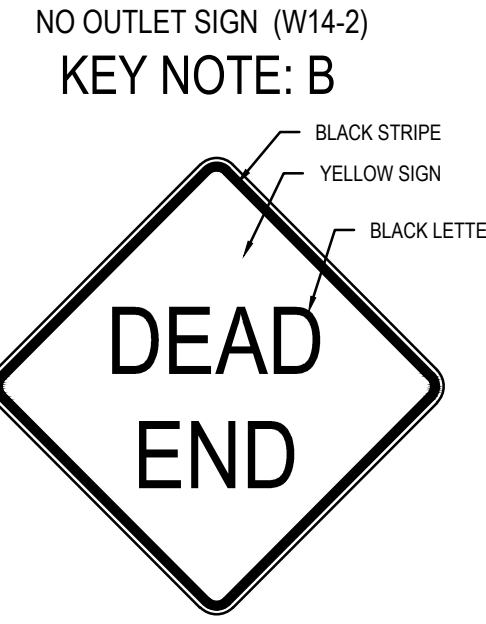
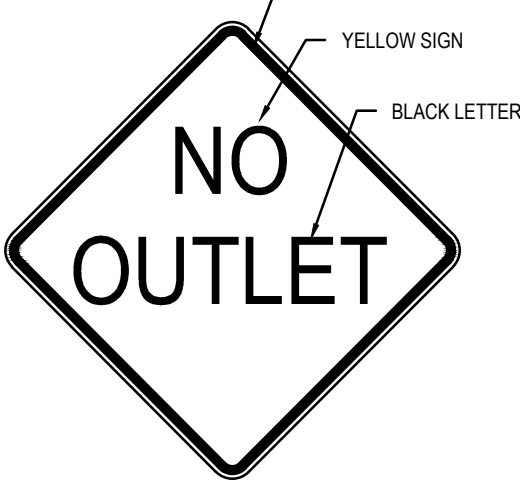
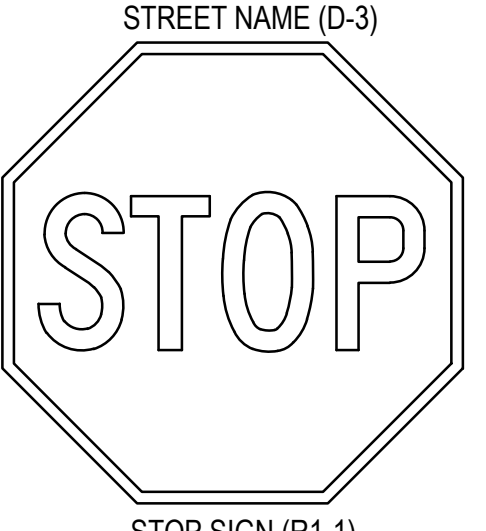
Provide all
cul-de-sac and
ROW and profile
geometry. Show
pavement and
shoulder.



SIKY THRED RD EAST - CL - 50' ROW
STA: 1+00 - 3+57.76
SCALE: H: 1"=40' V: 1"=10'



KEY NOTE: A
ROAD NAME



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**CHALLENGER
HOMES**

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

#	Date	Issue / Description	Init.
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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

THEDFORD COURT &
SILKY THREAD ROAD
STREET IMPROVEMENT PLAN



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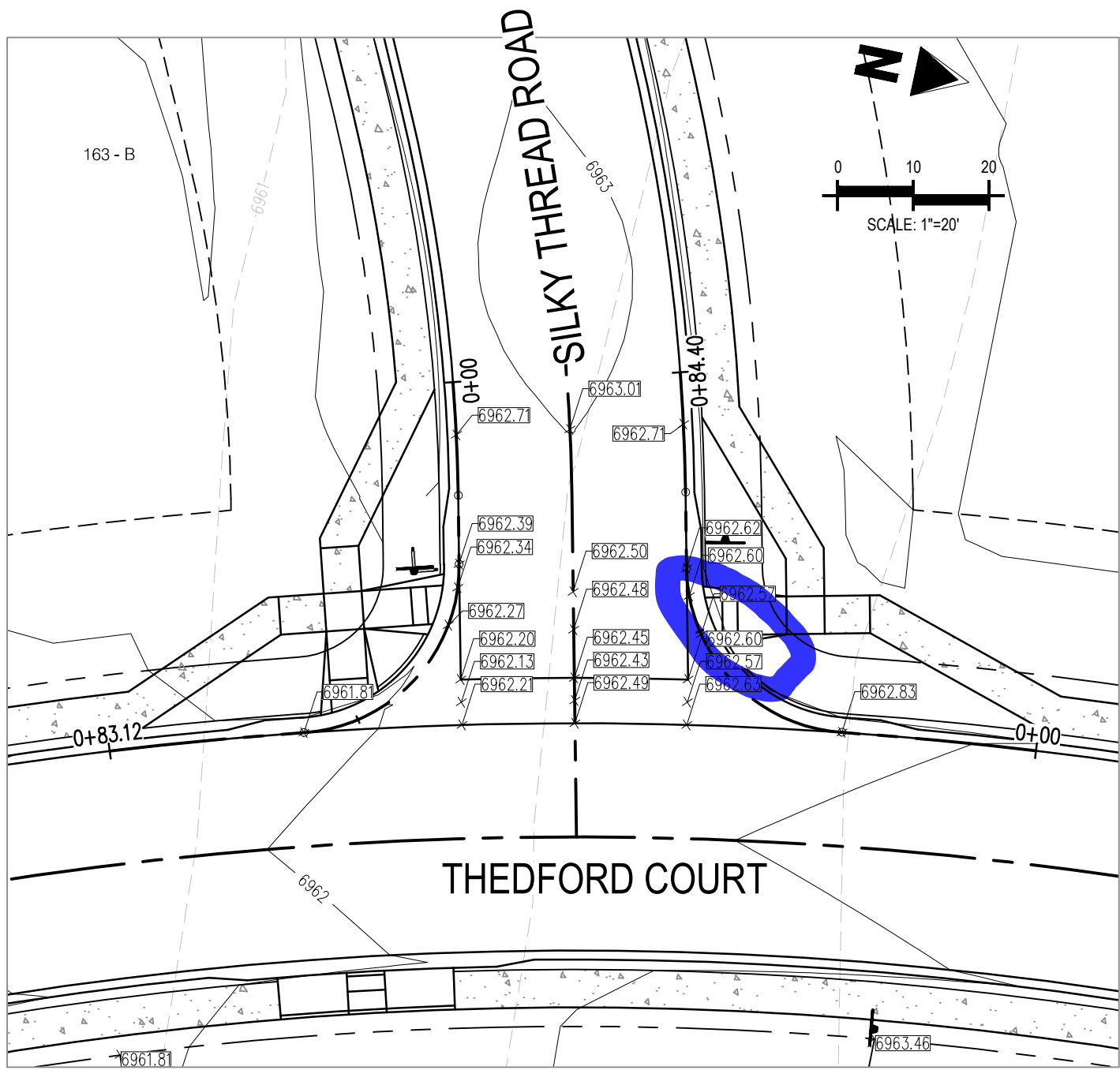
CHALLENGER
HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

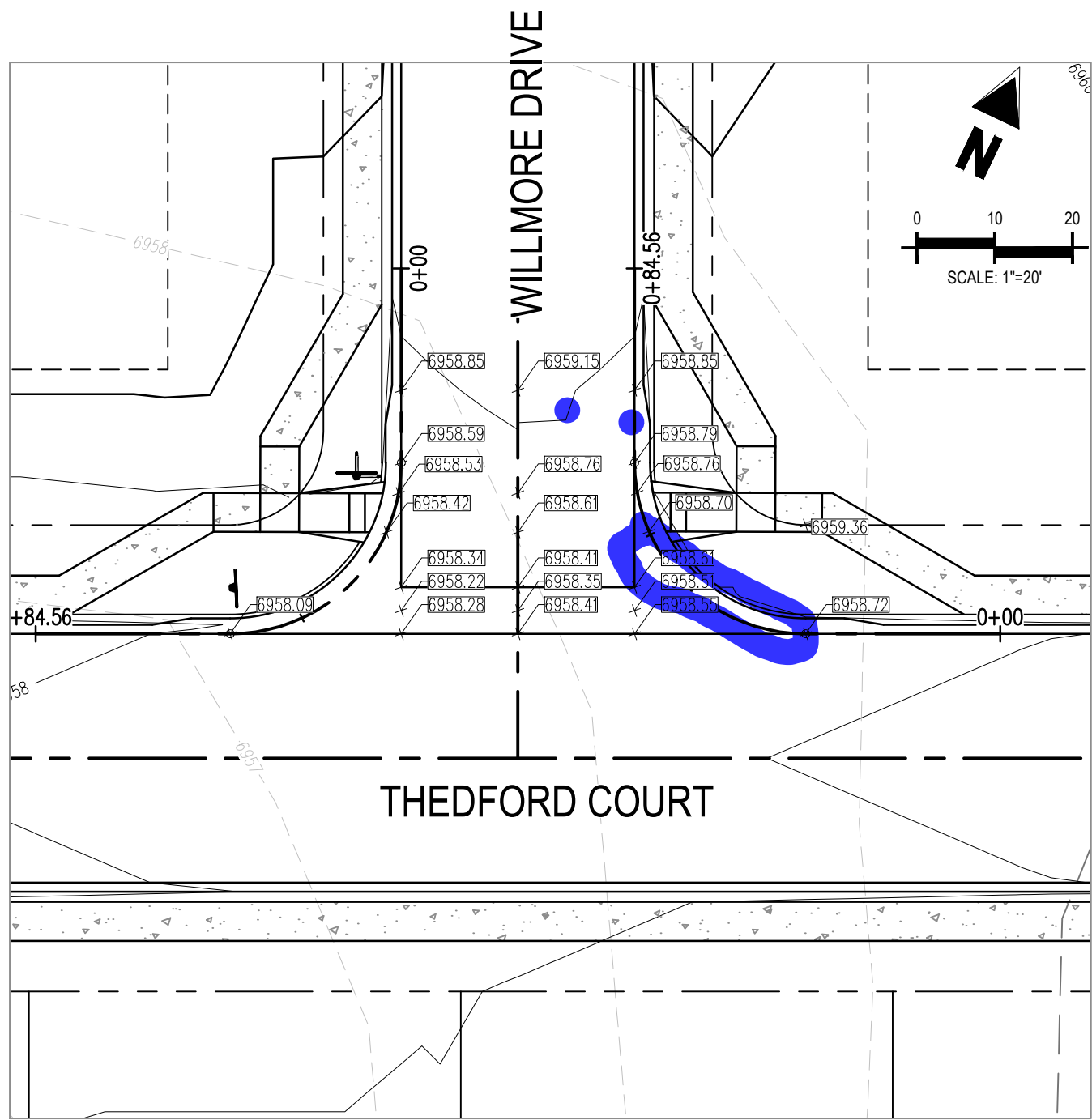
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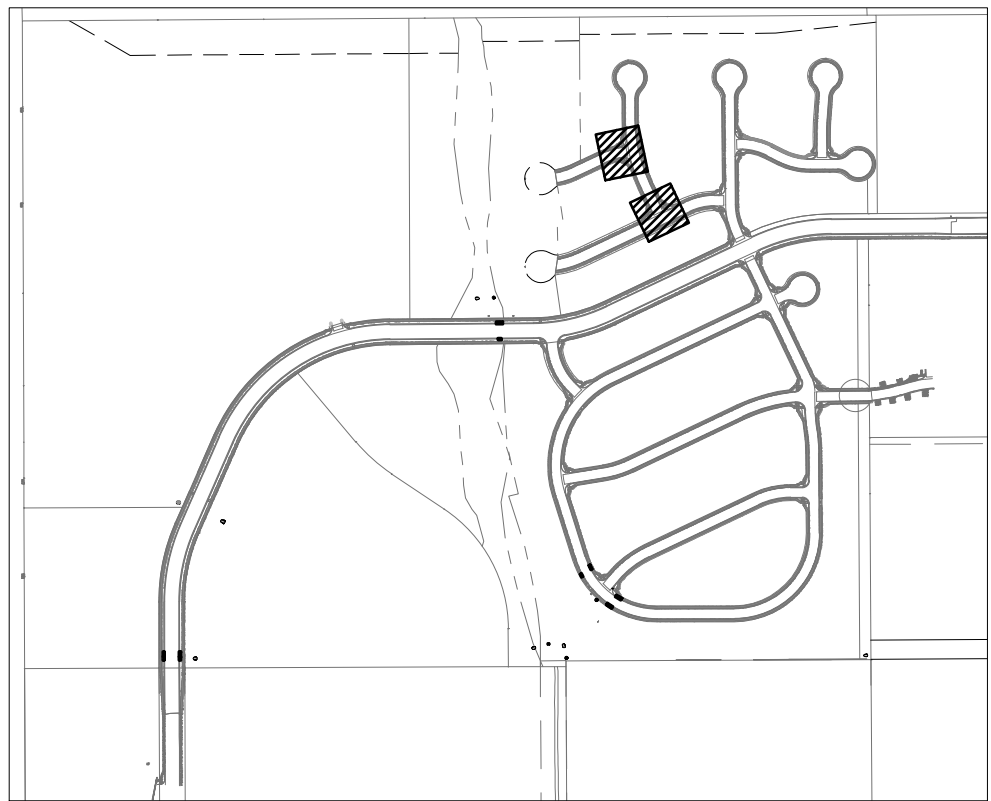
CROSS PAN &
CURB RETURN PROFILES



INTERSECTION OF:
THEDFORD COURT & SILKY THREAD ROAD



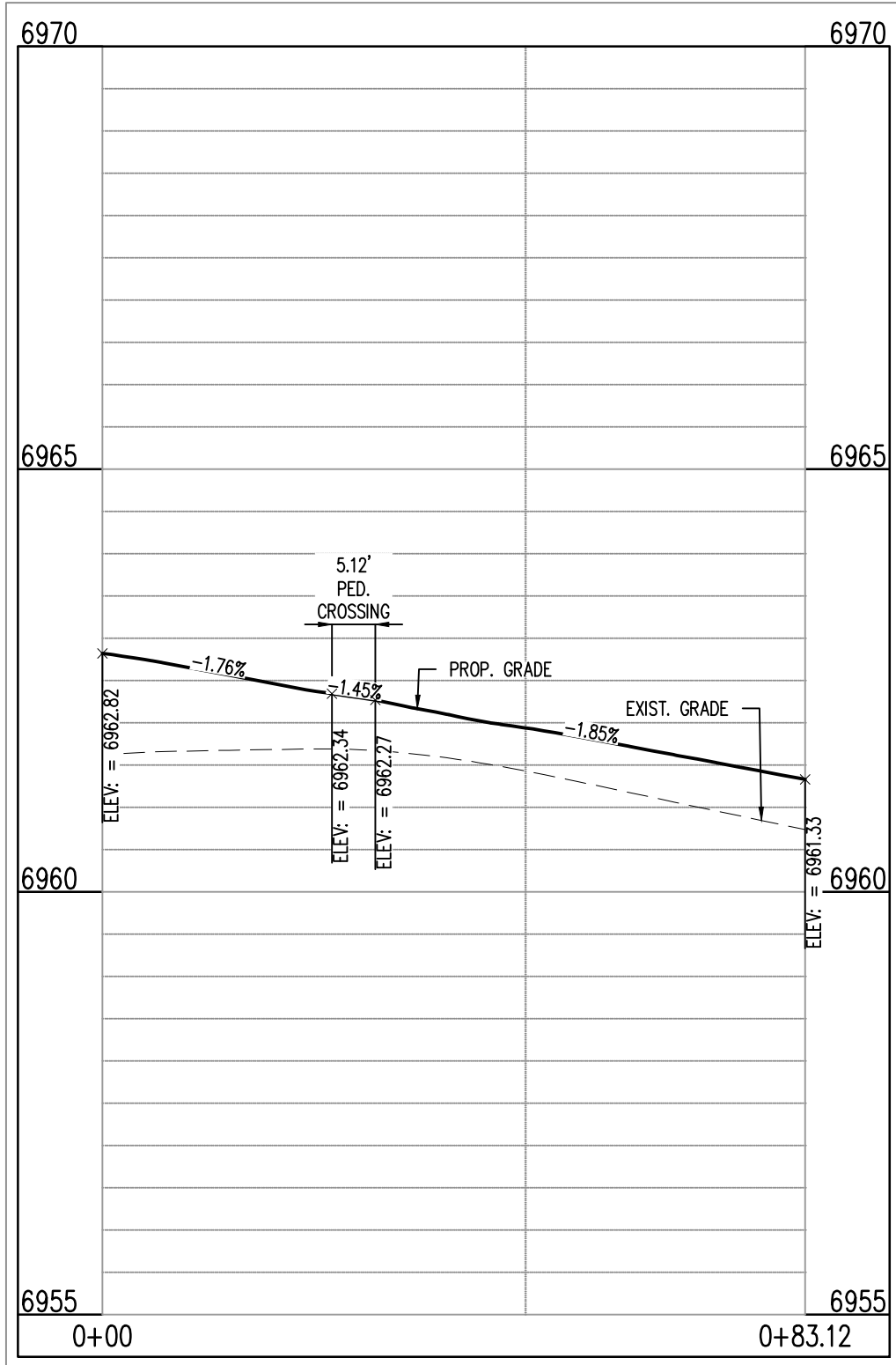
INTERSECTION OF:
WILLMORE DRIVE & THEDFORD COURT



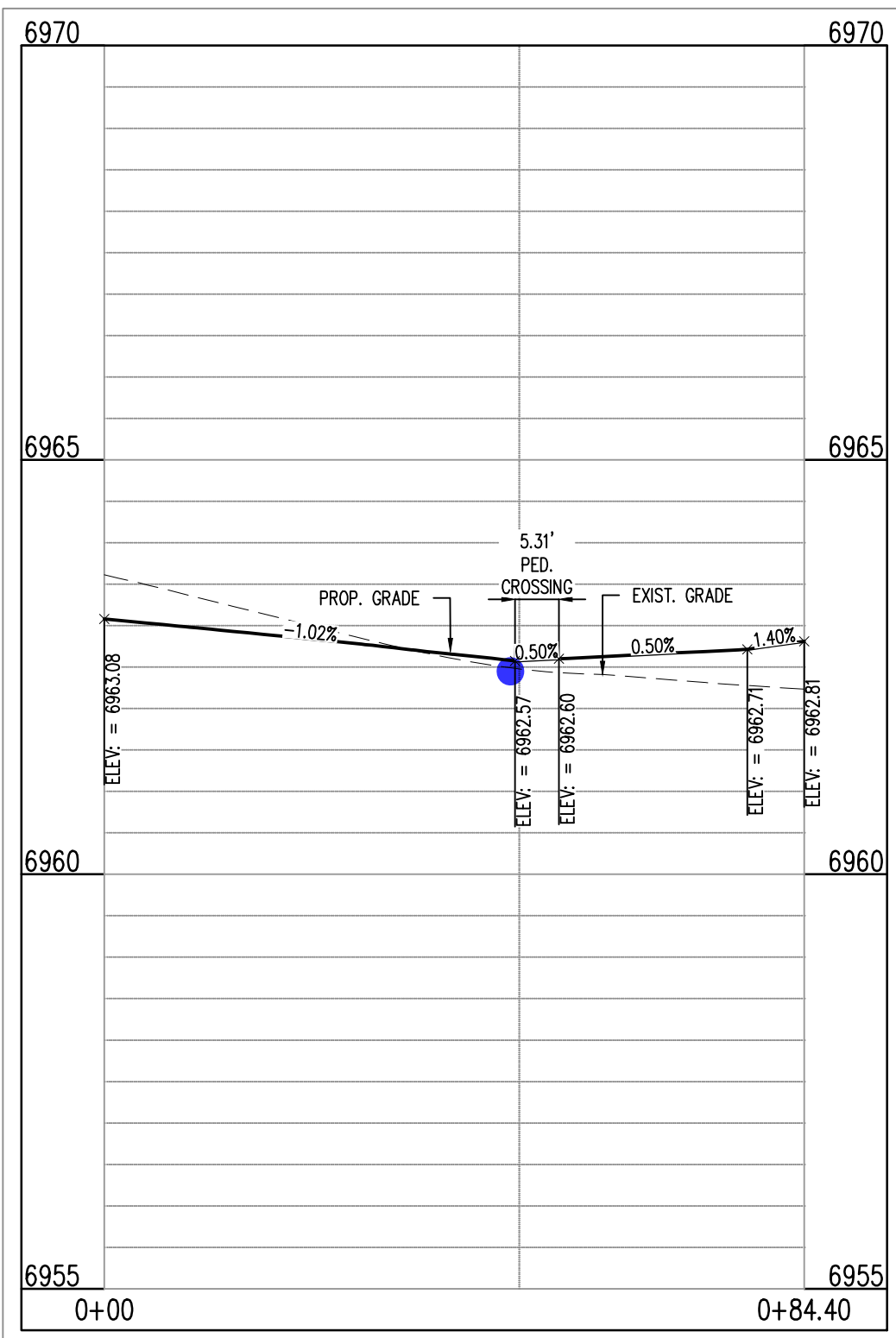
LOCATION MAP

GRADING LEGEND

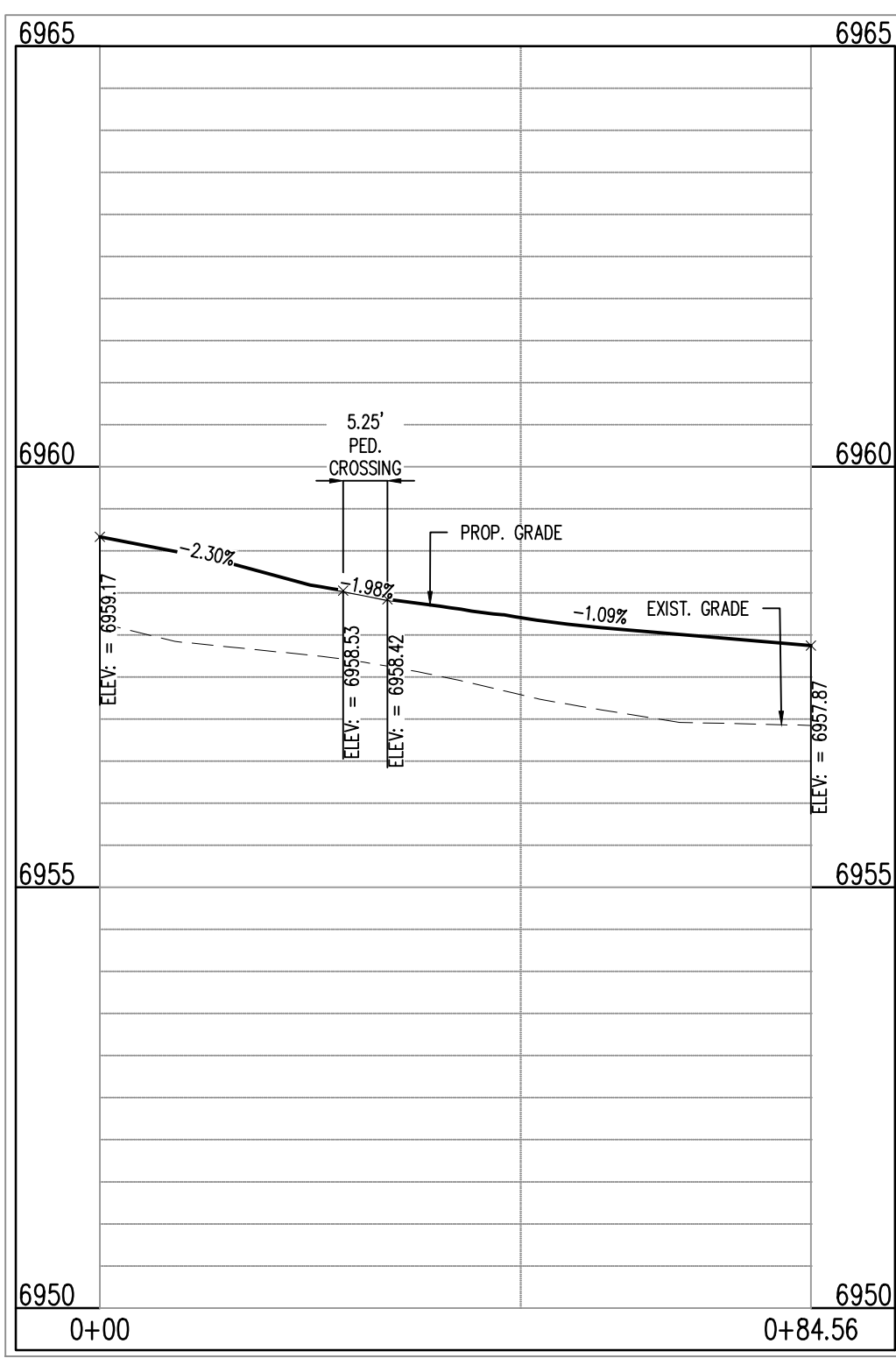
- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPERTY BOUNDARY
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING FLOW
- PROPOSED FLOW
- PROPOSED SPOT ELEVATION



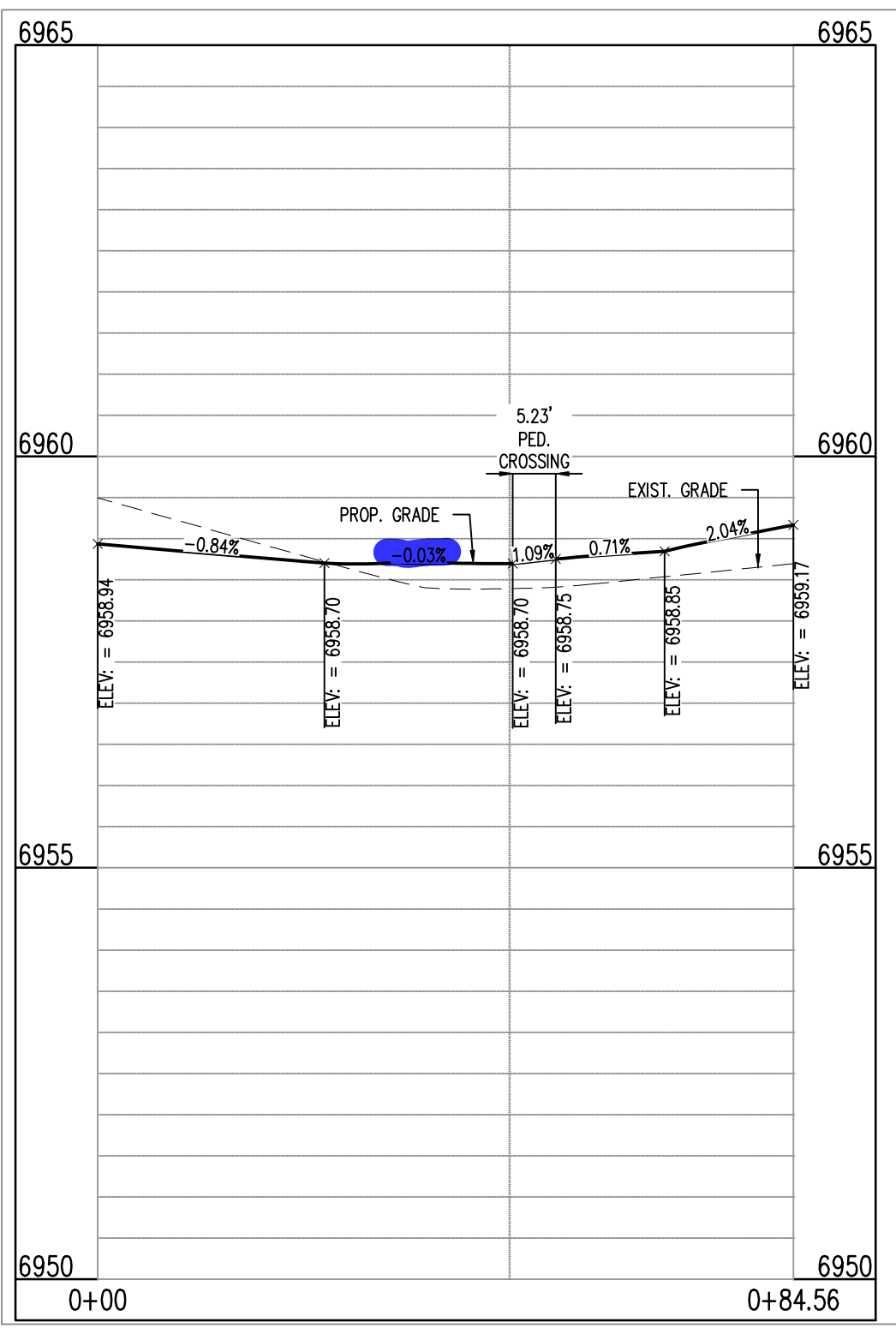
THEDFORD CT & SILK THREAD RD (SW): STA. 1+00.00 - 0+83.12
SCALE: H:1"=20', V:1"=10'



THEDFORD CT & SILK THREAD RD (NW): STA. 1+00.00 - 0+84.40
SCALE: H:1"=20', V:1"=10'



THEDFORD CT & WILLMORE DR (NW): STA. 1+00.00 - 0+84.56
SCALE: H:1"=20', V:1"=10'



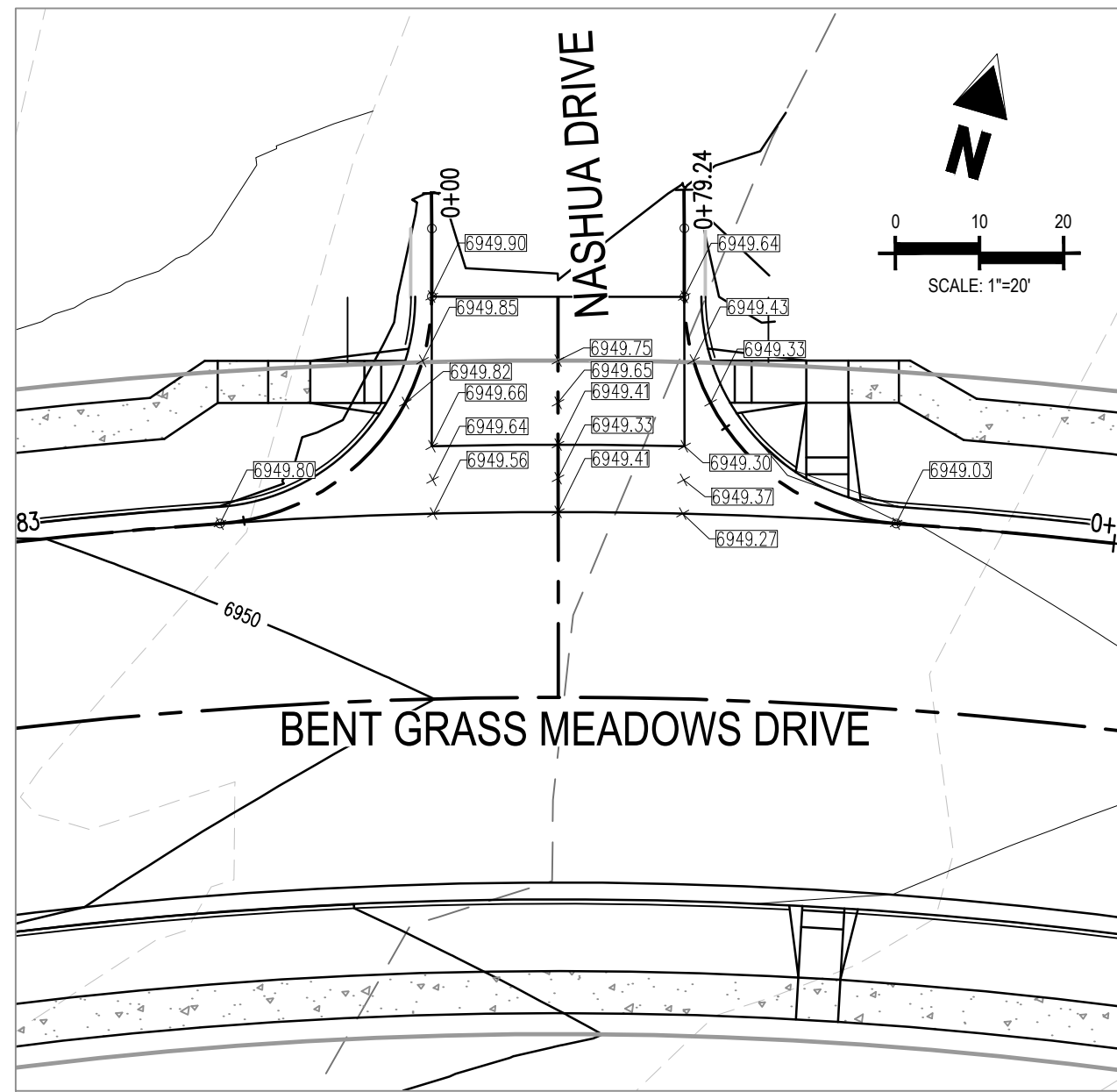
THEDFORD CT & WILLMORE DR (NE): STA. 1+00.00 - 0+84.56
SCALE: H:1"=20', V:1"=10'

CAUTION - NOTICE TO CONTRACTOR

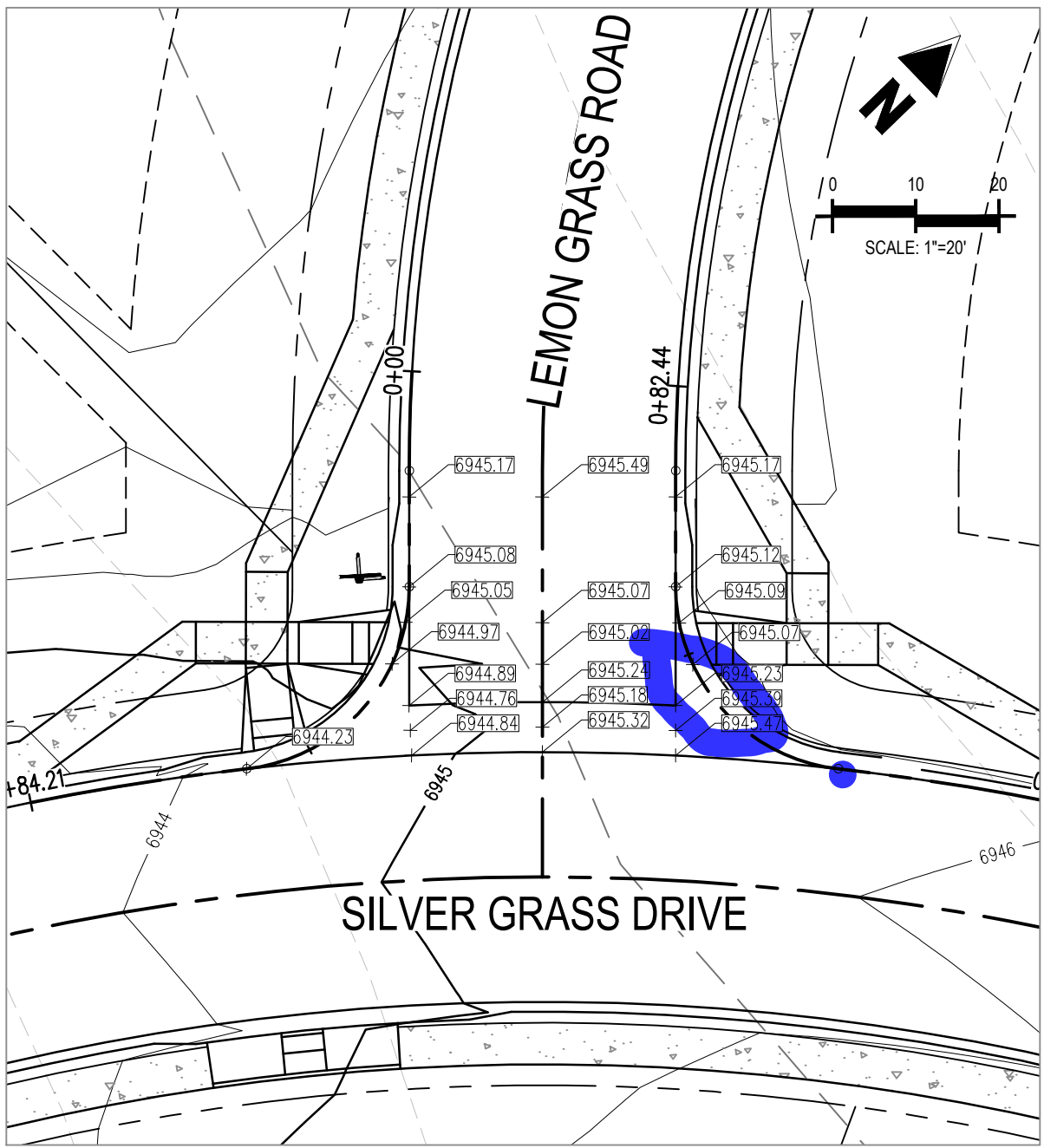
- ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURFACE EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN APPROXIMATE LOCATION ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION. REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.
- WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOLING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.



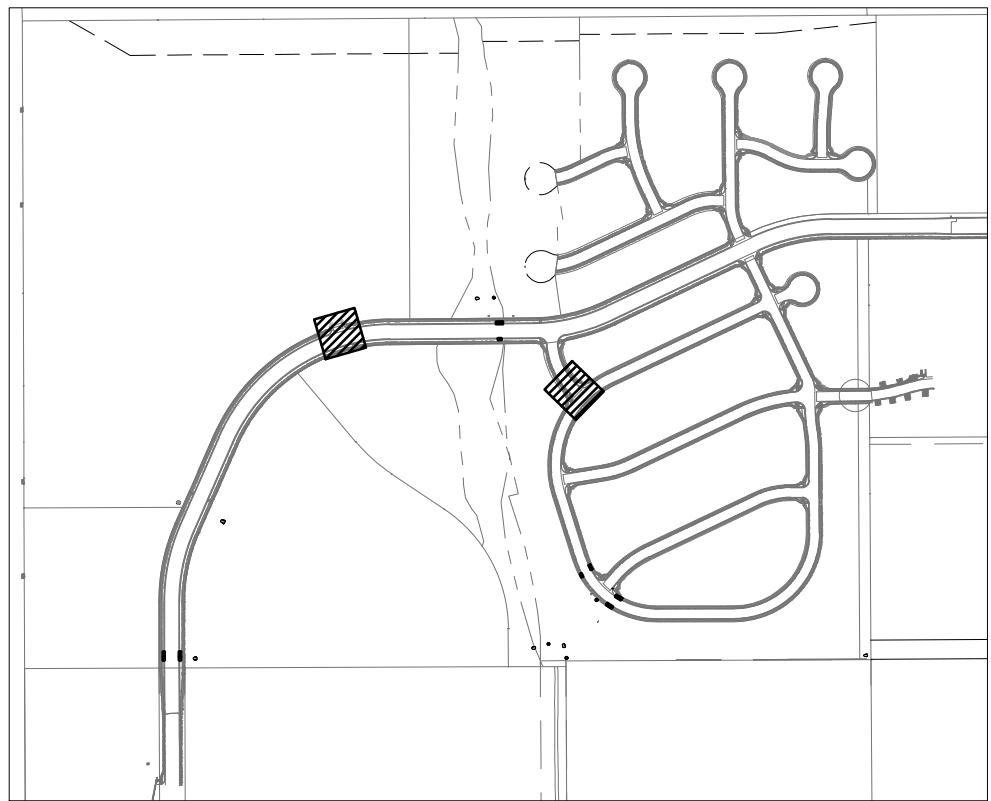
Know what's below.
Call before you dig.



INTERSECTION OF:
BENT GRASS MEADOWS DR & NASHUA DRIVE



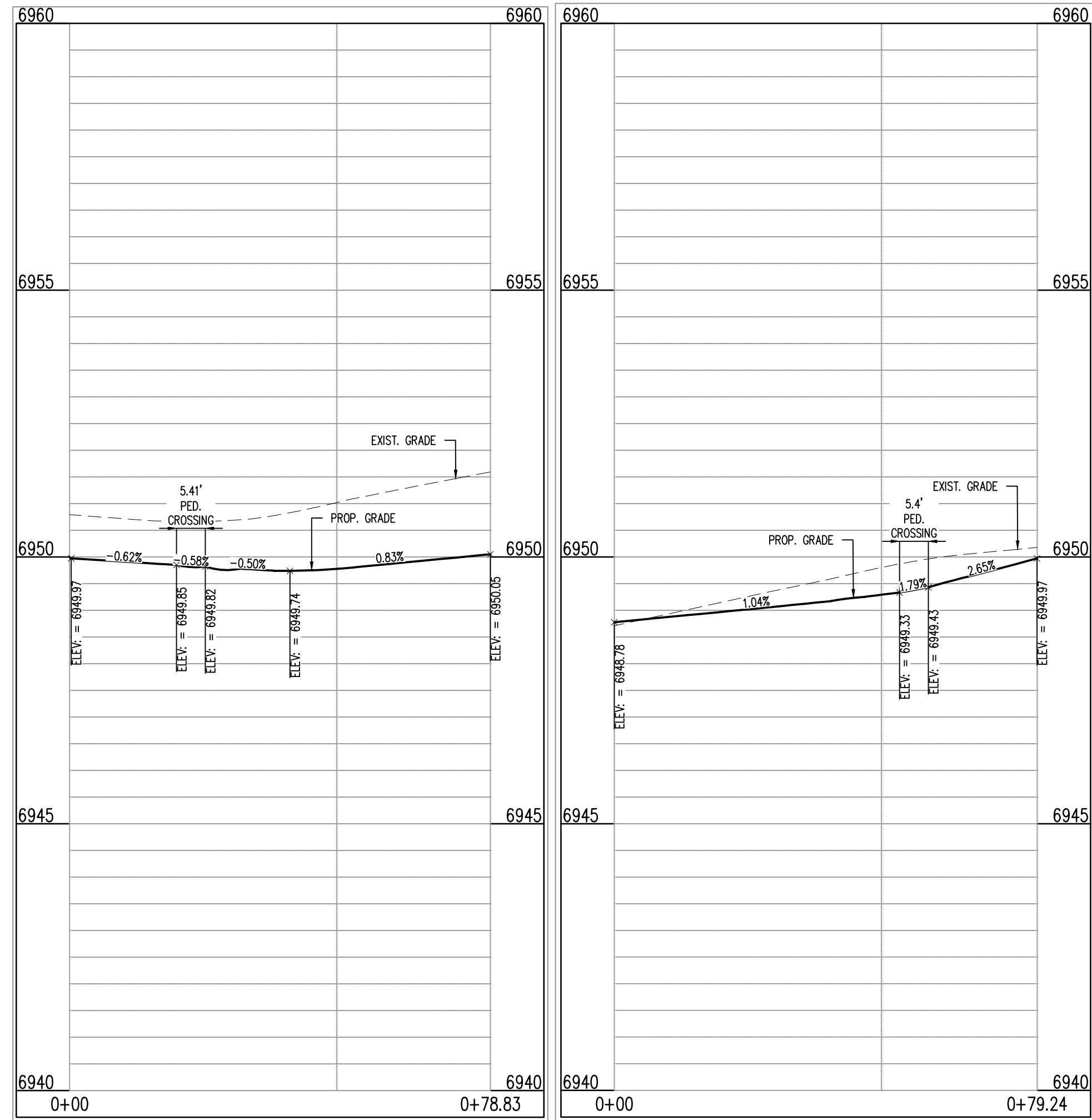
INTERSECTION OF:
SILVER GRASS DRIVE & LEMON GRASS ROAD



LOCATION MAP

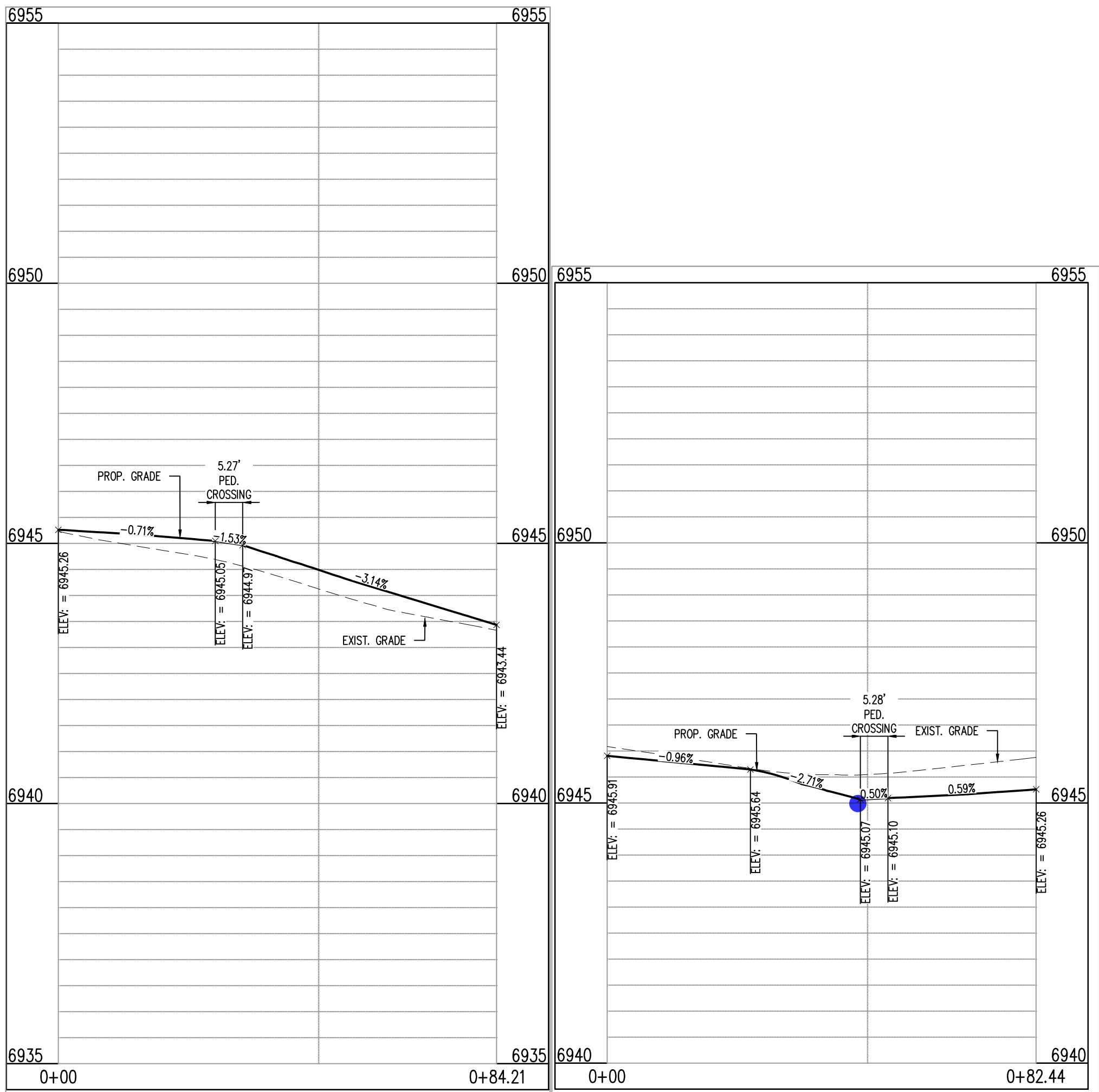
GRADING LEGEND

- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPERTY BOUNDARY
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING FLOW
- PROPOSED FLOW
- PROPOSED SPOT ELEVATION



BENT GRASS MEADOWS DR & NASHUA DR (NW): STA. 1+00.00 - 0+78.83
SCALE: H:1"=20', V:1"=10'

BENT GRASS MEADOWS DR & NASHUA DR (NE): STA. 1+00.00 - 0+79.24
SCALE: H:1"=20', V:1"=10'



SILVER GRASS DR & LEMON GRASS RD (SW): STA. 1+00.00 - 0+84.21
SCALE: H:1"=20', V:1"=10'

SILVER GRASS DR & LEMON GRASS RD (NW): STA. 1+00.00 - 0+82.44
SCALE: H:1"=20', V:1"=10'

CAUTION - NOTICE TO CONTRACTOR

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CHALLENGER
HOMES

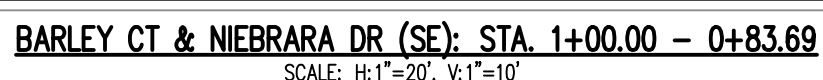
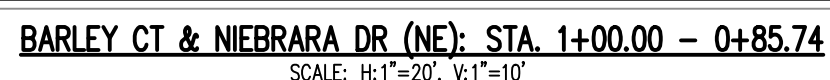
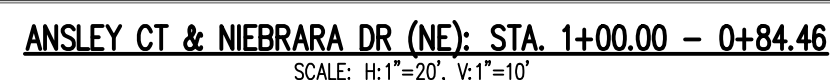
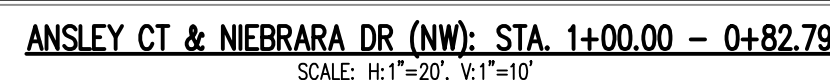
CONSTRUCTION DOCUMENTS
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CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGJ
Date: 2/10/2020

CROSS PAN &
CURB RETURN PROFILES

C3.02
Sheet 20 of 43



1. ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURFACE EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN APPROXIMATE LOCATION ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE FIELD LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION. REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.

Know what's **below**.
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CHALLENGER HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC

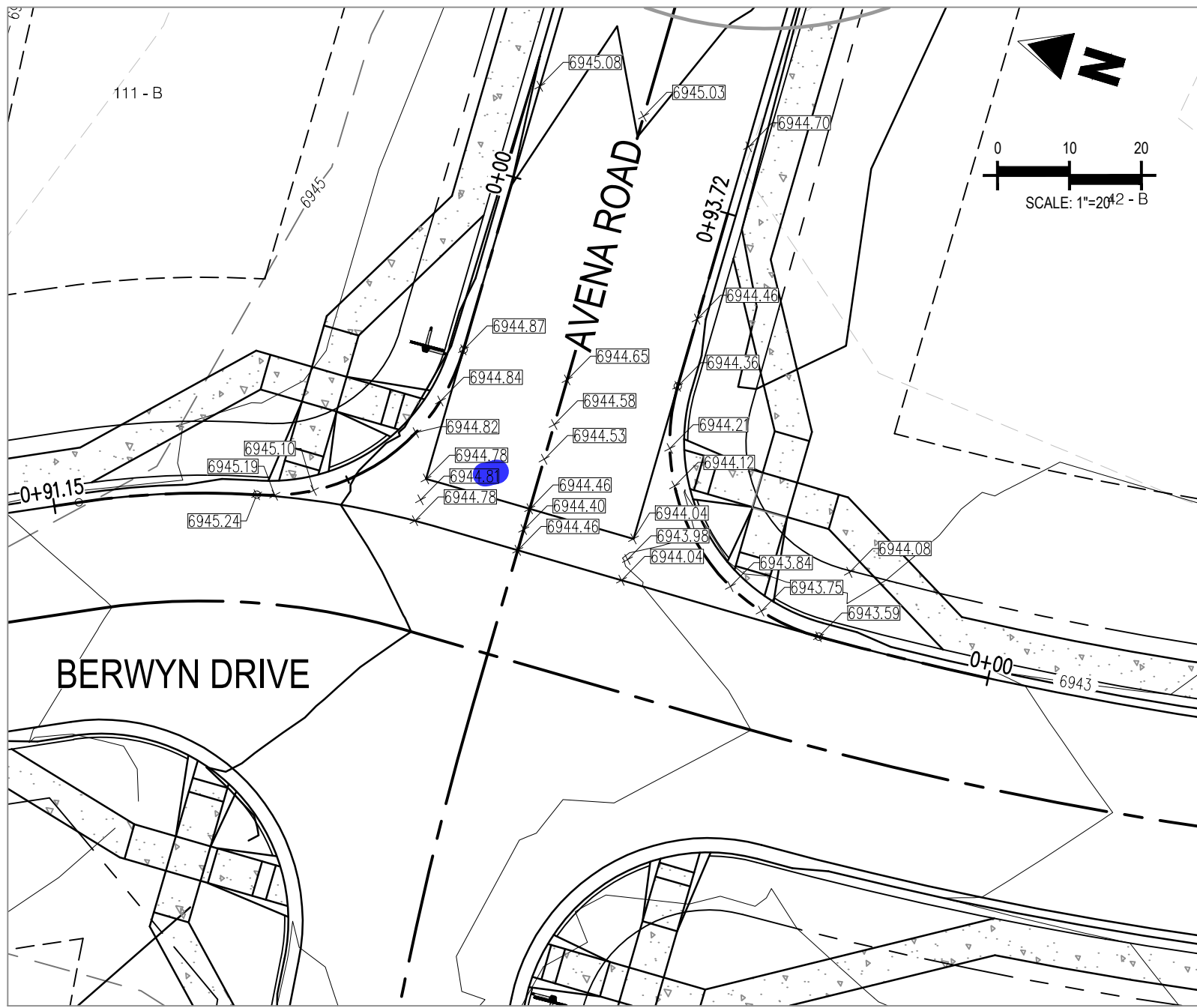
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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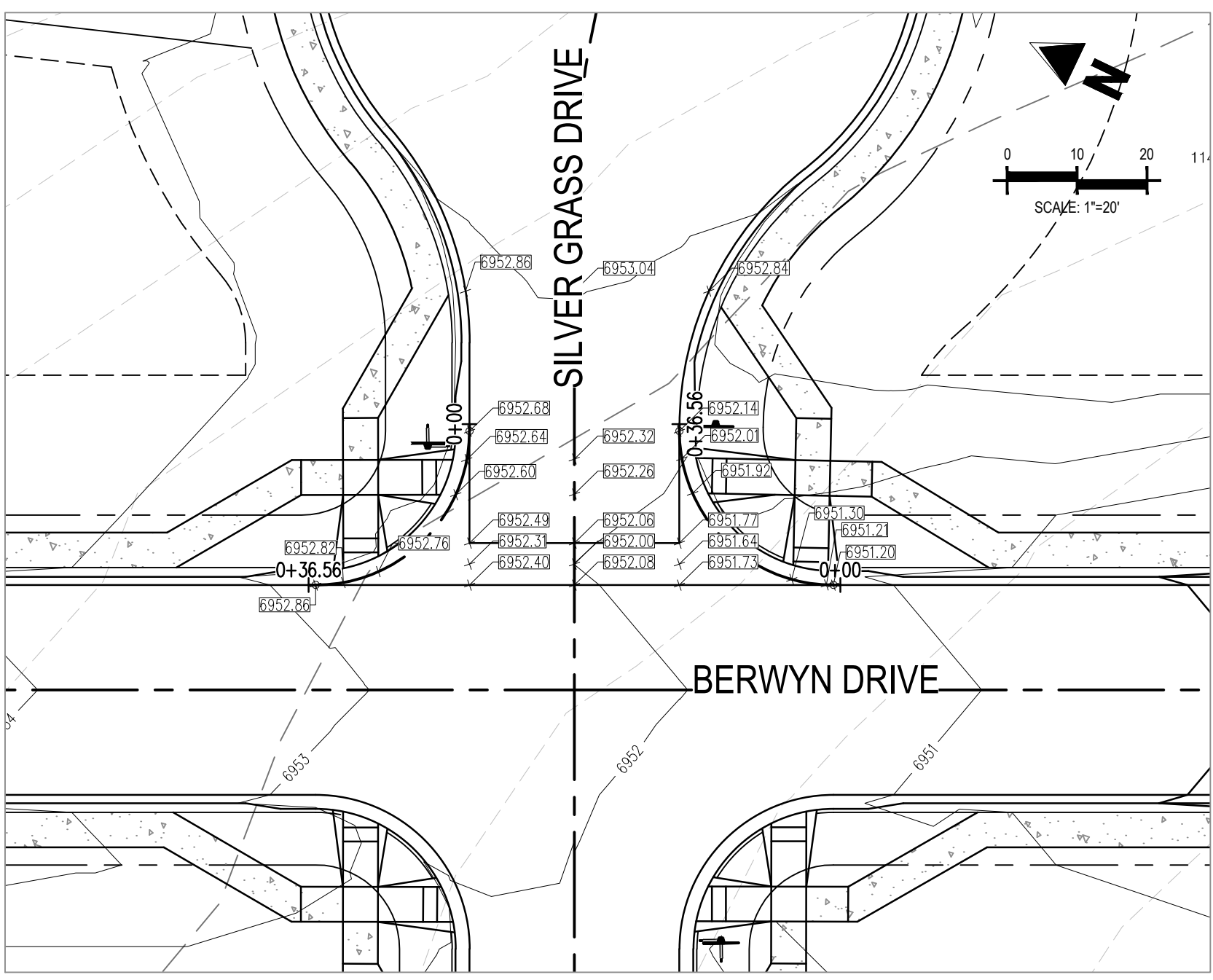
Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

CROSS PAN &
CURB RETURN PROFILES

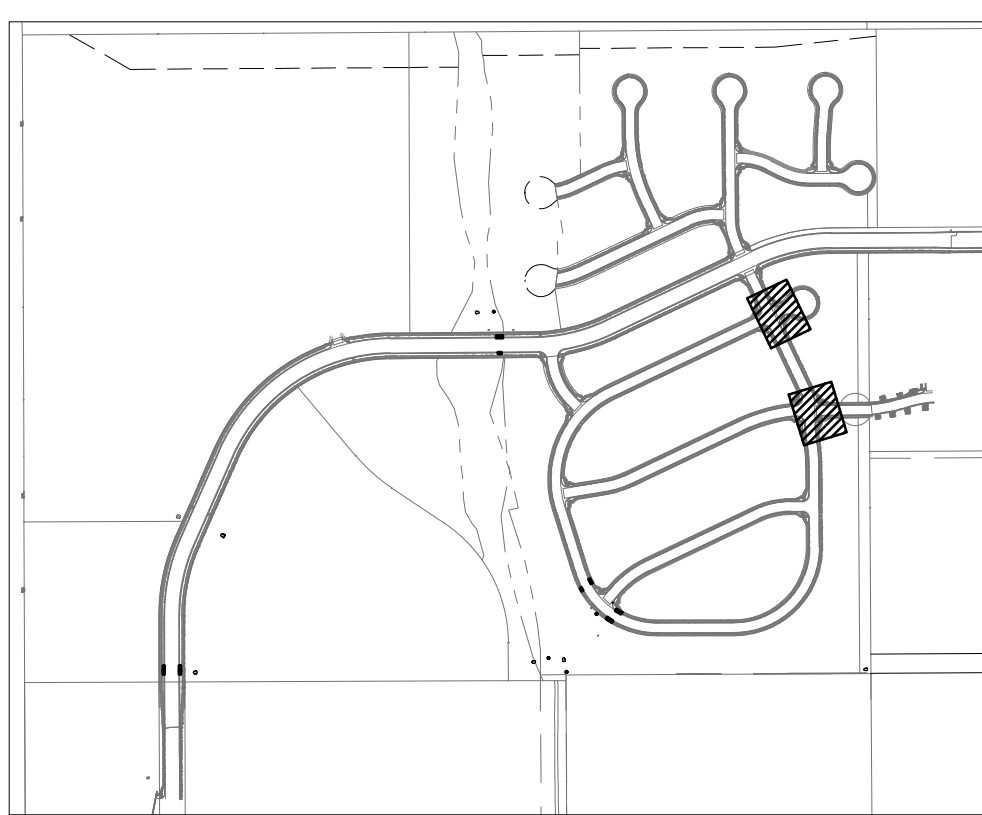
Sheet 21 of 43



INTERSECTION OF:
AVENA ROAD & BERWYN DRIVE



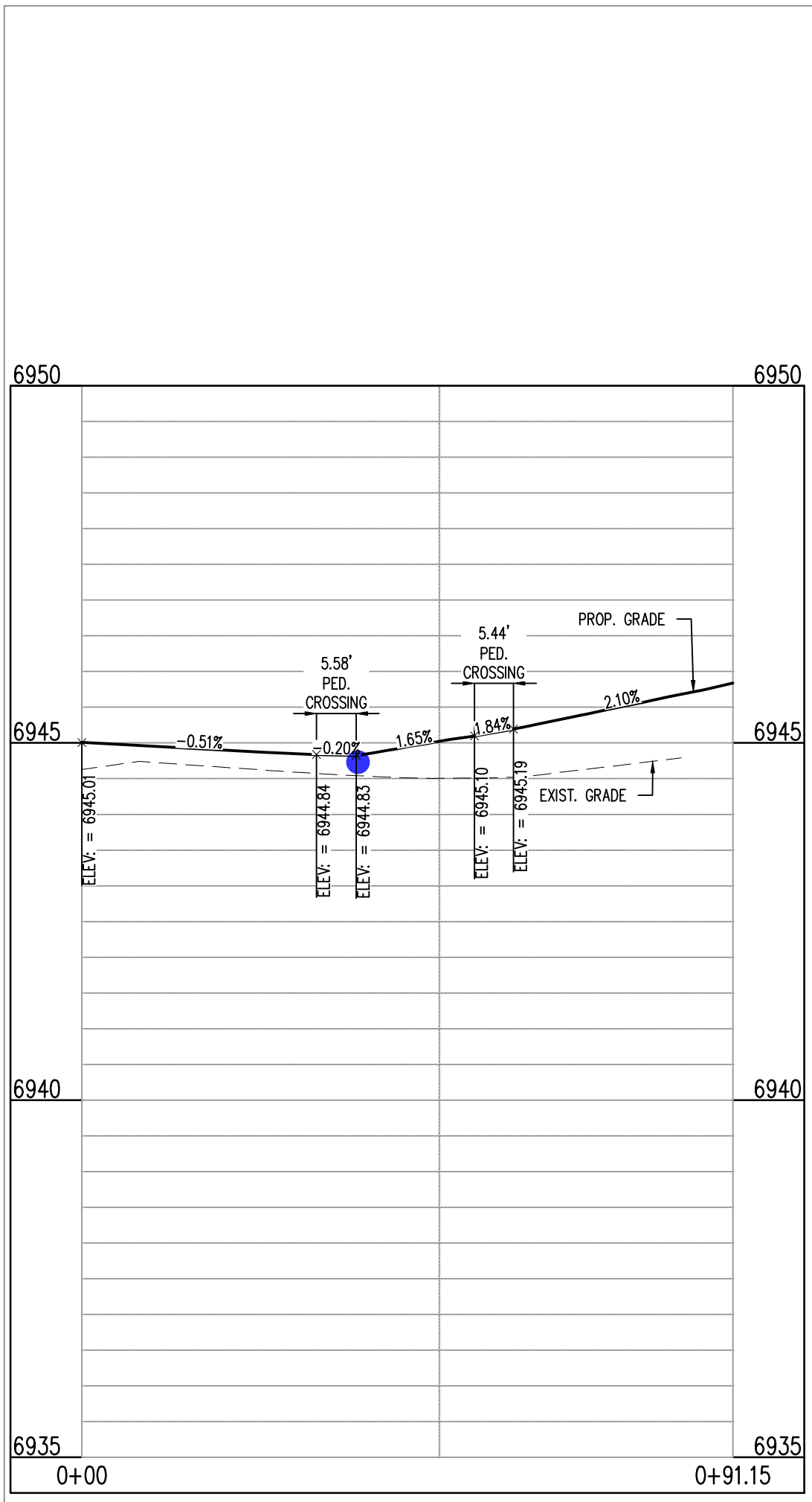
INTERSECTION OF:
SILVER GRASS DRIVE & BERWYN DRIVE



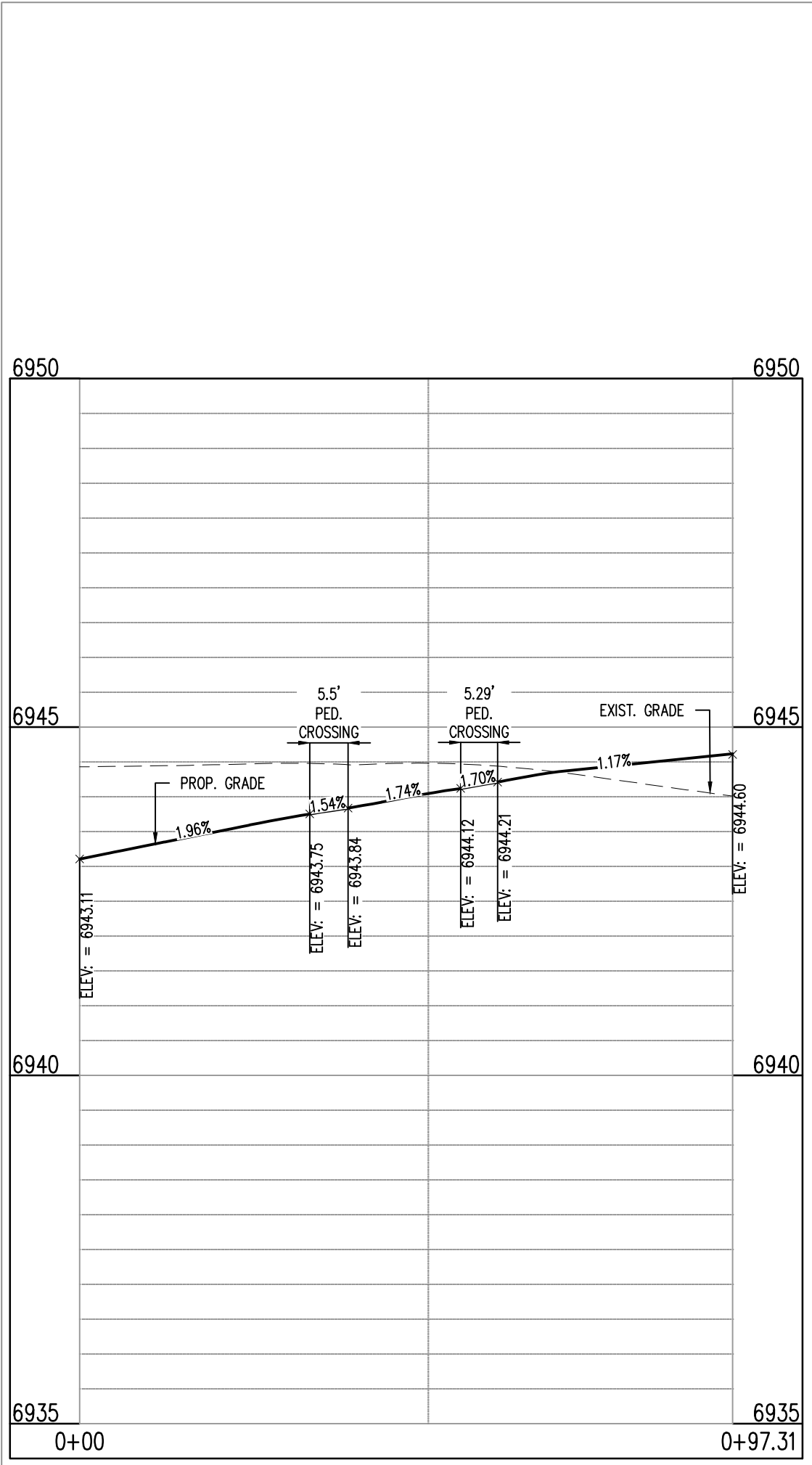
LOCATION MAP

GRADING LEGEND

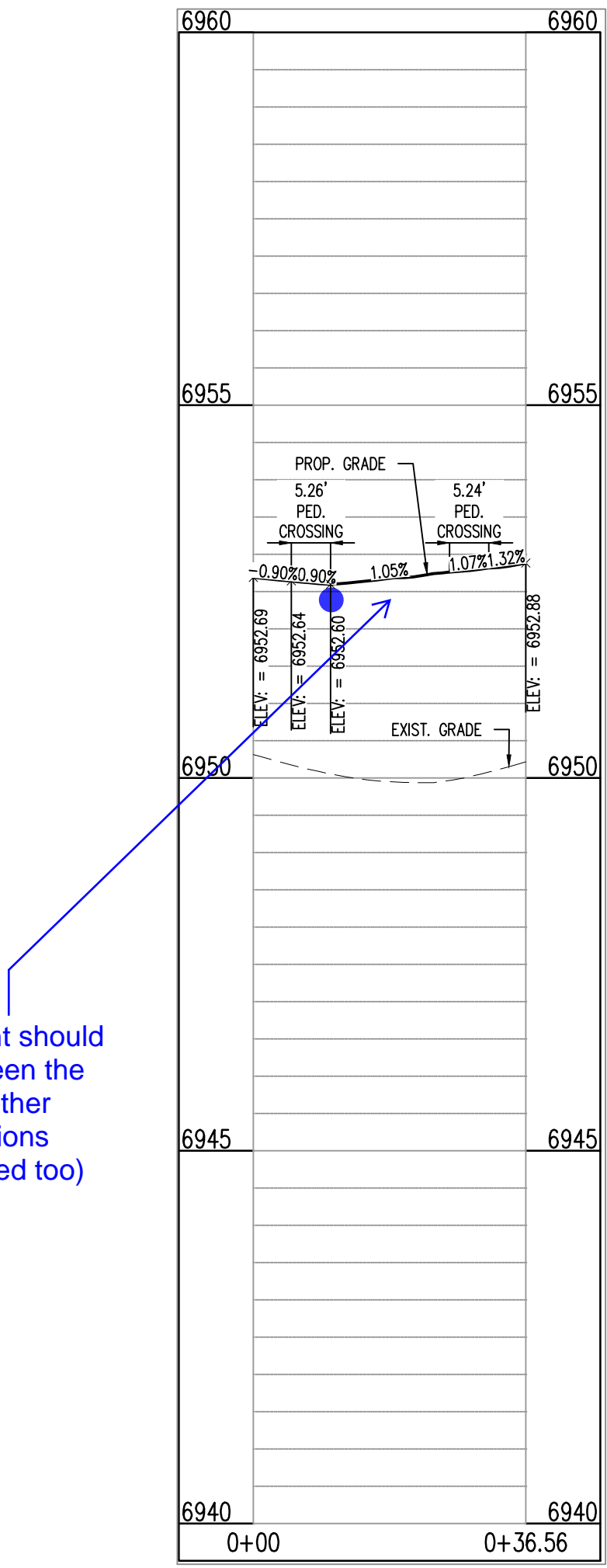
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- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPERTY BOUNDARY
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING FLOW
- PROPOSED FLOW
- PROPOSED SPOT ELEVATION



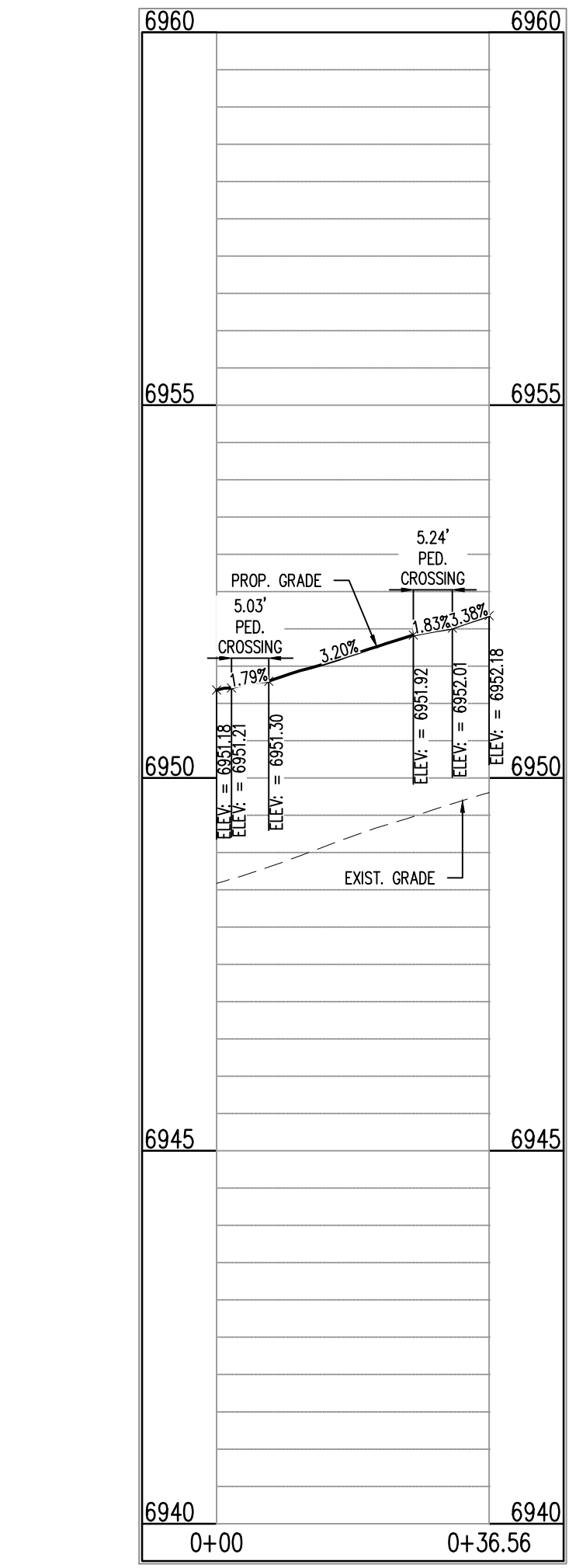
AVENA RD & BERWYN DR (NE): STA. 1+00.00 - 0+83.93
SCALE: H:1"=20', V:1"=10'



AVENA RD & BERWYN DR (SE): STA. 1+00.00 - 0+97.31
SCALE: H:1"=20', V:1"=10'



SILVER GRASS DR & BERWYN DR (NE): STA. 1+00.00 - 0+36.56
SCALE: H:1"=20', V:1"=10'



SILVER GRASS DR & BERWYN DR (SE): STA. 1+00.00 - 0+36.56
SCALE: H:1"=20', V:1"=10'

Low point should
be between the
ramps (other
intersections
highlighted too)

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CHALLENGER
HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

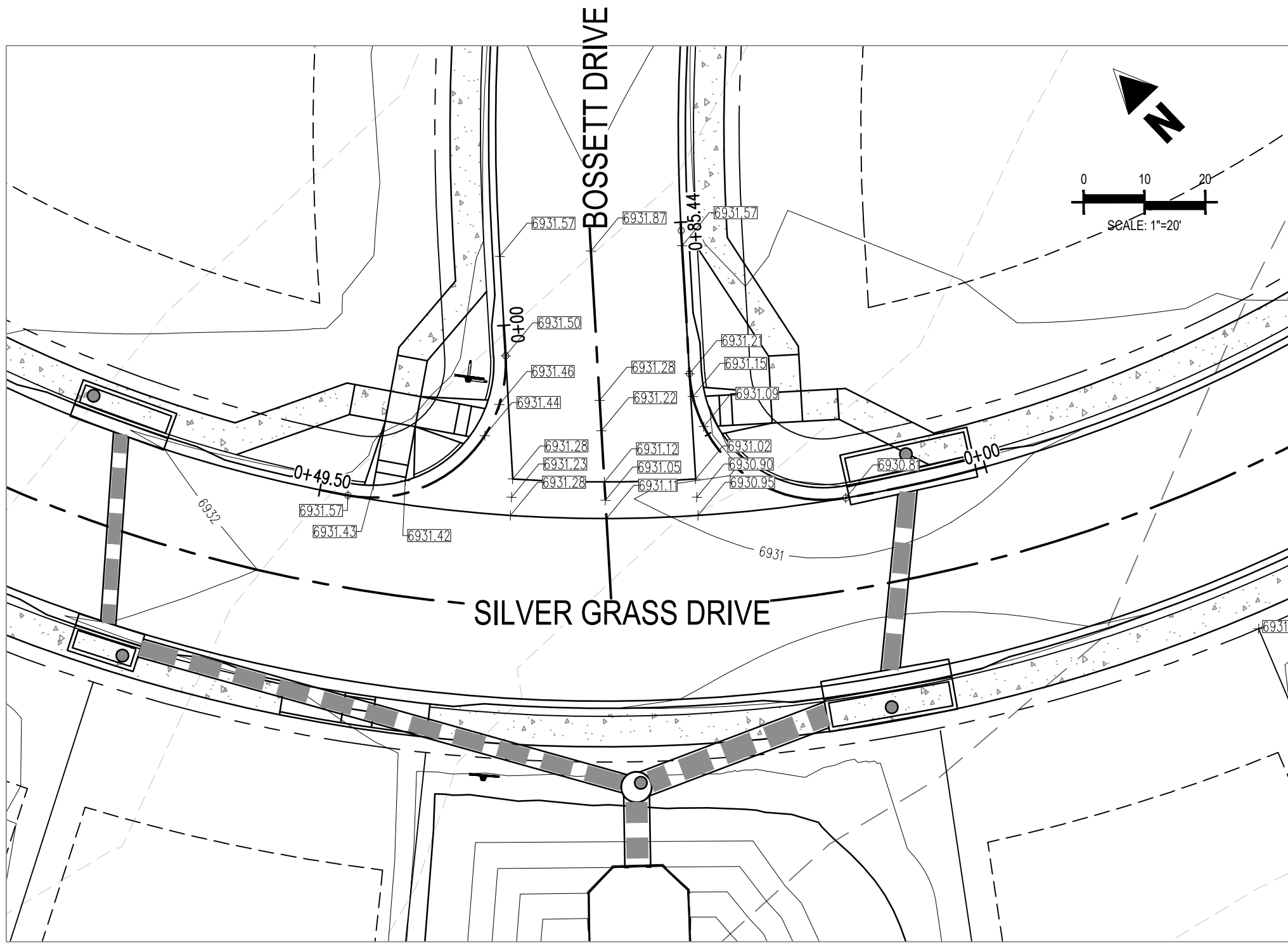
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

CROSS PAN &
CURB RETURN PROFILES

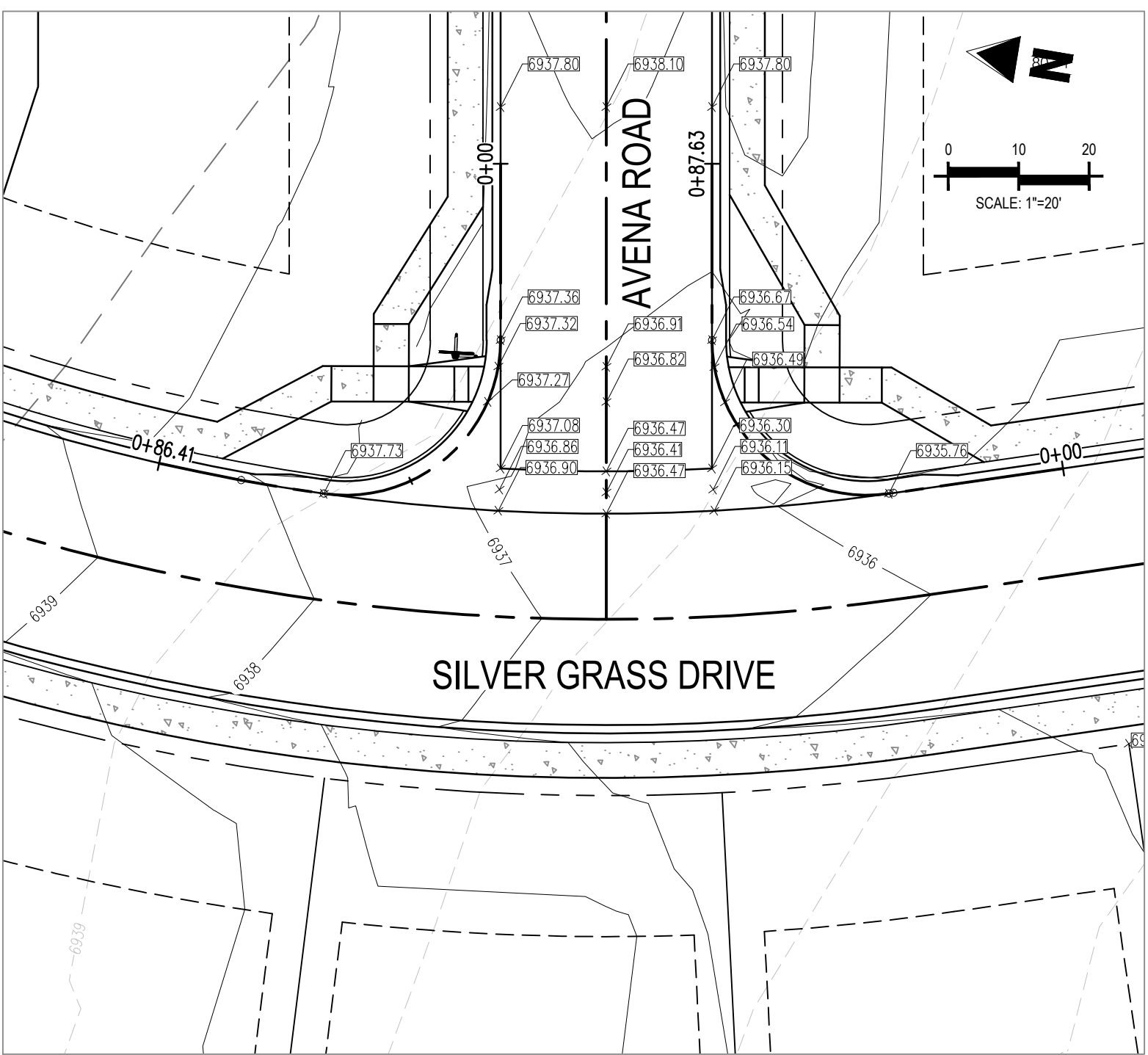
C3.04

Sheet 22 of 43

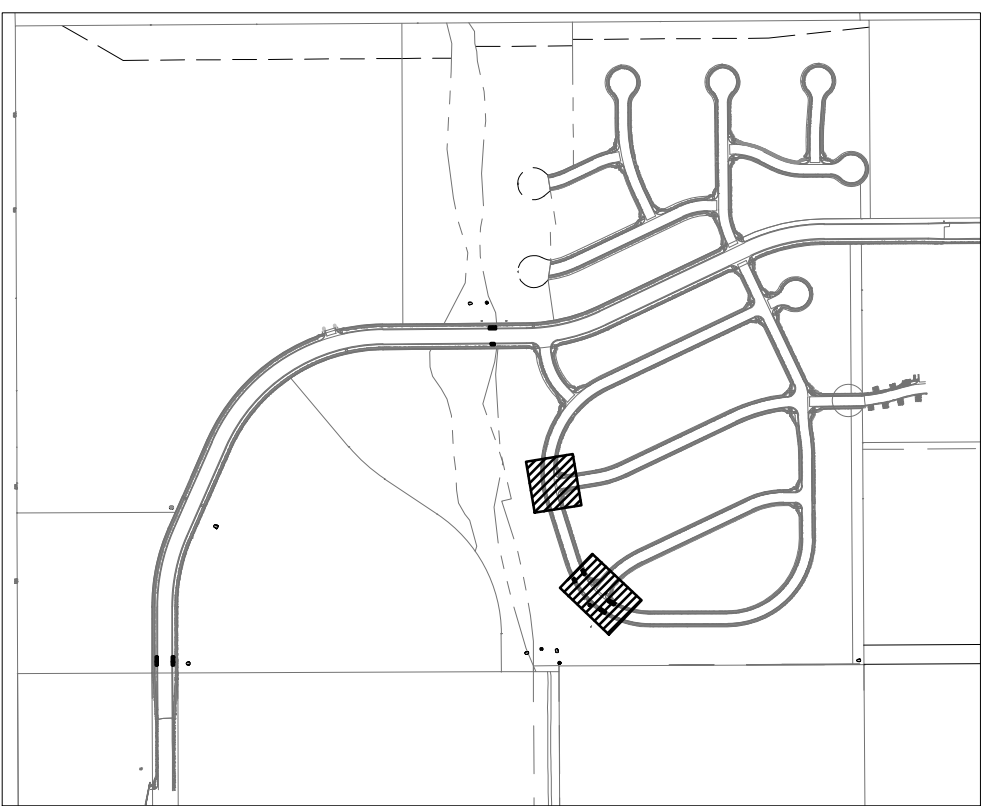


INTERSECTION OF:
BERWYN DRIVE & BOSSETT DRIVE

Provide missing
pedestrian crossings or
request deviations.



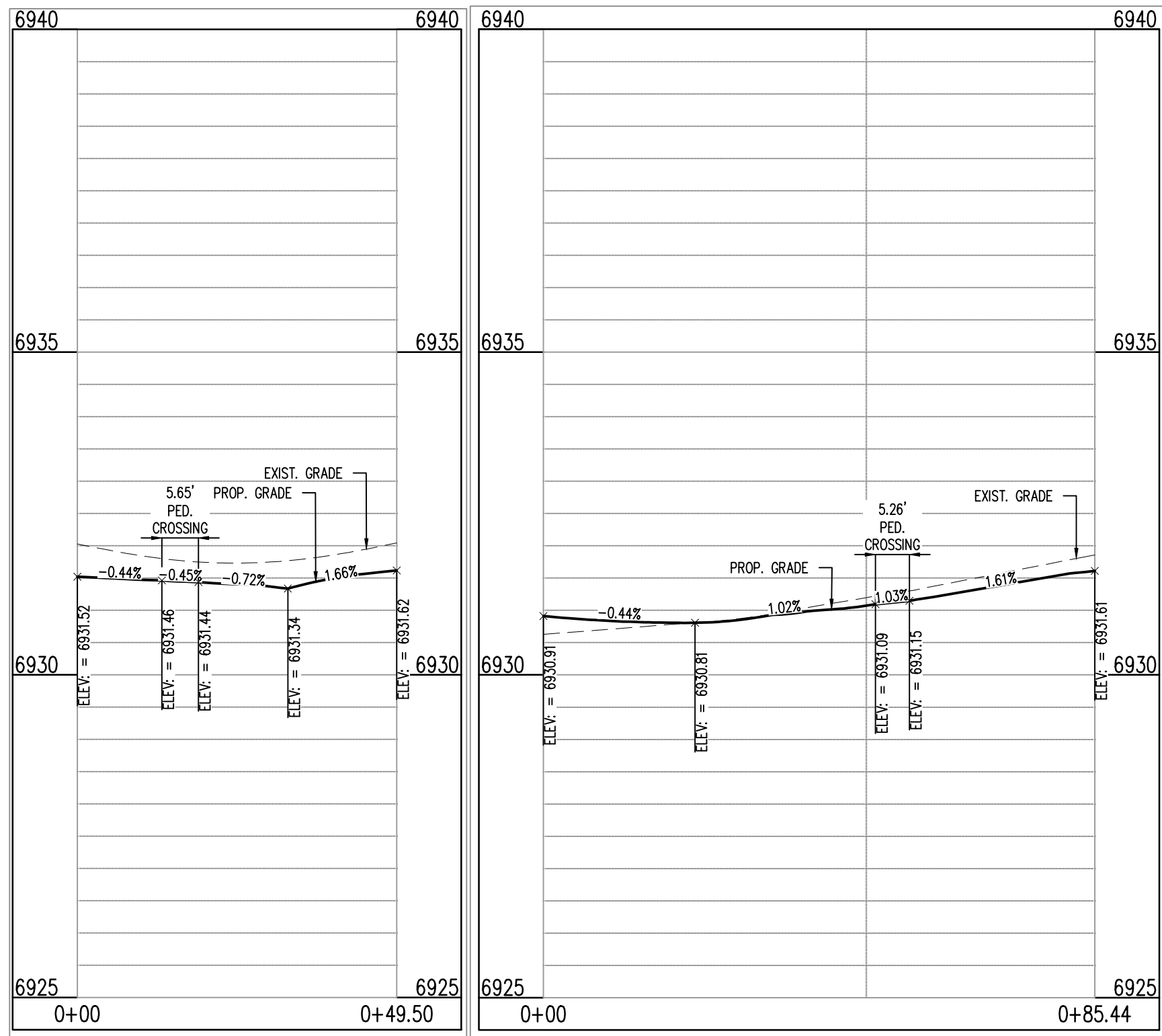
INTERSECTION OF:
SILVER GRASS DRIVE & AVENA ROAD



LOCATION MAP

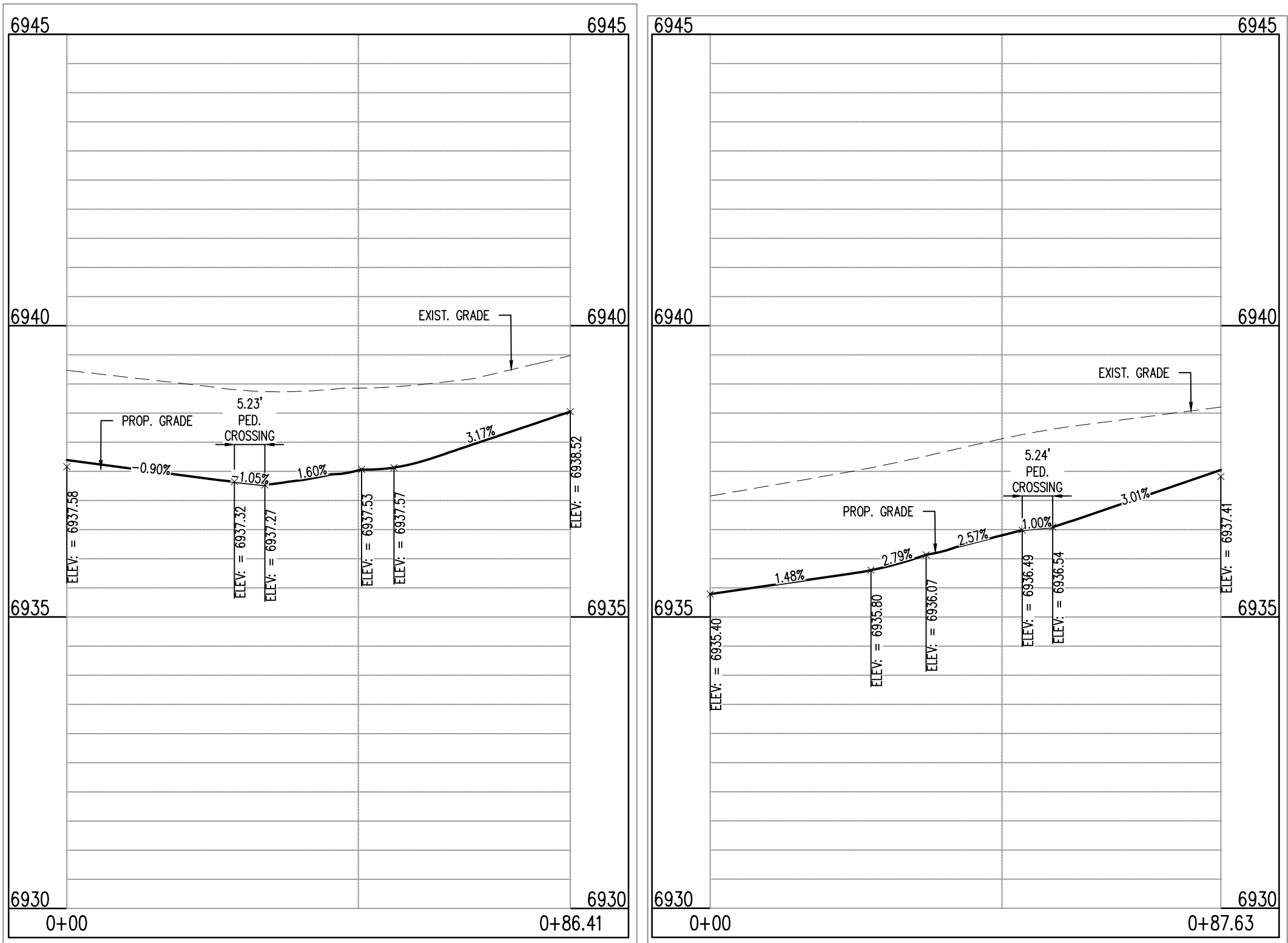
GRADING LEGEND

- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPERTY BOUNDARY
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING FLOW
- PROPOSED FLOW
- PROPOSED SPOT ELEVATION



SILVER GRASS DR & BOSSETT DR (NW): STA. 1+00.00 - 0+49.50
SCALE: H:1"=20', V:1"=10'

SILVER GRASS DR & BOSSETT DR (NE): STA. 1+00.00 - 0+85.44
SCALE: H:1"=20', V:1"=10'



SILVER GRASS DR & AVENA RD (NE): STA. 1+00.00 - 0+86.41
SCALE: H:1"=20', V:1"=10'

SILVER GRASS DR & AVENA RD (SE): STA. 1+00.00 - 0+87.63
SCALE: H:1"=20', V:1"=10'

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CHALLENGER
HOMES

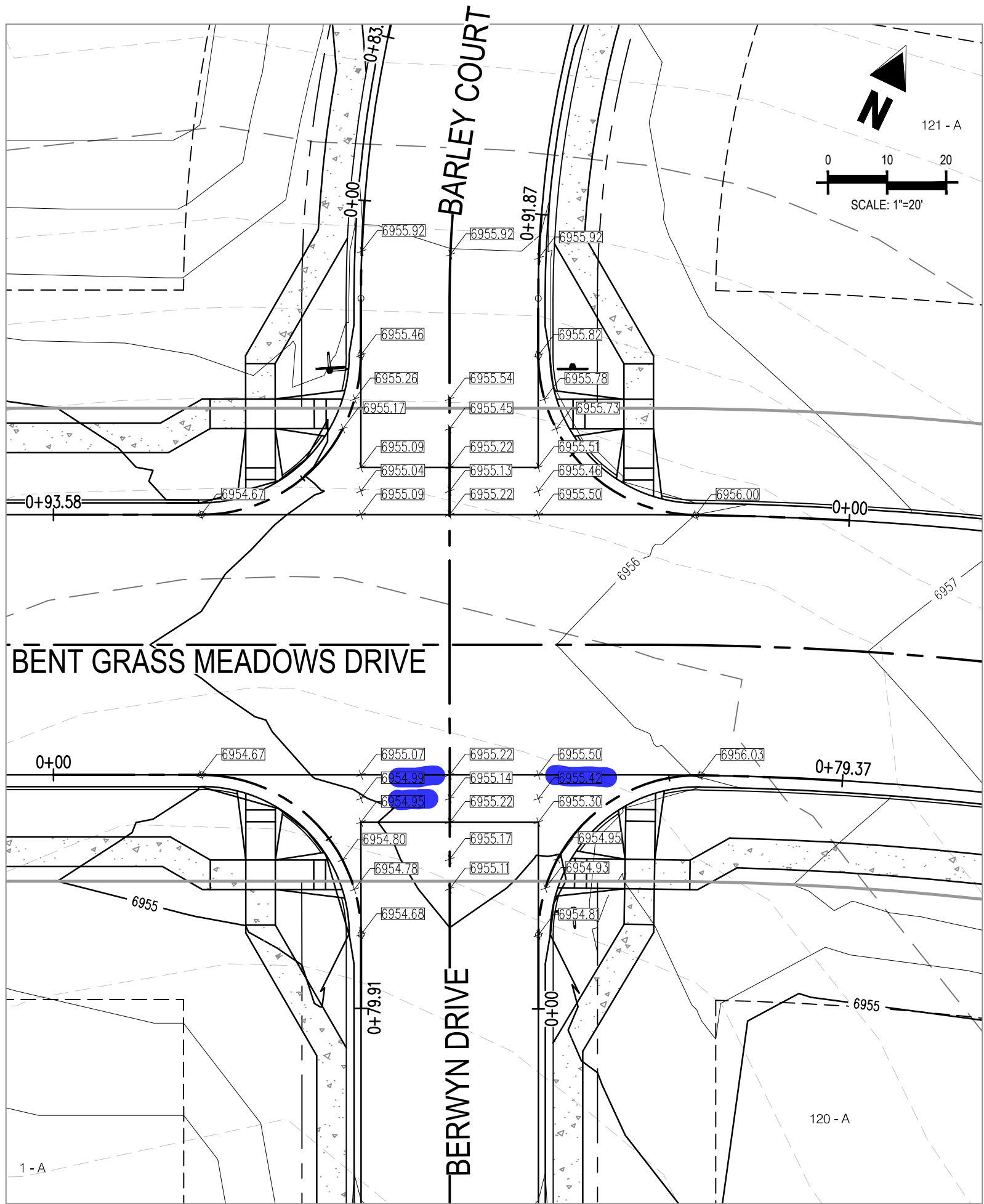
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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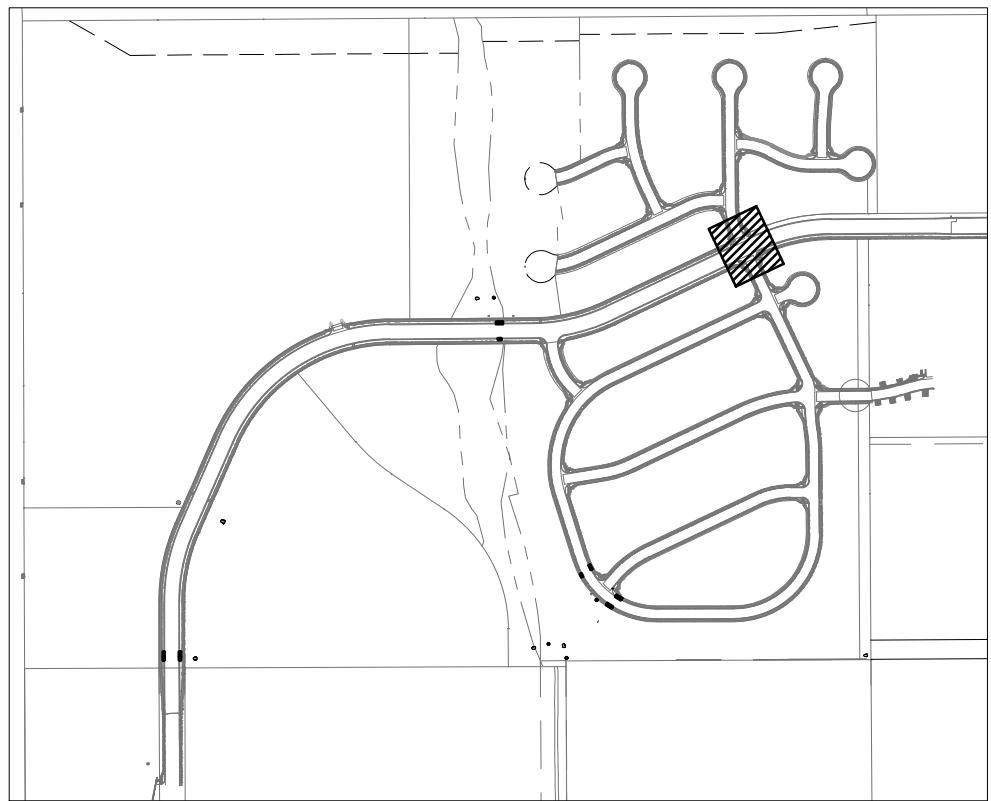
Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

CROSS PAN &
CURB RETURN PROFILES

C3.05
Sheet 23 of 43



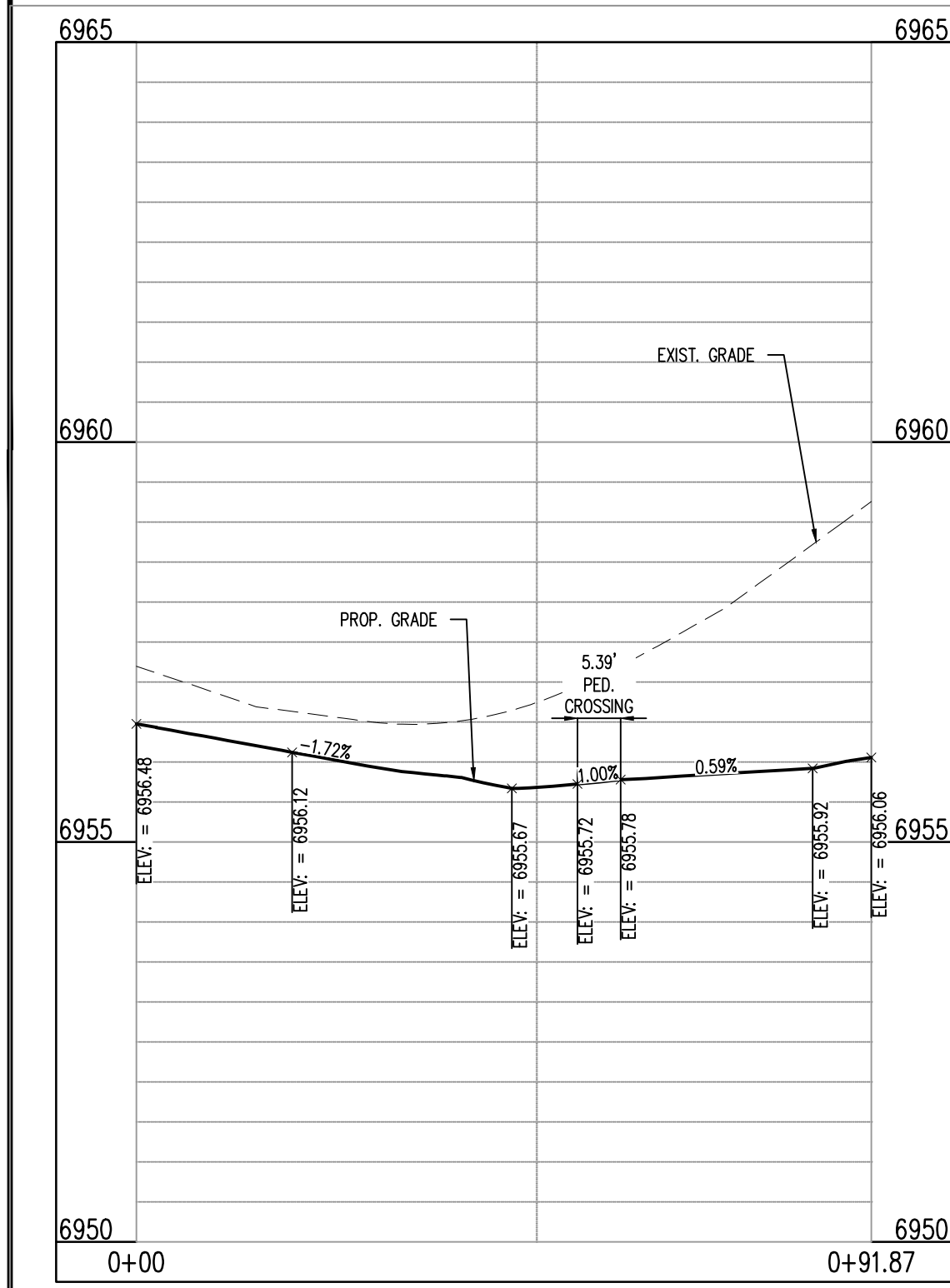
INTERSECTION OF:
BENT GRASS MEADOWS DRIVE, BARLEY COURT & BERWYN DRIVE



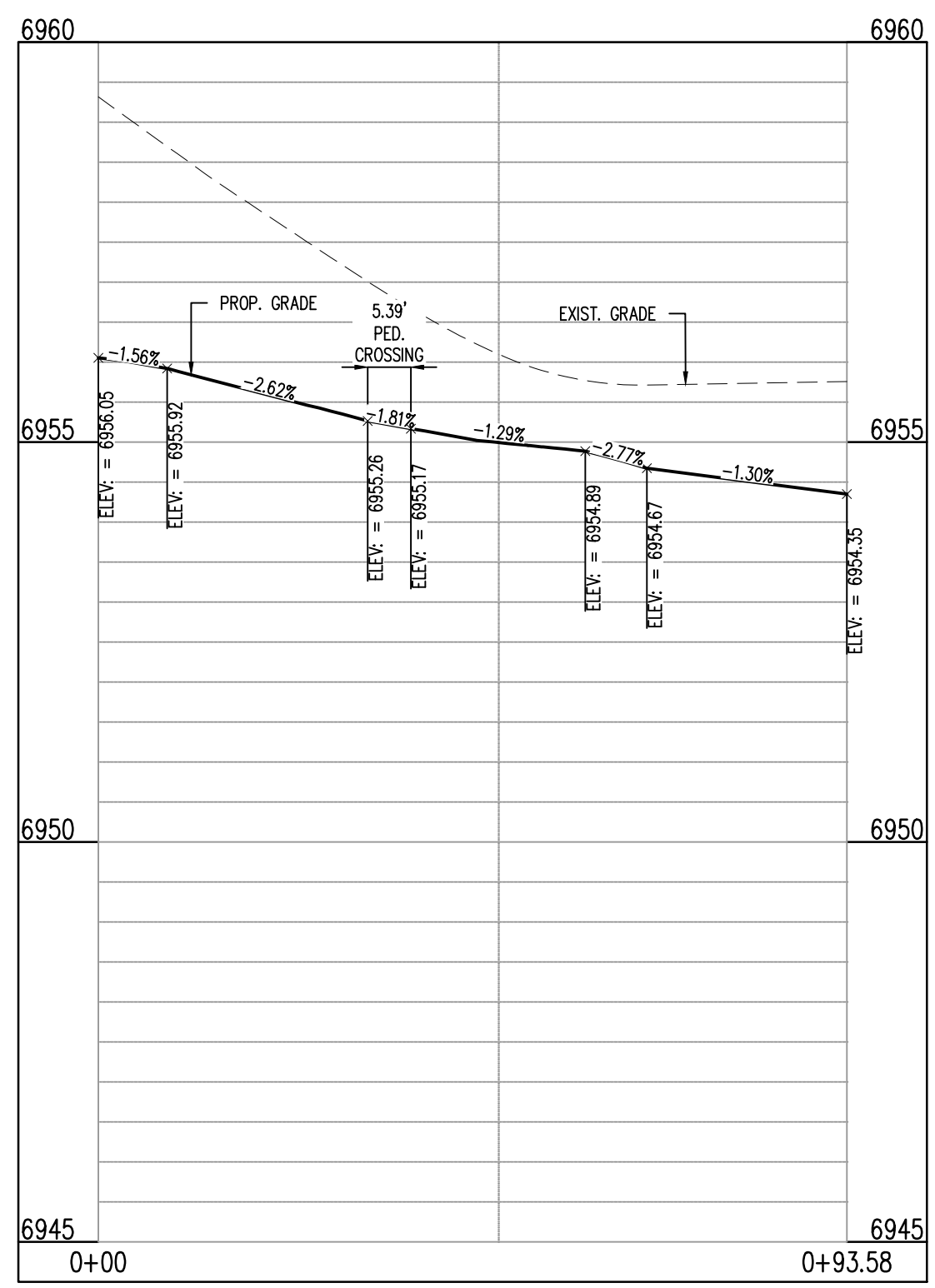
LOCATION MAP

GRADING LEGEND

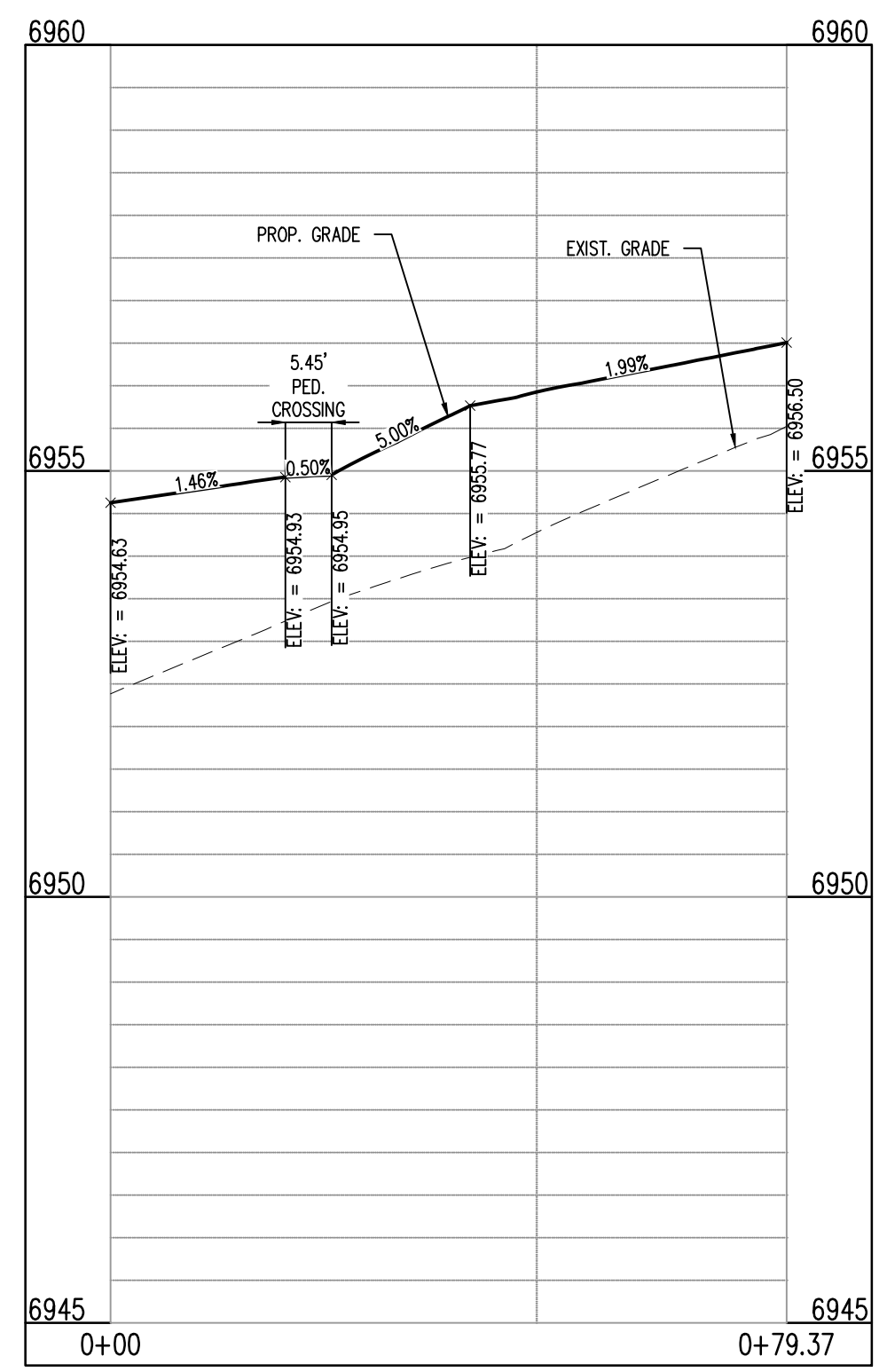
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- PROPERTY BOUNDARY
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING FLOW
- PROPOSED FLOW
- PROPOSED SPOT ELEVATION



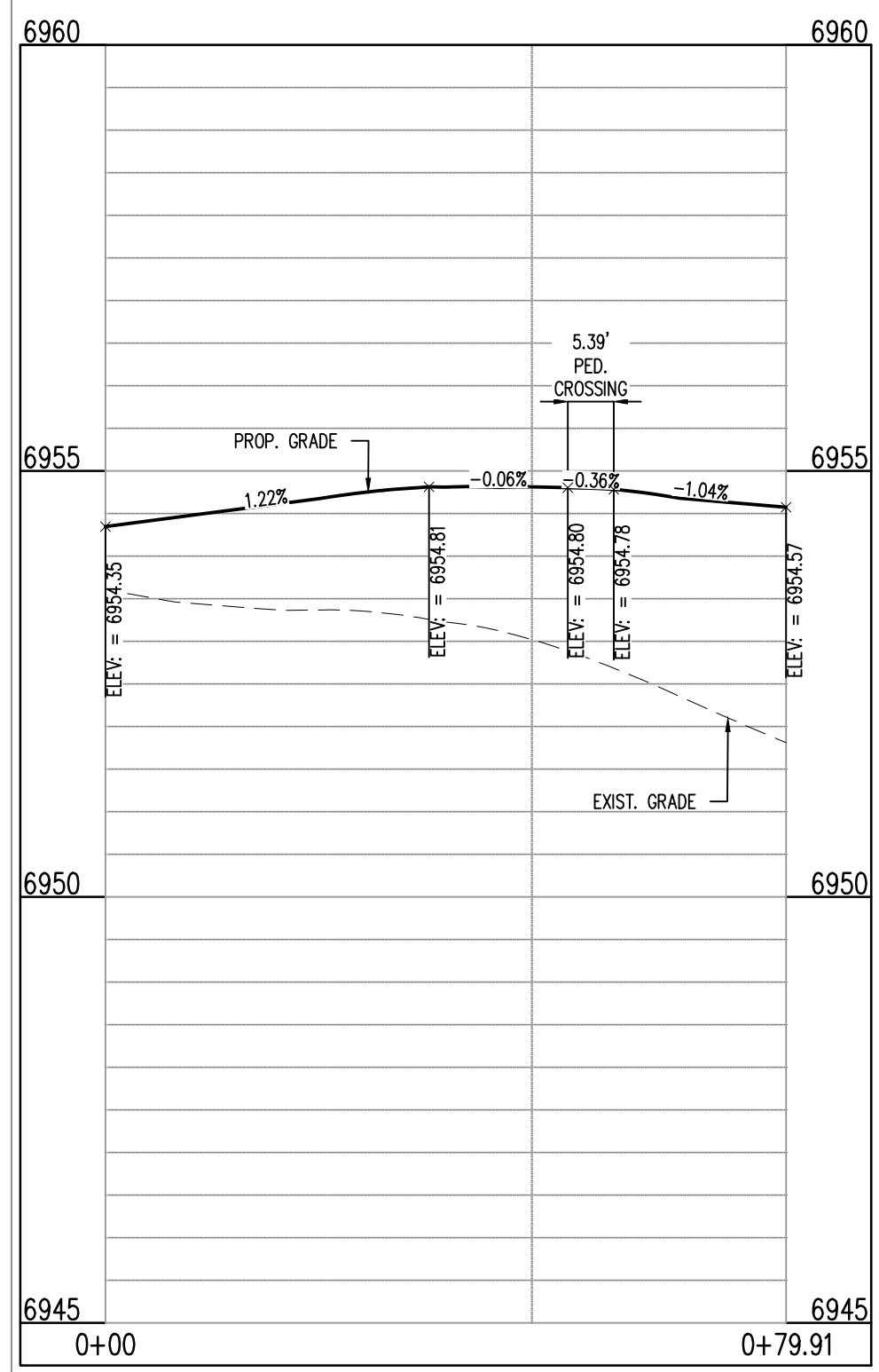
BENT GRASS MEADOWS DR & BARLEY CT (NE): STA. 1+00.00 - 0+91.87



BENT GRASS MEADOWS DR & BARLEY CT (NW): STA. 1+00.00 - 0+93.58



BENT GRASS MEADOWS DR & BERWYN DR (SE): STA. 1+00.00 - 0+79.37



BENT GRASS MEADOWS DR & BERWYN DR (SW): STA. 1+00.00 - 0+79.91

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CHALLENGER
HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

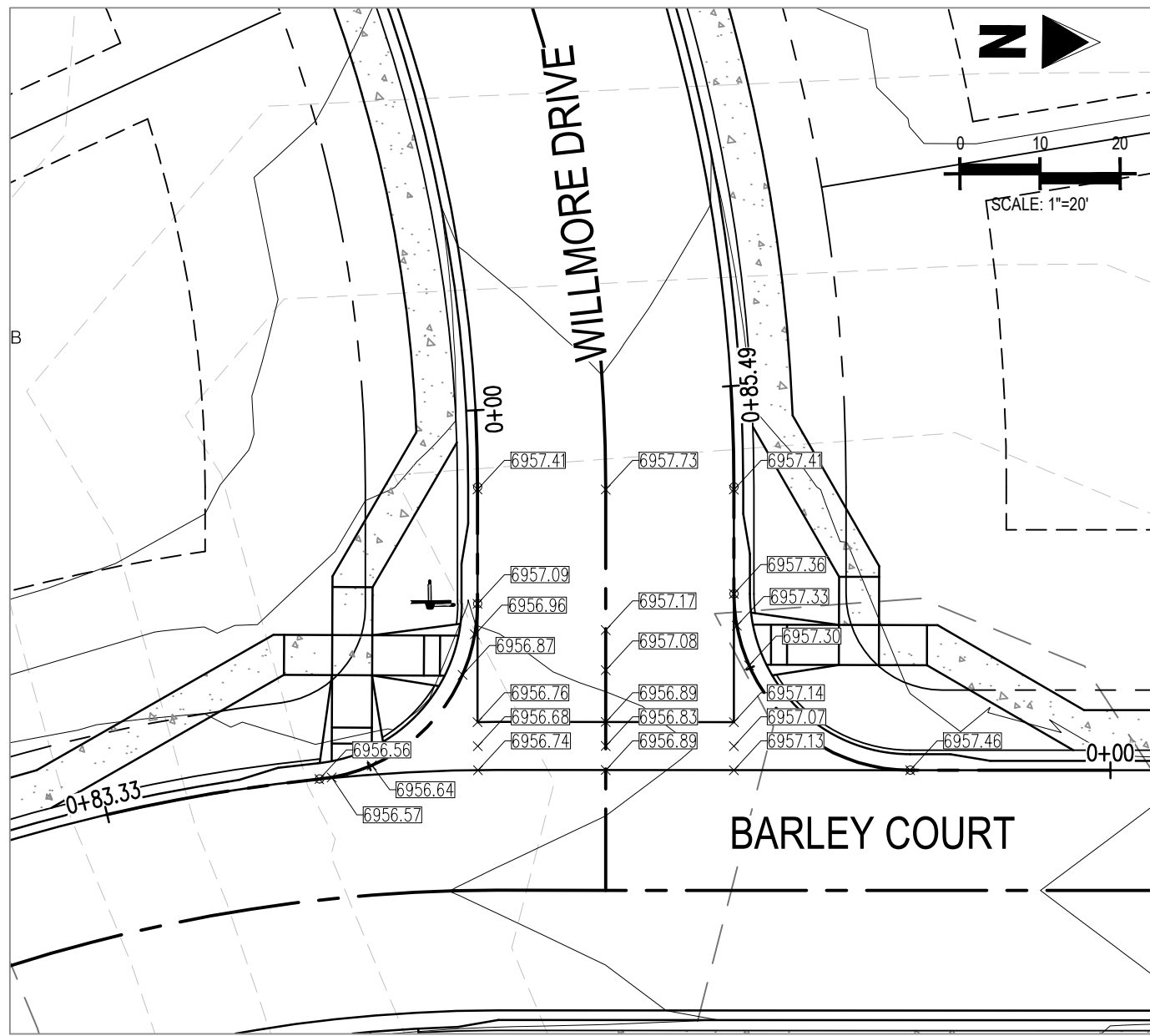
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Project No:	CLH000014.20
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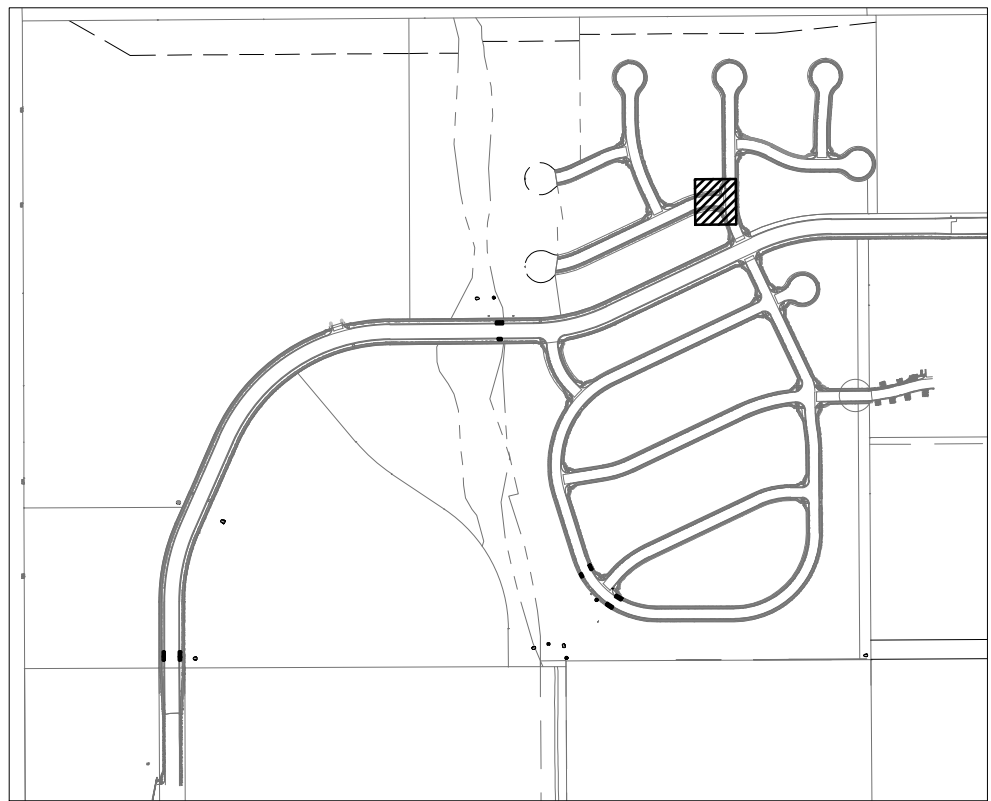
CROSS PAN &
CURB RETURN PROFILES

C3.06

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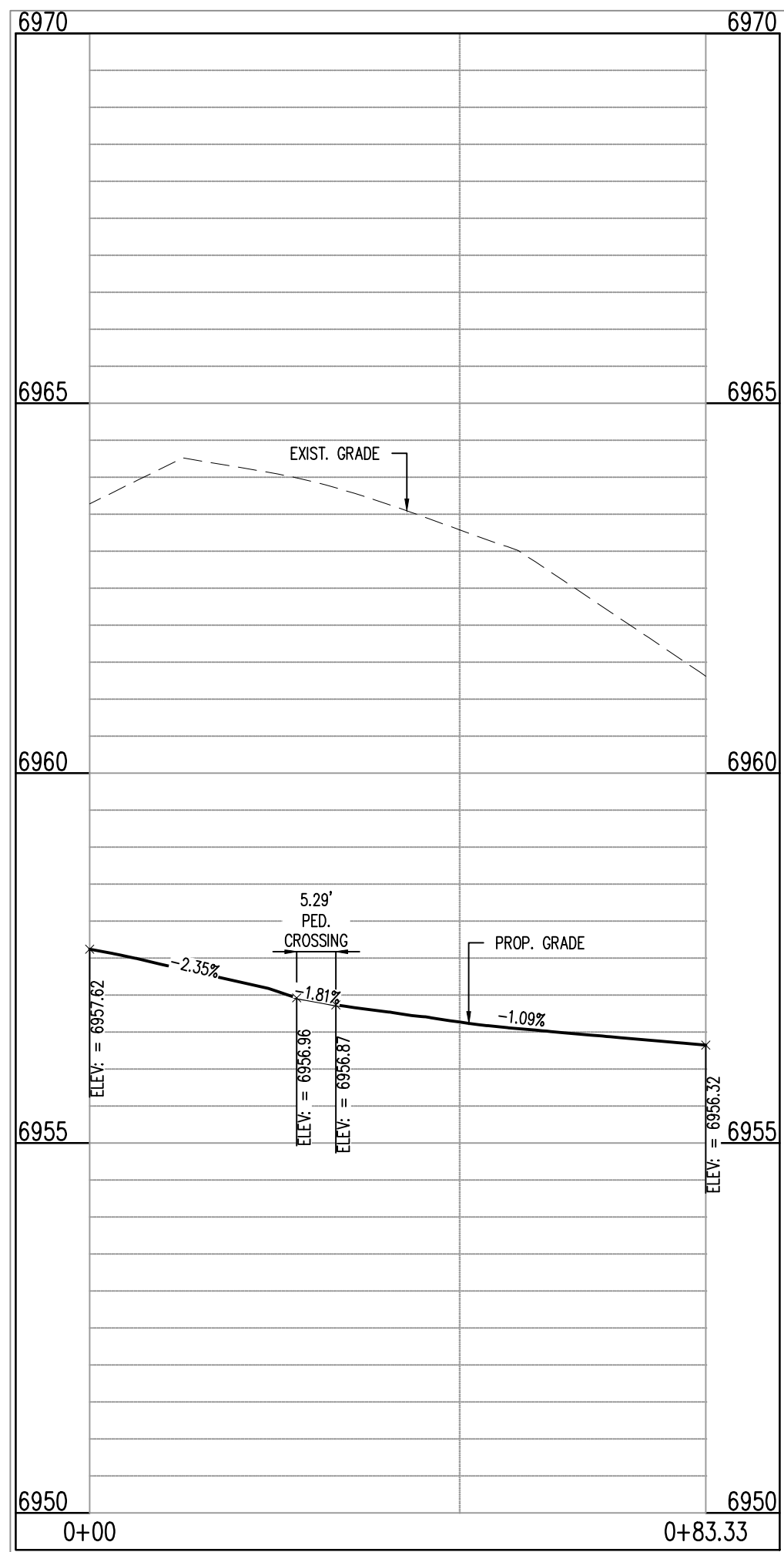
INTERSECTION OF:
WILLMORE DRIVE & BARLEY COURT



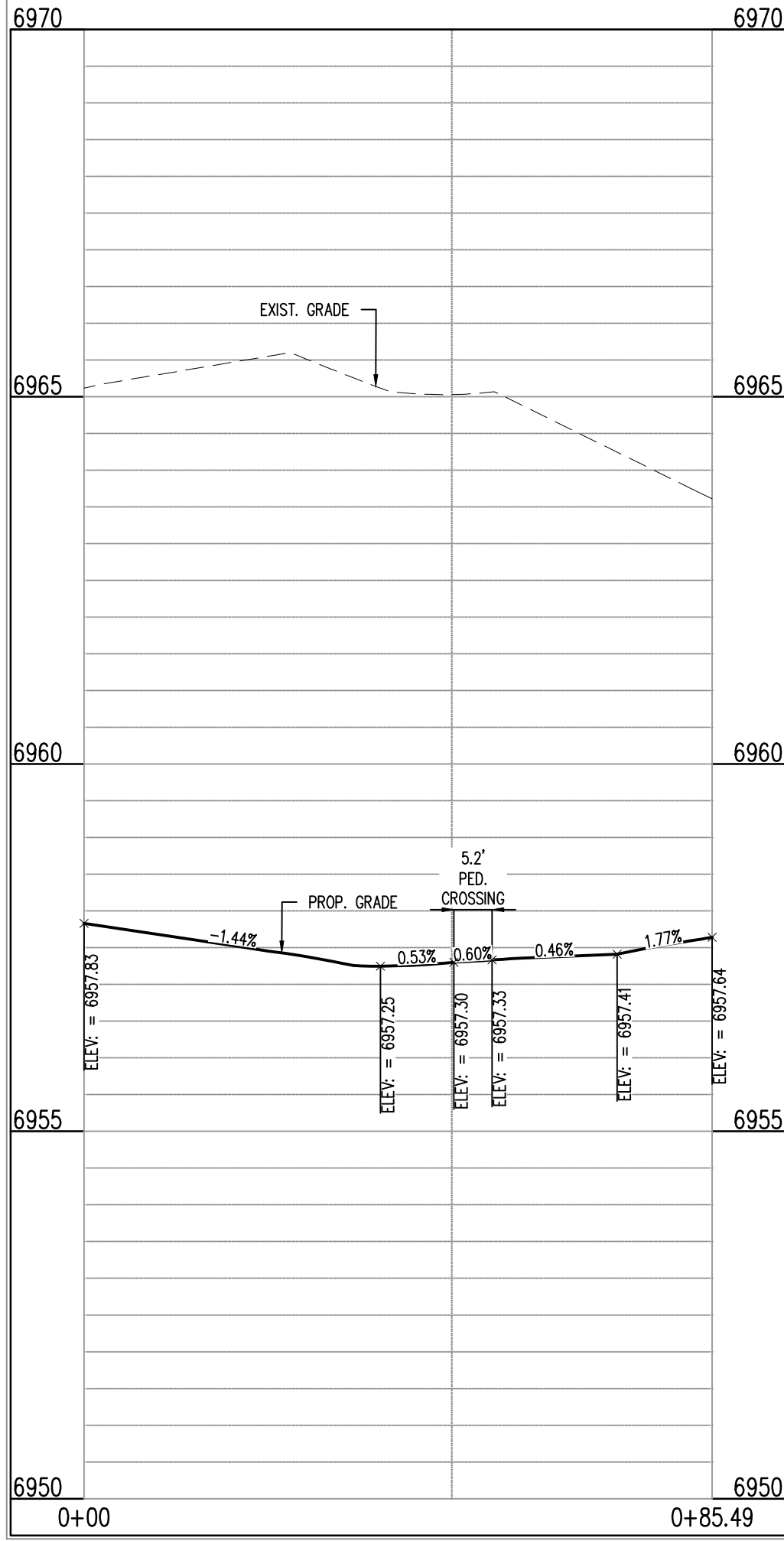
LOCATION MAP

GRADING LEGEND

- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPERTY BOUNDARY
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING FLOW
- PROPOSED FLOW
- PROPOSED SPOT ELEVATION



BARLEY CT & WILLMORE DR (SW): STA. 1+00.00 - 0+83.33
SCALE: H:1"=20', V:1"=10'



BARLEY CT & WILLMORE DR (NW): STA. 1+00.00 - 0+85.49
SCALE: H:1"=20', V:1"=10'

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CHALLENGER
HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR

CHALLENGER COMMUNITIES, LLC

BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

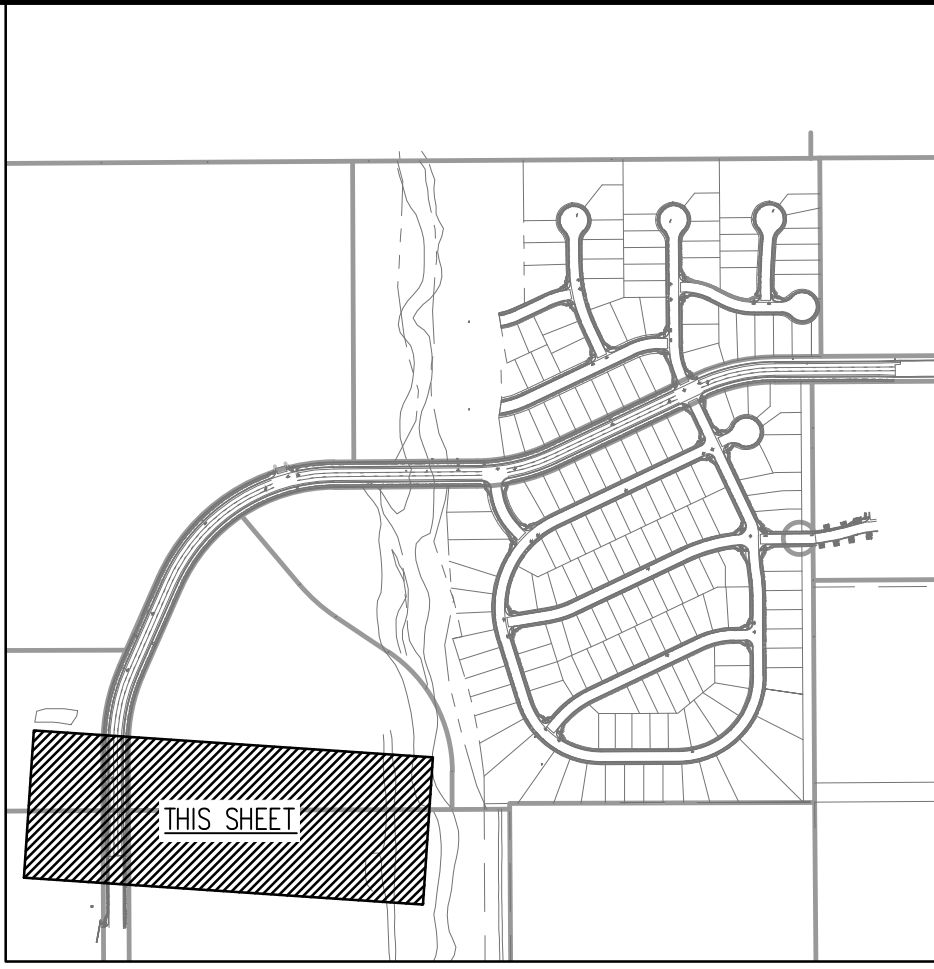
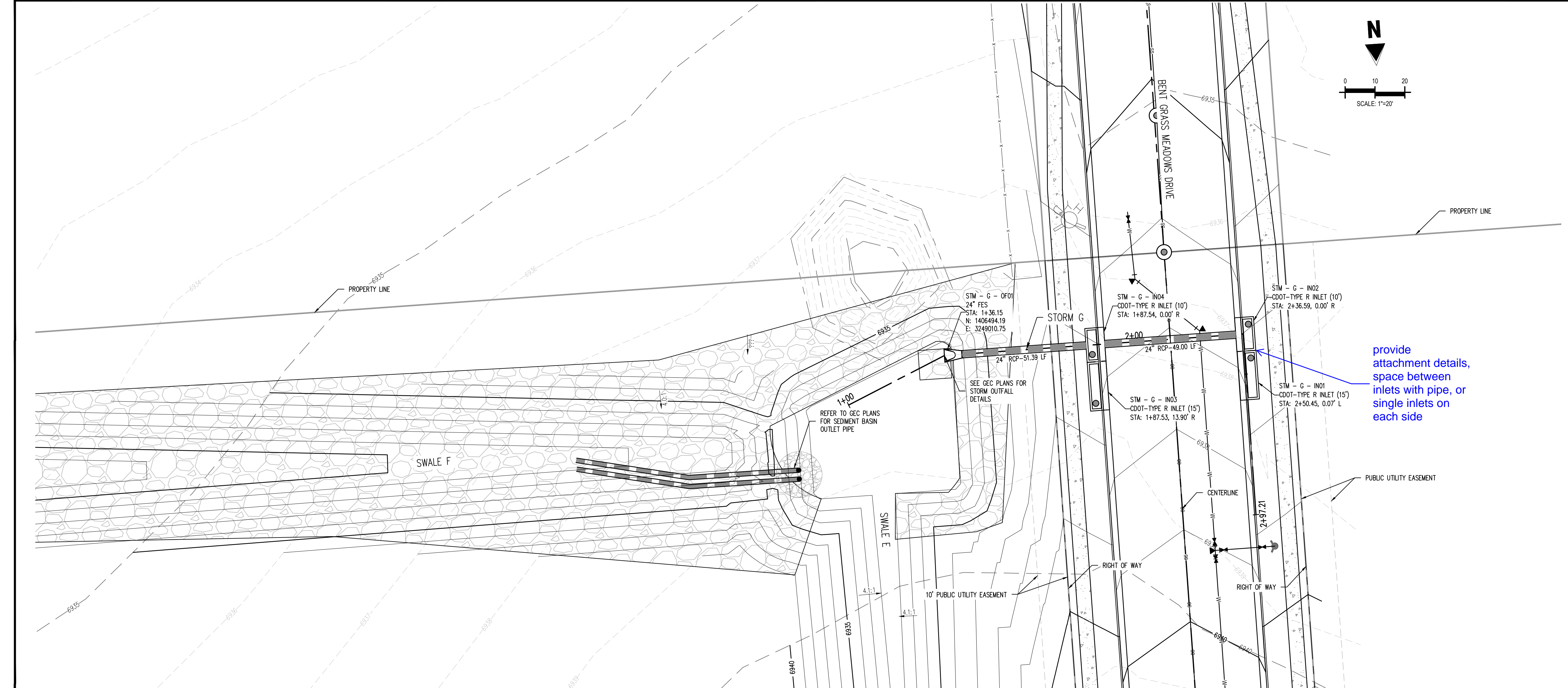
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

CROSS PAN &
CURB RETURN PROFILES

C3.07

Sheet 25 of 43



LEGEND

- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPERTY BOUNDARY
- 100 - YR FLOODPLAIN LINE
- EXISTING STORM SEWER
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- PROPOSED STORM SEWER
- PROPOSED RIPRAP
- PROPOSED GROUTED BOULDERS

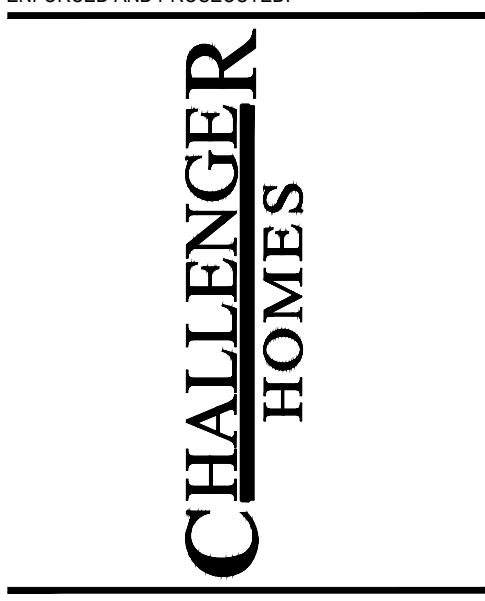
NOTE
UNLESS OTHERWISE NOTED, ALL RIPRAP SHOWN IS TO BE TYPE L-18" DEPTH.



ALIGN - STM - G
STA: 1+00 - 2+97.21
SCALE: H: 1"=20' V: 1"=10'



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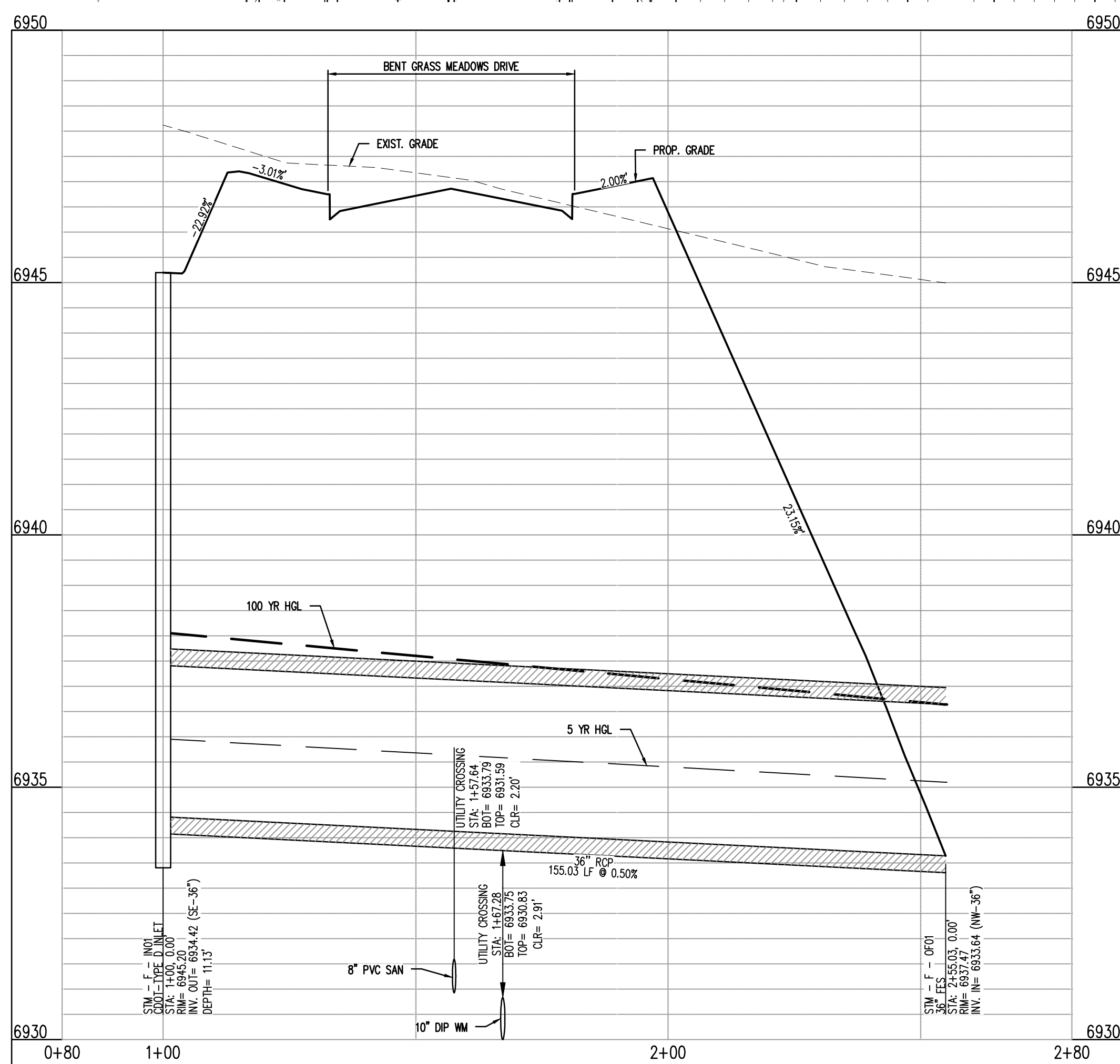
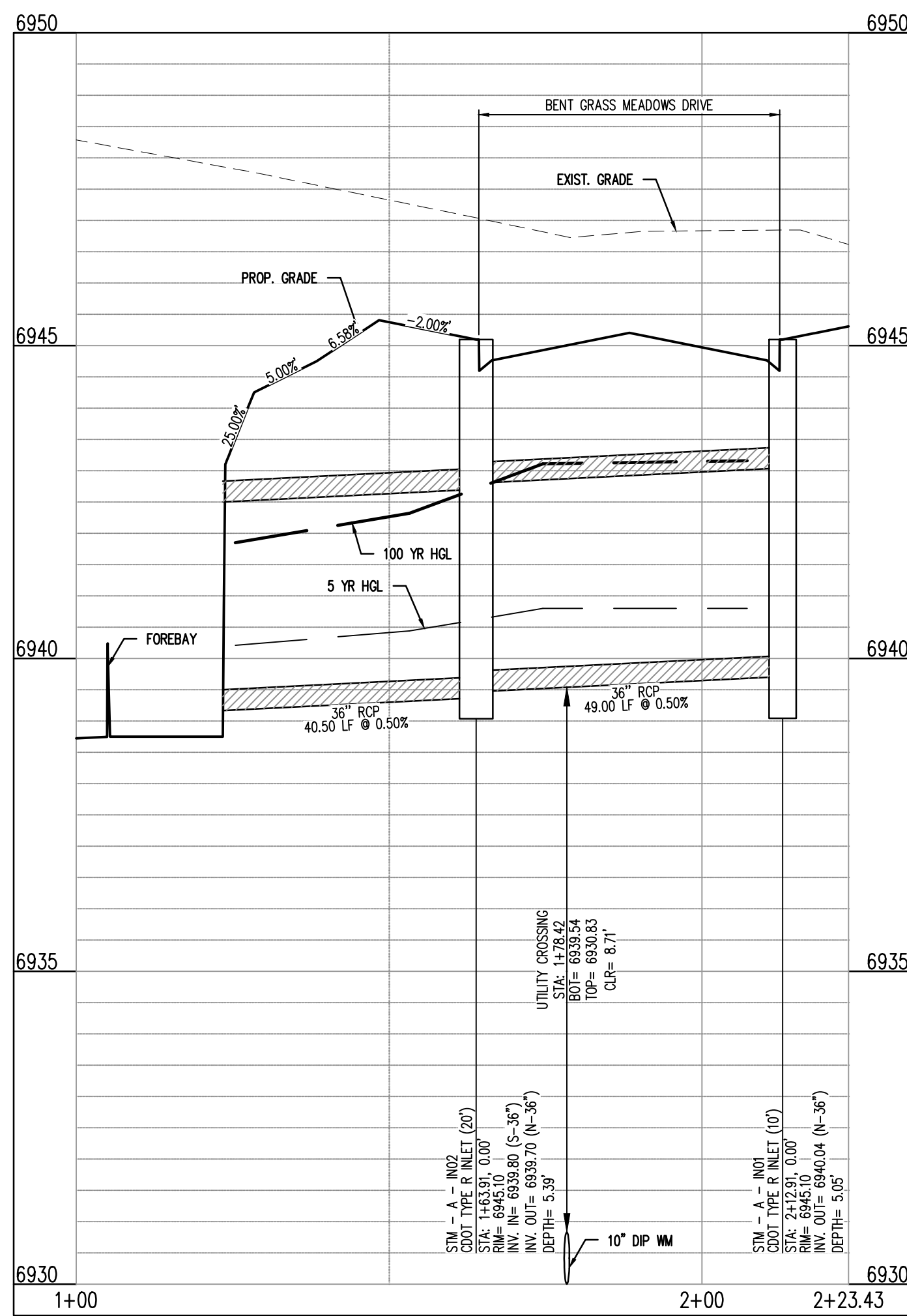
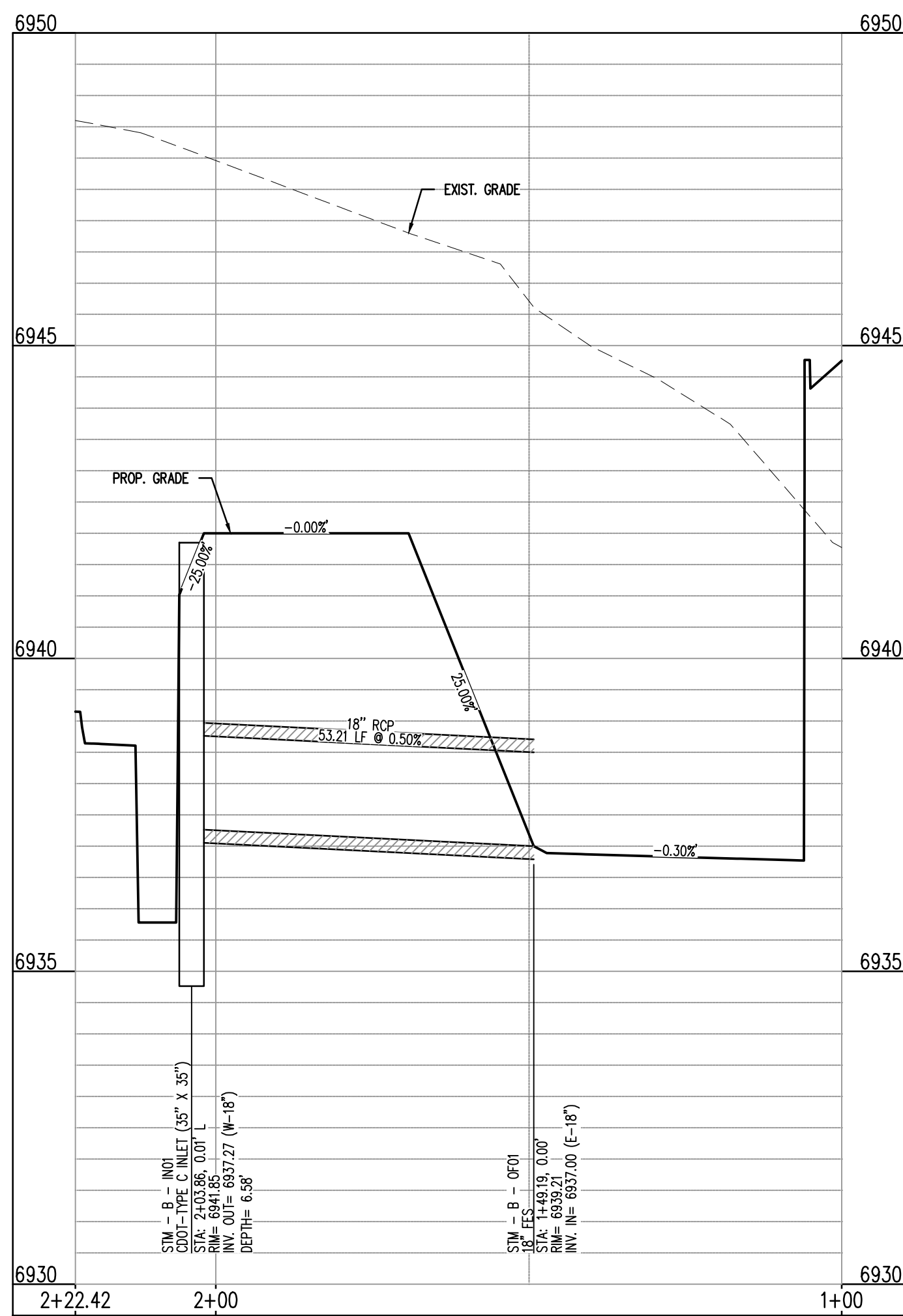
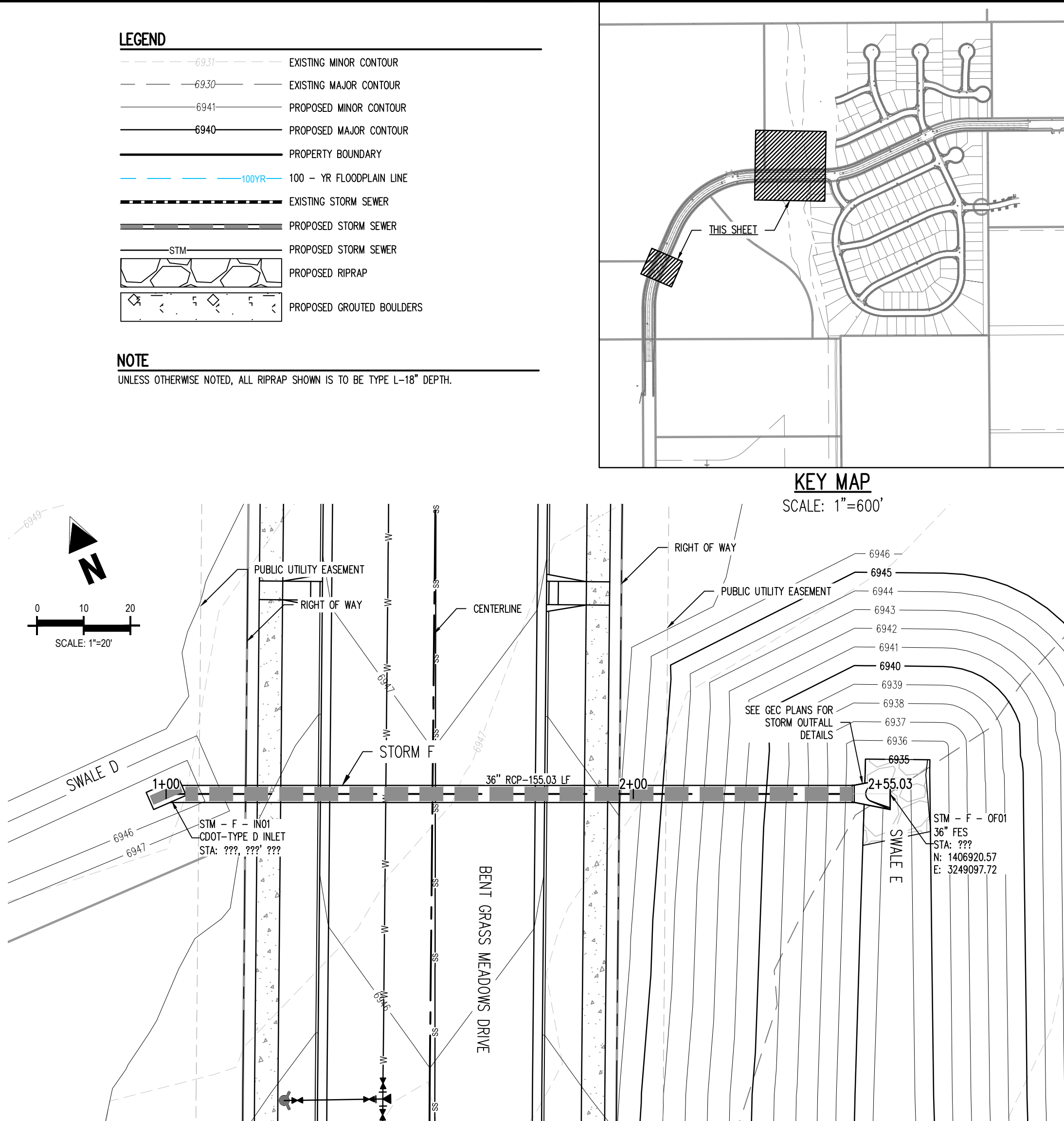
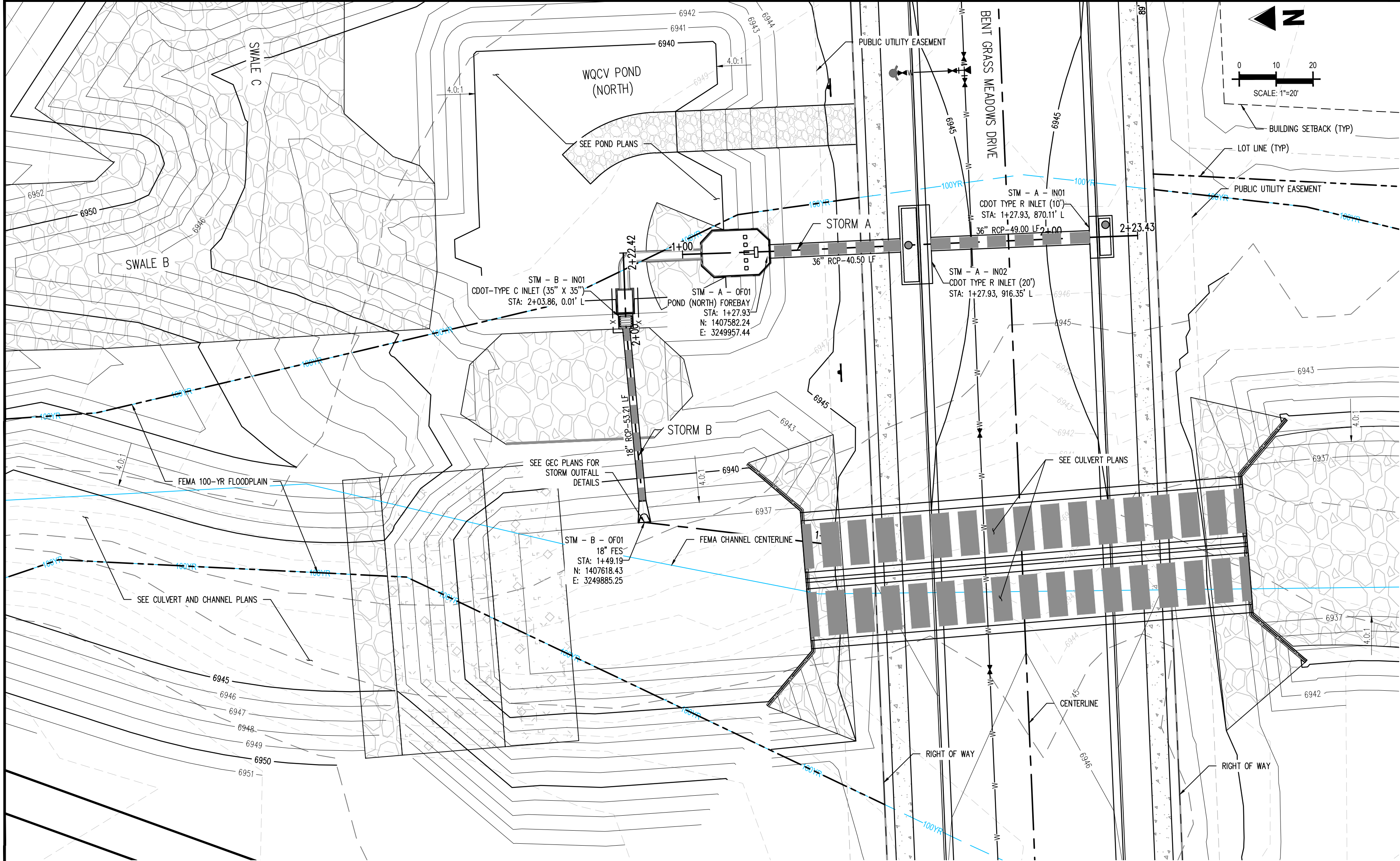


CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
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CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

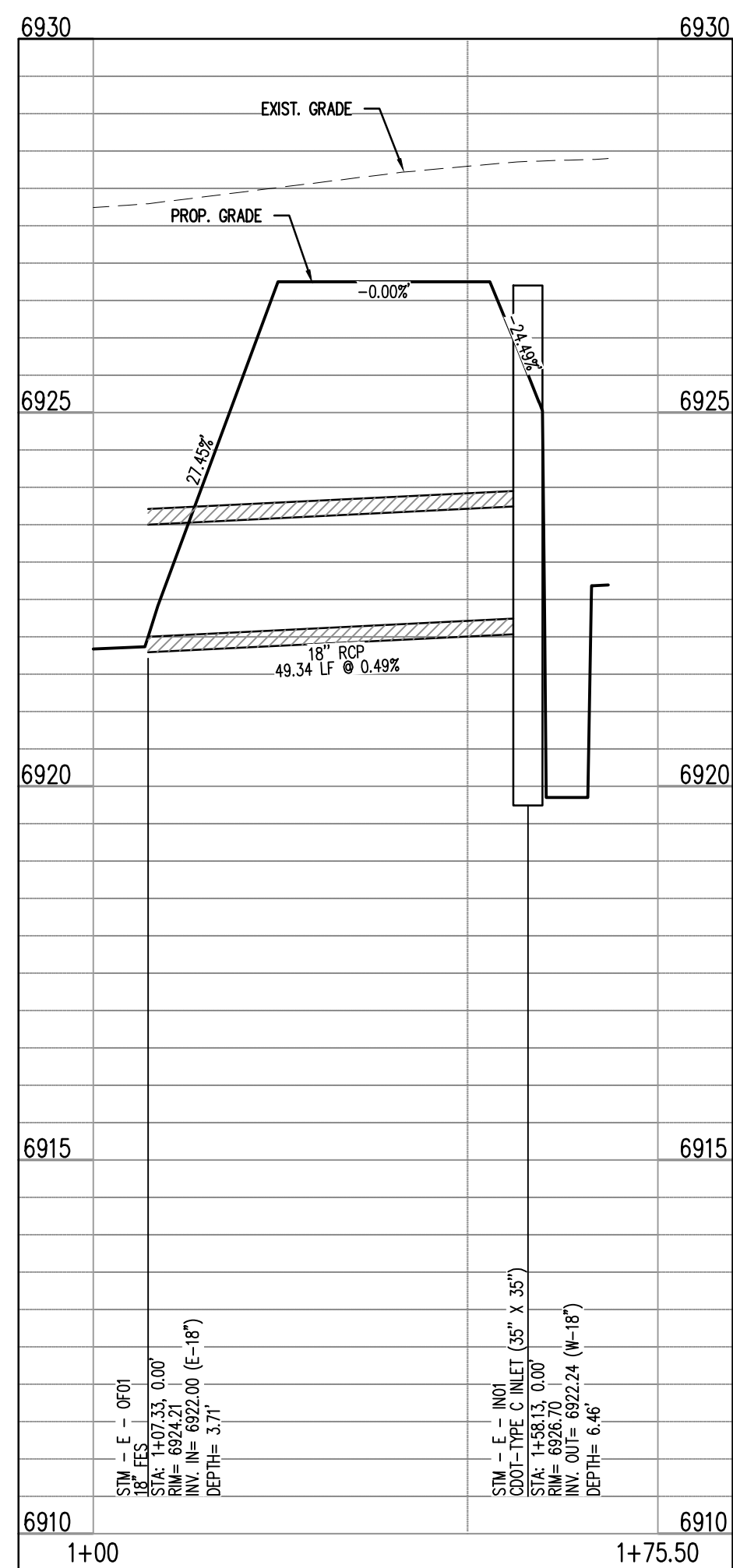
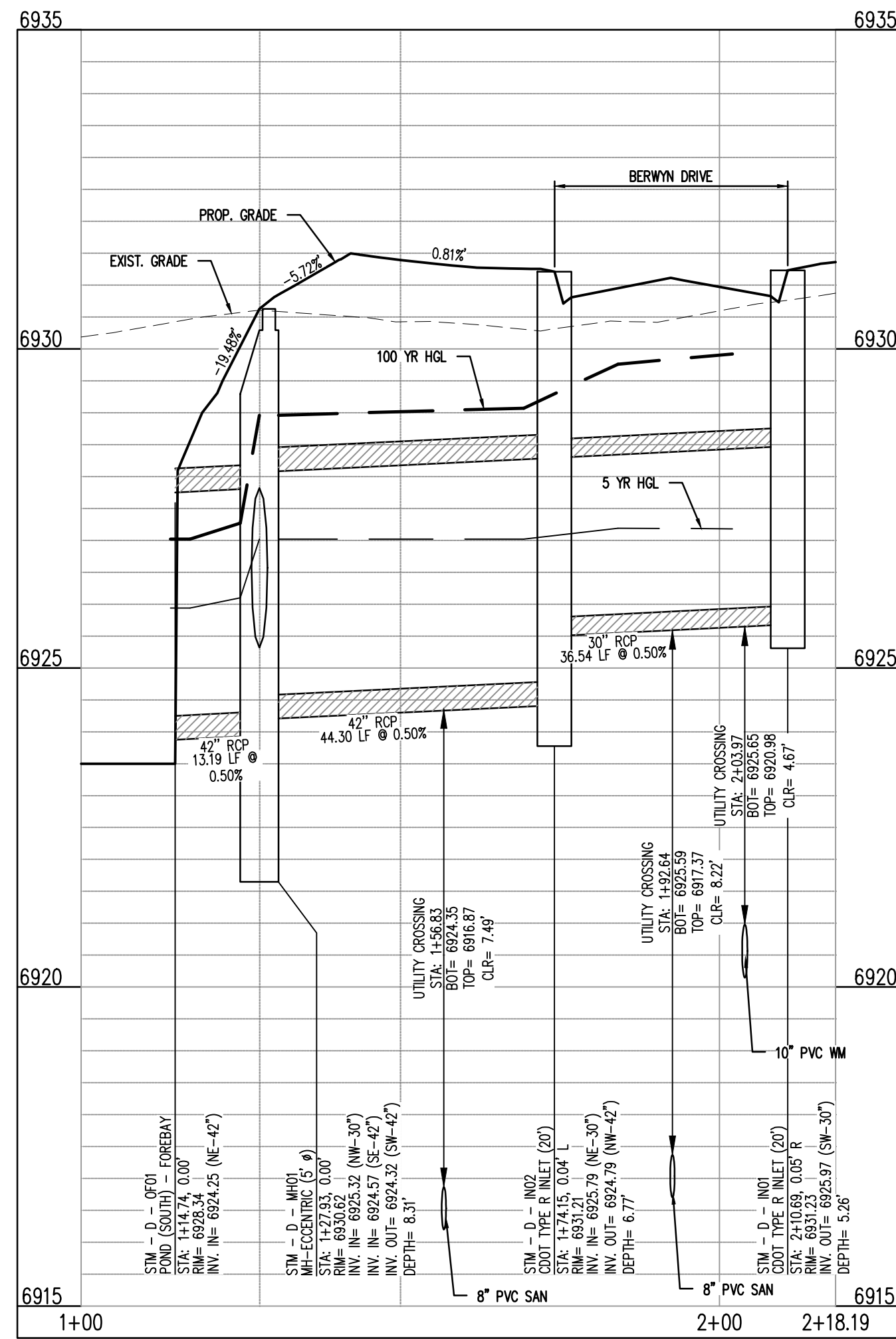
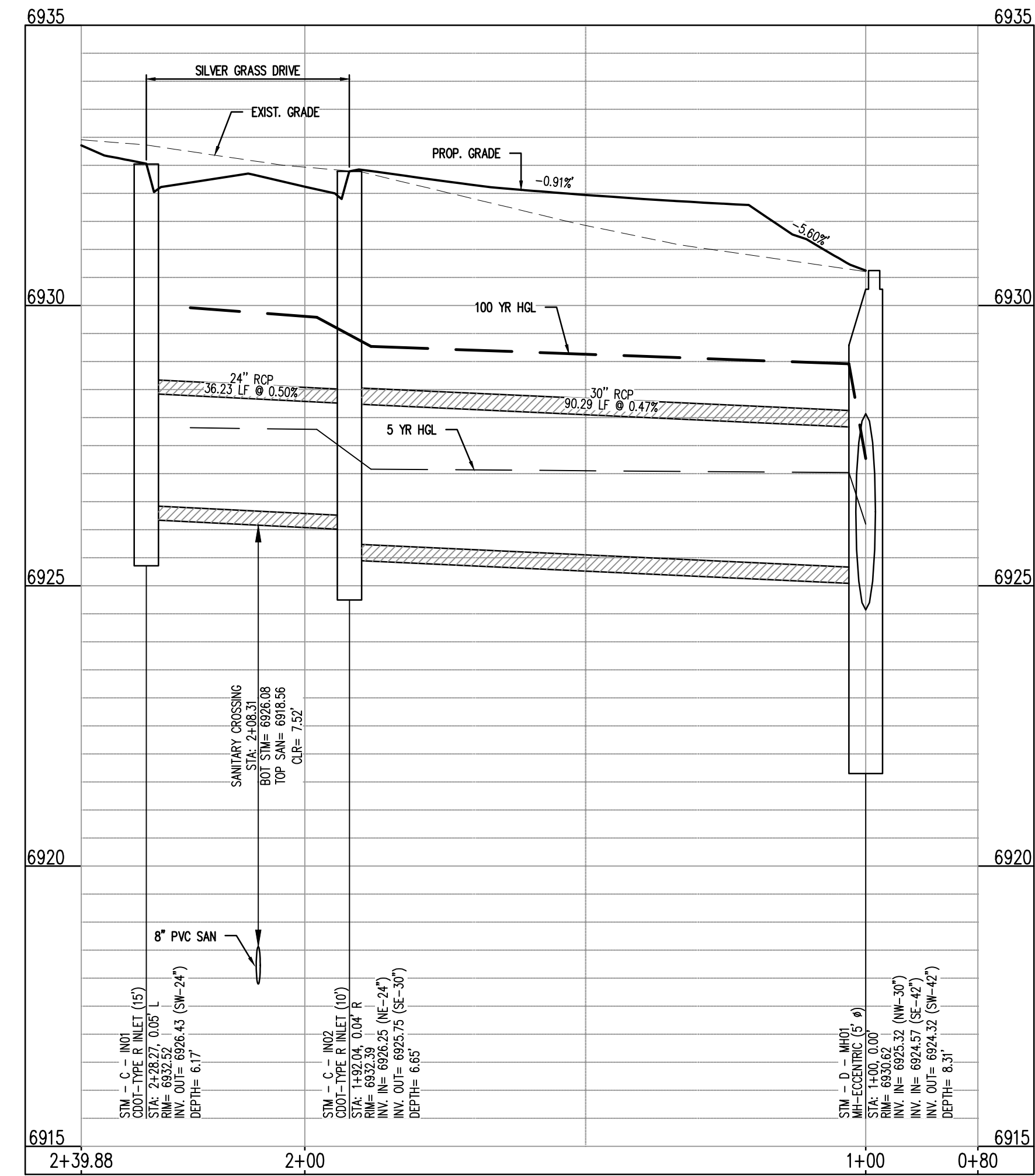
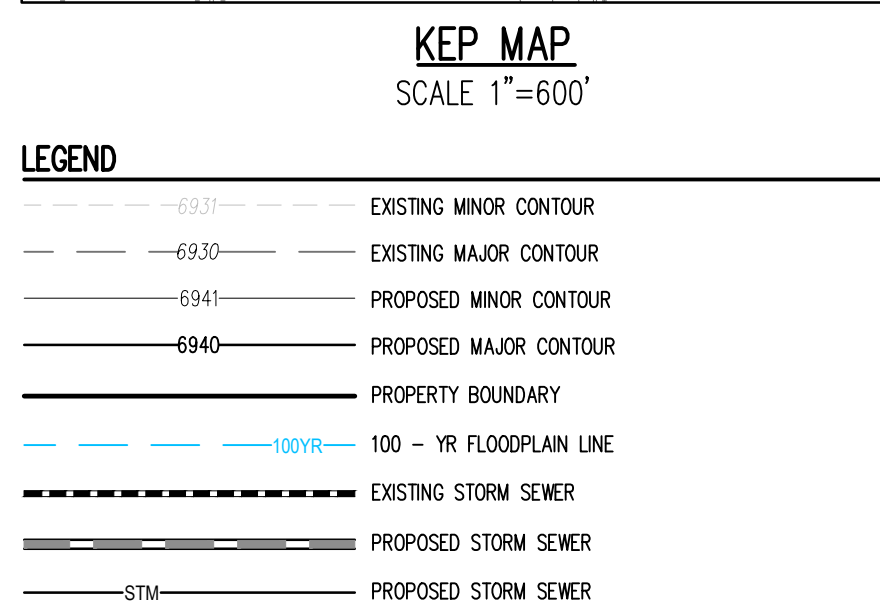
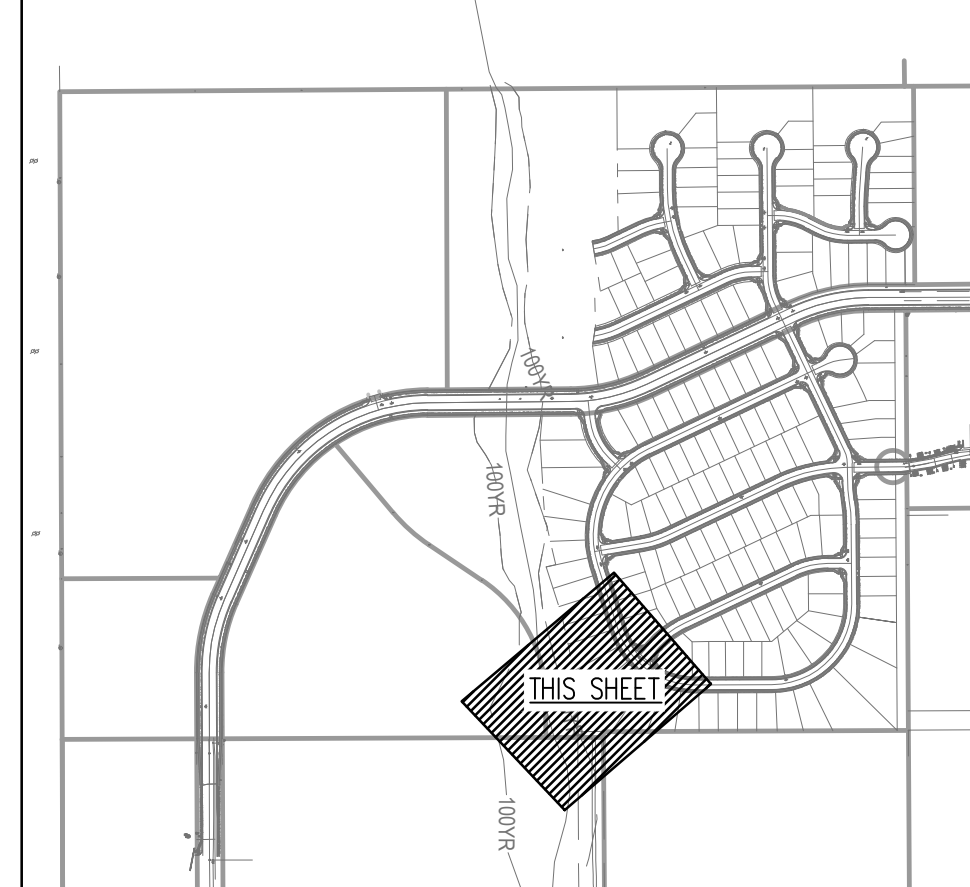
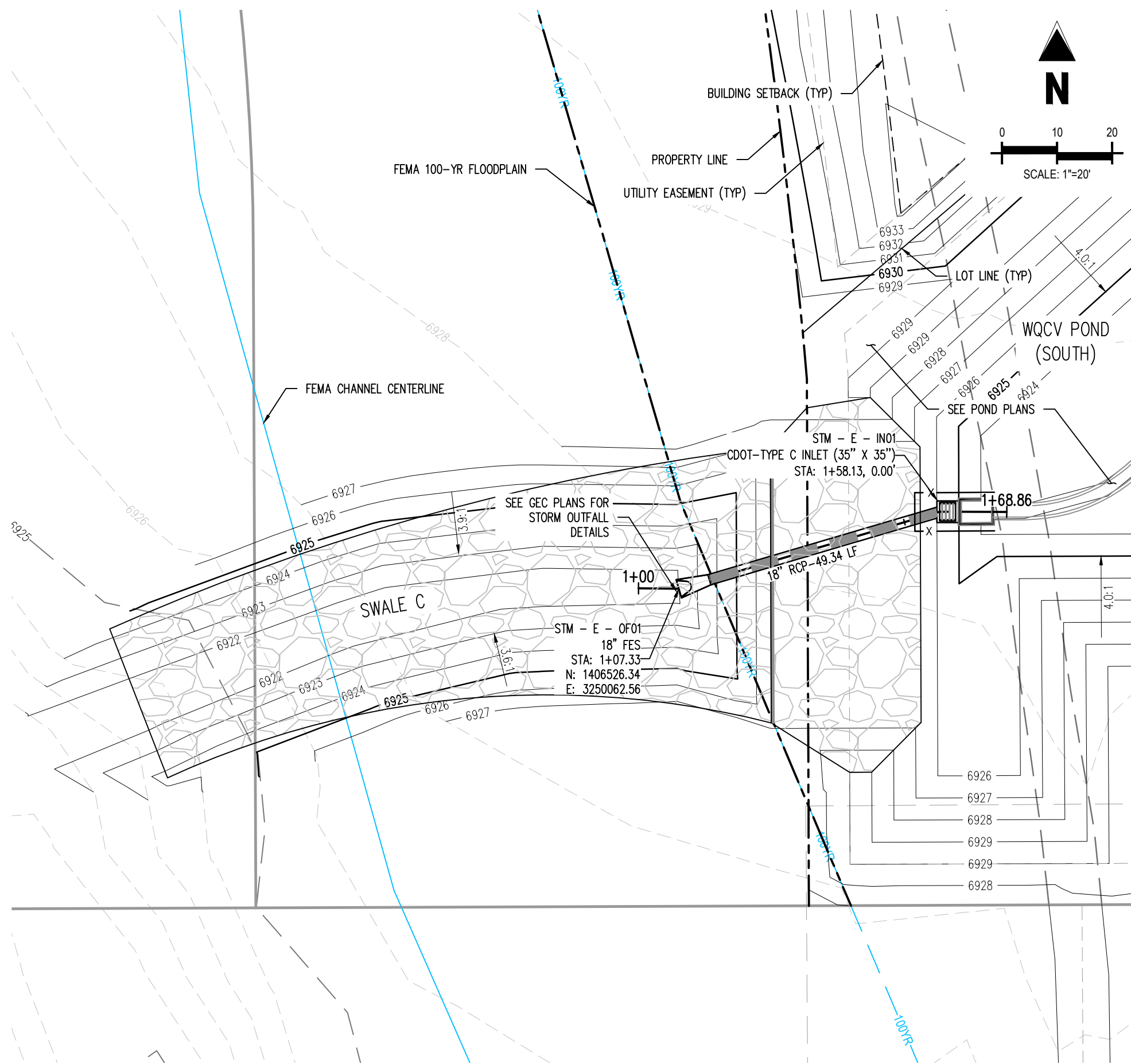
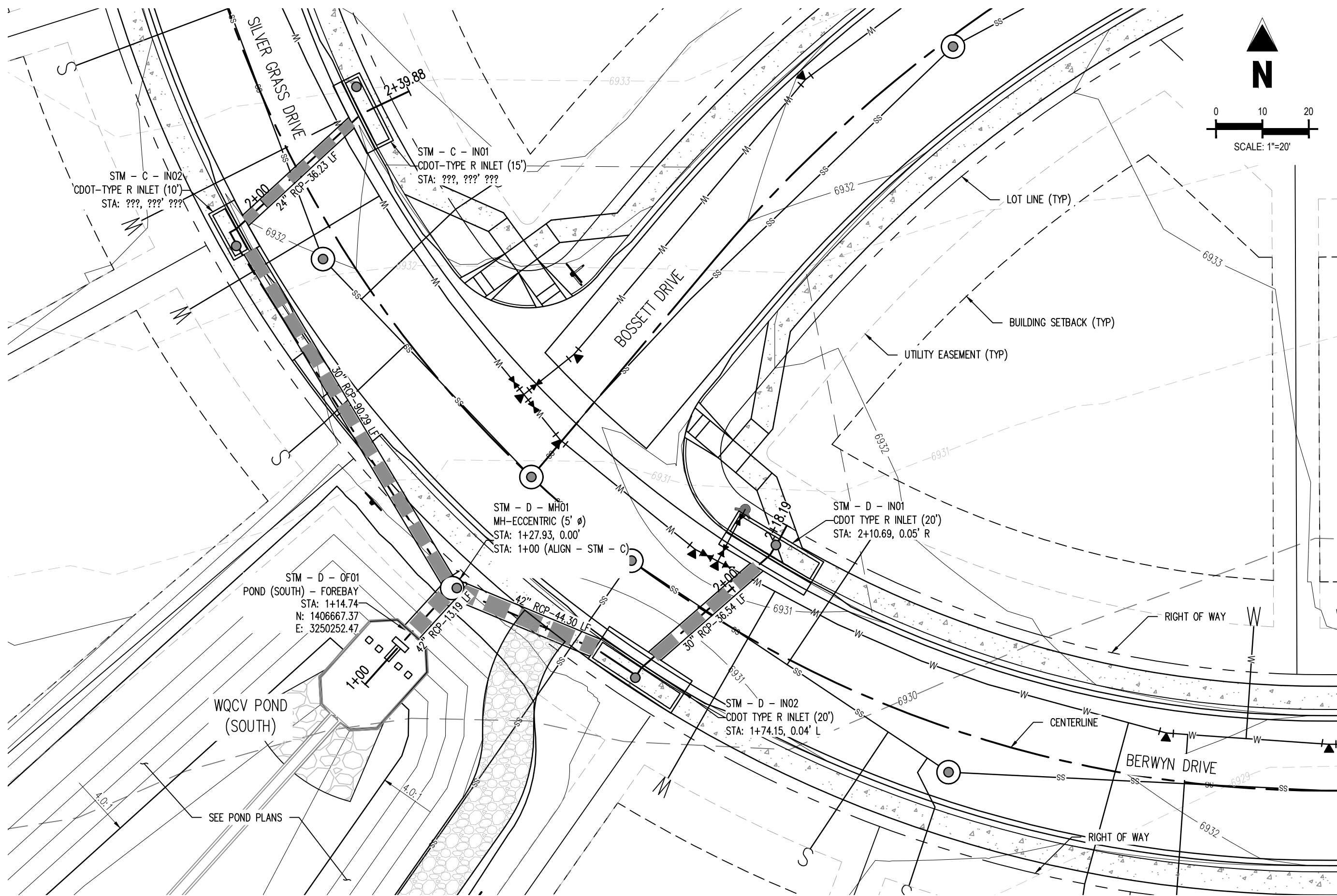
BENT GRASS MEADOWS
DRIVE STORM PLAN &
PROFILE



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Drawn By:	JDP
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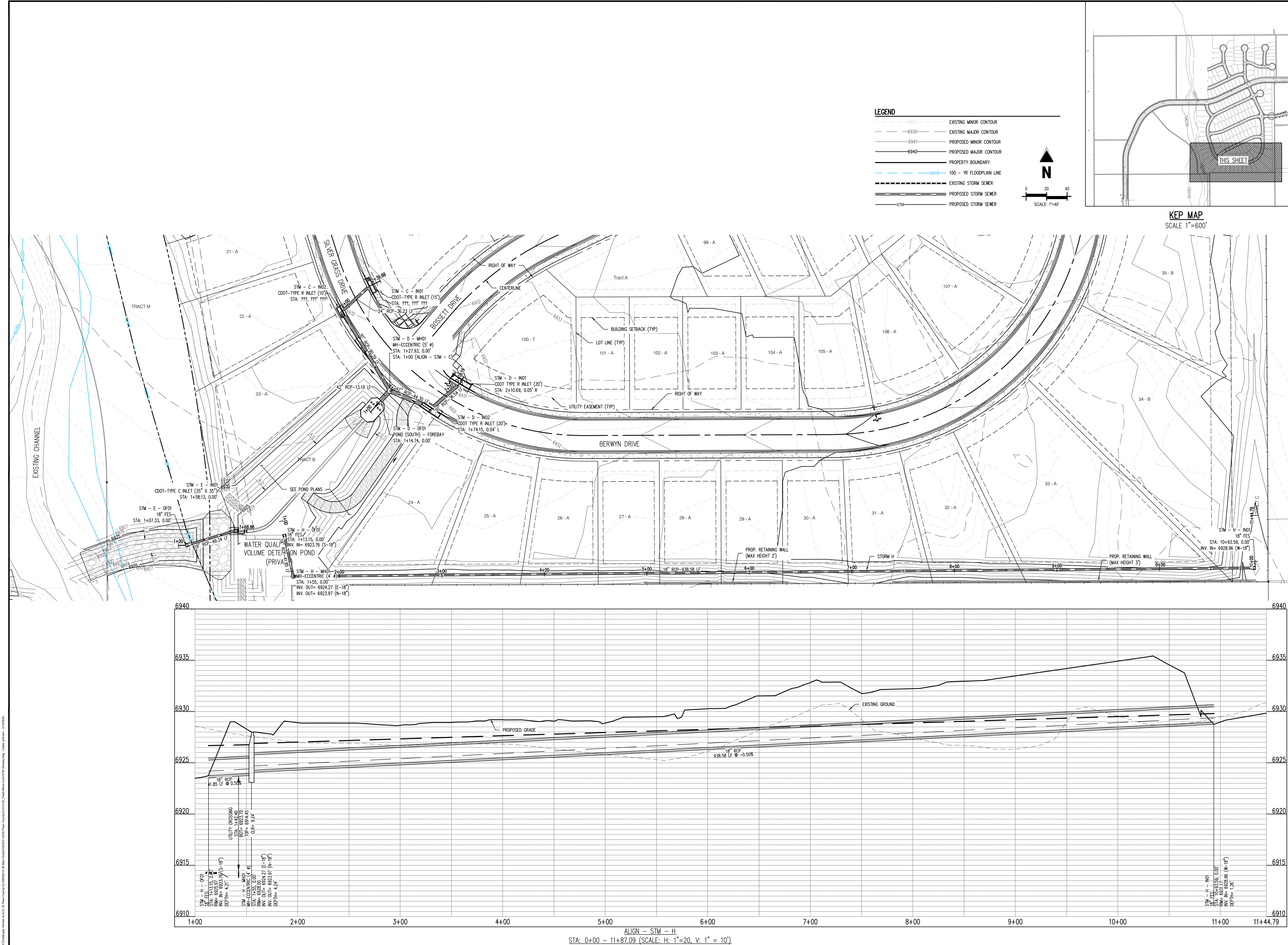
BENT GRASS MEADOWS DRIVE STORM PLAN & PROFILE



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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

C:\Projects\Bent Grass\Bent Grass\Bent Grass.dwg (2024) Plot: Storm Drainage (11/14/24) Plot Date: 11/14/24 Plot Time: 11:14:24 Plot User: JDP



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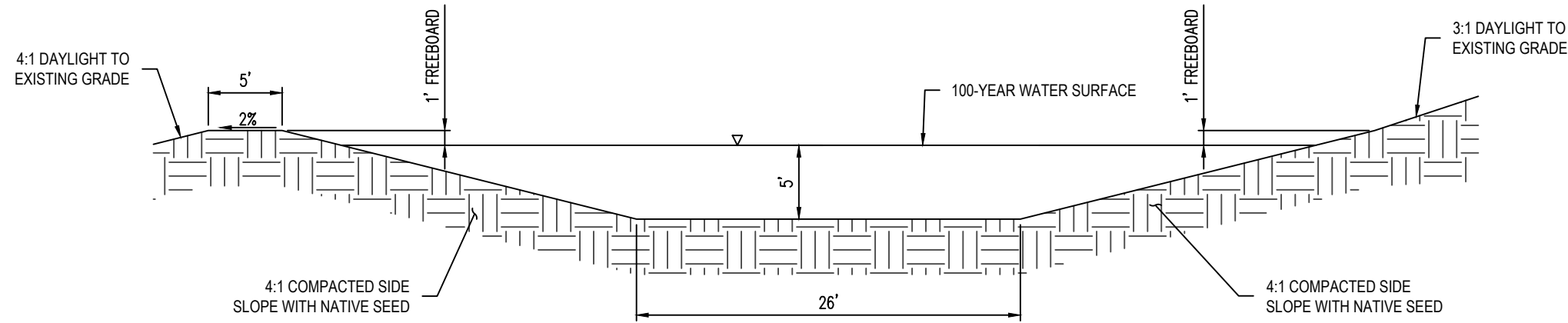
PROFESSIONAL ENGINEER
02/10/2020

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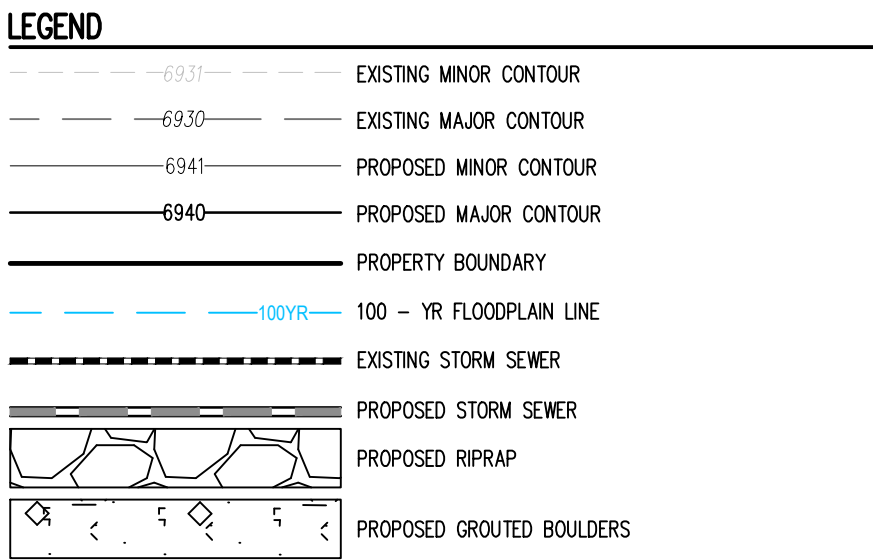
CHALLENGER HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

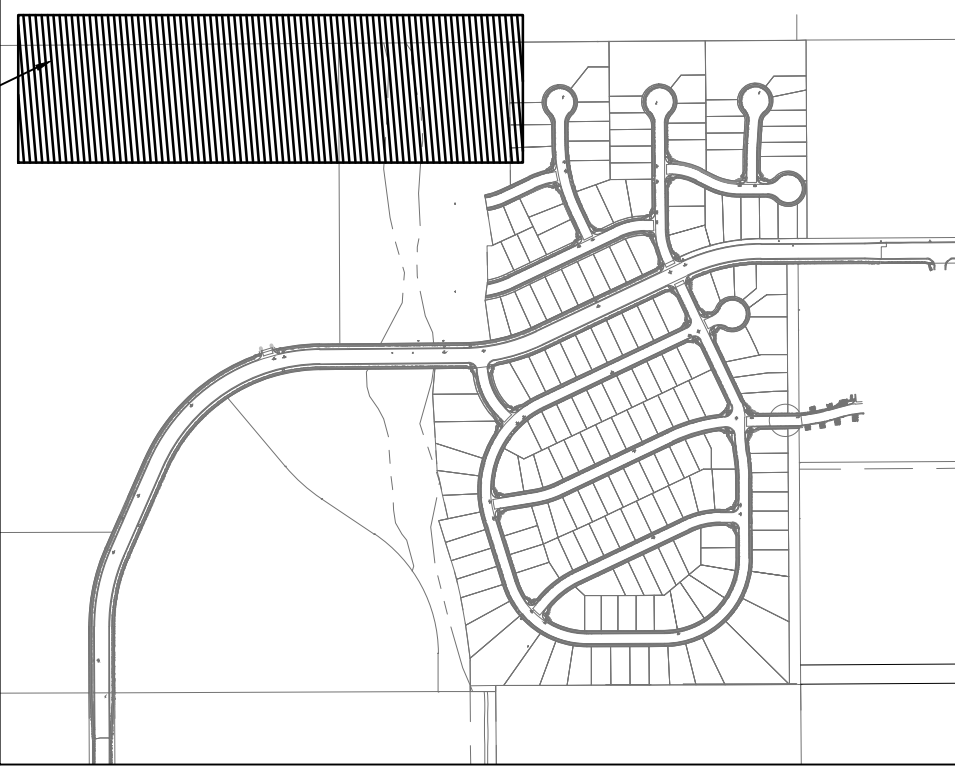
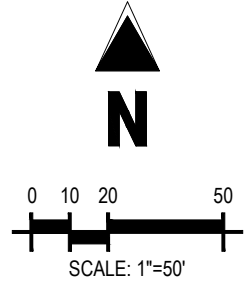
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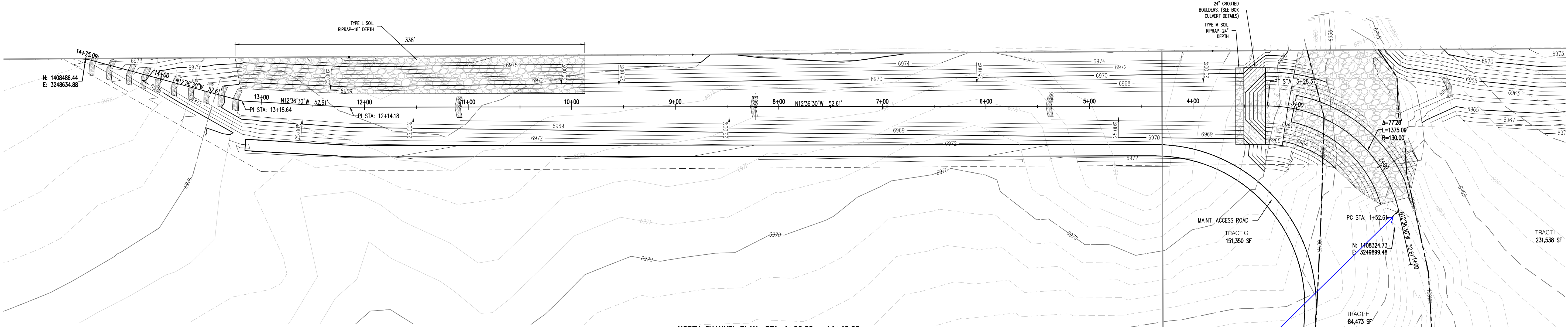
NORTH CHANNEL TYPICAL SECTION
SCALE: 1"=10'



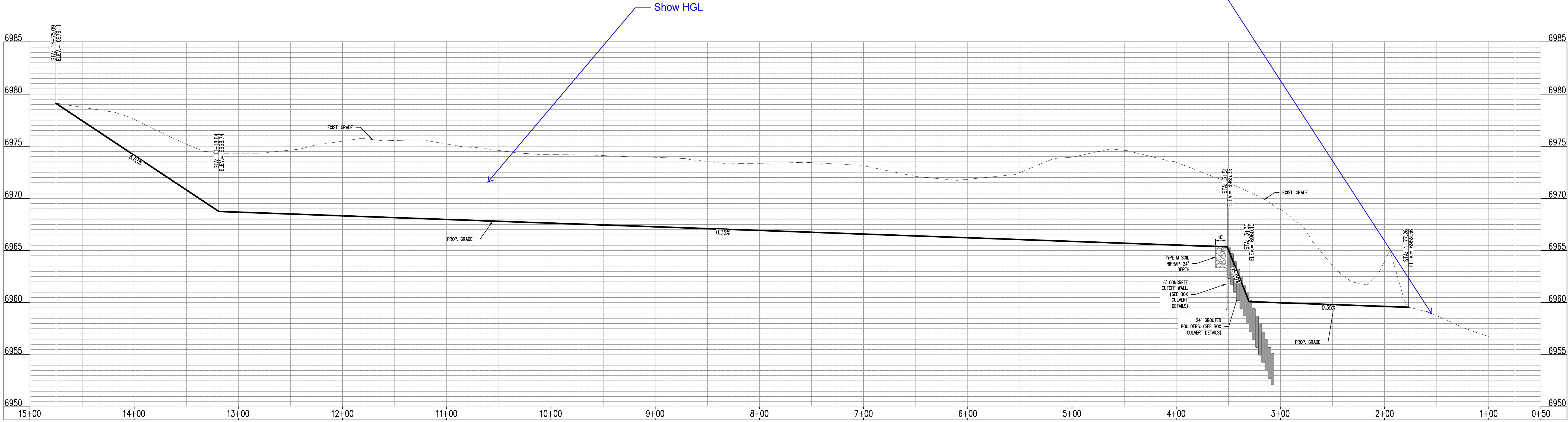
NOTE
UNLESS OTHERWISE NOTED, ALL RIPRAP SHOWN IS TO BE TYPE L-18" DEPTH.



SITE MAP
SCALE: 1"=600'



NORTH CHANNEL PLAN: STA. 1+00.00 - 14+40.00
SCALE: 1"=50'



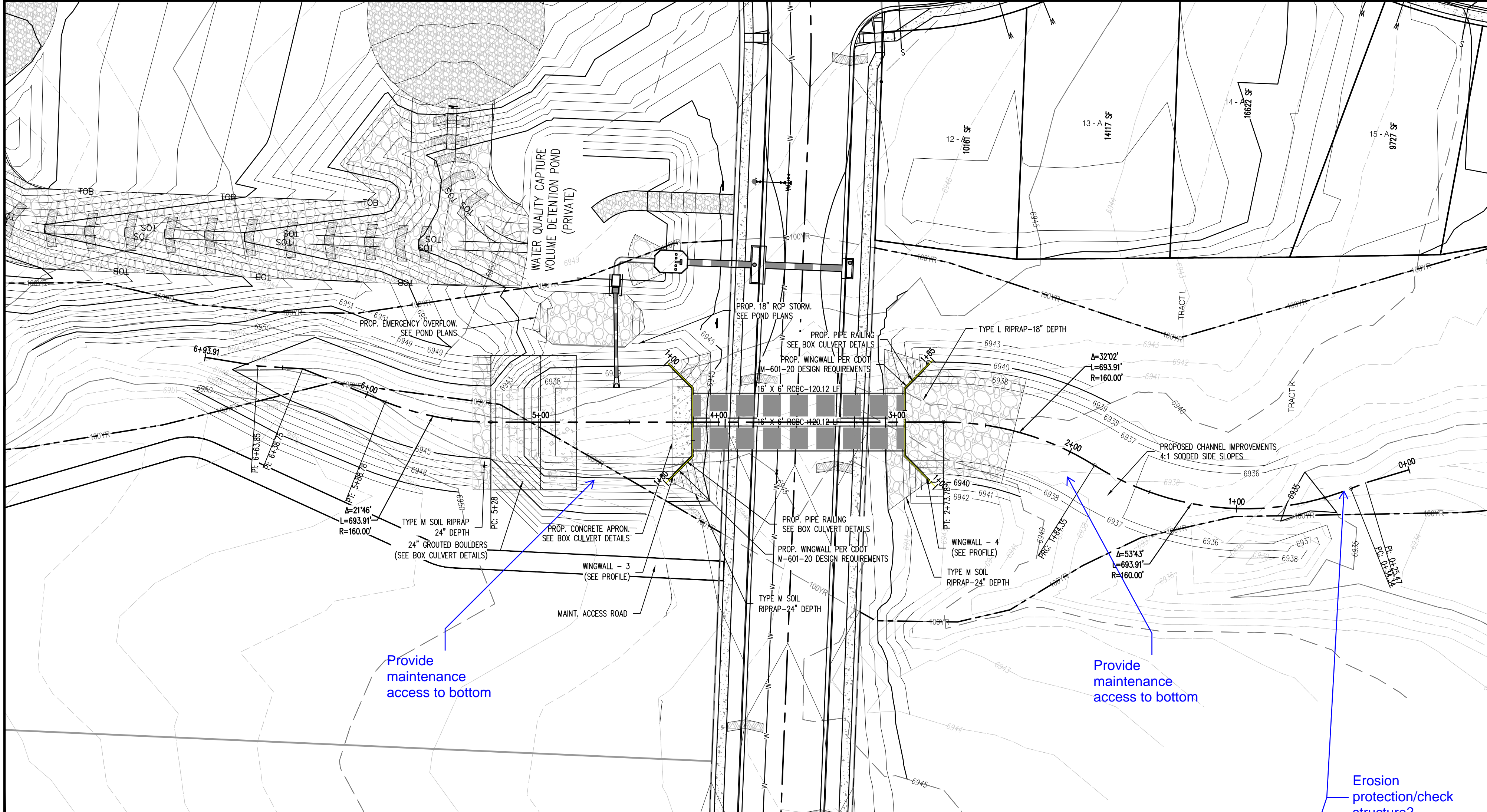
NORTH CHANNEL PROFILE: STA. 0+80.00 - 14+40.00
SCALE: H:1"=50', V:1"=5'

Erosion
protection/check
structure?

Show HGL

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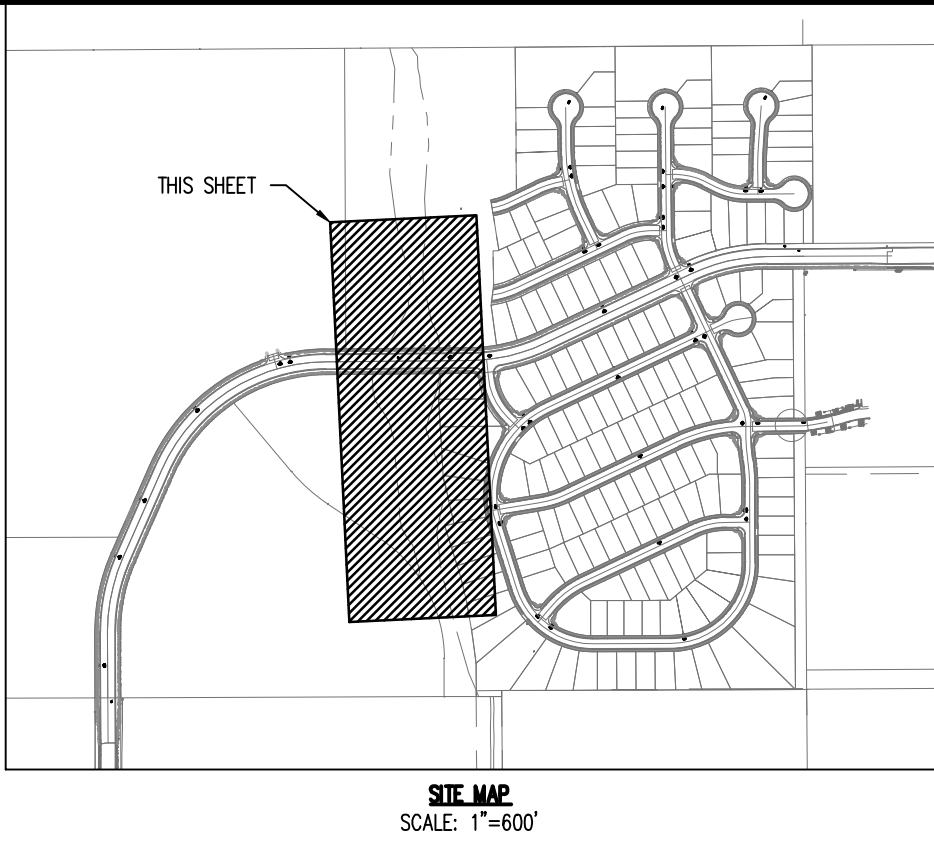
Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020



LEGEND

- 6931 - EXISTING MINOR CONTOUR
- 6930 - EXISTING MAJOR CONTOUR
- 6941 - PROPOSED MINOR CONTOUR
- 6940 - PROPOSED MAJOR CONTOUR
- 100YR - 100 - YR FLOODPLAIN LINE
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED RIPRAP
- PROPOSED GROUTED BOULDERS

NOTE
UNLESS OTHERWISE NOTED, ALL RIPRAP SHOWN IS TO BE TYPE L-18" DEPTH.



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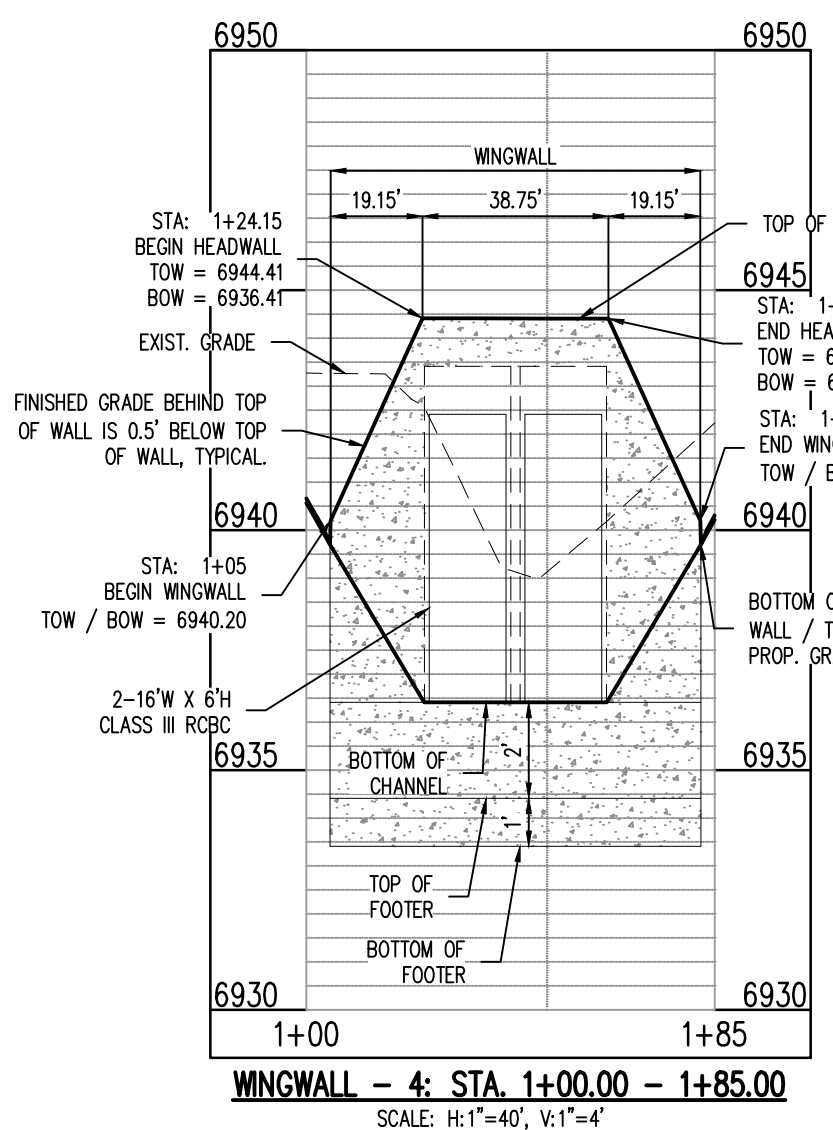
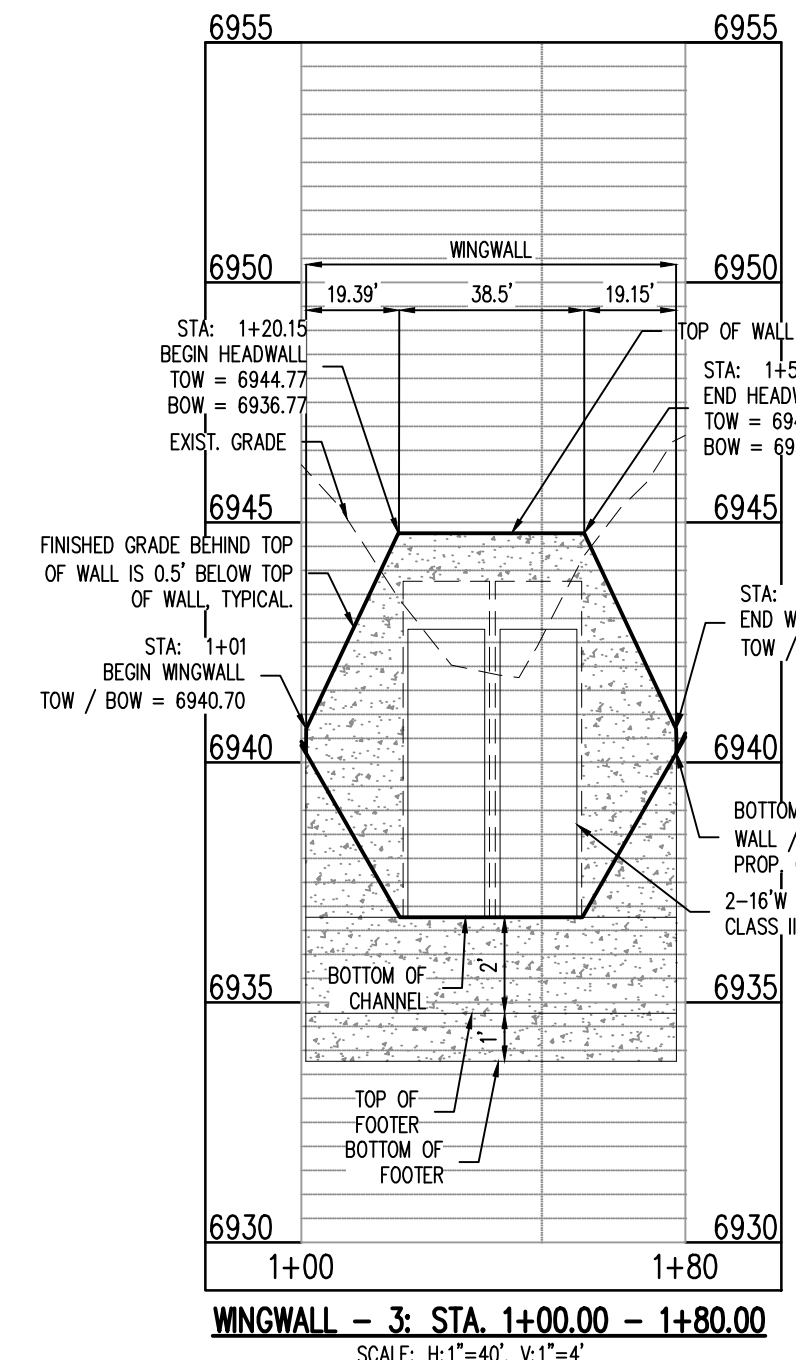
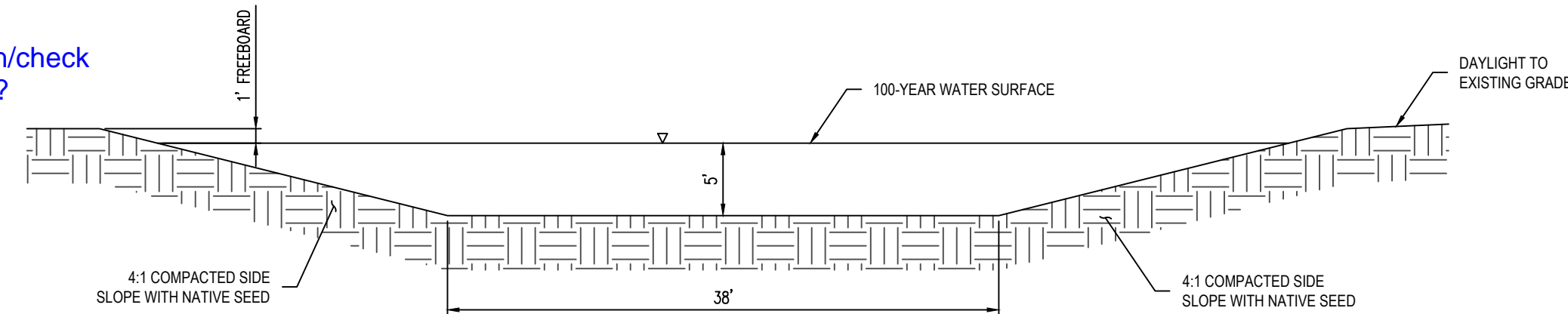
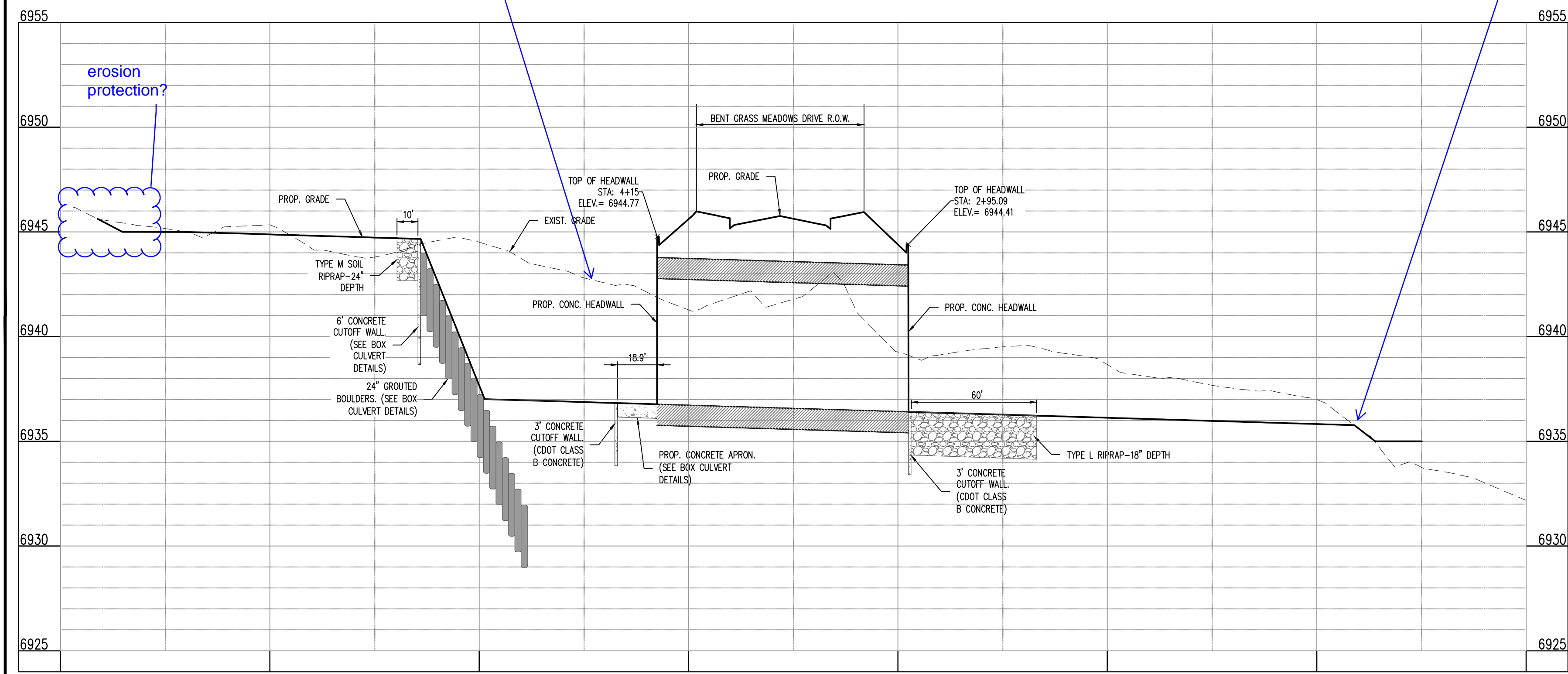
CHALLENGER HOMES

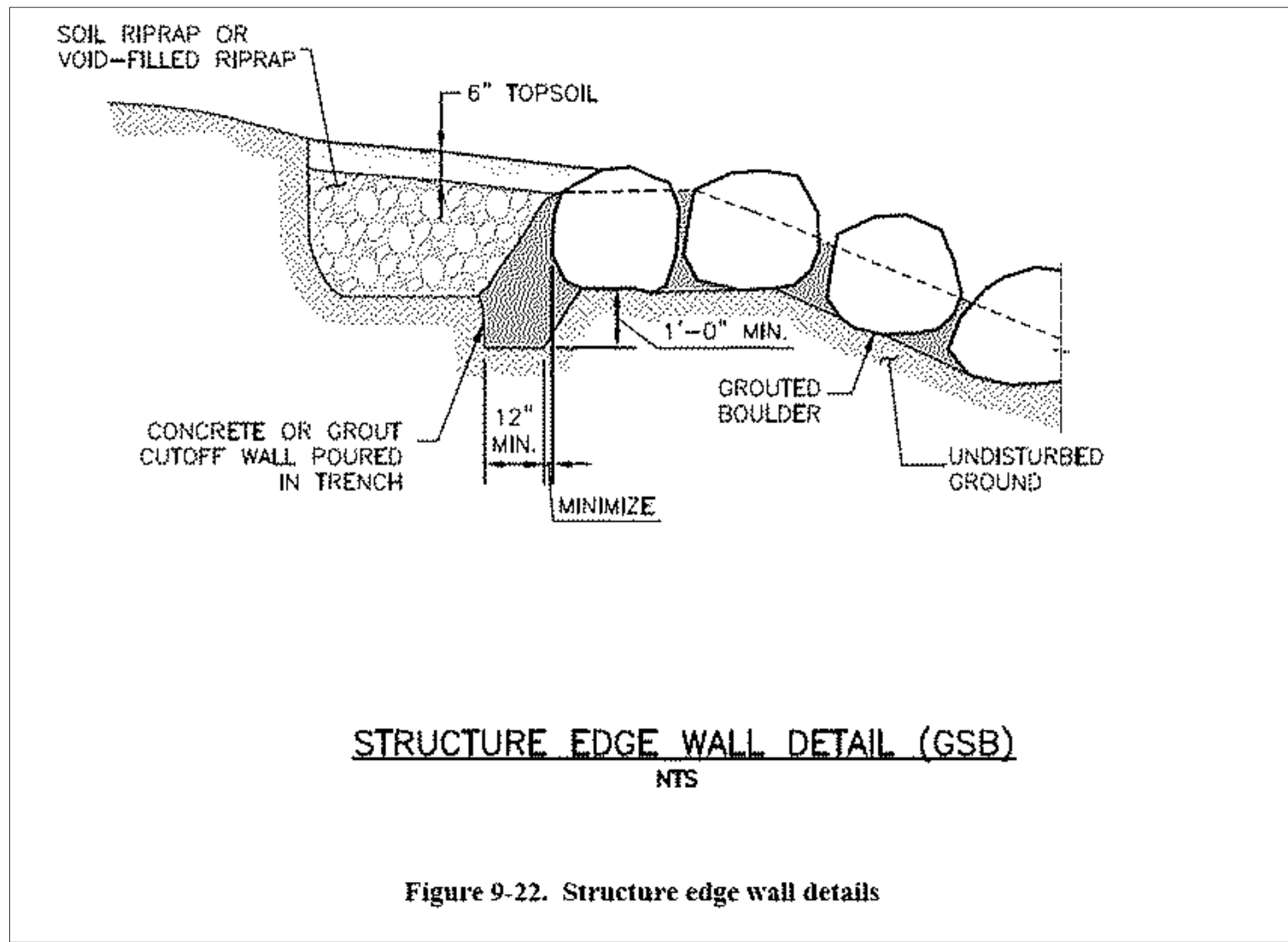
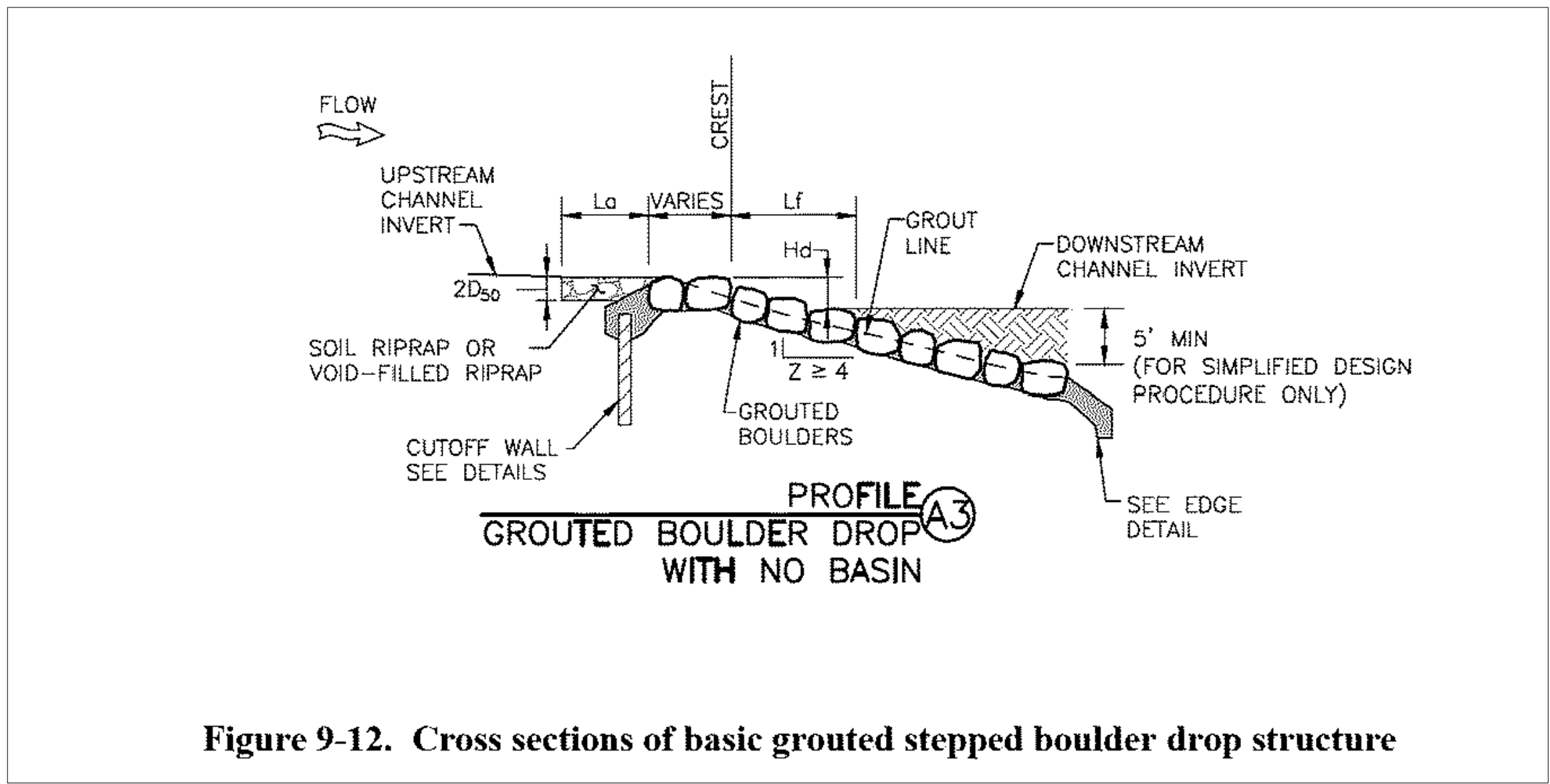
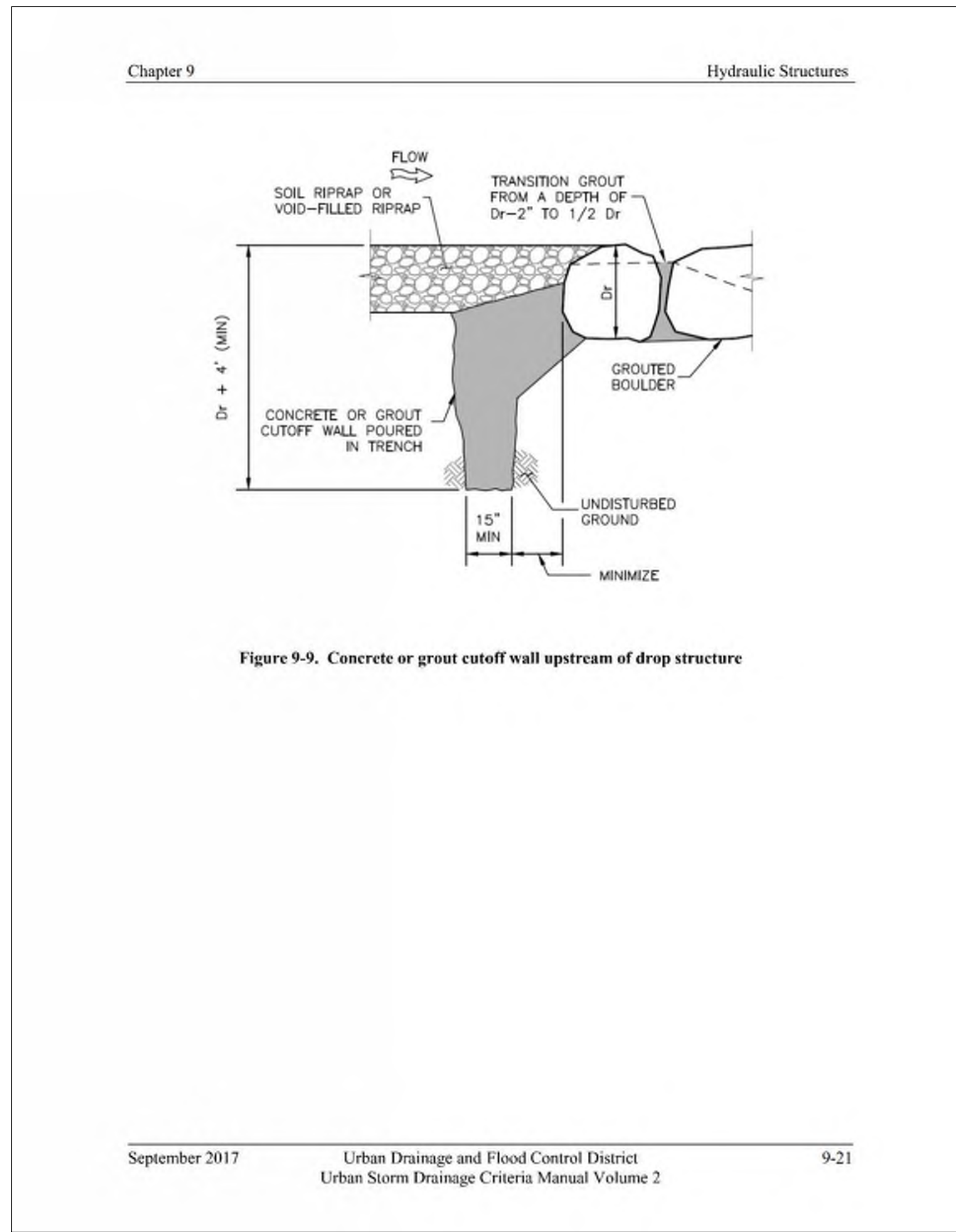
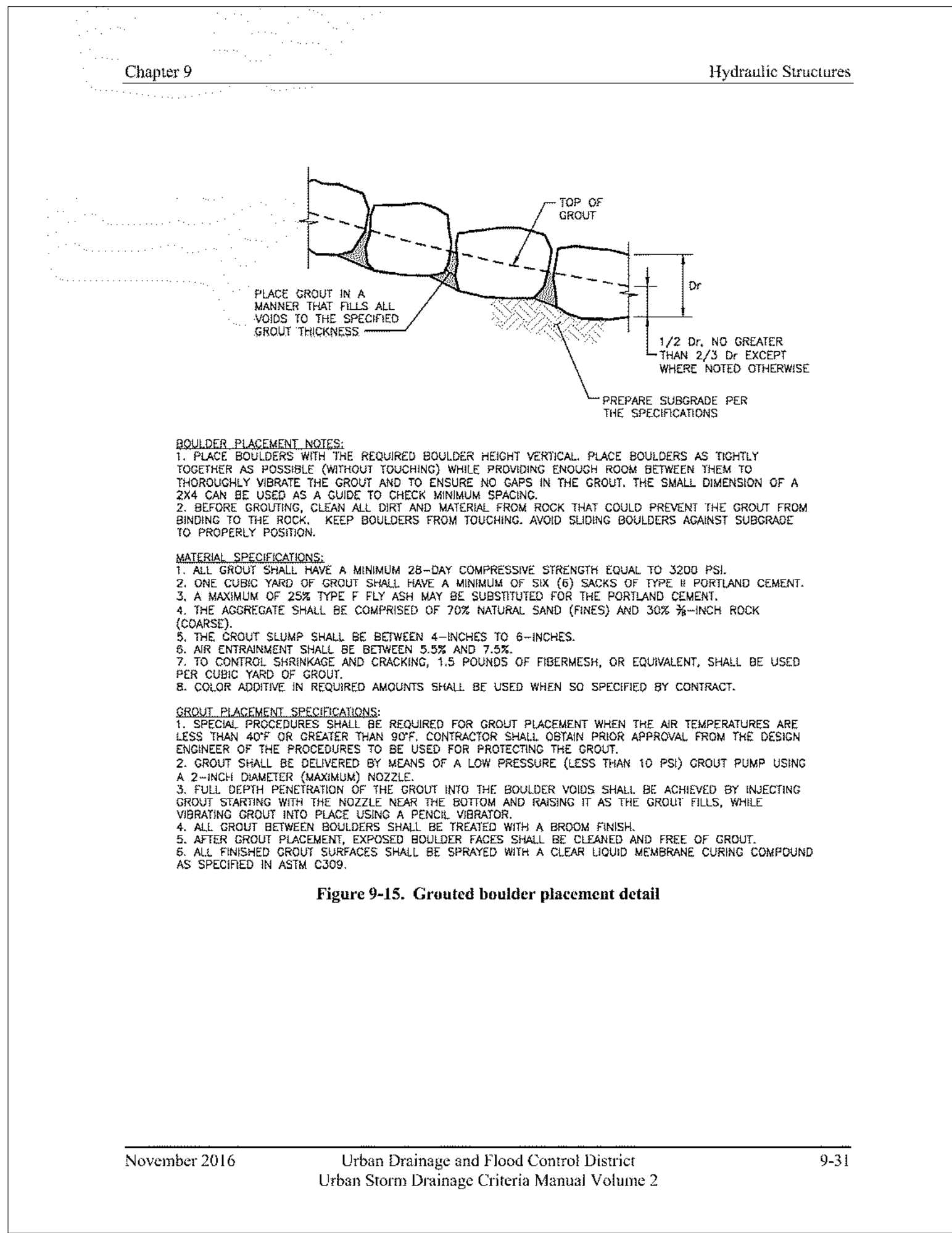
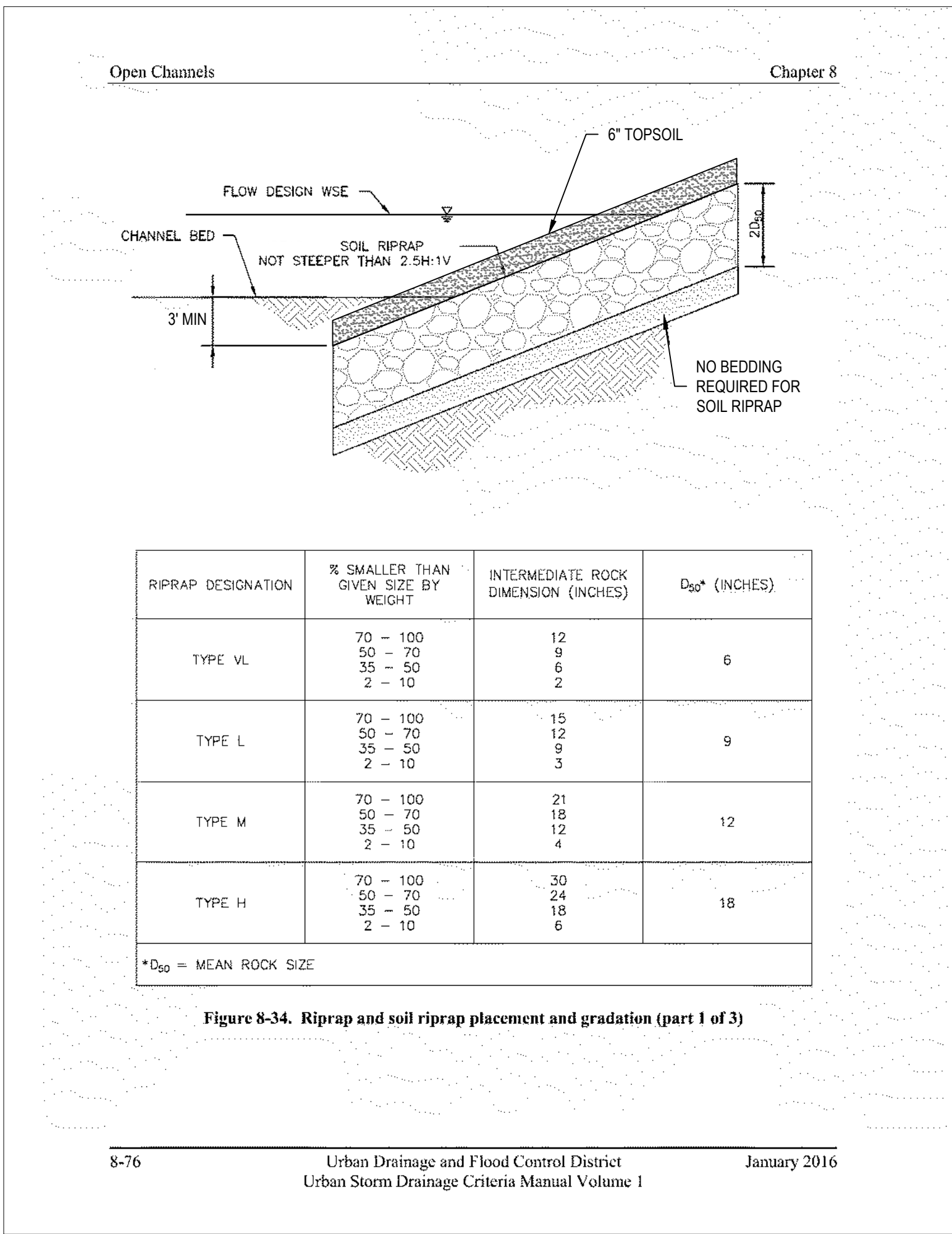
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

CULVERT & WINGWALL PLAN & PROFILE





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CHALLENGER
HOMES

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FOR
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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

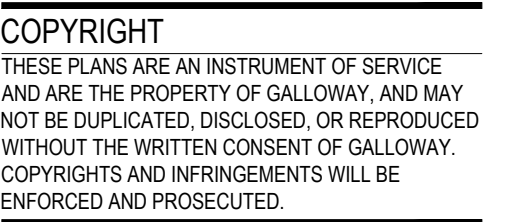
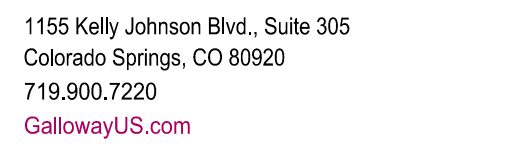
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

BOX CULVERT DETAILS

C4.08

Sheet 33 of 43



CHALLENGER HOMES

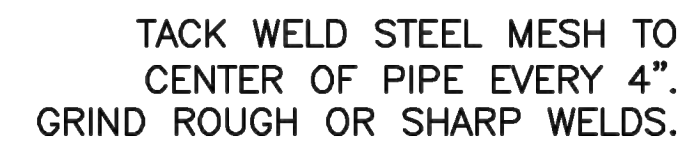
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BENT GRASS RESIDENTIAL FILING NO. 2
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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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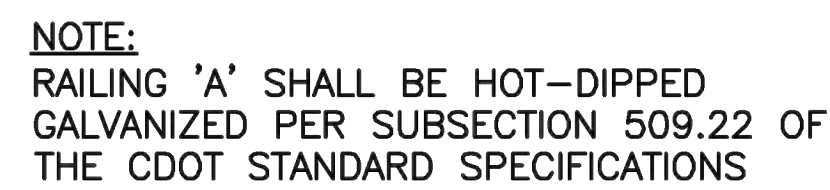
Project No:	CLH000014.20
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Checked By:	RGD
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BOX CULVERT DETAILS

Sheet 34 of 43

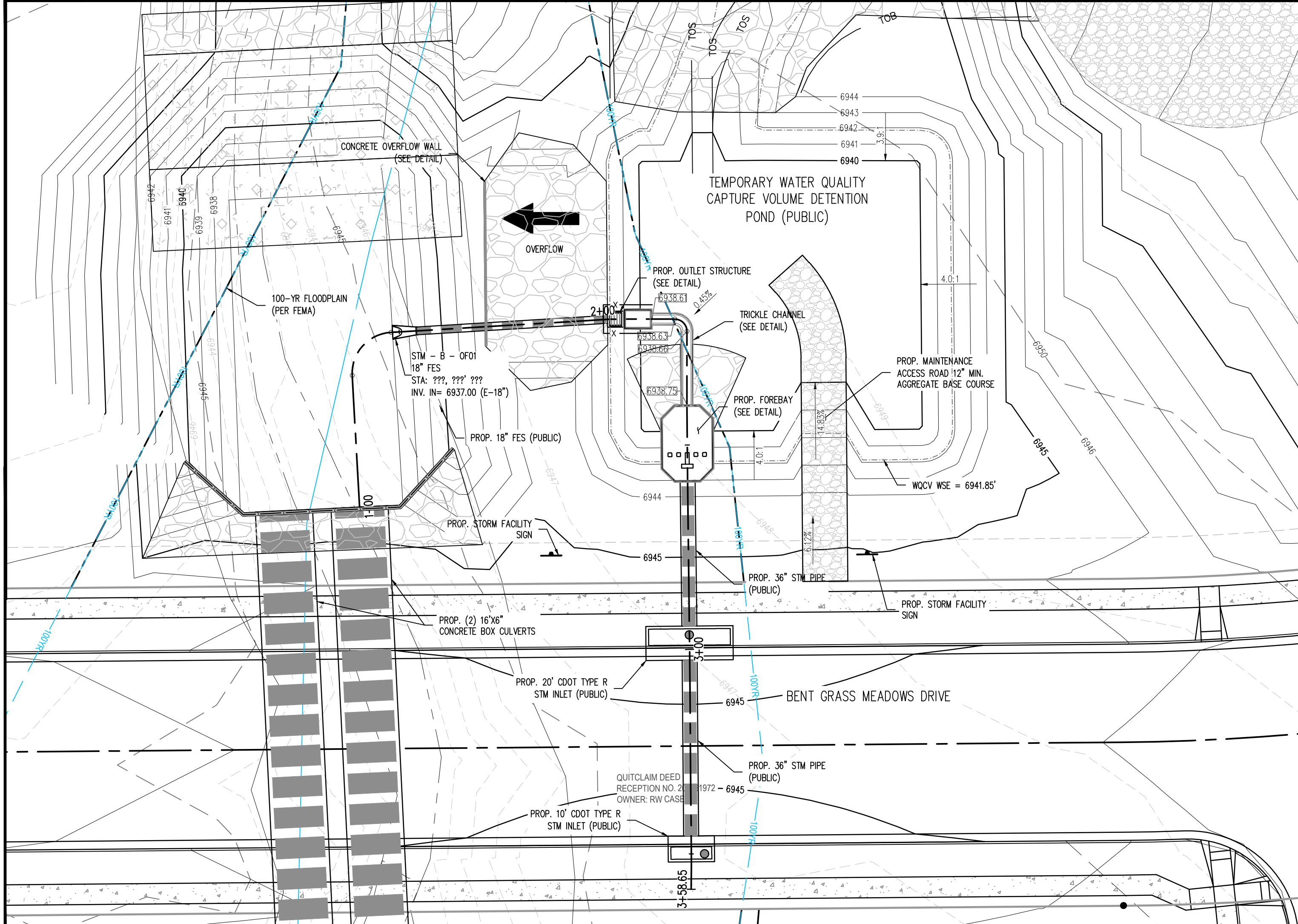


1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST VERSION OF THE CDOT STANDARD SPECIFICATIONS FOR ROAD & BRIDGE CONSTRUCTION.
2. STEEL PIPE TO BE ASTM A53 GRADE B; WITH $F_y = 35$ KSI.
3. STEEL PLATES, CHANNELS & ANGLES TO BE ASTM A36; WITH $F_y = 36$ KSI.
4. ALL WELDING TO CONFORM TO CURRENT AWS D1.1 REQUIREMENTS.
5. COMPLETE ASSEMBLY OR SUB-ASSEMBLIES, BRACKETS, RAILING AND MISCELLANEOUS STEEL PIECES SHALL BE HOT-DIP GALVANIZED AFTER FABRICATION.
6. STRUCTURAL BOLTS, NUTS & WASHERS SHALL BE HOT-DIP GALVANIZED.
7. EXPANSION ANCHORS TO BE HILTI KWIK-BOLT II OR APPROVED EQUAL. INSTALL EXPANSION ANCHORS PER MANUFACTURER'S WRITTEN INSTRUCTIONS.
8. EXPANSION ANCHOR BOLTS, NUTS & WASHERS SHALL BE STAINLESS STEEL.
9. REFER TO OTHER DETAILS FOR CONCRETE REINFORCING REQUIREMENTS.
10. COORDINATE REBAR PLACEMENT IN CONCRETE TO MISS ANCHOR BOLTS & INSERTS.
11. GALVANIZED AREAS THAT ARE DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED WITH A APPROVED COLD-GALVANIZING COMPOUND.
12. EPOXY SET ANCHORS SHALL UTILIZE HILTI HIT HY 150 CONSTRUCTION ADHESIVE AND HAS GALVANIZED OR STAINLESS STEEL THREADED RODS.
13. FIELD SPLICES SHALL BE MADE UTILIZING GALVANIZED STEEL PIPE SLEEVE INSERTS AND HOT-DIP GALVANIZED STEEL BOLTS, NUTS, AND WASHERS.
14. CROSS REFERENCE ASSOCIATED STANDARD DETAILS AS NECESSARY FOR STEEL FABRICATIONS.

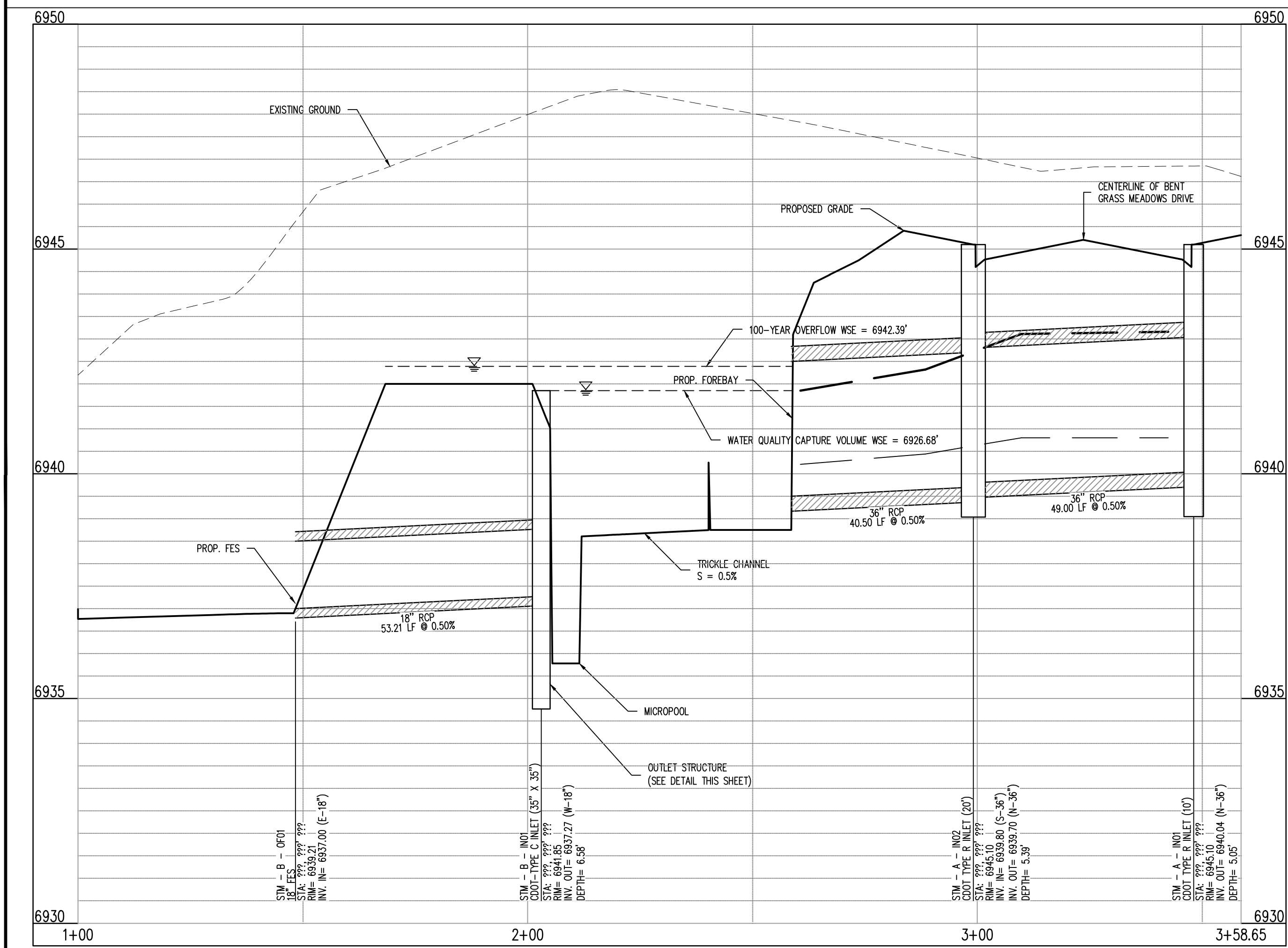


PIPE RAILING DETAIL
SCALE: NOT TO SCALE

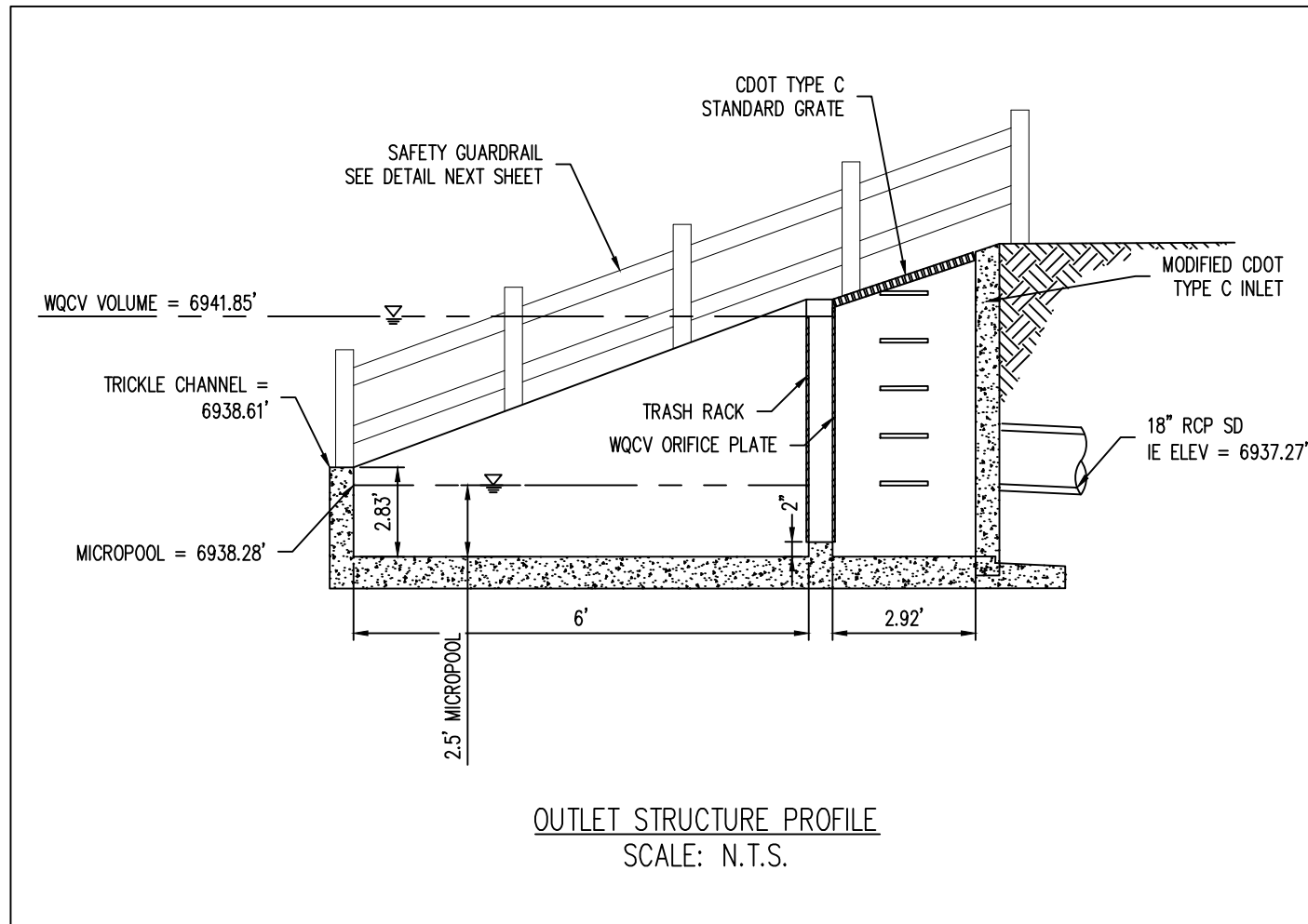
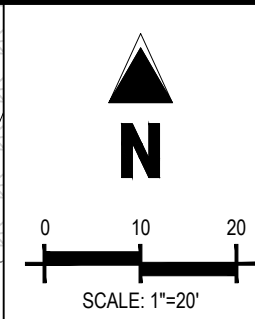




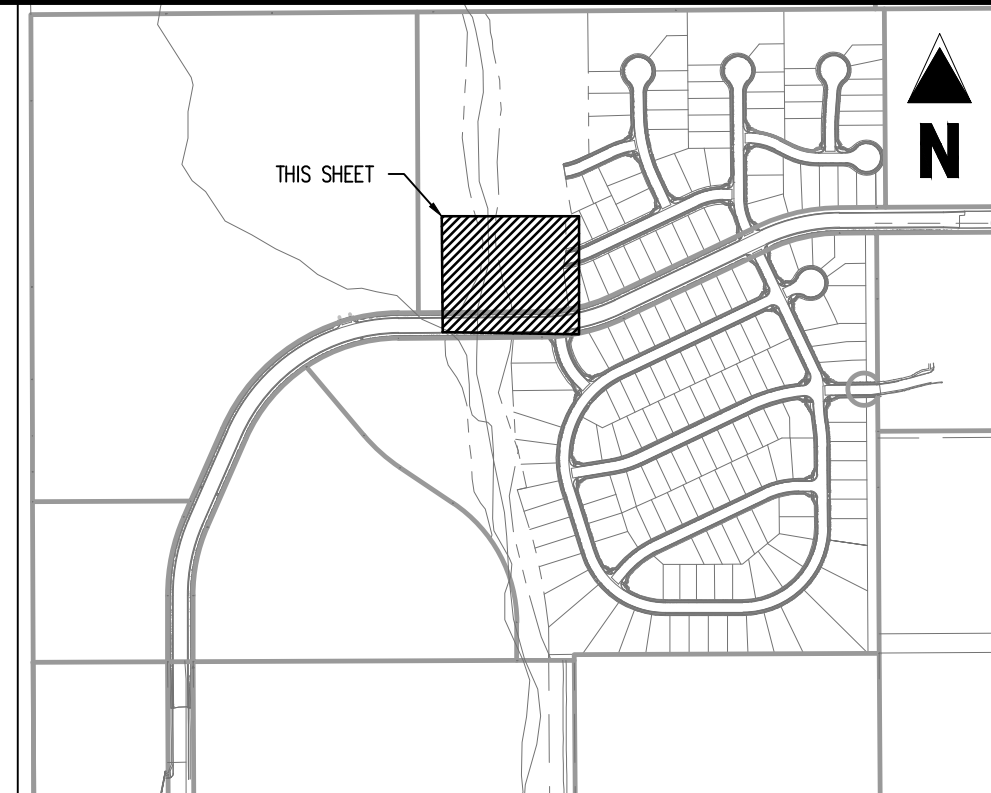
POND (NORTH) PLAN VIEW
SCALE: 1"=20'



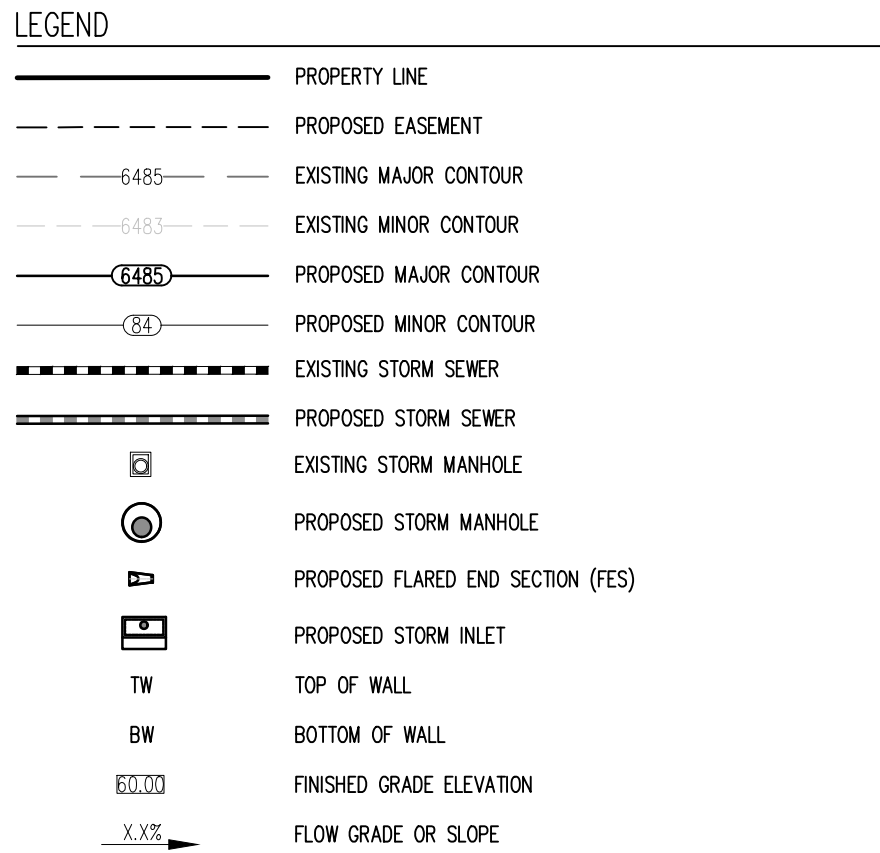
POND (NORTH) PROFILE
SCALE: H: 1"=20' V: 1"=2'



OUTLET STRUCTURE PROFILE
SCALE: N.T.S.



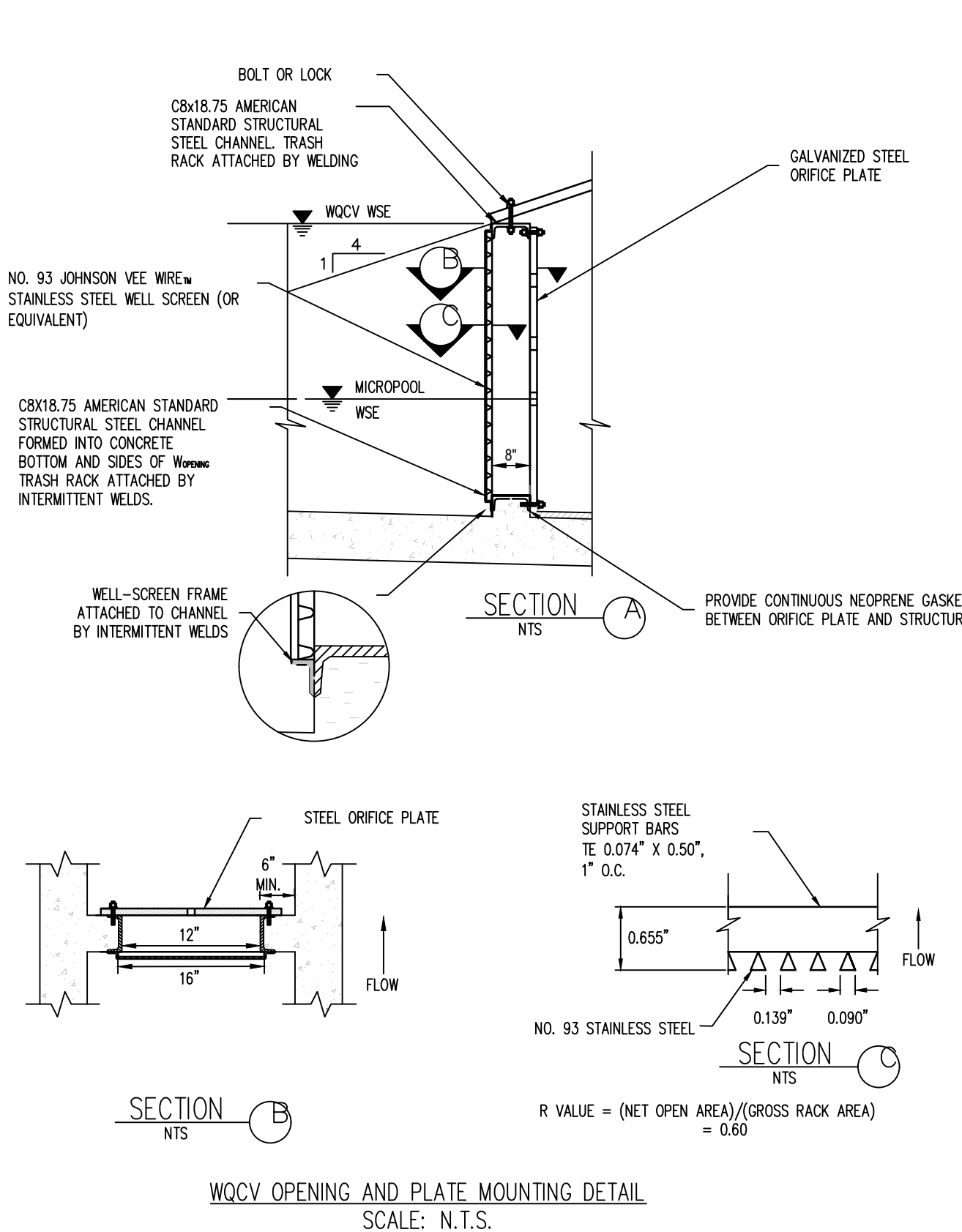
KEY MAP
SCALE: 1"=600'



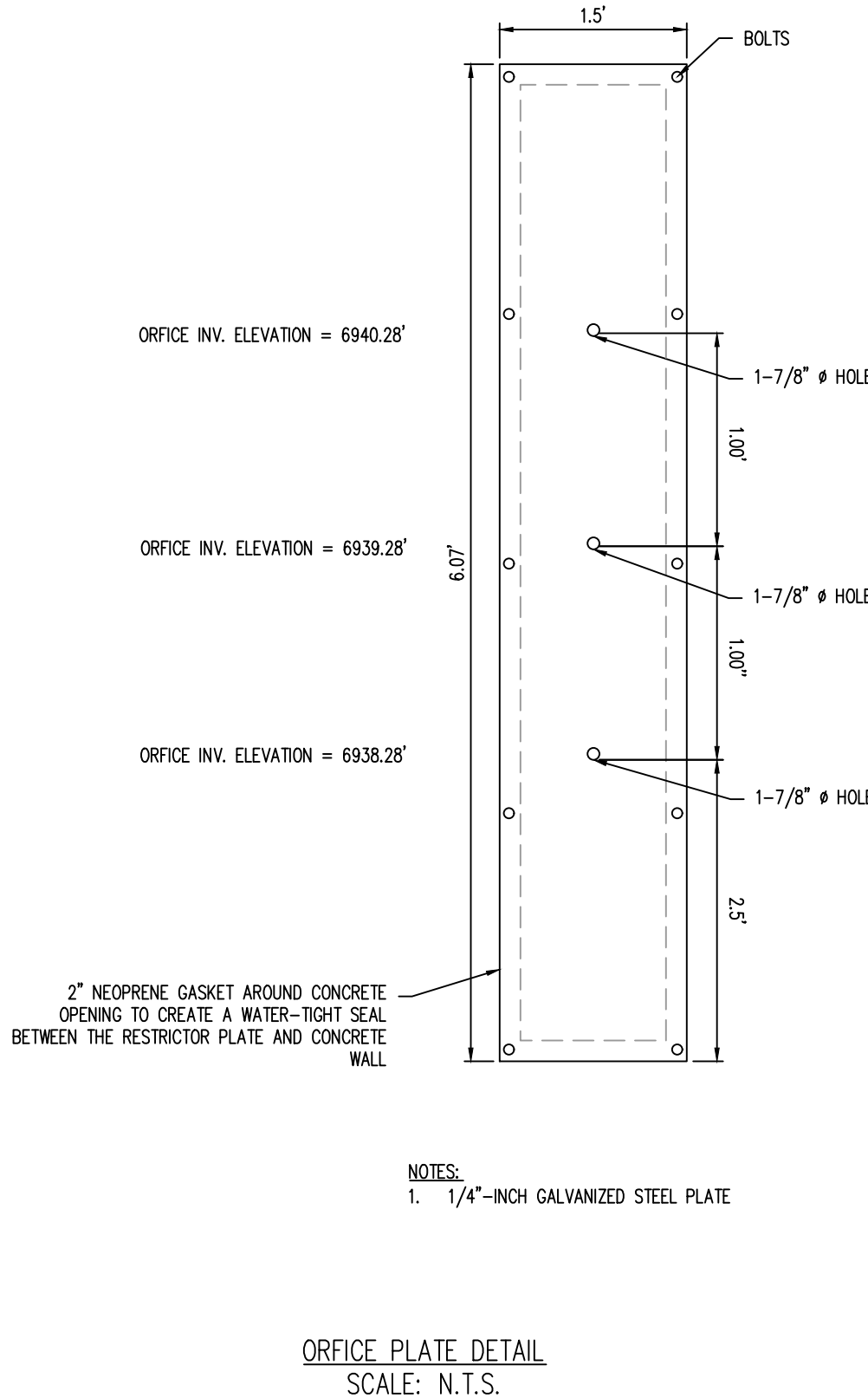
- NOTES
1. ADD 6900 TO ALL SPOT ELEVATIONS.
 2. ALL STORM PIPE SHALL BE RCP OR HDPE IN ACCORDANCE WITH COUNTY STANDARD SPECIFICATIONS, UNLESS OTHERWISE NOTED.

WARNING
THIS AREA IS A STORMWATER FACILITY
AND IS SUBJECT TO PERIODIC FLOODING

- DETENTION POND SIGNAGE
- NOTES:
- TWO SIGNS WITH THE ABOVE MESSAGE, EACH WITH A MINIMUM AREA OF 3 SQUARE FEET SHALL BE PROVIDED AROUND THE PERIMETER OF THE DETENTION POND, AS SHOWN ON THESE PLANS.
 - SIGNS SHALL BE DURABLE MATERIALS, SUCH AS METAL OR PLASTIC, USING RED LETTERING ON A WHITE BACKGROUND.



WQV OPENING AND PLATE MOUNTING DETAIL
SCALE: N.T.S.



- NOTES:
1. 1/4"-INCH GALVANIZED STEEL PLATE

ORIFICE PLATE DETAIL
SCALE: N.T.S.

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CHALLENGER
HOMES

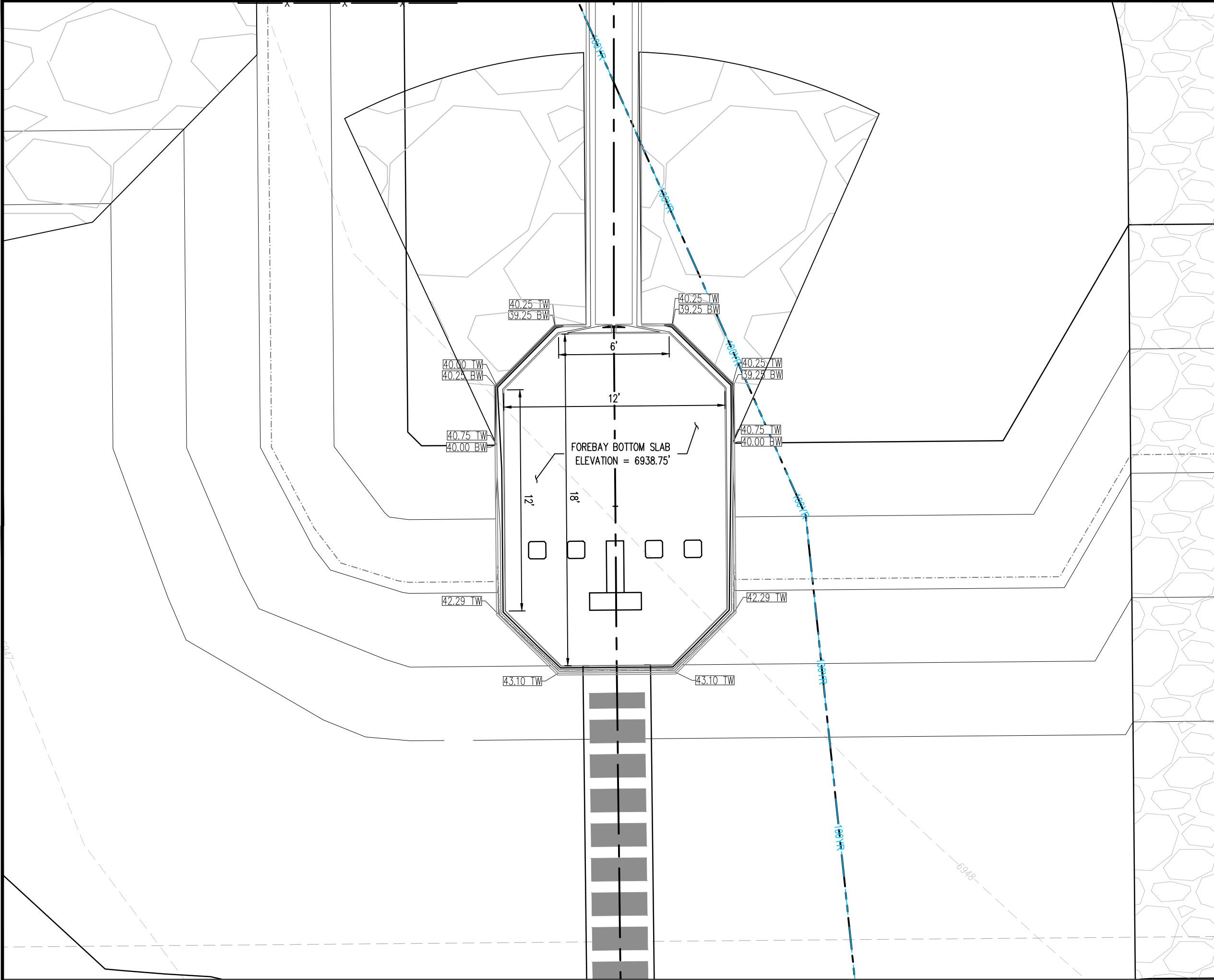
CONSTRUCTION DOCUMENTS
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FALCON, CO 80831 - EL PASO COUNTY

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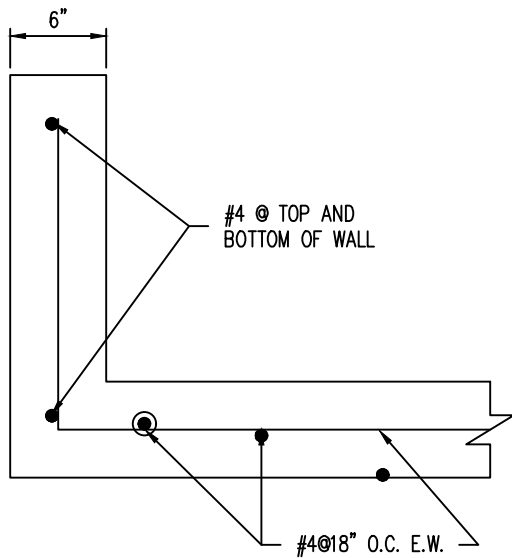
Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

POND PLAN (NORTH)

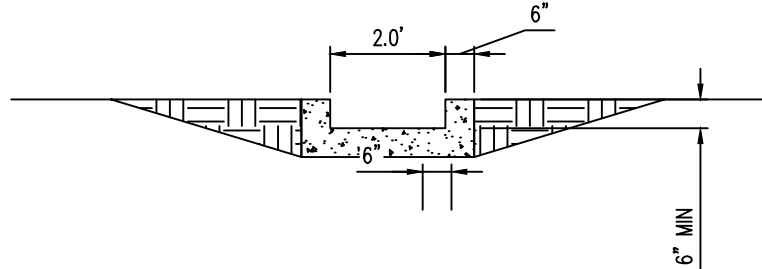
C5.01
Sheet 35 of 43



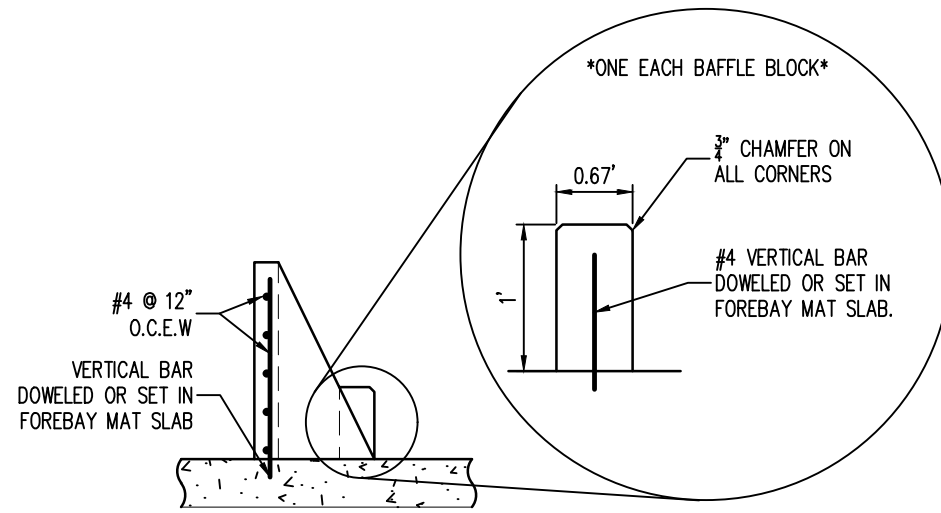
FOREBAY DETAIL
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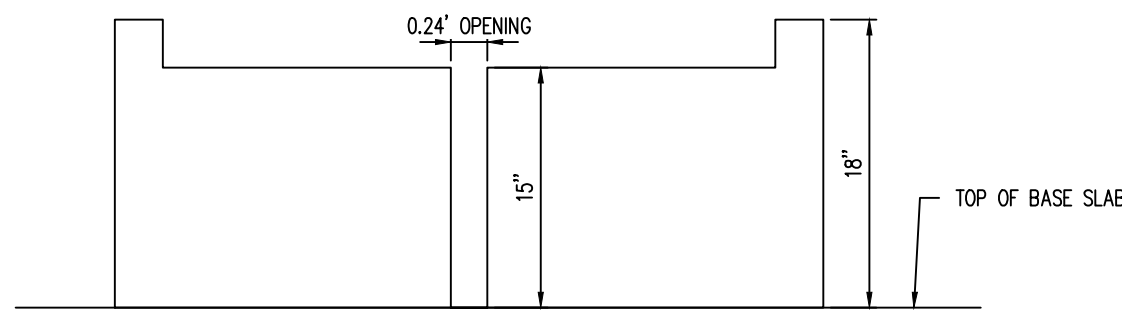
FOREBAY REINFORCING DETAIL
SCALE: N.T.S.



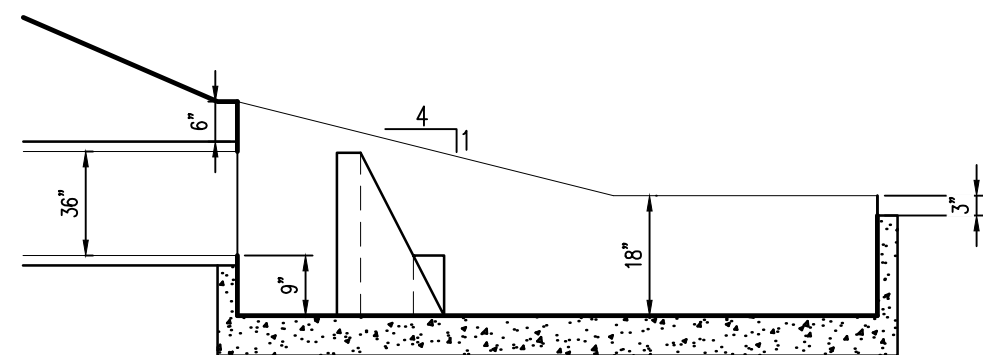
CONCRETE TRICKLE CHANNEL
SCALE: N.T.S.



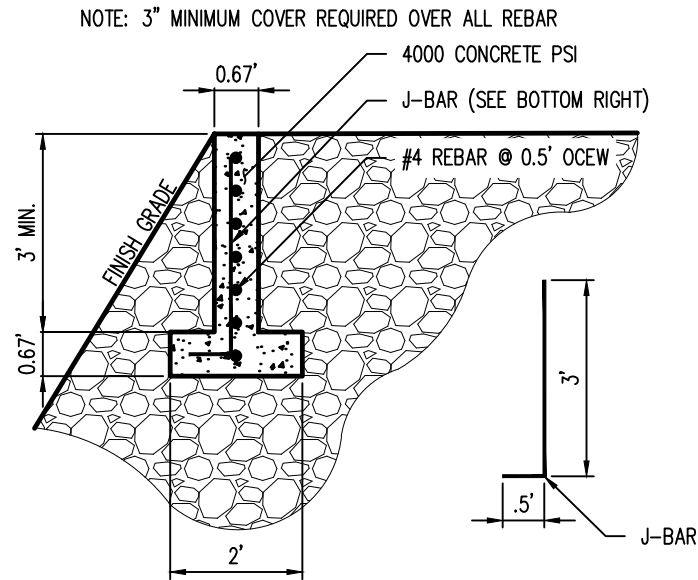
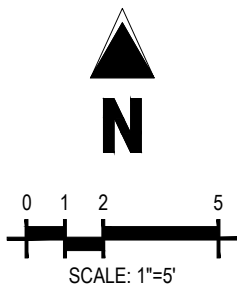
MODIFIED IMPACT STILLING BASIN - REINFORCING DETAIL
SCALE: N.T.S.



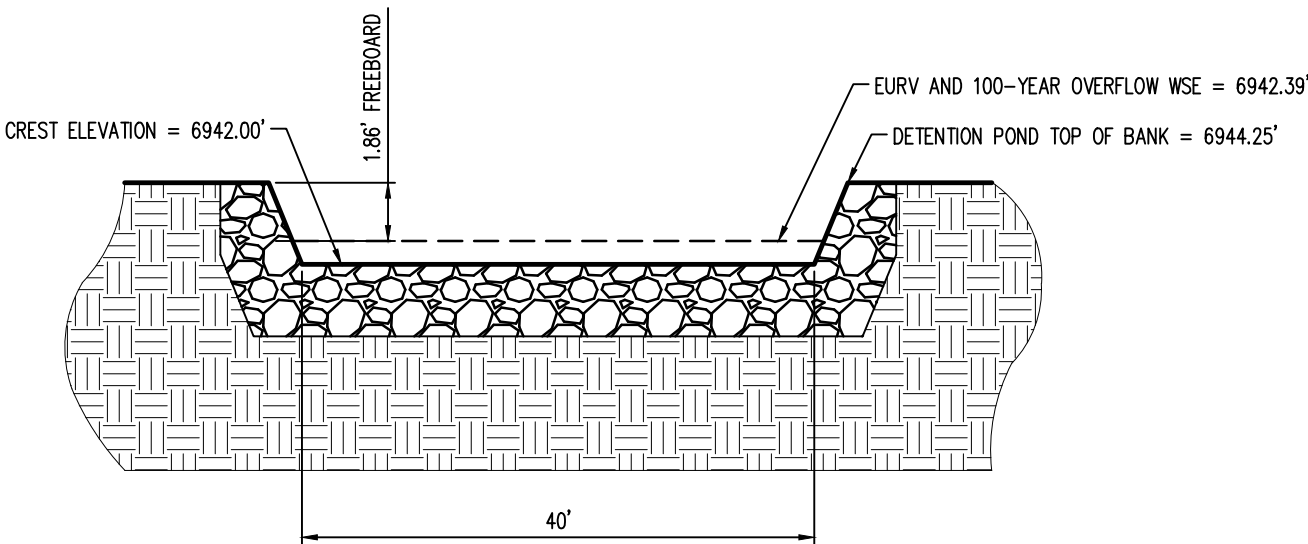
FOREBAY SLOT DETAIL
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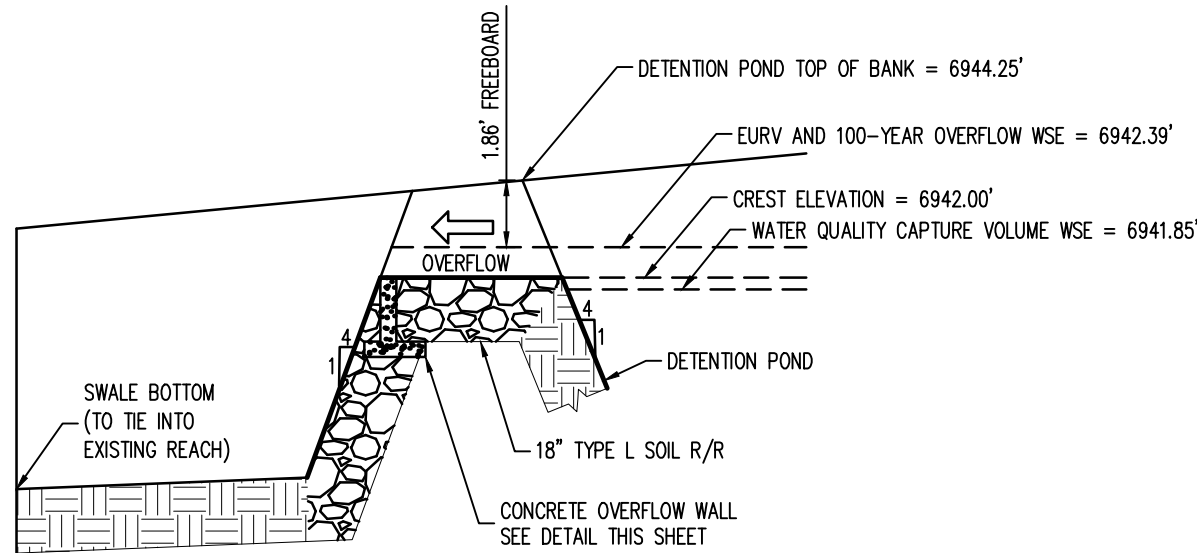
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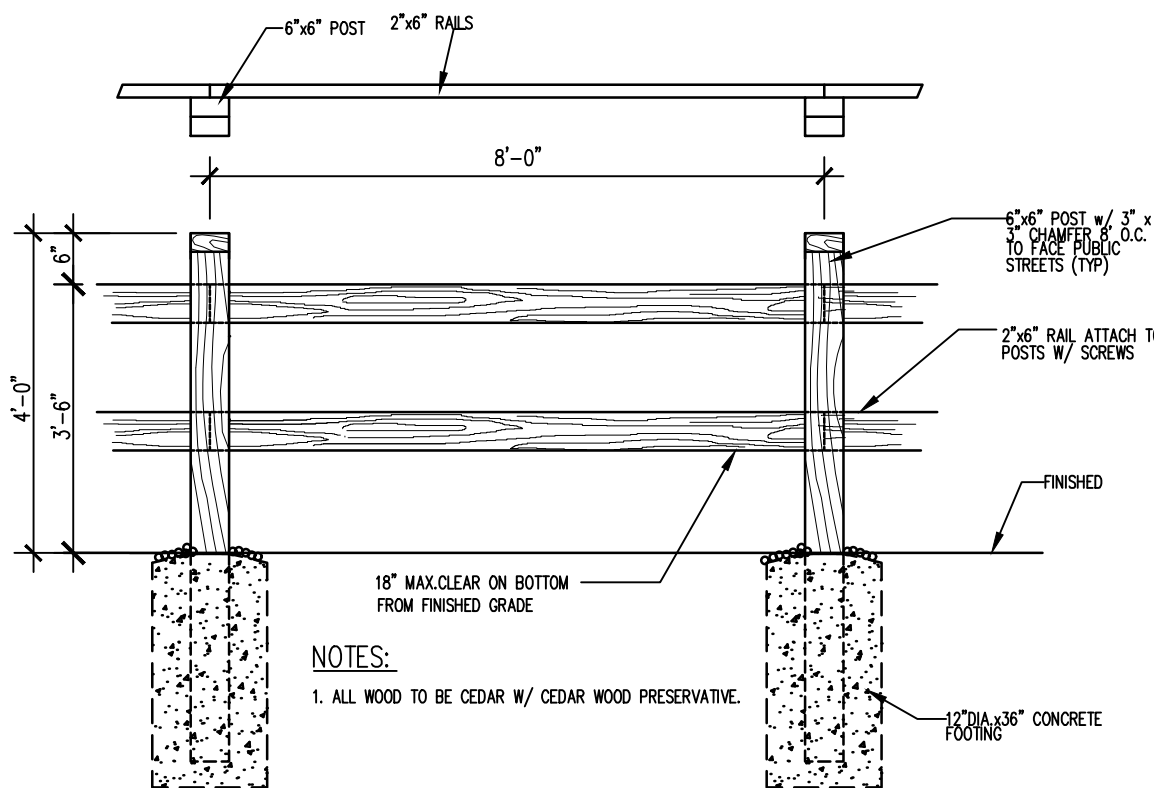
CONCRETE OVERFLOW WALL
SCALE: N.T.S.



EURV AND 100-YEAR OVERFLOW - CROSS SECTION
SCALE: N.T.S.



EURV AND 100-YEAR OVERFLOW
SCALE: N.T.S.



SAFETY GUARDRAIL
SCALE: N.T.S.

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**CHALLENGER
HOMES**

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR

CHALLENGER COMMUNITIES, LLC

BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

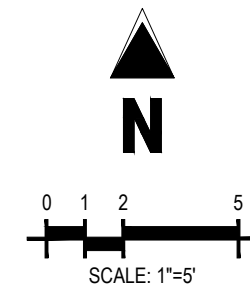
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

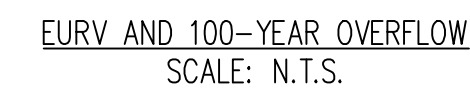
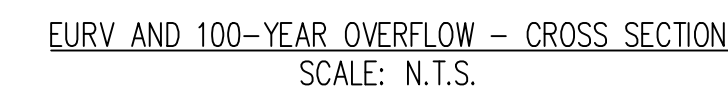
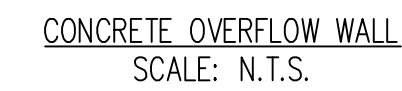
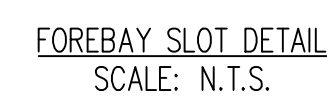
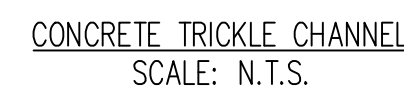
POND DETAILS (NORTH)

C5.02

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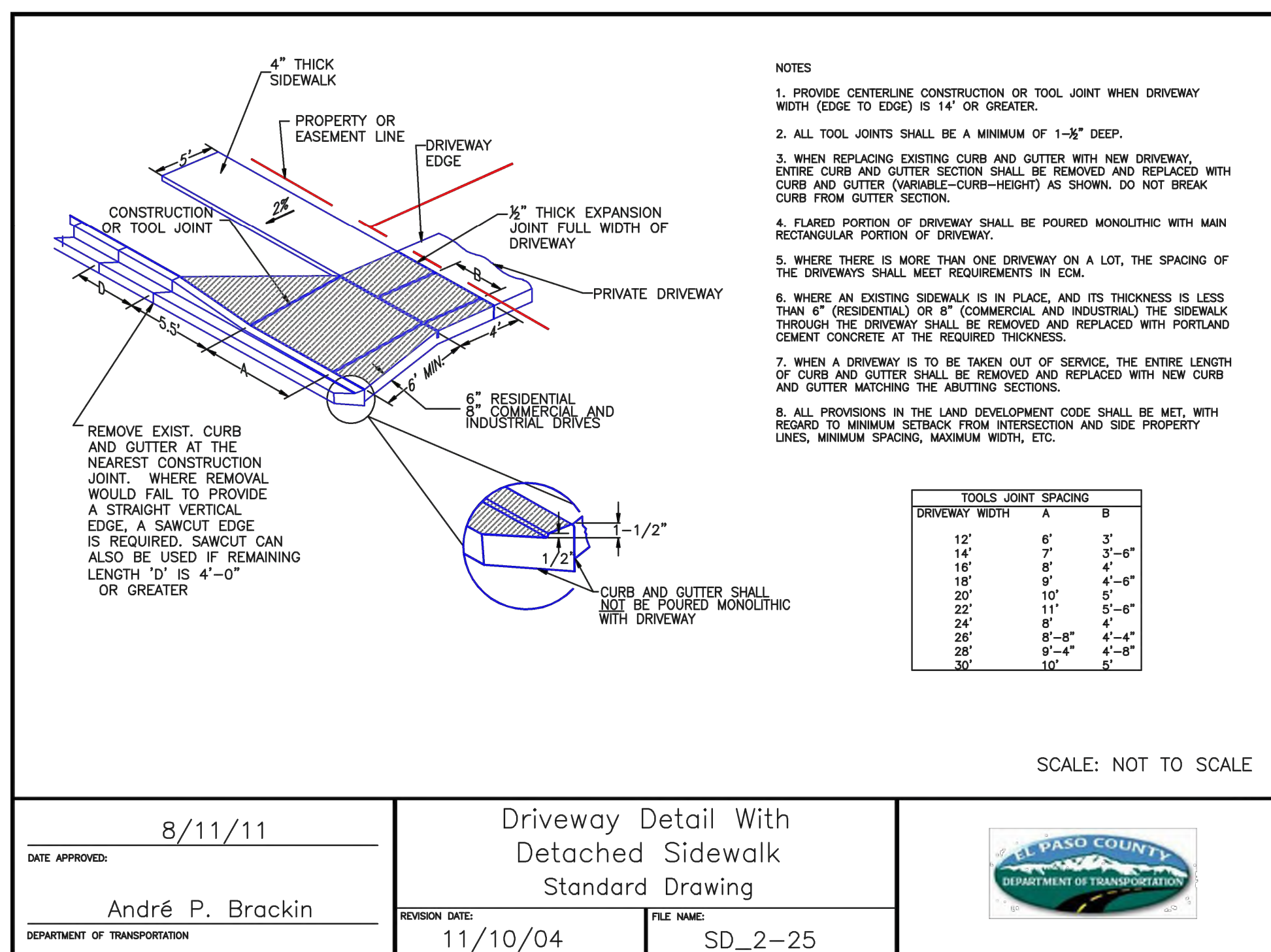
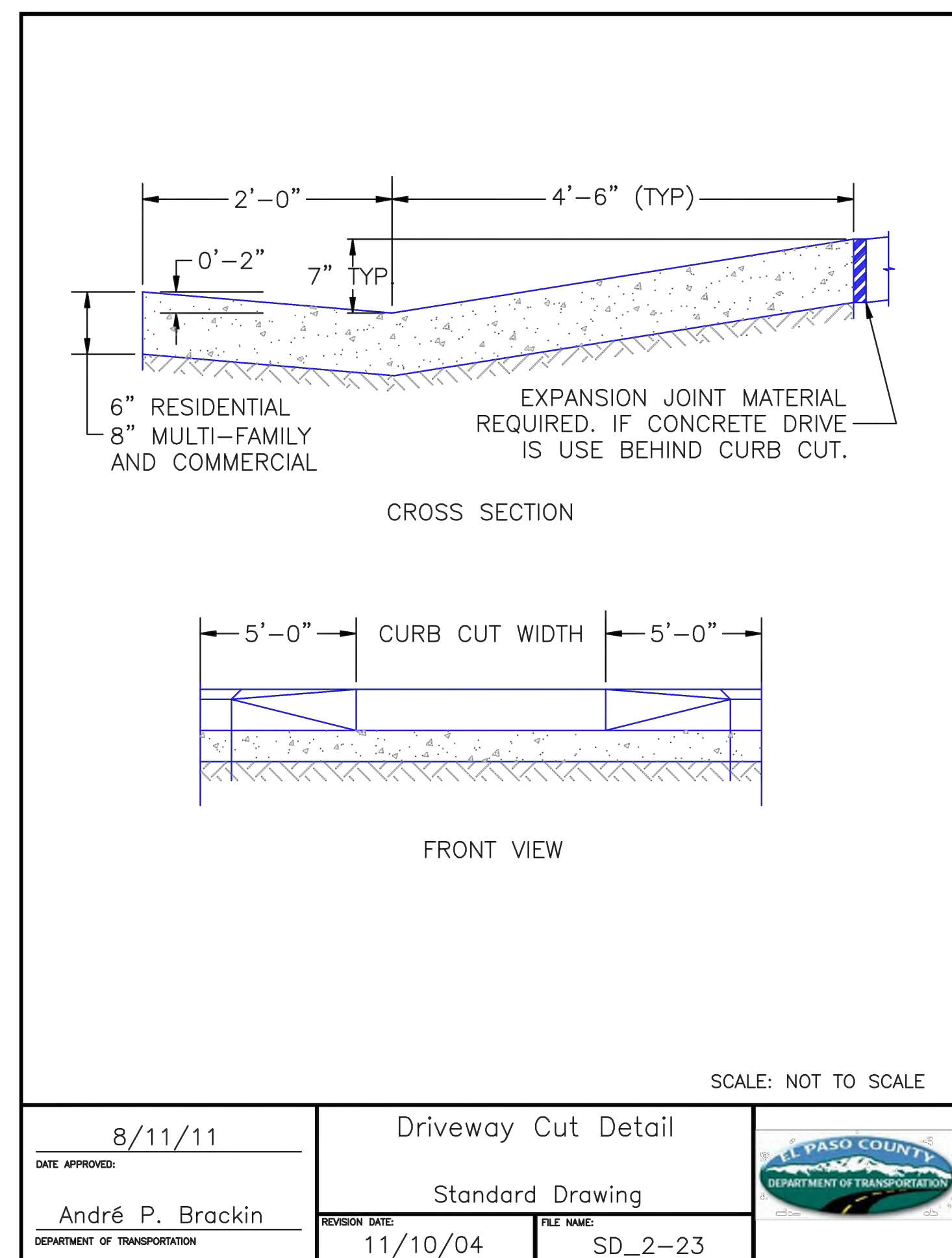
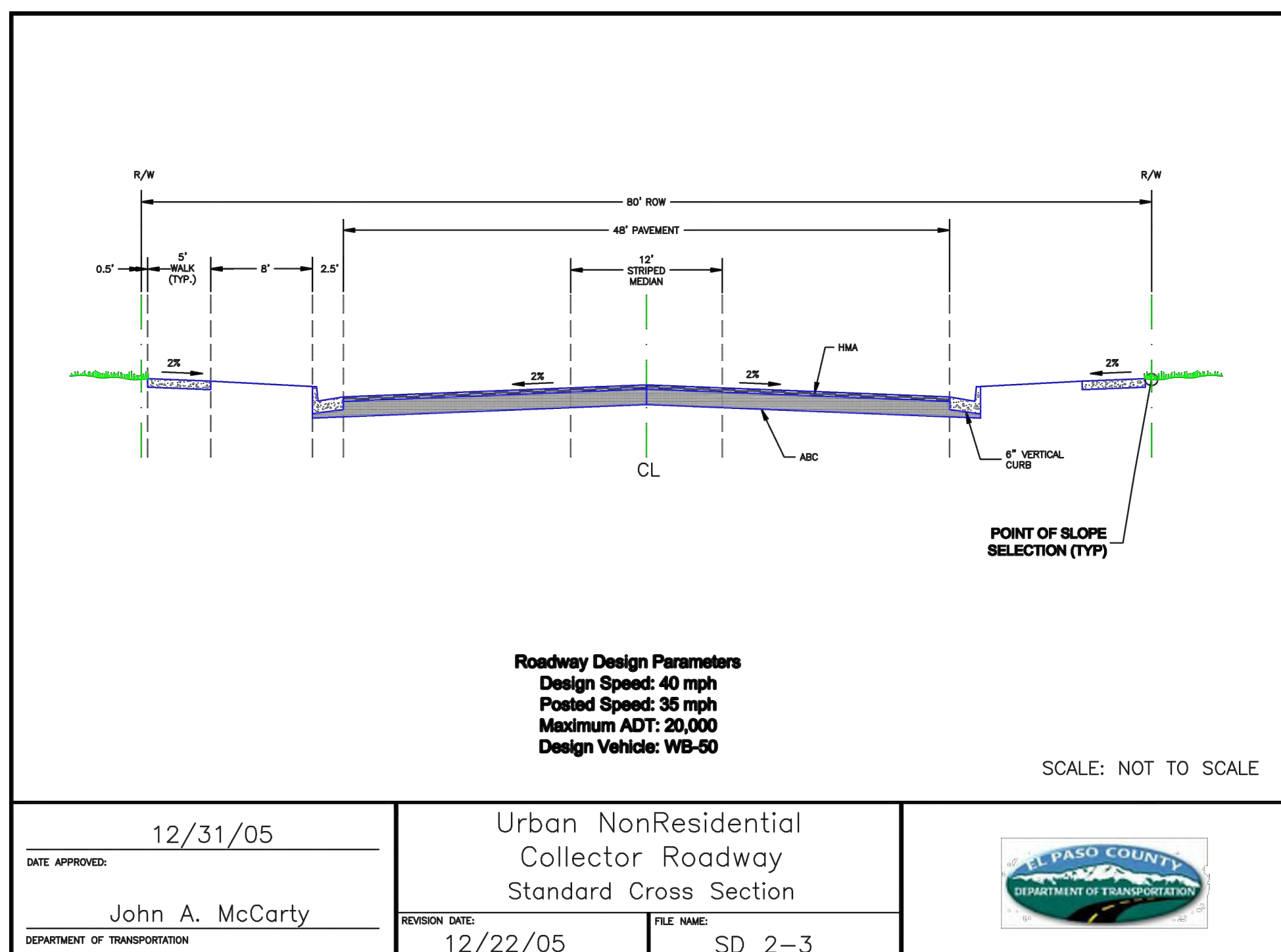
FOREBAY DETAIL
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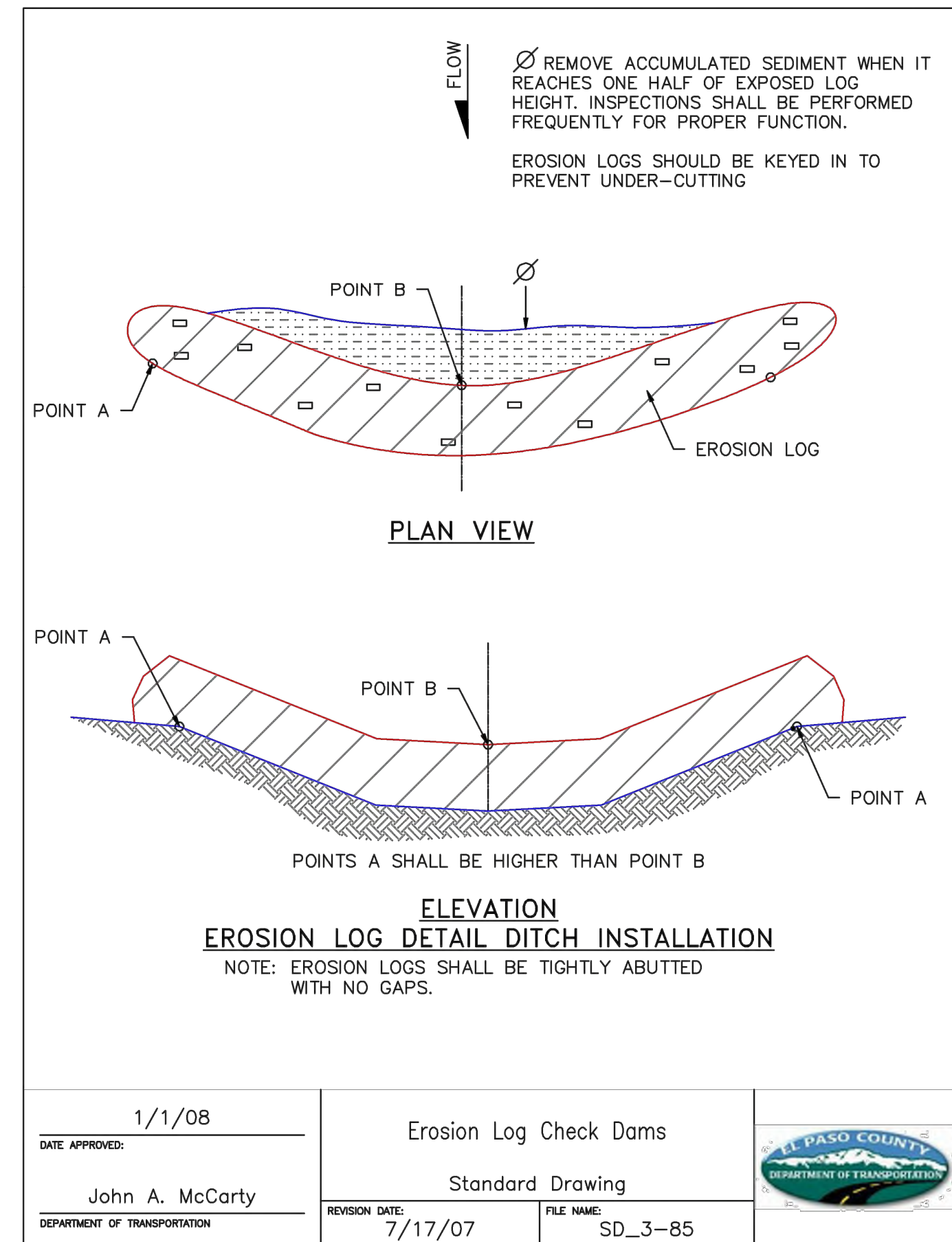
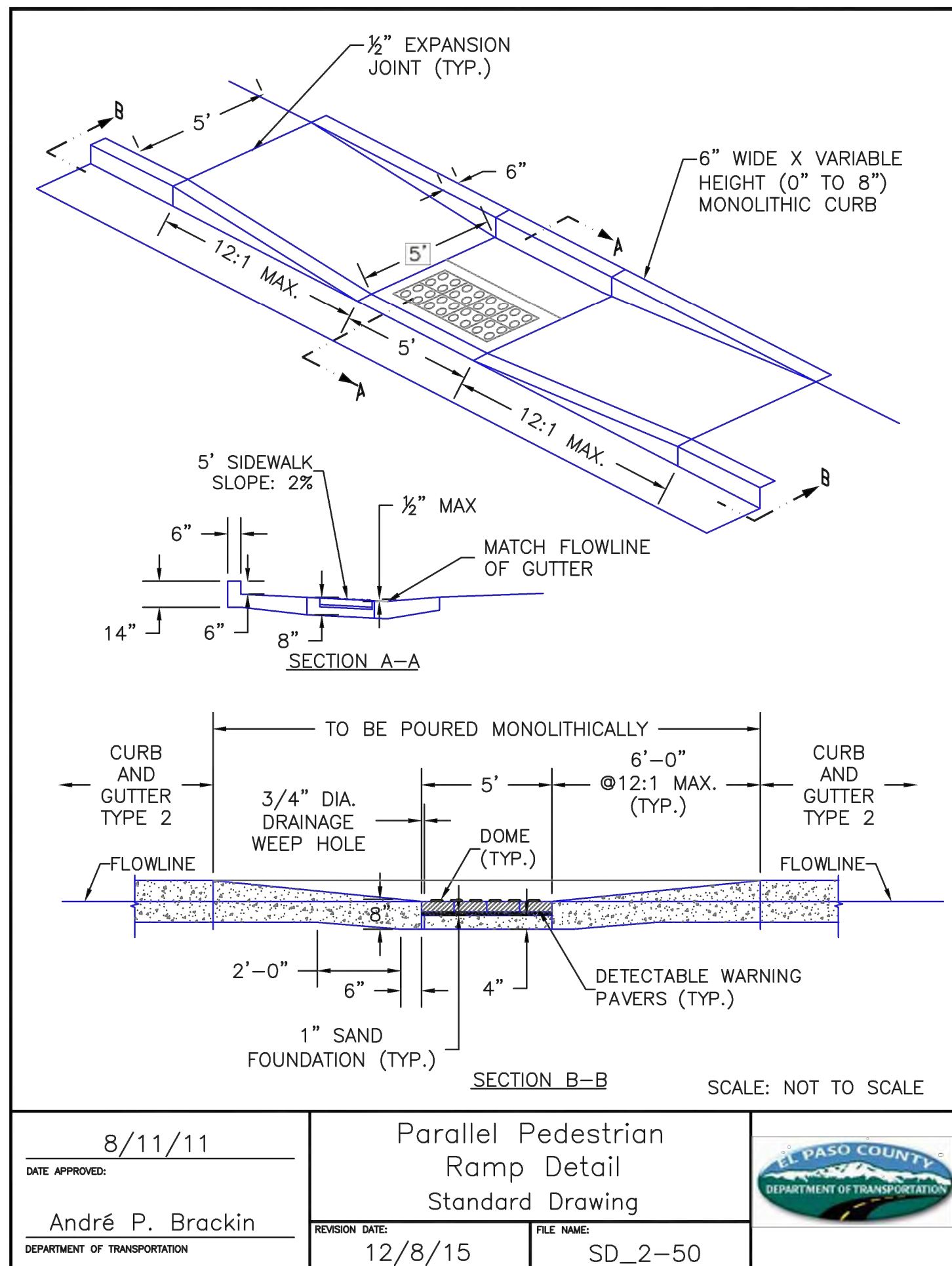
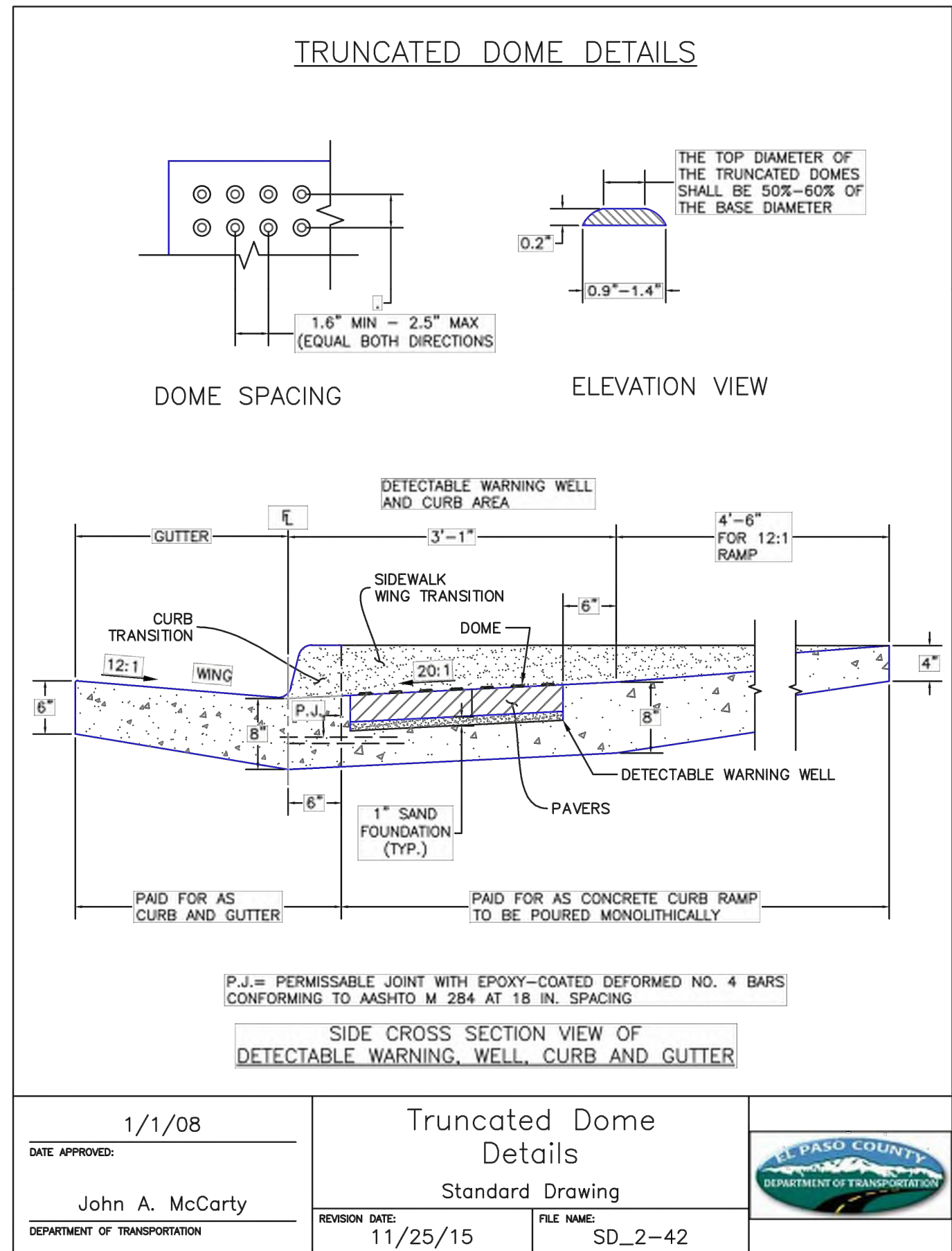
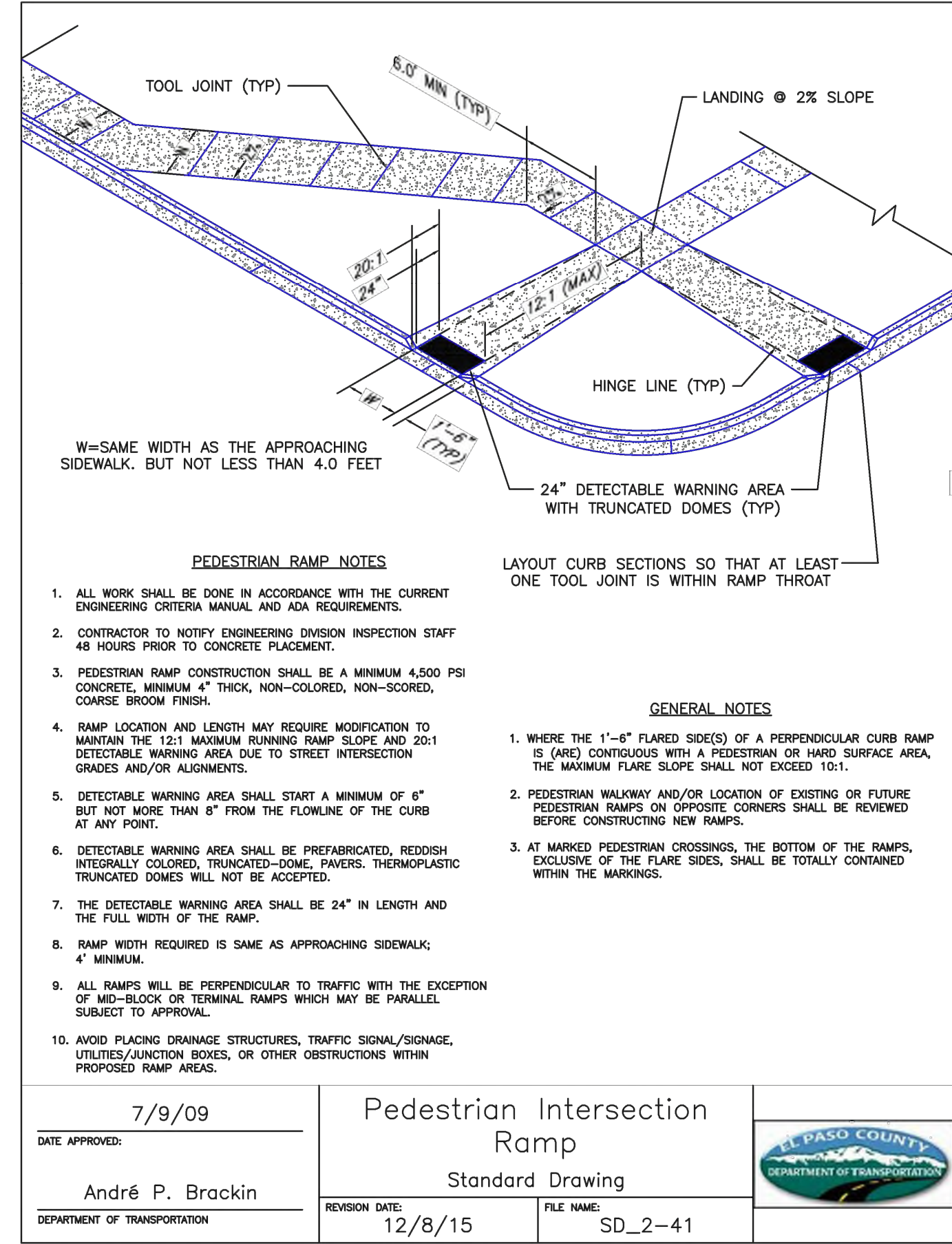
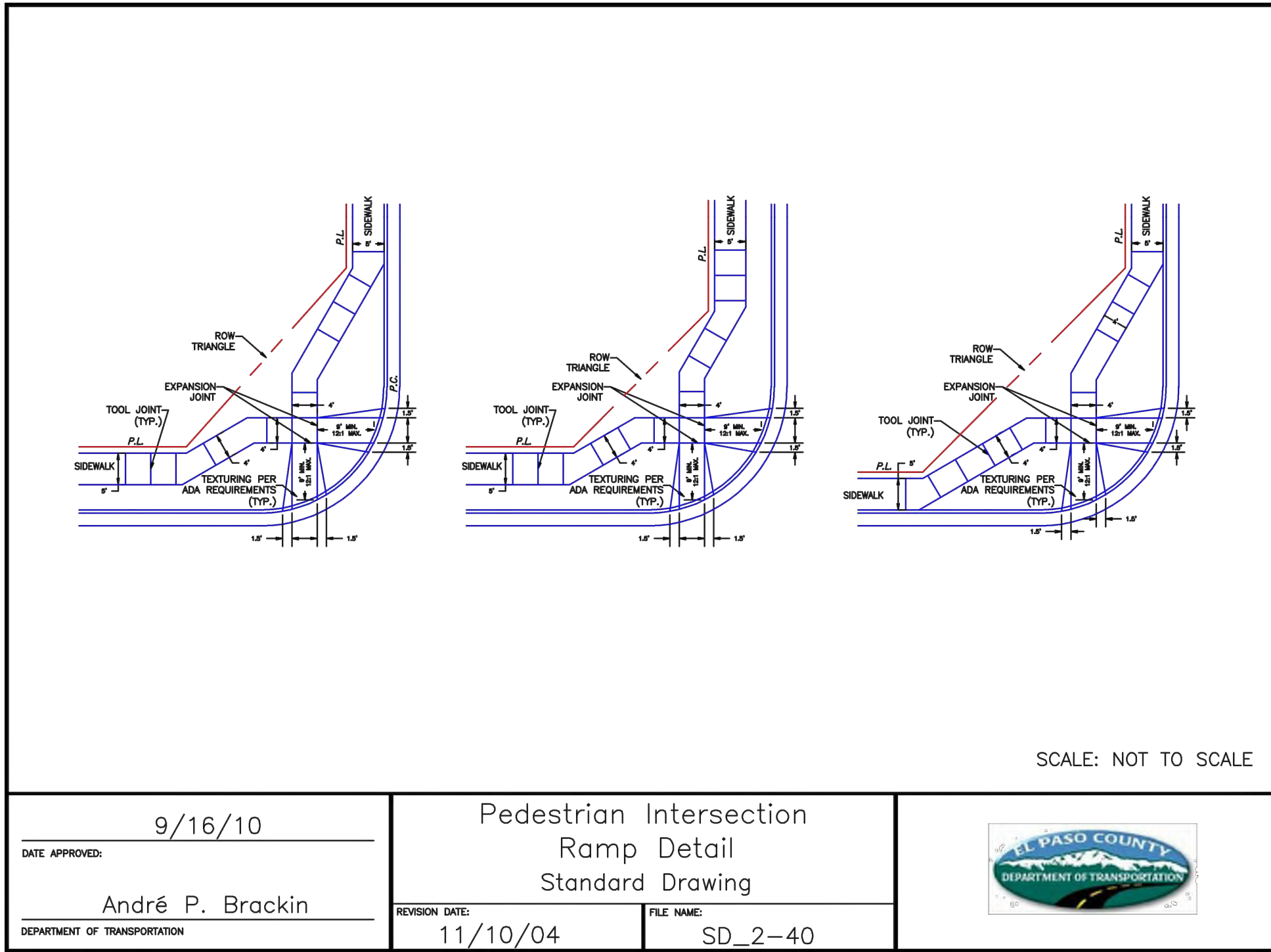
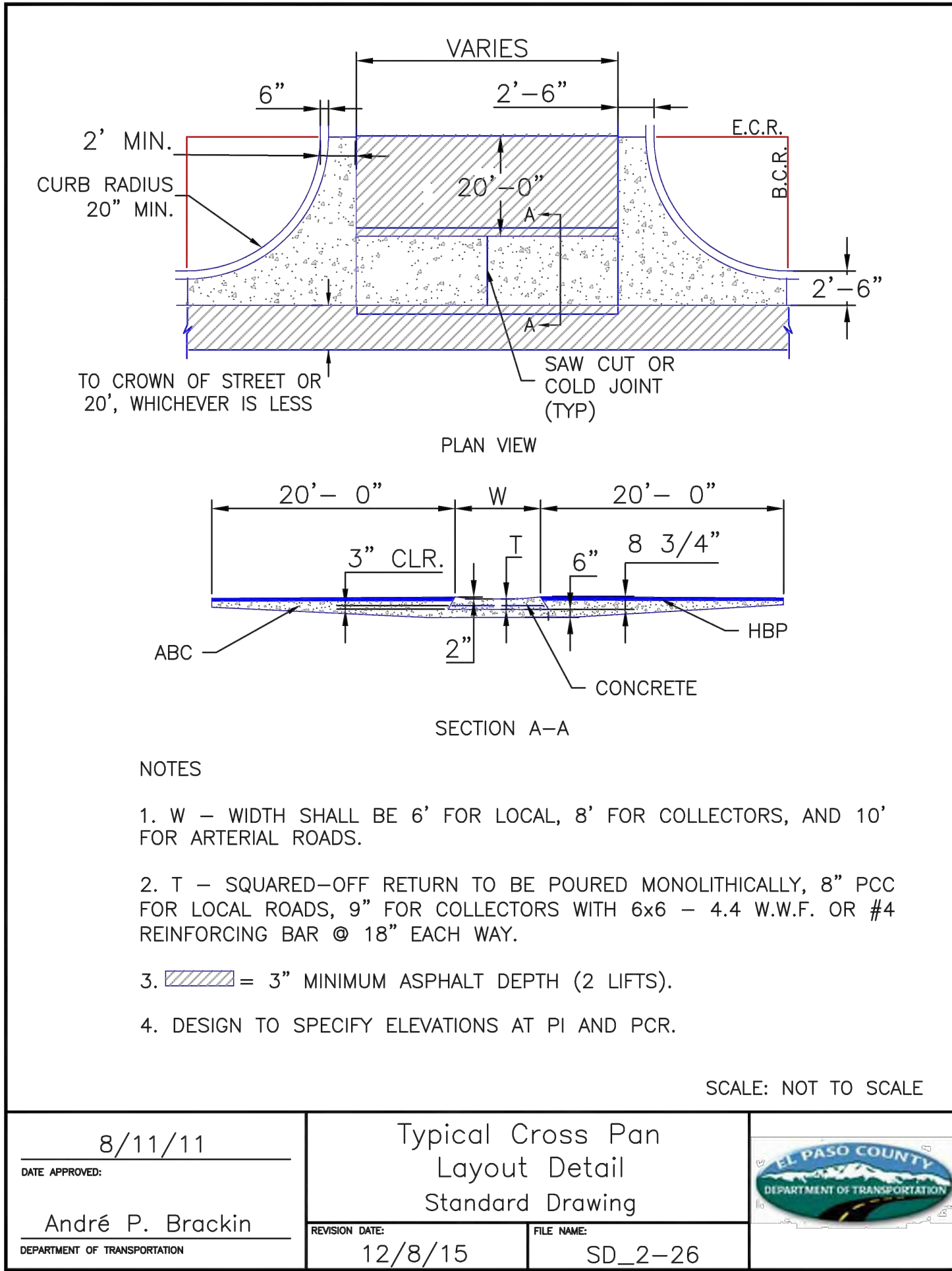


CHALLENGER HOMES

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POND DETAILS (SOUTH)





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CHALLENGER
HOMES

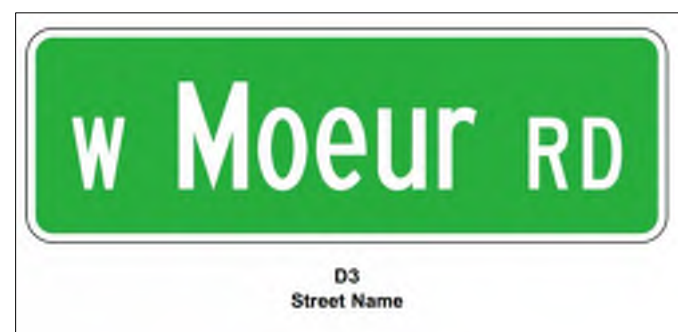
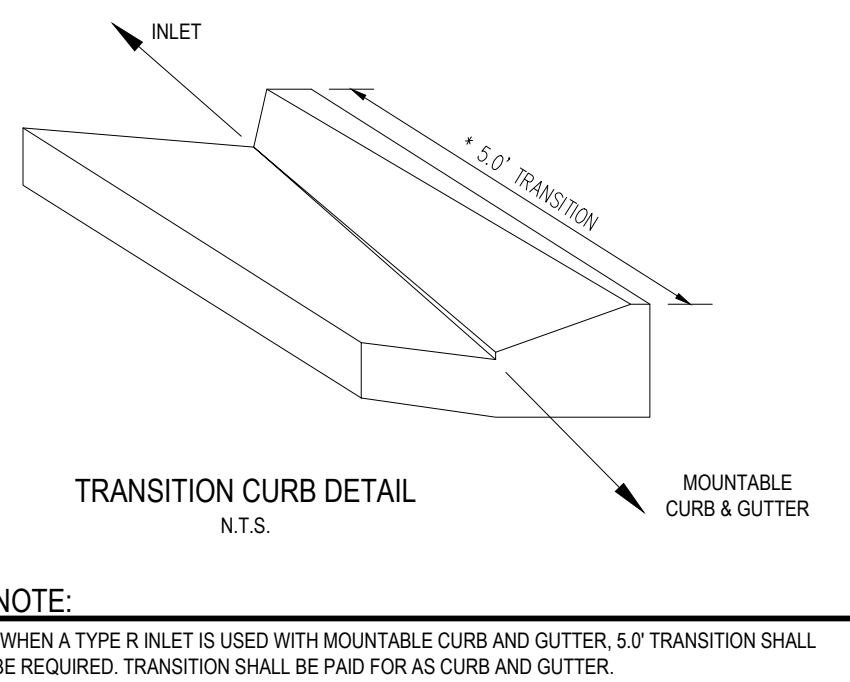
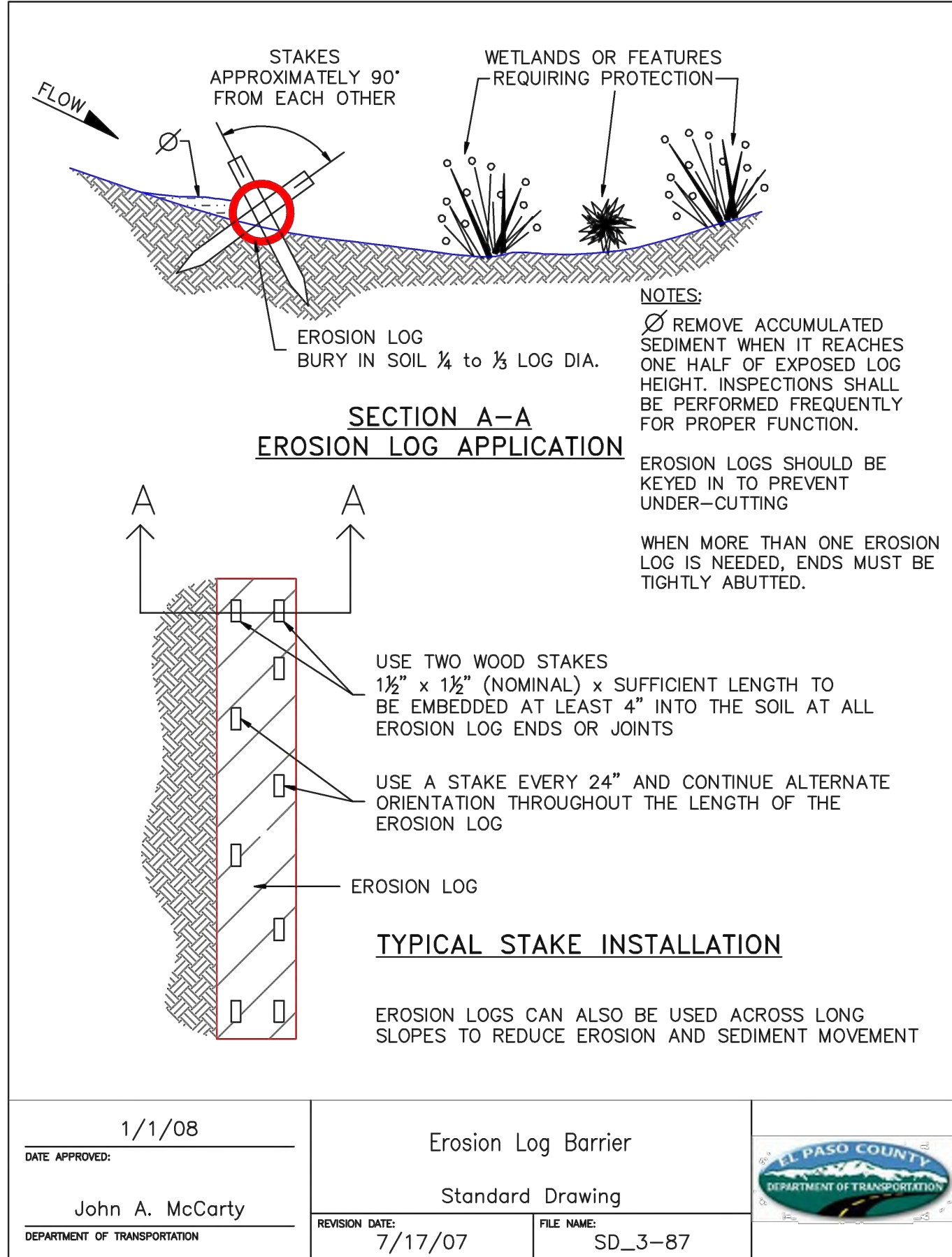
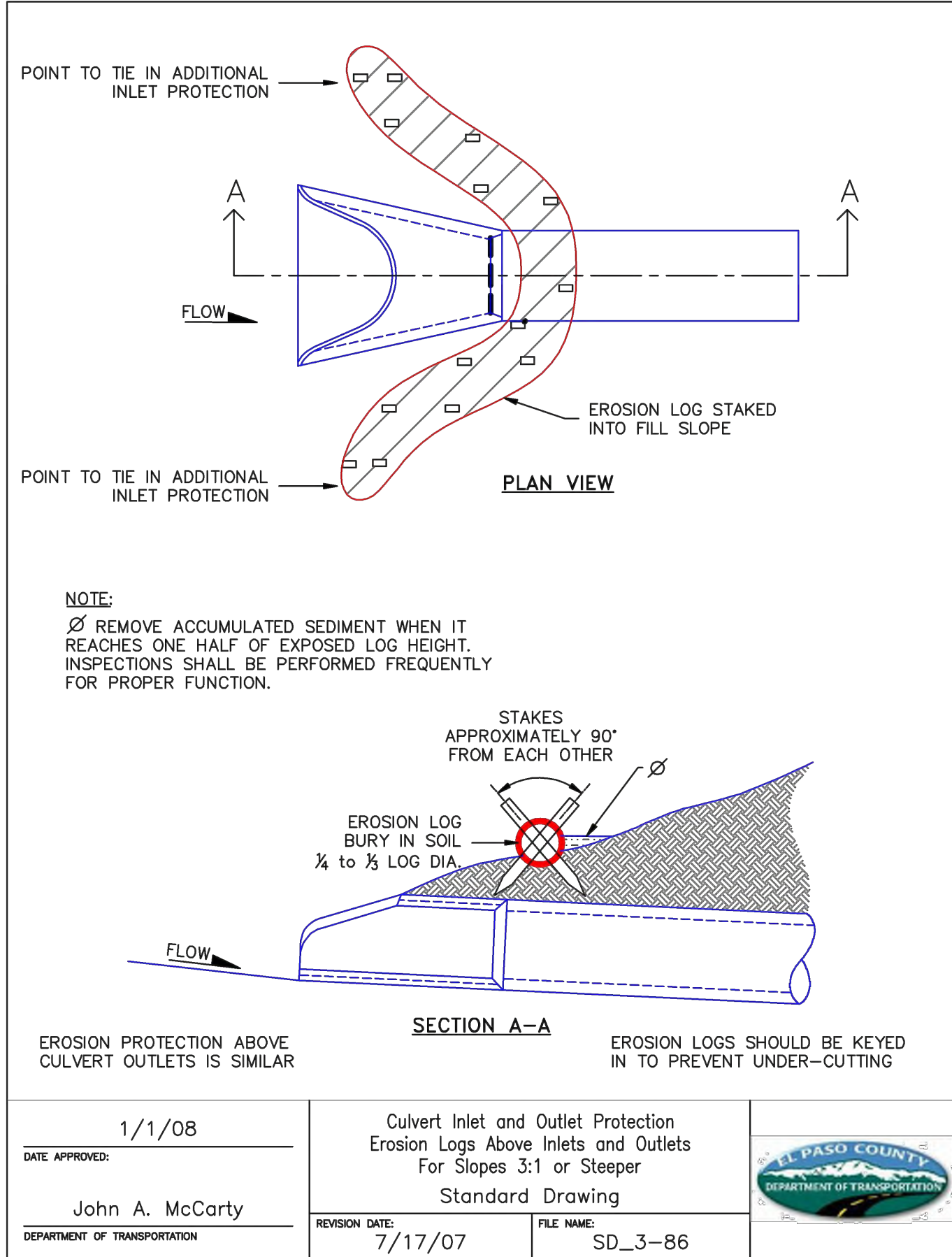
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC

BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

#	Date	Issue / Description	Init.
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

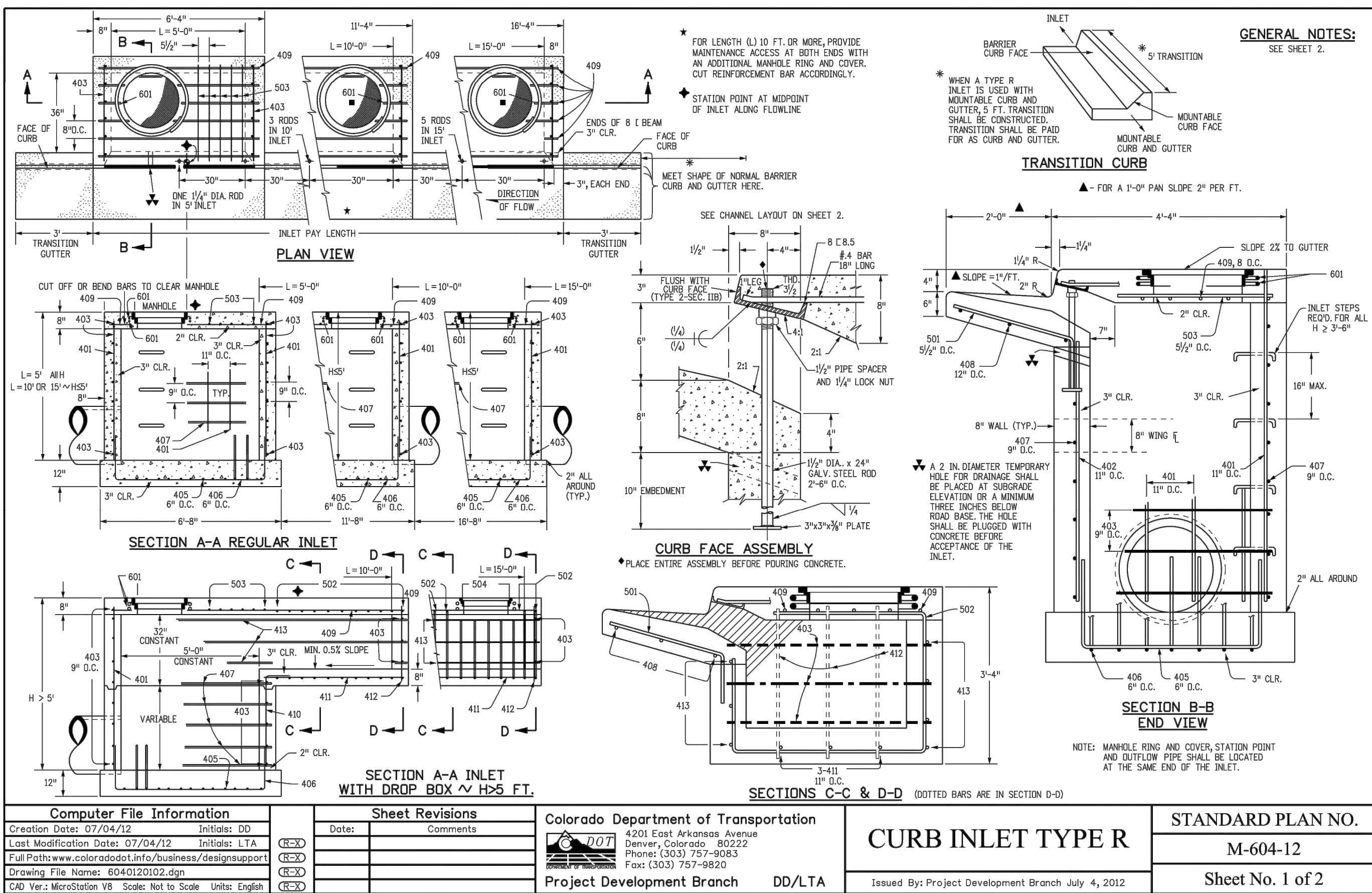
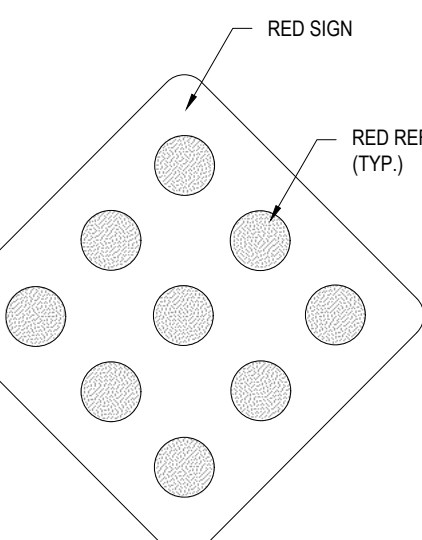
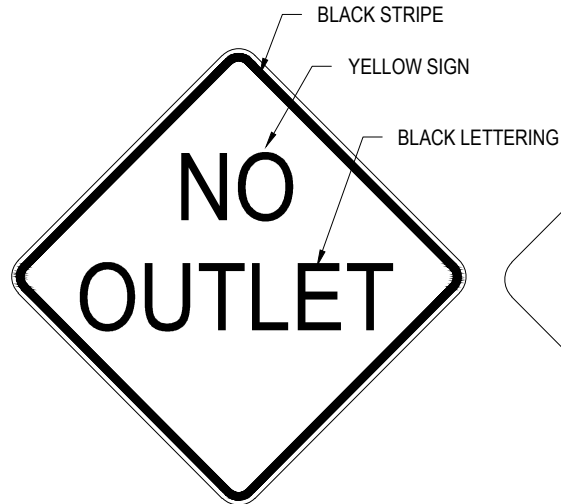
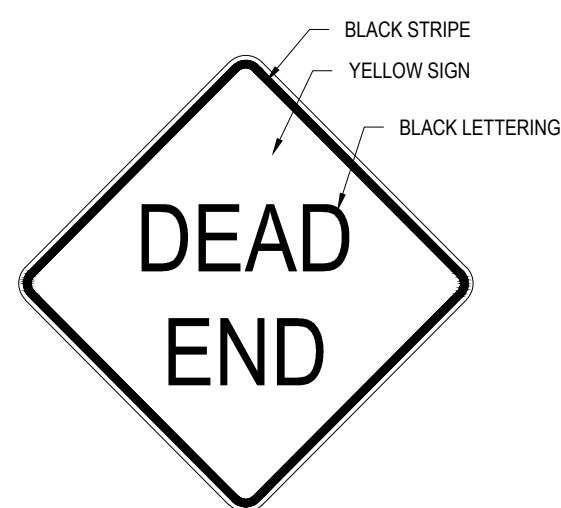
SITE DETAILS



NOTE:
 ALL INTERNAL SIGNS SHALL BE 4" FONT LETTER SIZE

NOTE TO CONTRACTOR:
 SIGNS AND POLES SHALL BE PER CDOT STANDARDS S-614-2, S-614-3 AND S-614-3, LATEST REVISION

ALL SIGNAGE INSTALLATION IS TO BE IN COMPLIANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.



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CHALLENGER HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

#	Date	Issue / Description	Init.
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Project No: CLH000014.20

Drawn By: JDP

Checked By: RGD

Date: 2/10/2020

SITE DETAILS

C6.03

Sheet 42 of 43



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CHALLENGER HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

#	Date	Issue / Description	Init.
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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

SITE DETAILS

C6.04

Sheet 43 of 43

MARK	BAR # OR SIZE	O.C. SPACING	TYPE	ALL INLETS				INLETS H ≤ 5 FT.				INLETS H > 5 FT.			
				L = 5 FT.		L = 10 FT.		L = 15 FT.		L = 10 FT.		L = 15 FT.		L = 15 FT.	
				NO. REQ'D.	LENGTH	NO. REQ'D.	LENGTH	NO. REQ'D.	LENGTH	NO. REQ'D.	LENGTH	NO. REQ'D.	LENGTH	NO. REQ'D.	LENGTH
401	4	11"	II	15	*	21	*	26	*	11	*	11	*		
402	4	11"	II	7	*	13	*	18	*	7	*	7	*		
403	4	9"	II	*	4'-0"	*	4'-0"	*	4'-0"	*	4'-0"	*	4'-0"	*	4'-0"
405	4	6"	VI	11	6'-10"	21	6'-10"	31	6'-10"	11	6'-10"	11	6'-10"		
406	4	6"	VIII	7	8'-10"	7	13'-10"	7	18'-10"	7	8'-10"	7	8'-10"		
407	4	6"	II	*	10'-10"	*	15'-10"	*	19'-10"	*	5'-10"	*	5'-10"		
408	4	12"	II	3	6'-10"	3	11'-10"	3	16'-10"	3	11'-10"	3	16'-10"		
409	4	8"	II	6	5'-10"	6	10'-10"	6	15'-10"	6	5'-10"	6	15'-10"		
410	4	11"	VII												
411	4	11"	II							3	5'-2"	3	3	3	10'-2"
412	4	11"	II							3	2'-9"	3	2'-9"		
413	4	8"	II							7	10'-10"	7	15'-10"		
501	5	5/8"	IV	11	3'-4"	22	3'-4"	33	3'-4"	22	3'-4"	33	3'-4"		
502	5	5/8"	III												
503	5	5/8"	II	5	3'-6"	16	3'-6"	27	3'-6"	6	3'-6"	6	3'-6"	17	11'-5"
504	5	5/8"	IX											8	8'-4"
601	6	2 1/4"	V	2	8'-10"	2	8'-10"	2	8'-10"	2	8'-10"	4	8'-10"		
REG. 5				1	5'-10"	1	10'-10"	1	15'-10"	1	10'-10"	1	15'-10"		
				2 BARS, 1 ROD		4 BARS, 3 RODS		8 BARS, 5 RODS		4 BARS, 3 RODS		8 BARS, 5 RODS			

* VARIABLE REFER TO TABLE TWO.
■ INCLUDE #4, 18 IN BARS (SEE CHANNEL LAYOUT).

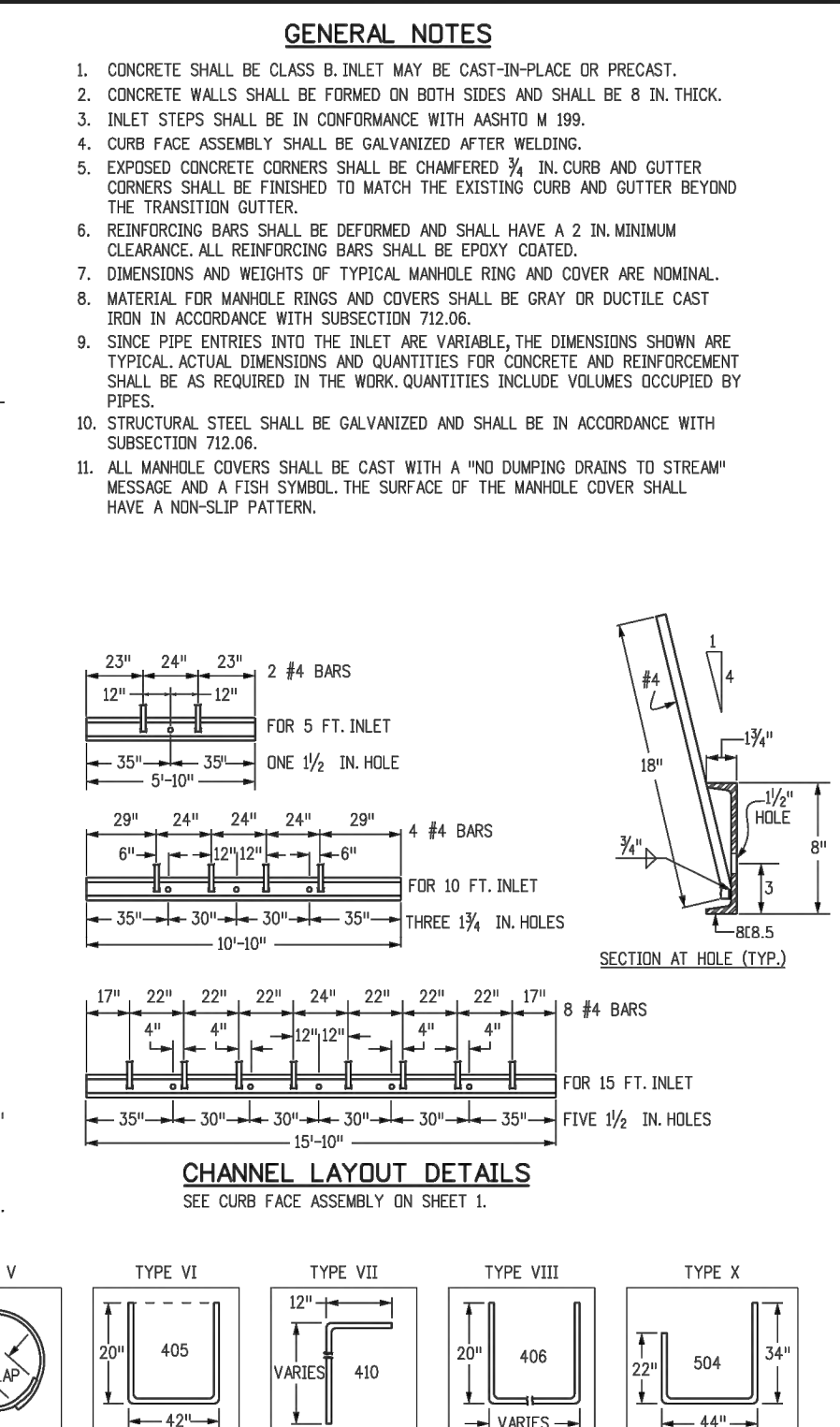
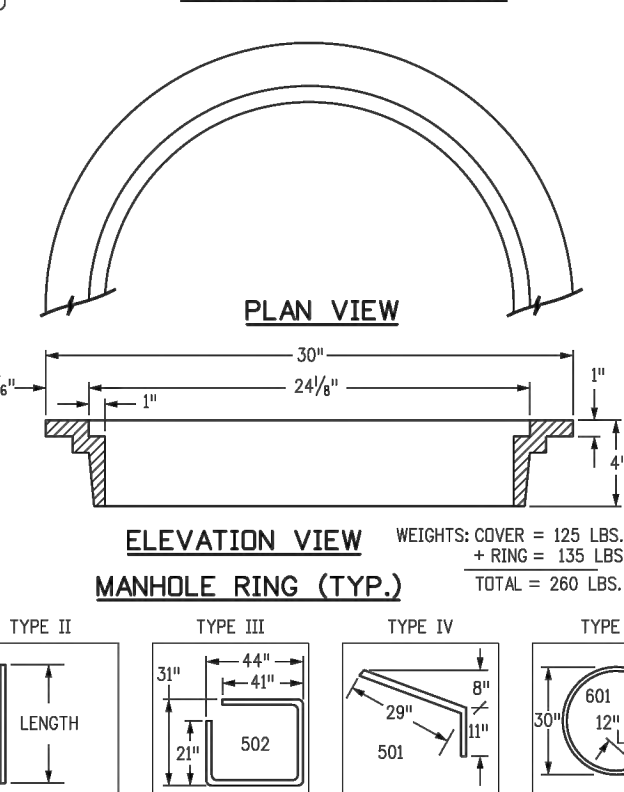
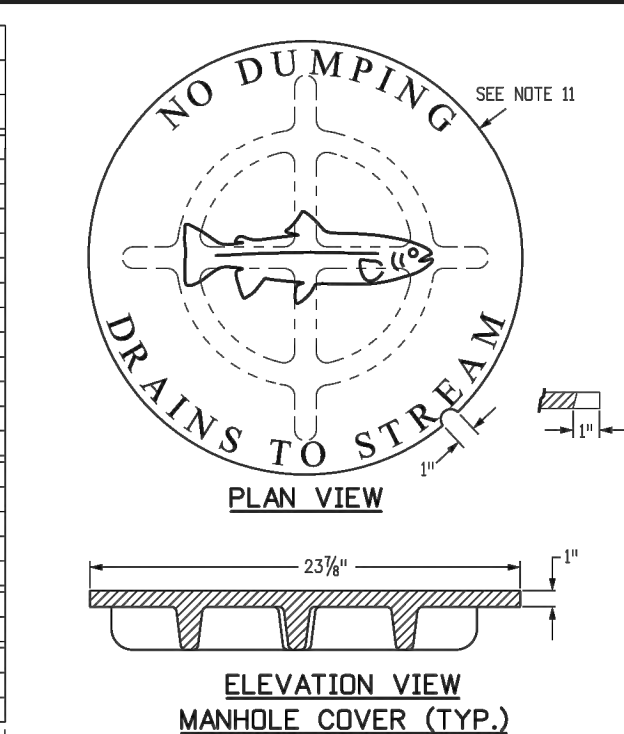
TABLE ONE ~ BAR LIST FOR CURB INLETS, TYPE "R"

H"	LENGTH	NO. REQ'D.		L = 5 FT.	NO. REQ'D.		L = 10 FT.	NO. REQ'D.		L = 15 FT.
		REGULAR	DROP BOX		REGULAR	DROP BOX		REGULAR	DROP BOX	
3'-0"	2'-8"	1'-8"	10	7	3.2	285	5.3	497	7.4	706
3'-6"	3'-2"	2'-2"	10	7	3.4	305	5.7	528	7.9	747
4'-0"	3'-6"	2'-6"	12	9	3.7	326	6.0	559	8.4	788
4'-6"	4'-2"	3'-2"	12	9	3.9	334	6.4	571	8.8	803
5'-0"	4'-8"	3'-8"	14	11	4.1	354	6.7	602	9.3	844
5'-6"	5'-2"	4'-2"	16	13	4.4	375	7.0	627	7.4	850
6'-0"	5'-8"	4'-8"	18	15	4.6	385	7.2	636	7.6	860
6'-6"	6'-2"	5'-2"	18	15	4.8	402	7.4	637	7.8	880
7'-0"	6'-8"	5'-8"	20	17	5.0	423	7.6	654	8.0	897
7'-6"	7'-2"	6'-2"	20	17	5.3	430	7.8	664	8.3	907
8'-0"	7'-8"	6'-8"	22	19	5.5	451	8.1	684	8.5	927
8'-6"	8'-2"	7'-2"	24	21	5.7	471	8.3	702	8.7	944
9'-0"	8'-8"	7'-8"	24	21	6.0	479	7.8	711	9.0	954
9'-6"	9'-2"	8'-2"	26	23	6.2	499	7.8	732	9.2	974
10'-0"	9'-8"	8'-8"	28	25	6.4	520	8.0	749	9.4	992
10'-6"	10'-2"	9'-2"	28	25	6.7	527	8.3	759	9.7	1001
11'-0"	10'-8"	9'-8"	30	27	6.9	547	8.5	779	9.9	1022

NOTES: FOR L = 5 FT, L = 10 FT, AND L = 15 FT.
REGULAR INLETS TOTAL QUANTITIES NEEDED ARE OUTSIDE THE HEAVY BLACK LINE.
DROP BOX INLETS TOTAL QUANTITIES NEEDED ARE INSIDE THE HEAVY BLACK LINE.
STEEL WEIGHTS DO NOT INCLUDE STRUCTURAL STEEL CHANNEL.

TABLE TWO ~ BARS AND QUANTITIES VARIABLE WITH "H"

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Creation Date: 07/04/12	Initials: DD	Date:	Comments:	4201 East Arkansas Avenue			
Last Modification Date: 07/04/12	Initials: LTA			Denver, Colorado 80222			
Full Path: www.coloradodot.info/business/designsupport				Phone: (303) 757-9083			
Drawing File Name: M06040200203.dgn				Fax: (303) 757-9820			
CAD Ver.: MicroStation V8	Scale: Not to Scale	Units: English		Project Development Branch	DD/LTA		

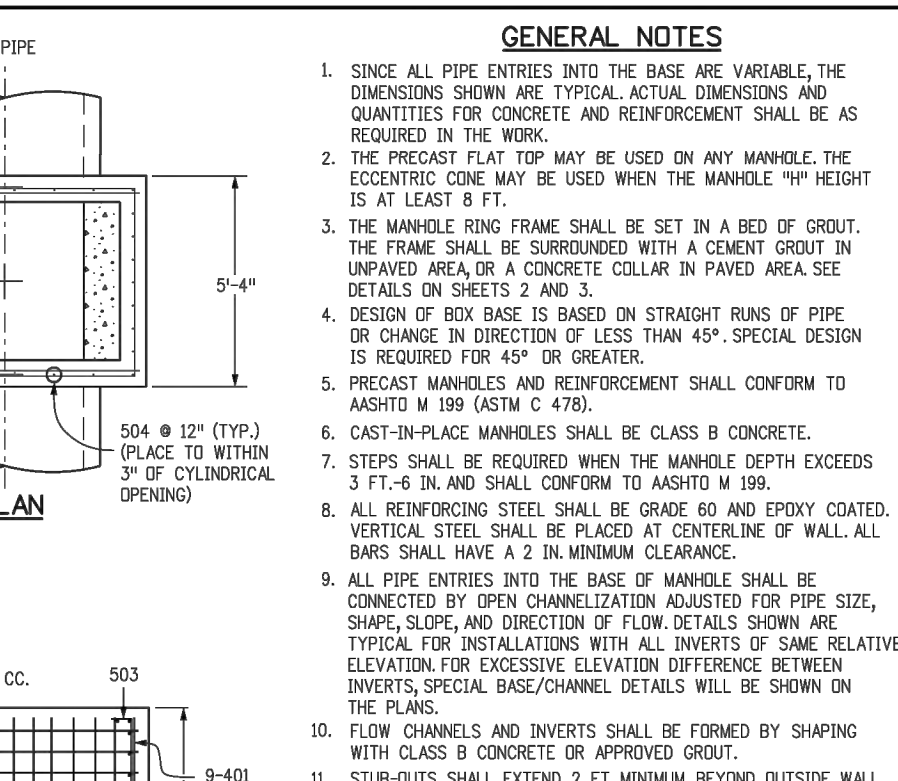
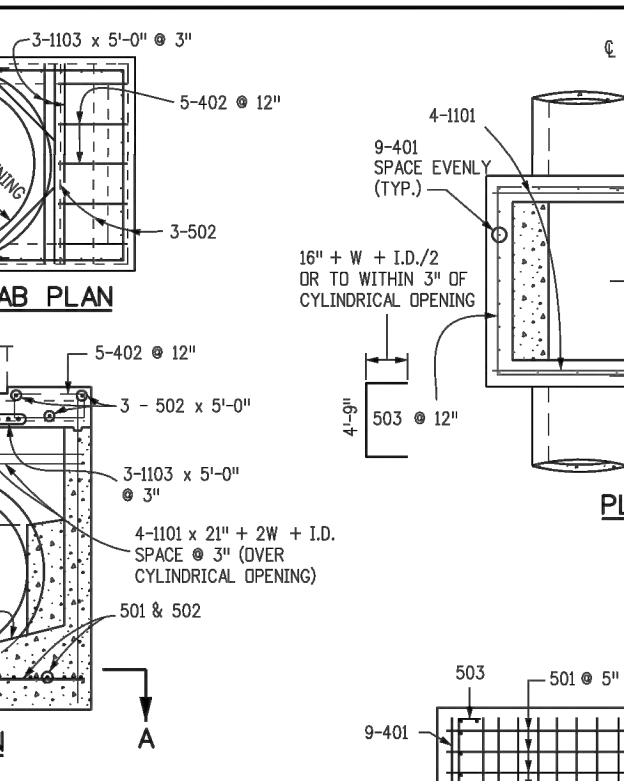
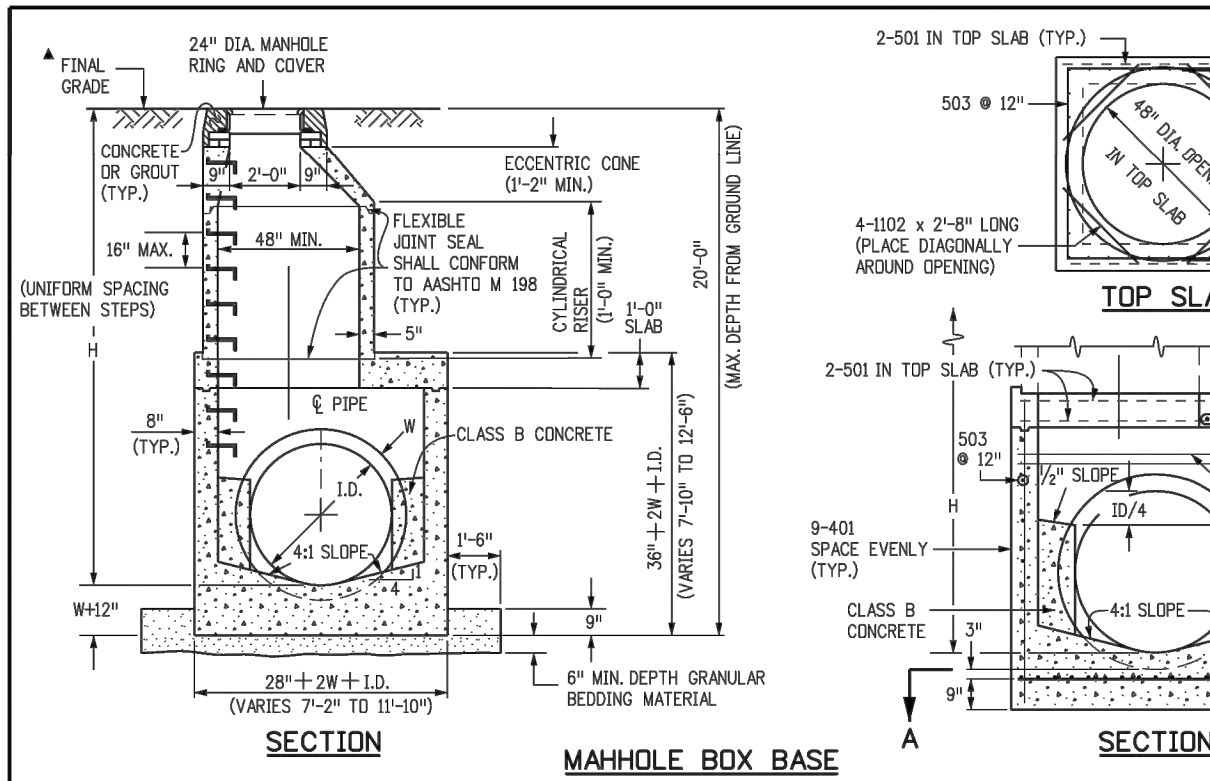


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Last Modification Date: 07/04/12	Initials: LTA			Denver, Colorado 80222			
Full Path: www.coloradodot.info/business/designsupport				Phone: (303) 757-9083			
Drawing File Name: M06040200203.dgn				Fax: (303) 757-9820			
CAD Ver.: MicroStation V8	Scale: Not to Scale	Units: English		Project Development Branch	DD/LTA		

CURB INLET TYPE R

M-604-12

Sheet No. 2 of 2



MARK	SIZE	TYPE	WT. W/T.	BARS	I.D.						FORMULAS	
					54"	60"	66"	72"	84"	96"		
401	4	I	0.668	(ND. REPO. LENGTH WEIGHT *)	18 32.1 97.2	18 32.1 97.2	18 32.1 97.2	18 32.1 97.2	18 32.1 97.2	18 32.1 97.2	401 BAR LENGTH = 32" + 2W + I.D.	
402	4	III	0.668	(ND. REPO. LENGTH WEIGHT *)	5-0 9.0 27.0	6-0 10.8 32.4	6-0 10.8 32.4	7-0 12.6 37.8	8-0 14.4 43.2	9-0 16.2 48.6	402 BAR LENGTH = I.D. + 2W	
501	5	I	1.043	(ND. REPO. LENGTH WEIGHT *)	17 28.5 85.5	17 28.5 85.5	17 28.5 85.5	17 28.5 85.5	17 28.5 85.5	17 28.5 85.5	501 BAR LENGTH = 24" + I.D. + 2W	
502	5	I	1.043	(ND. REPO. LENGTH WEIGHT *)	22 37.4 112.2	22 37.4 112.2	22 37.4 112.2	22 37.4 112.2	22 37.4 112.2	22 37.4 112.2	502 NUMBER BARS REQD. = $3 + \frac{(24+I.D.)(2W+I.D.)}{57}$	
503	5	II	1.043	(ND. REPO. LENGTH WEIGHT *)	17-10 28.5 85.5	17-10 28.5 85.5	17-10 28.5 85.5	17-10 28.5 85.5	17-10 28.5 85.5	17-10 28.5 85.5	503 NUMBER BARS REQD. = $2 + \frac{(17+I.D.)(2W+I.D.)}{57}$	
504	5	I	1.043	(ND. REPO. LENGTH WEIGHT *)	12-14 20.7 62.1	12-14 20.7 62.1	12-14 20.7 62.1	12-14 20.7 62.1	12-14 20.7 62.1	12-14 20.7 62.1	504 NUMBER BARS REQD. = $2 + \frac{(12+I.D.)(2W+I.D.)}{57}$	
1101	11	I	5.313	(ND. REPO. LENGTH WEIGHT *)	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	1101 BAR LENGTH = 22" + I.D. + 2W	
1102	11	I	5.313	(ND. REPO. LENGTH WEIGHT *)	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	1102 BAR LENGTH = 22" + I.D. + 2W	
1103	11	I	5.313	(ND. REPO. LENGTH WEIGHT *)	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	4-2 6.9 20.7	1103 BAR LENGTH = 22" + I.D. + 2W	
REINFORCING STEEL TOTAL					955.8	1,037.5	1,127.2	1,204.0	1,380.2	1,601.8		
CONCRETE - CUBIC YARDS - TOTAL					6.0	6.6	7.3	8.0	9.5	11.1		
NET QUANTITIES ARE BASED ON 1" DIA. BAR WITH 1" DIA. END ORANGE												
BASE AND A 4 FT. MANHOLE ENTRANCE TO THE DRAIN OUT FROM												

NOTE: QUANTITIES ARE BASED ON SAME SIZE PIPE ENTRANCE TO AND EXIT FROM BASE, AND A 4" T. MANHOLE.

QUANTITIES FOR CONCRETE MANHOLE BOX BASE

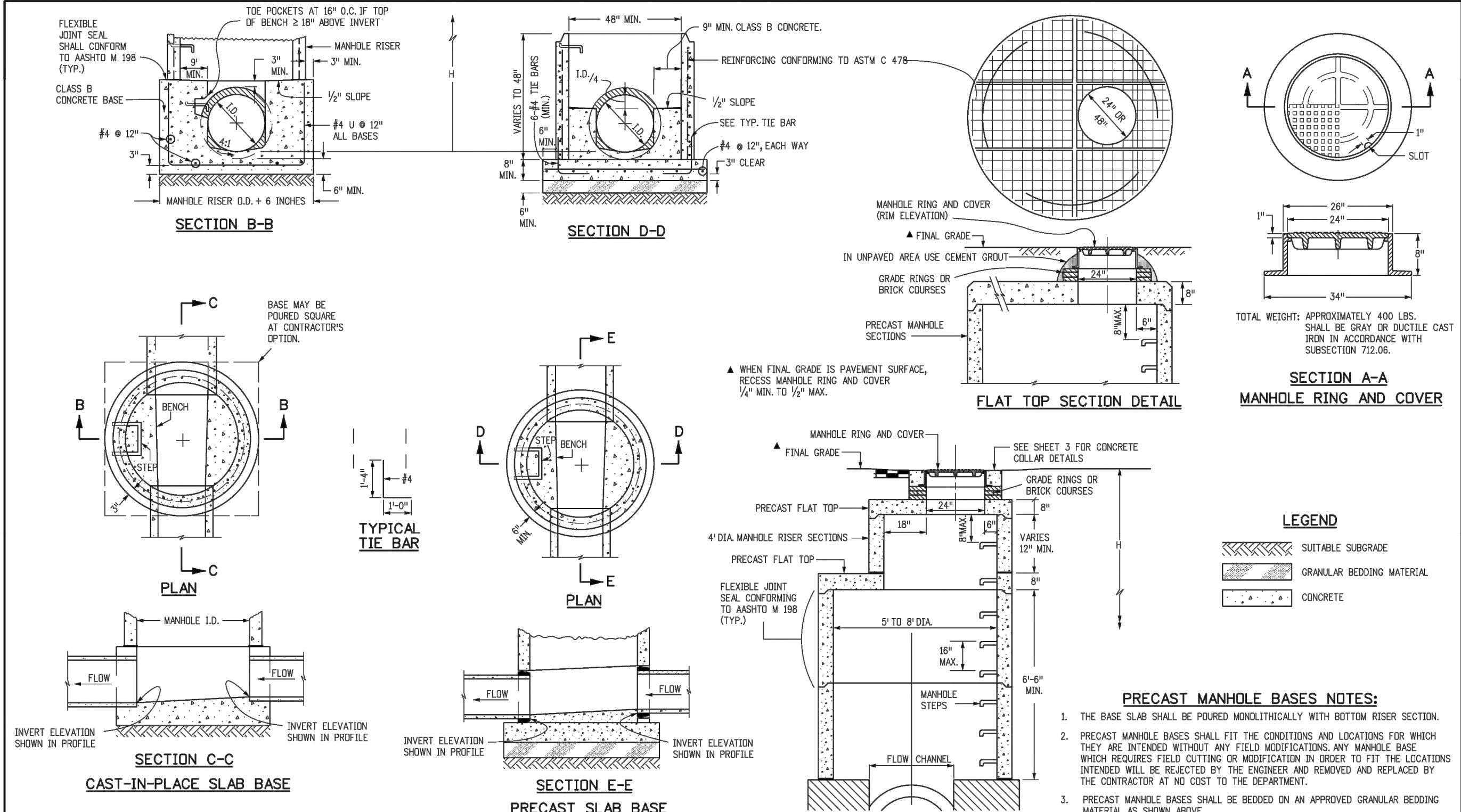
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MANHOLES

M-604-20

Sheet No. 1 of 3

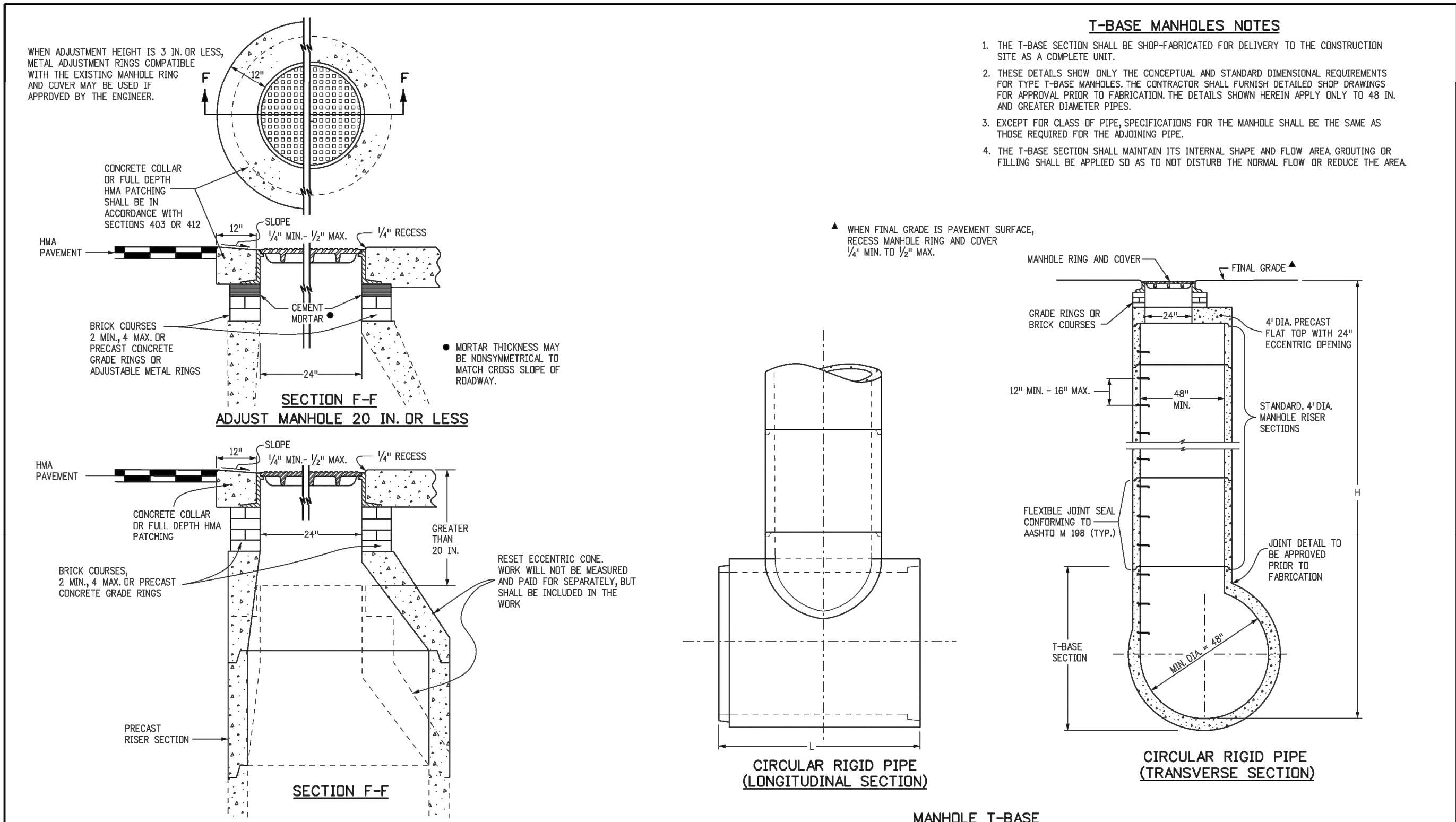


Computer File Information		Sheet Revisions		Colorado Department of Transportation		STANDARD PLAN NO.	
Creation Date: 07/04/12	Initials: DD	Date:	Comments:	4201 East Arkansas Avenue			
Last Modification Date: 07/04/12	Initials: LTA			Denver, Colorado 80222			
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MANHOLES

M-604-20

Sheet No. 2 of 3



Computer File Information		Sheet Revisions		Colorado Department of Transportation		STANDARD PLAN NO.	
Creation Date: 07/04/12	Initials: DD	Date:	Comments:	4201 East Arkansas Avenue			
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MANHOLES

M-604-20

Sheet No. 3 of 3

GENERAL NOTES:

1. ALL UTILITY CONSTRUCTION TO BE CONDUCTED IN CONFORMANCE WITH THE CURRENT WOODMEN HILLS METROPOLITAN DISTRICT (WHMD, THE DISTRICT) SPECIFICATIONS.
2. ALL PLANS ON THE JOB SITE SHALL BE SIGNED BY THE DISTRICT AND THE DISTRICTS ENGINEER. ANY REVISION TO THE PLANS SHALL BE SO NOTED WITH THE OLD DRAWING MARKED NOT VALID.
3. ALL STATIONING IS CENTER LINE UNLESS OTHERWISE NOTED. ALL ELEVATIONS ARE CENTER LINE UNLESS OTHERWISE NOTED.
4. ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE DISTRICT. THE DISTRICT RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
5. ALL WATER AND SEWER SERVICE LOCATIONS SHALL BE CLEARLY MARKED ON EITHER THE CURB HEAD OR THE FACE OF THE CURB, WITH AN 'S' FOR SEWER AND A 'W' FOR WATER.
6. DUCTILE IRON PIPES, INCLUDING FITTINGS, VALVES AND FIRE HYDRANTS, SHALL BE WRAPPED WITH POLYETHYLENE TUBING, DOUBLE BONDED AT EACH JOINT AND ELECTRICALLY ISOLATED. BONDING AND ANODE CONNECTIONS SHALL BE THOROUGHLY COATED WITH BITUMINOUS COATINGS.
7. ALL DUCTILE IRON PIPE LESS THAN 12 INCHES AND FITTINGS SHALL HAVE CATHODIC PROTECTION USING TWO NO. 6 WIRES WITH 17 LB. MAGNESIUM ANODES EVERY 400 FEET AND 9 LB. MAGNESIUM ANODES AT EACH FITTING. ALL DUCTILE IRON PIPE 12 INCHES AND GREATER AND FITTINGS SHALL HAVE CATHODIC PROTECTION USING TWO NO. 6 WIRES WITH 17 LB. MAGNESIUM ANODES EVERY 300 FEET AND 9 LB. MAGNESIUM ANODES AT EACH FITTING.
8. ALL PIPE MATERIAL, BACKFILL AND INSTALLATION SHALL CONFORM TO THE APPLICABLE SPECIFICATIONS OF THE CITY OF COLORADO SPRINGS, COLORADO DEPARTMENT OF TRANSPORTATION, EL PASO COUNTY DEPARTMENT OF TRANSPORTATION, COLORADO SPRINGS UTILITIES AND THE GEOTECHNICAL ENGINEER.
9. COMPACTION TESTS SHALL BE 95% STANDARD PROCTOR AS DETERMINED BY ASTM D698, UNLESS OTHERWISE APPROVED BY THE DISTRICT OR HIGHER STANDARD AS IMPOSED BY ANOTHER AGENCIES HAVING RIGHT-OF-WAY JURISDICTION. THIS SHALL INCLUDE ALL VALVES, FIRE HYDRANT RUNS, WATER & SEWER SERVICE LINES AND MANHOLES. ALL REPORTS SHALL BE SUBMITTED TO THE DISTRICT FOR REVIEW AND APPROVAL.
10. THE LOCATION OF ALL EXISTING UTILITIES SHOWN ON THE DRAWINGS ARE APPROXIMATE ONLY. THE LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES. THE DISTRICT SHALL BE NOTIFIED OF ANY DEVIATIONS TO THE LINE AND/OR GRADE AS DEPICTED ON THE PLANS. CONTRACTOR SHALL SUBMIT TO THE DISTRICT AND THE ENGINEER OF RECORD A REPORT OF THE FIELD VERIFIED INFORMATION PRIOR TO THE START OF CONSTRUCTION.
11. ALL BENDS SHALL BE FIELD STAKED PRIOR TO THE START OF CONSTRUCTION.
12. BENDS, DEFLECTION & CUT PIPE LENGTHS SHALL BE USED TO HOLD HORIZONTAL ALIGNMENT OF SEWER AND WATER LINES TO NO MORE THAN 0.5' FROM THE DESIGNED ALIGNMENT. CONSTRUCTION STAKES TO BE AT 25' INTERVALS ALONG CURVES TO ASSURE LOCATION OF PIPE LINE CONSTRUCTION.
13. AT ALL LOCATIONS WHERE CAP AND STUB IS NOTED ON DRAWINGS, PROVIDE A PLUG AT THE END OF THE PIPE JOINT NEAREST THE SPECIFIED STATION. PROVIDE A REVERSE ANCHOR AT ALL WATER LINE PLUGS.
14. ALL UNUSED SALVAGED WATER UTILITY MATERIAL SHALL BE RETURNED TO THE METROPOLITAN DISTRICT AS REQUESTED.
15. AT THE CONTRACTOR'S EXPENSE, ALL UTILITY MAINS SHALL BE SUPPORTED AND PROTECTED SUCH THAT THEY SHALL FUNCTION CONTINUOUSLY DURING CONSTRUCTION OPERATIONS. SHOULD A UTILITY MAIN FAIL AS A RESULT OF THE CONTRACTOR'S OPERATION, IT SHALL BE REPLACED IMMEDIATELY BY THE CONTRACTOR OR BY THE DISTRICT AT FULL COST OF LABOR AND MATERIALS TO THE CONTRACTOR/DEVELOPER.
16. PUMPING OR BYPASS OPERATIONS SHALL BE REVIEWED AND APPROVED BY BOTH THE DISTRICT AND THE DISTRICT ENGINEER PRIOR TO EXECUTION.
17. THE CONTRACTOR SHALL REPLACE OR REPAIR DAMAGE TO ALL SURFACE IMPROVEMENTS, INCLUDING BUT NOT LIMITED TO FENCES, LANDSCAPING, CURB AND GUTTER AND/OR ASPHALT THAT MAY BE CAUSED DURING CONSTRUCTION.
18. ALL CONTRACTORS WORKING ON OR NEAR A WATER OR SEWER FACILITY (TO INCLUDE SERVICE LINES) SHALL HAVE LIABILITY INSURANCE NAMING THE DISTRICT AS AN ADDITIONAL INSURED AND SHALL PROVIDE A CURRENT COPY OF WORKERS COMPENSATION INSURANCE ON FILE WITH THE DISTRICT. NO WORK CAN PROCEED WITHOUT CURRENT CERTIFICATES ON FILE AT THE DISTRICTS OFFICE.
19. THE CONTRACTOR SHALL NOTIFY THE DISTRICT AND ALL AFFECTED UTILITY COMPANIES ADJACENT TO THE PROPOSED UTILITY CONSTRUCTION A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A WEEKLY CONSTRUCTION MEETING SHALL BE REQUIRED WITH THE CONTRACTOR, DISTRICT ENGINEER AND ALL OTHER PARTIES AS DEEMED NECESSARY BY THE DISTRICT.
20. COMMENCEMENT OF CONSTRUCTION OF WATER/SEWER SYSTEMS WITHIN METROPOLITAN DISTRICT:
 - A) PRIOR TO THE START OF CONSTRUCTION, A PRE-CONSTRUCTION MEETING IS REQUIRED A MINIMUM OF 48 HOURS IN ADVANCE OF COMMENCEMENT OF WORK. A REPRESENTATIVE OF THE OWNER OR DEVELOPER, A REPRESENTATIVE OF THE CONTRACTOR AND DESIGN ENGINEER ARE REQUIRED TO ATTEND. CONTACT THE DISTRICT TO SCHEDULE THE PRE-CONSTRUCTION MEETING. NO PRE-CONSTRUCTION MEETING CAN BE SCHEDULED PRIOR TO FOUR (4) SIGNED/APPROVED PLAN SETS ARE RECEIVED BY THE DISTRICT.
 - B) THE CONTRACTOR IS REQUIRED TO NOTIFY THE DISTRICT A MINIMUM OF 48 HOURS AND A MAXIMUM OF 2 WEEKS PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL ALSO NOTIFY AFFECTED UTILITY COMPANIES AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION ADJACENT TO THE KNOWN UTILITY LINES.
21. TESTING OF FACILITIES:
 - A) THE CONTRACTOR SHALL NOTIFY THE DISTRICT A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF ANY TESTING.
 - B) ALL SECTIONS OF WATER LINE ARE TO MEET THE FOLLOWING PRESSURE TESTING REQUIREMENTS
 - TEST 100 % OF ALL LINES
 - MUST PASS PRESSURE TEST TO 200 PSI FOR TWO HOURS (UNLESS OTHERWISE APPROVED ON THE PLANS).
 - C) ALL SANITARY SEWER FACILITIES ARE TO MEET THE FOLLOWING TESTING REQUIREMENTS
 - ALL LINES SHALL BE JET CLEANED PRIOR TO VACUUM OR PRESSURE TESTING
 - ALL MANHOLES SHALL BE VACUUM TESTED WITH DISTRICT STAFF PRESENT PRIOR TO CCTV INSPECTION.
 - SEWER MAINS TO BE PRESSURE TEST PRIOR TO CCTV INSPECTION
 - ALL LINES SHALL BE CCTV INSPECTED AND VIDEO SHALL TO BE SUBMITTED TO THE DISTRICT FOR REVIEW AND APPROVAL.
22. PRELIMINARY ACCEPTANCE SHALL BE DEFINED AS THE POINT IN TIME THAT THE DISTRICT ACCEPTS THE FACILITY FOR USE. ALL SURFACE IMPROVEMENTS AND RESTORATION SHALL BE COMPLETED WITHIN 30 DAYS OF COMMENCEMENT. SHOULD THE CONTRACTOR FAIL TO COMPLETE ALL SURFACE IMPROVEMENTS AND RESTORATION WITHIN 30 DAYS OF COMMENCEMENT OF SERVICE, THE DISTRICT, AT THEIR DISCRETION, MAY ELECT TO COMPLETE THE IMPROVEMENTS AT THE CONTRACTORS COST.
23. FINAL ACCEPTANCE BY THE DISTRICT OF ANY LINE OR SYSTEM SHALL NOT OCCUR UNTIL COMPLETION OF FINAL ASPHALT LAYERS AND/OR FINAL COMPLETION AND/OR RESTORATION OF ALL SURFACE IMPROVEMENTS. THE WARRANTY PERIOD FOR ALL FACILITIES PRIOR TO FINAL ACCEPTANCE SHALL BE 24 MONTHS COMMENCING AFTER PRELIMINARY ACCEPTANCE.
24. ACCEPTANCE:
 - A) THE DISTRICT MAY GIVE PRELIMINARY ACCEPTANCE ONCE ALL OF THE TESTS ON ALL THE LINES HAVE BEEN COMPLETED AND A WALK-THRU HAS OCCURRED.
 - B) A SECOND ACCEPTANCE MAY OCCUR ONCE FIRST LIFT OF ASPHALT GOES DOWN AND A SECOND WALK-THRU OF THE SYSTEM OCCURS. IF ALL FACILITIES ARE CLEAN AND ACCESSIBLE, A FINAL ACCEPTANCE MAY OCCUR (THE DISTRICT MAY REQUIRE CLEANING AND RE-VIDEO OF THE SYSTEM, DEPENDING ON THE SEVERITY OF THE CONTAMINATION).
25. ALL WATER AND SEWER MAINS, INCLUDING SERVICE LINES, SHALL HAVE "AS-BUILT" DRAWINGS PREPARED AND APPROVED PRIOR TO PRELIMINARY ACCEPTANCE BY THE DISTRICT.
26. ALL COMMERCIAL/BUSINESS DEVELOPMENTS SHALL HAVE AN EIGHT INCH (MIN.) WATER MAIN LOOPED THROUGH THE PROPOSED PROPERTY WITH GATE VALVES LOCATED WHERE THE MAIN ENTERS THE PROPERTY LINE. AN EIGHT INCH SEWER MAIN SHALL BE INSTALLED FOR SERVICE TO COMMERCIAL/BUSINESS DEVELOPMENTS, AND A MANHOLE SHALL BE LOCATED WHERE THE MAIN ENTERS THE PROPERTY. THE END OF THE MAINS SHALL BE MARKED WITH THE APPROPRIATE COLORED CARSONITE MARKER ALONG WITH TRACER WIRE.
27. AFTER REVIEW AND APPROVAL OF PLANS FOR THE EXTENSION OF LINES, FACILITIES AND/OR SERVICES, CONSTRUCTION MUST BE COMMENCED WITHIN 18 MONTHS FOR RESIDENTIAL SUBDIVISIONS AND 12 MONTHS FOR ANY COMMERCIAL INSTALLATIONS.
28. INSPECTION FEES: CALL THE DISTRICT (719-495-2500) FOR FEE SCHEDULE.

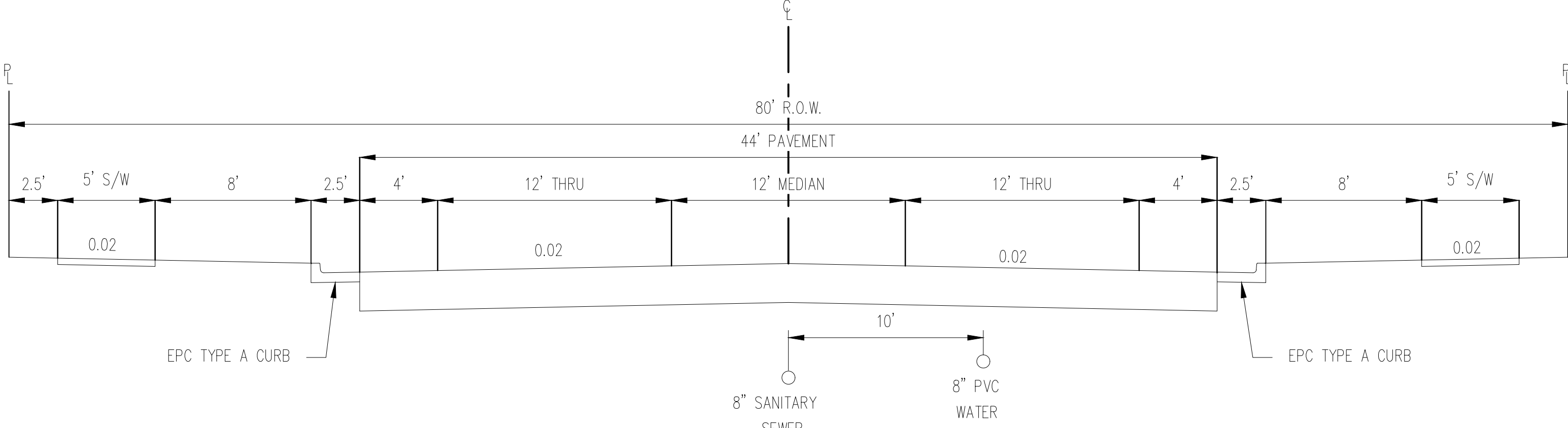
WATER SYSTEM INSTALLATION NOTES

29. ALL WATER AND FORCE MAIN PIPE SHALL BE AWWA C900 PVC, OR EQUAL, PRESSURE CLASS 200. ALL WATER AND FORCE MAIN FITTINGS SHALL HAVE MECHANICAL RESTRAINTS AND THRUST BLOCKS. ALL WATER AND FORCE MAIN PIPE SHALL HAVE A MINIMUM COVER DEPTH OF FIVE AND ONE-HALF (5.5) FEET.
30. ALL WATER VALVES ASSOCIATED WITH THE POTABLE WATER SYSTEM SHALL BE OPEN CLOCKWISE. ALL VALVES INSTALLED IN LANDSCAPED AREAS AND/OR NOT WITHIN PAVED STREETS SHALL BE MARKED WITH CARSONITE MARKERS. ALL VALVES ASSOCIATED WITH THE RAW WATER SYSTEM SHALL BE OPEN COUNTERCLOCKWISE AND MARKED WITH CARSONITE MARKERS AS APPLICABLE.
31. THE DEVELOPER OR HIS ENGINEER SHALL LOCATE ALL FIRE HYDRANTS AND SERVICE STUB OUTS FOR FUTURE DEVELOPMENT. ANY REQUIRED REALIGNMENT, (HORIZONTAL OR VERTICAL), SHALL BE AT THE EXPENSE OF THE DEVELOPER. FIRE HYDRANT LOCATION SHALL BE REVIEWED AND APPROVED BY THE APPLICABLE FIRE AUTHORITY.
32. FIRE HYDRANTS SHALL BE OPEN RIGHT WITH 7/8" X 7/8" SQUARE TAPERED ALONG WITH SERVICE CAPS. LUBRICATION TYPE: (GREASE). ACCEPTABLE BRANDS ARE AMERICAN AVK SERIES 2700 (MODERN) AND MUELLER SUPER CENTURION 250.
33. ALL MAIN LINES (PVC & DUCTILE IRON) SHALL BE INSTALLED WITH COATED #12 TRACER WIRE WITH TEST STATIONS AT INTERVALS NO GREATER THAN 500 FT (VALVE BOXES CAN BE USED AT INTERSECTIONS AND SERVICE STUBS).
34. CONTRACTOR SHALL MAKE CONNECTIONS TO EXISTING WATER LINE WITHOUT SHUTDOWN, OR ELSE NOTIFY THE DISTRICT OF ANY SERVICE SHUTDOWNS NECESSARY TO CONNECT TO EXISTING LINES.
35. IRRIGATION SERVICES SHALL HAVE A STOP AND WASTE CURB STOP VALVE INSTALLED ALONG WITH TRACER WIRE EXTENDING BACK TO THE MAIN LINE.
36. COMMENCEMENT OF USE OF WATER LINES AND/OR SYSTEMS:

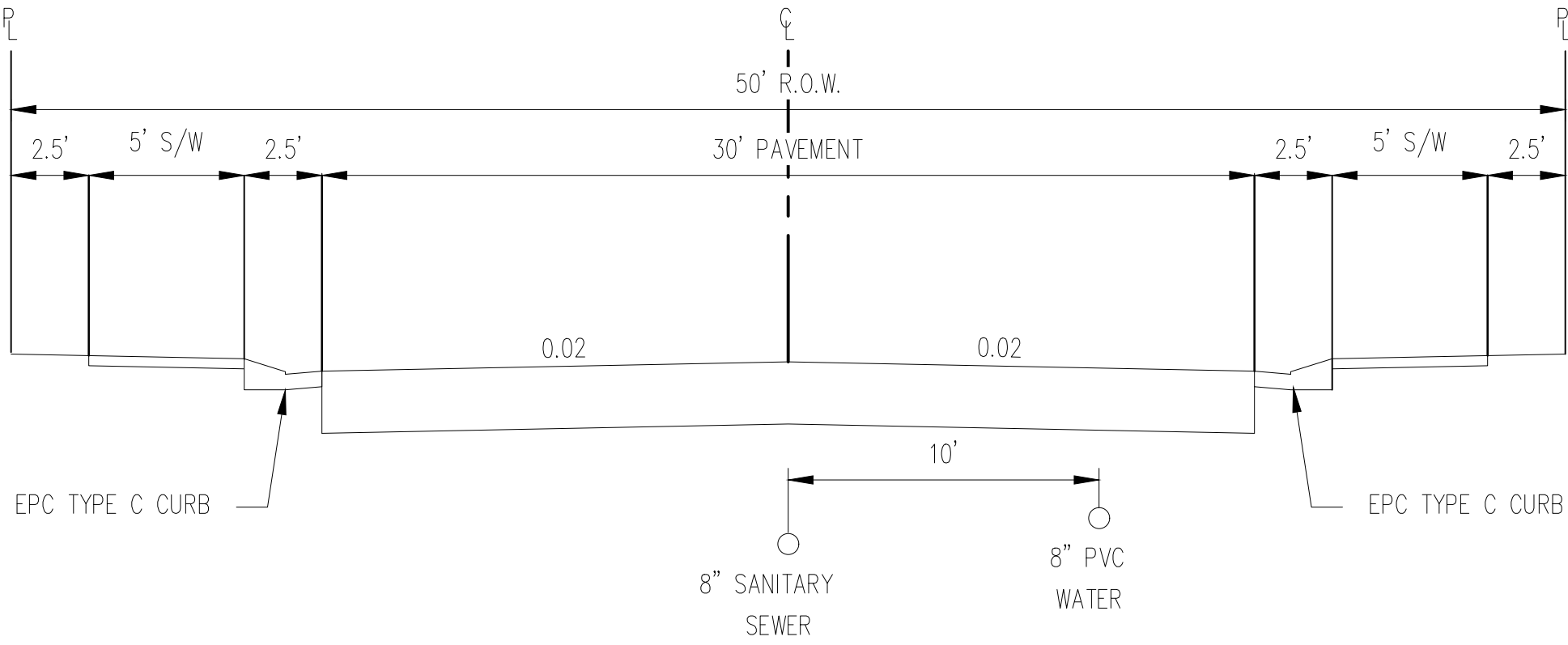
- A) NO WATER FACILITY SHALL BE PLACED IN SERVICE UNTIL AFTER THE COMPLETION OF ALL PRESSURE TESTING, FLUSHING, BAC-T TESTING, COMPACTION TESTING, AND AS-BUILT DRAWINGS ARE SUBMITTED AND APPROVED BY THE DISTRICT.
- B) NO WATER FACILITY SHALL BE PLACED IN SERVICE UNTIL ALL SERVICE LINES ARE COMPLETED AND THE FIRST LIFT OF ASPHALT IS COMPLETED OVER THE LINE. IN THE CASE WHERE NO ASPHALT IS TO BE PLACED OVER THE LINE, SURFACE IMPROVEMENTS SHALL BE COMPLETED PRIOR TO USE OF THE FACILITY.
- C) ALL EASEMENTS (PLATTED OR DEEDED) ARE DEDICATED, EXECUTED BY THE DISTRICT, AND RECORDED.

WASTEWATER SYSTEM INSTALLATION NOTES

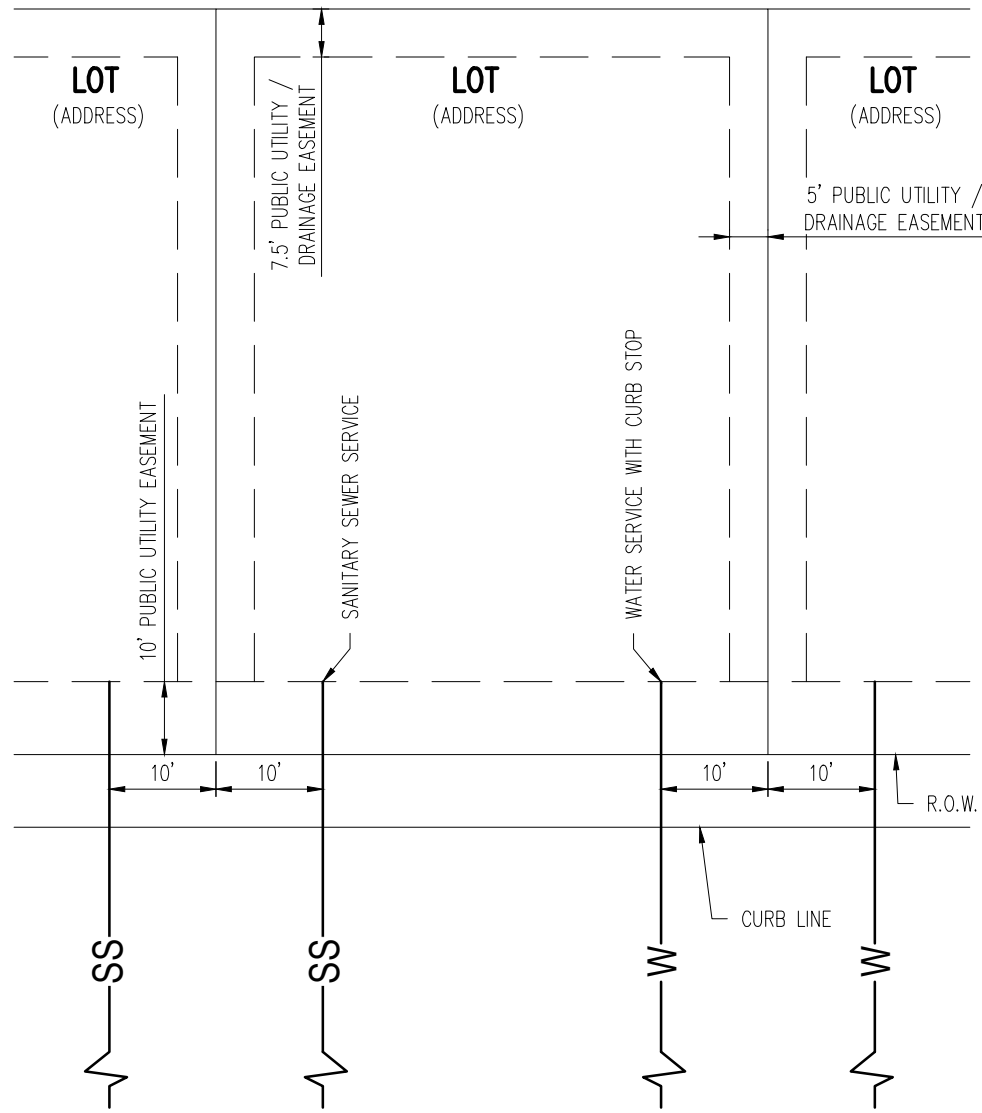
37. SANITARY SEWER LENGTHS ARE MH CENTER-MH CENTER. ALL SANITARY SEWER PIPES SHALL BE SDR 35 PVC OR EQUAL. SEWER LINES MAY NOT EXCEED 7% GRADE FOR ANY SIZE WITHOUT PRIOR APPROVAL OF THE DISTRICT. ALL NEWLY CONSTRUCTED RESIDENTIAL SANITARY SEWER TAPS SHALL USE PRE-MANUFACTURED IN-LINE PVC PUSH-ON WYES. TAPPING SADDLES MAY ONLY BE USED FOR TAPPING PRE-EXISTING MAINS.
 38. ALL SANITARY SEWER MANHOLES SHALL BE WRAPPED WITH RU116 - RUBR-NEK JOINT WRAP OR EQUIVALENT AND COATED.
 39. COMMENCEMENT OF USE OF SEWER LINES AND/OR SYSTEMS:
 - A) NO SANITARY SEWER FACILITY SHALL BE PLACED IN SERVICE UNTIL THE COMPLETION OF ALL JET CLEANING, PRESSURE TESTING, VACUUM TESTING, CCTV INSPECTION, COMPACTION TESTING, AND AS-BUILT DRAWINGS ARE SUBMITTED AND APPROVED BY THE DISTRICT.
 - B) NO SANITARY SEWER FACILITY SHALL BE PLACED IN SERVICE UNTIL ALL SERVICE LINES ARE COMPLETED AND THE FIRST LIFT OF ASPHALT IS COMPLETED OVER THE LINE. IN THE CASE WHERE NO ASPHALT IS TO BE PLACED OVER THE LINE, ANY REQUIRED SURFACE IMPROVEMENTS SHALL BE COMPLETED PRIOR TO USE OF THE FACILITY.
 - C) ALL NECESSARY EASEMENTS (PLATTED OR DEEDED) ARE DEDICATED, EXECUTED BY THE DISTRICT, AND RECORDED.
 - D) DOWNSTREAM PLUG CAN BE REMOVED ONCE FIRST LIFT OF ASPHALT IS DOWN AND THE ABOVE REQUIREMENTS ARE MET.
- THE ABOVE GUIDELINES ARE SUBJECT TO CHANGE AT ANY TIME.



**TYPICAL STREET SECTION – BENT GRASS MEADOWS DRIVE
URBAN NON-RESIDENTIAL COLLECTOR ROADWAY
N.T.S.**



**TYPICAL STREET SECTION
URBAN LOCAL ROADWAY
N.T.S.**



**DETAIL: UTILITY SERVICE LOCATIONS
N.T.S.**

UTILITY NOTES

1. CONTRACTOR TO OBTAIN WORK IN THE ROW PERMIT FROM EL PASO COUNTY PRIOR TO CONSTRUCTION.
2. CONTRACTOR TO POTHOLE AND VERIFY EXISTENCE OF OTHER UTILITIES WITHIN ANY PUBLIC RIGHT OF WAYS.
3. FOR SANITARY SEWER RIGHT OF WAY CONNECTION, A 30' UTILITY EASEMENT MUST BE GRANTED PRIOR TO CONSTRUCTION.
4. CONTRACTOR TO MAINTAIN 5.0' MINIMUM COVER OVER ALL WATER MAINS CONSTRUCTED.
5. ALL VALVES SHOWN FOR SCHEMATIC PURPOSES ONLY. NO VALVES SHALL BE INSTALLED IN CURB AND GUTTER OR CROSSPANS.
6. STANDARD WATER SERVICE LOCATIONS TO BE 10' FROM "DOWNSTREAM" PROPERTY LINE AND EXTENDED THROUGH THE 10' PUBLIC UTILITY EASEMENT.
7. SEWER SERVICE TO BE PLACED APPROXIMATELY FROM THE SIDE LOT LINE AND BE EXTENDED THROUGH THE 10' PUBLIC UTILITY EASEMENT.
8. SERVICES SHALL BE MARKED WITH 2"x4" POST AT TERMINATION POINT FOR LOCATION DURING CONNECTION TO RESIDENCE. POSTS SHALL BE SPRAY-PAINTED BLUE FOR WATER AND GREEN FOR SEWER.
9. CURB STOPS ARE NOT TO BE INSTALLED IN CONCRETE CURB, CROSSPANS, SIDEWALKS, OR DRIVEWAYS.
10. SANITARY SEWER MAINLINE DEPTH (<10' DEPTH); NO BASEMENTS ALLOWED UNLESS PUMPED TO MAIN ELEVATION.
11. LOTS WITH FRONTAGE OF 40' OR LESS, WATER SERVICE LOCATION WILL BE 2.0' OFF THE DOWN HILL LOT CORNER.
12. SEE PLAN FOR CORNER LOT LOCATIONS.

Galloway

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Colorado Springs, CO 80920
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gallowayUS.com



02/10/2020

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**CHALLENGER
HOMES**

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC

BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

GENERAL NOTES AND
TYPICAL ROADWAY
SECTIONS

U0.02

Sheet 2 of 32



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CHALLENGER HOMES

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FOR
CHALLENGER COMMUNITIES, LLC

BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

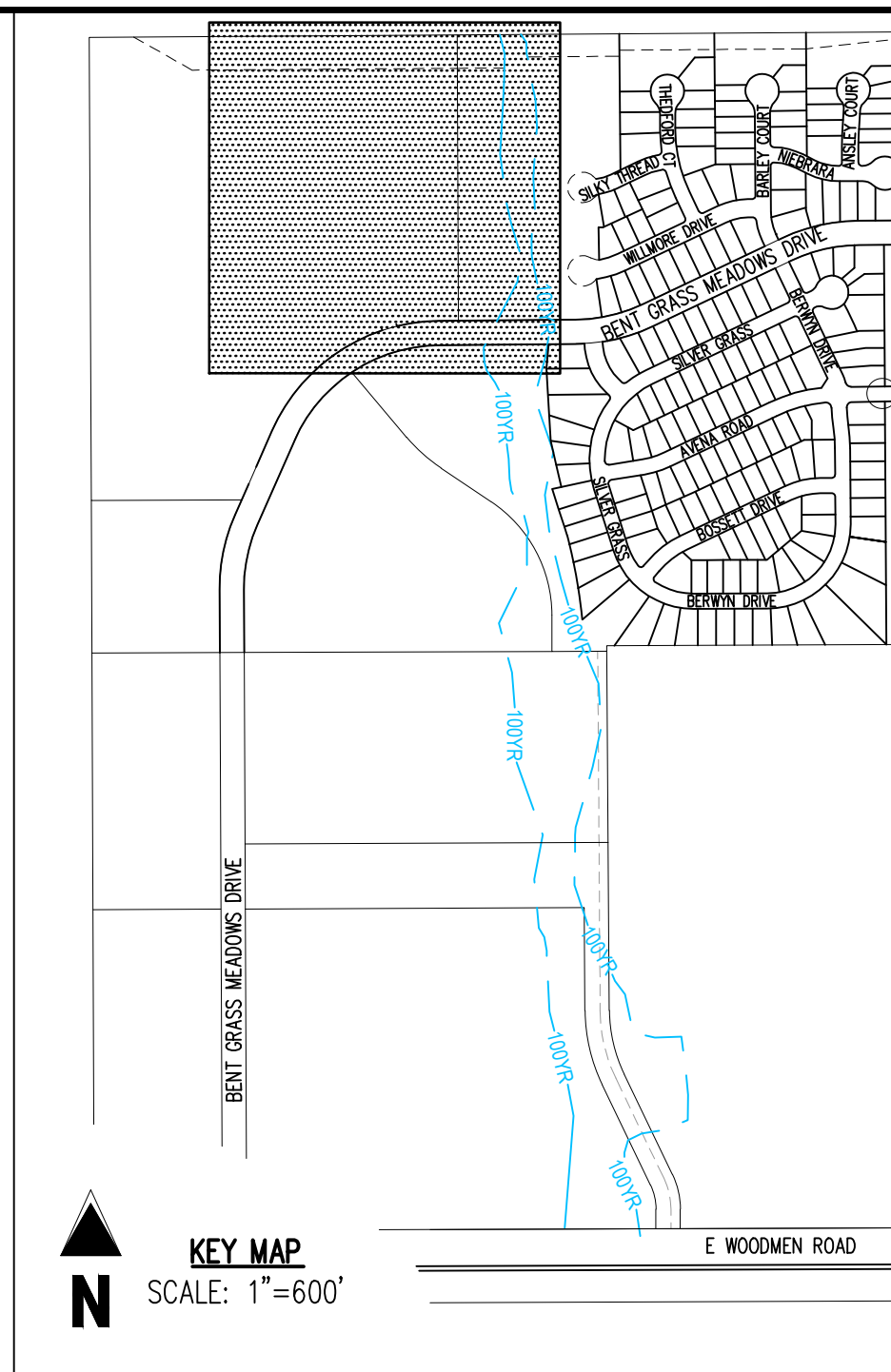
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SANITARY LAYOUT PLAN

U1.02

Sheet 5 of 32



LEGEND

- | | |
|--|------------------------------------|
| | EXISTING PROPERTY BOUNDARY |
| | PROPOSED PROPERTY BOUNDARY |
| | PROPOSED R.O.W. |
| | LOT LINE |
| | EXISTING EASEMENT LINE |
| | PROPOSED EASEMENT LINE |
| | FEMA 100-YEAR FLOODPLAIN BOUNDARY |
| | MATCHLINE |
| | EXISTING STORM SEWER |
| | PROPOSED STORM SEWER |
| | EXISTING WATER LINE |
| | PROPOSED WATER LINE |
| | EXISTING SANITARY SEWER |
| | PROPOSED SANITARY SEWER |
| | EXISTING GAS LINE |
| | EXISTING UNDERGROUND ELECTRIC LINE |
| | EXISTING OVERHEAD ELECTRIC LINE |
| | EXISTING FENCE |
| | EXISTING SANITARY SEWER MANHOLE |
| | PROPOSED SANITARY SEWER MANHOLE |
| | EXISTING FIRE HYDRANT |
| | PROPOSED FIRE HYDRANT AND FITTINGS |

NOTES

1. SEE SHEETS U3.01 - U3.15 FOR ALL SANITARY CROSSINGS

LEGAL DESCRIPTION

BENT GRASS RESIDENTIAL FILING NO. 2. LOCATED IN THE NORTHWEST QUARTER AND SOUTHWEST QUARTER OF SECTION 1 T13S R65W OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF EL PASO, STATE OF COLORADO

BASIS OF BEARINGS

ALL BEARINGS ARE GRID BEARINGS OF THE COLORADO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE, NORTH AMERICAN DATUM 1983. THE BEARING OF THE LINE BETWEEN THE SOUTHWEST CORNER OF SECTION 1, T13S, R65W AND THE WEST QUARTER CORNER SECTION 1, T13S, R65W IS $N00^{\circ}13'46''W$ AND MONUMENTED AS SHOWN:

BENCHMARK

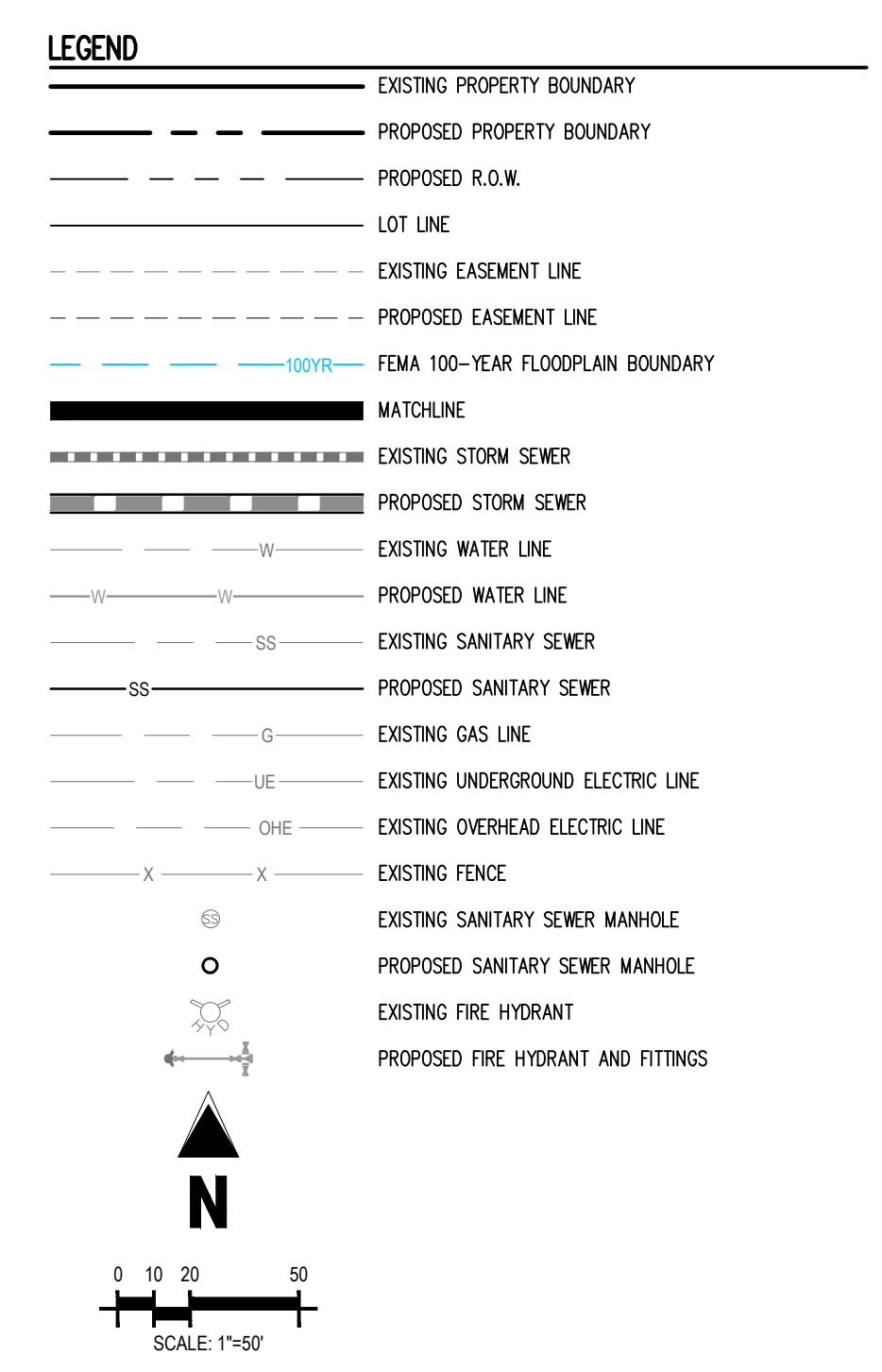
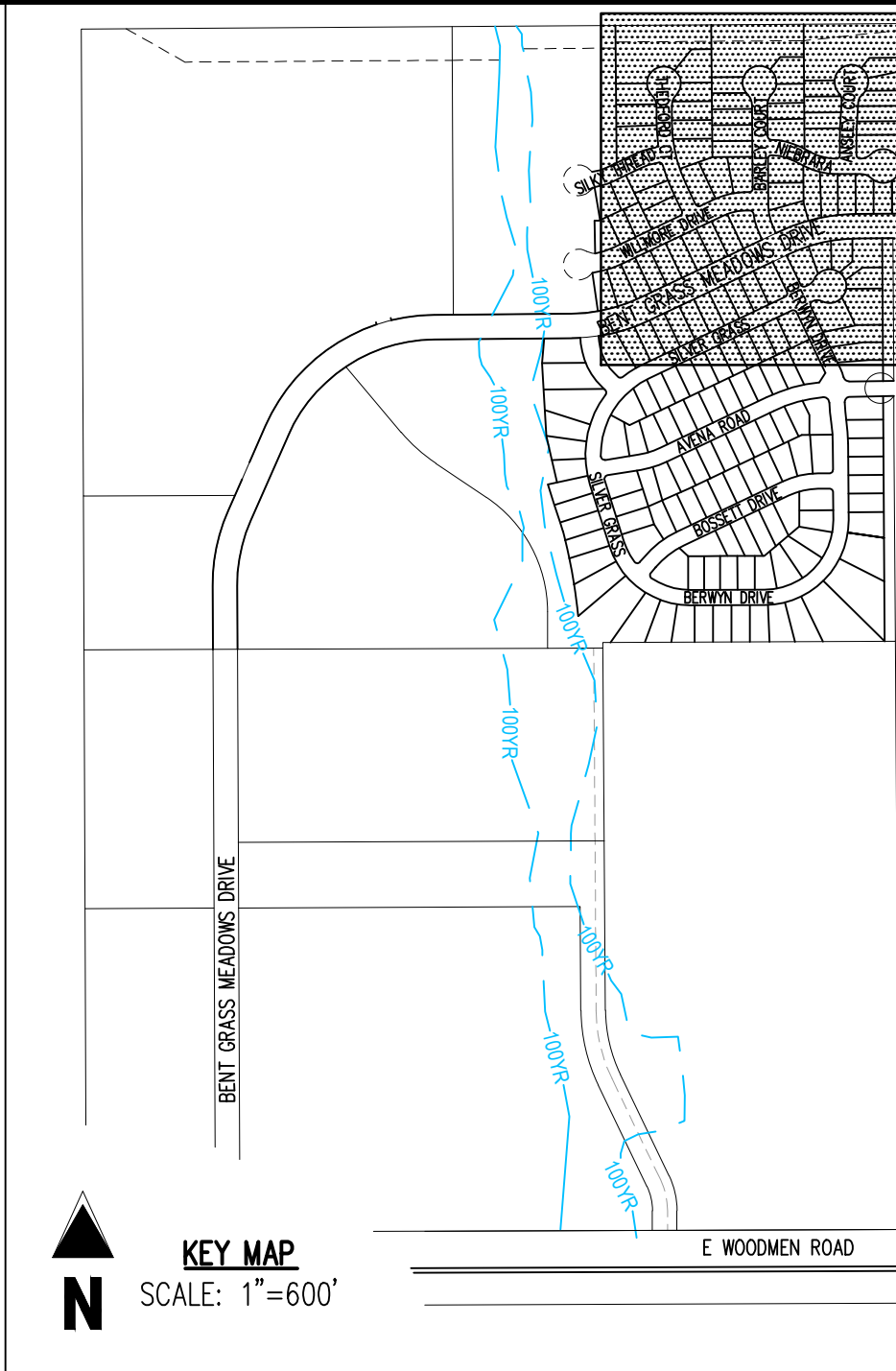
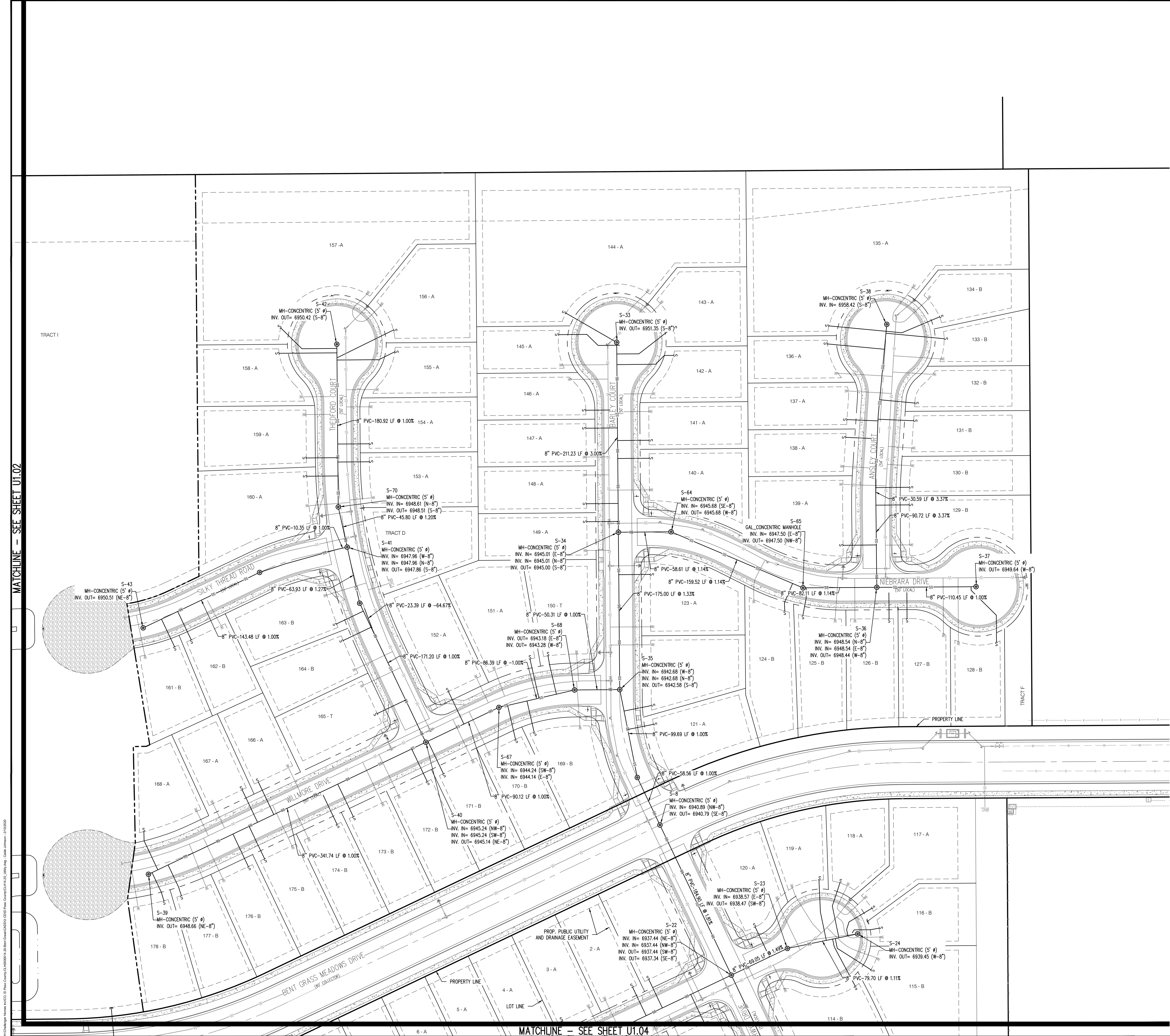
THE SOUTHWESTERLY CORNER OF LOT 1 WOODMEN HILLS FILING NO. 4. MONUMENTED BY A YELLOW PLASTIC SURVEYORS CAP ON A NO. 4 REBAR LùS# 24954 ELEVATION = 6947.67

CAUTION – NOTICE TO CONTRACTOR

1. ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURFACE EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN APPROXIMATE LOCATION ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION. REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.
2. WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY EITHER THROUGH POT-HOING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.



Know what's **below**.
Call before you dig.



LEGAL DESCRIPTION

BENT GRASS RESIDENTIAL FILING NO. 2, LOCATED IN THE NORTHWEST QUARTER AND SOUTHWEST QUARTER OF SECTION 1 T13S R65W OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF EL PASO, STATE OF COLORADO.

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BENCHMARK

THE SOUTHWESTERLY CORNER OF LOT 1 WOODMEN HILLS FILING NO. 4, MONUMENTED BY A YELLOW PLASTIC SURVEYORS CAP ON A NO. 4 REBAR L5# 24954 ELEVATION = 6947.67

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- WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOLING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.

811
Know what's below.
Call before you dig.

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02/10/2020

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CHALLENGER HOMES

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FALCON, CO 80831 - EL PASO COUNTY

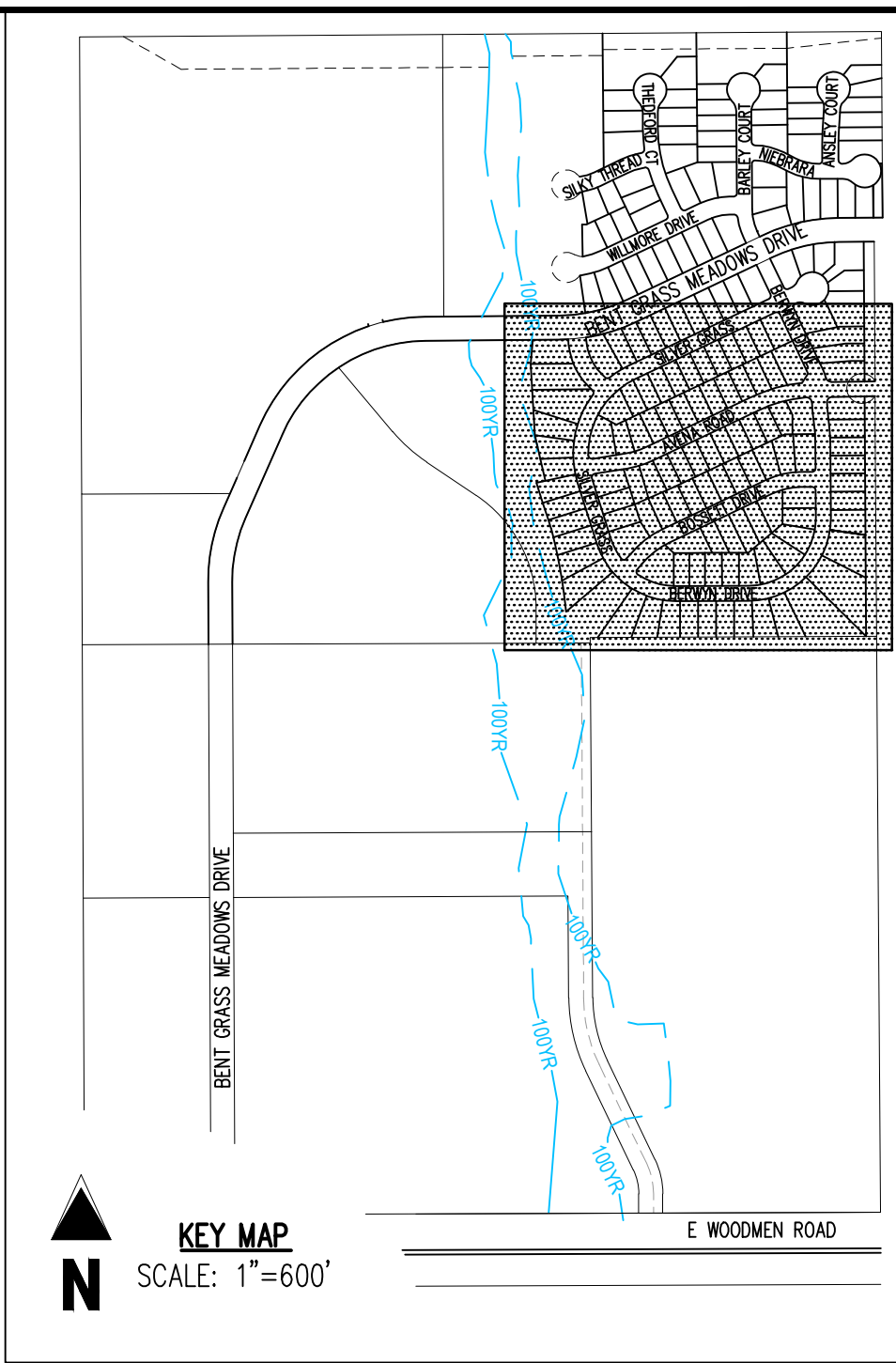
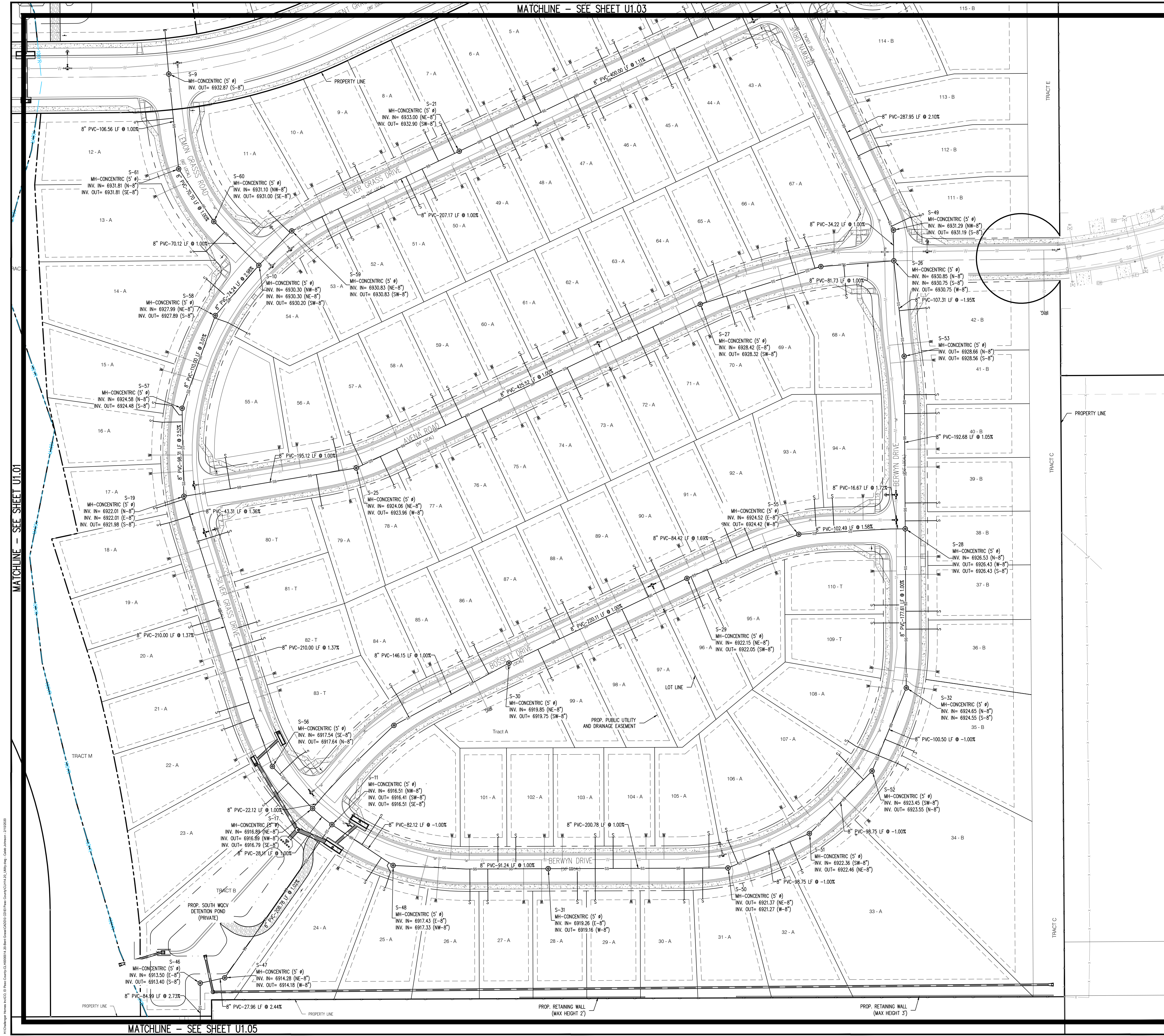
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Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

SANITARY LAYOUT PLAN

U1.03

Sheet 6 of 32



LEGEND

- EXISTING PROPERTY BOUNDARY
- PROPOSED PROPERTY BOUNDARY
- PROPOSED R.O.W.
- LOT LINE
- EXISTING EASEMENT LINE
- PROPOSED EASEMENT LINE
- FEMA 100-YEAR FLOODPLAIN BOUNDARY
- MATCHLINE
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- EXISTING WATER LINE
- PROPOSED WATER LINE
- EXISTING SANITARY SEWER
- PROPOSED SANITARY SEWER
- EXISTING GAS LINE
- EXISTING UNDERGROUND ELECTRIC LINE
- EXISTING OVERHEAD ELECTRIC LINE
- EXISTING FENCE
- EXISTING SANITARY SEWER MANHOLE
- PROPOSED SANITARY SEWER MANHOLE
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT AND FITTINGS

NOTES

- SEE SHEETS U3.01 - U3.15 FOR ALL SANITARY CROSSINGS

LEGAL DESCRIPTION

BENT GRASS RESIDENTIAL FILING NO. 2, LOCATED IN THE NORTHWEST QUARTER AND SOUTHWEST QUARTER OF SECTION 1 T13S R65W OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF EL PASO, STATE OF COLORADO

BASIS OF BEARINGS

ALL BEARINGS ARE GRID BEARINGS OF THE COLORADO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE, NORTH AMERICAN DATUM 1983. THE BEARING OF THE LINE BETWEEN THE SOUTHWEST CORNER OF SECTION 1, T13S, R65W AND THE WEST QUARTER CORNER SECTION 1, T13S, R65W IS N007°34'46"W AND MONUMENTED AS SHOWN.

BENCHMARK

THE SOUTHWESTERLY CORNER OF LOT 1 WOODMEN HILLS FILING NO. 4, MONUMENTED BY A YELLOW PLASTIC SURVEYORS CAP ON A NO. 4 REBAR L5# 24954 ELEVATION = 6947.67

CAUTION - NOTICE TO CONTRACTOR

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CHALLENGER HOMES

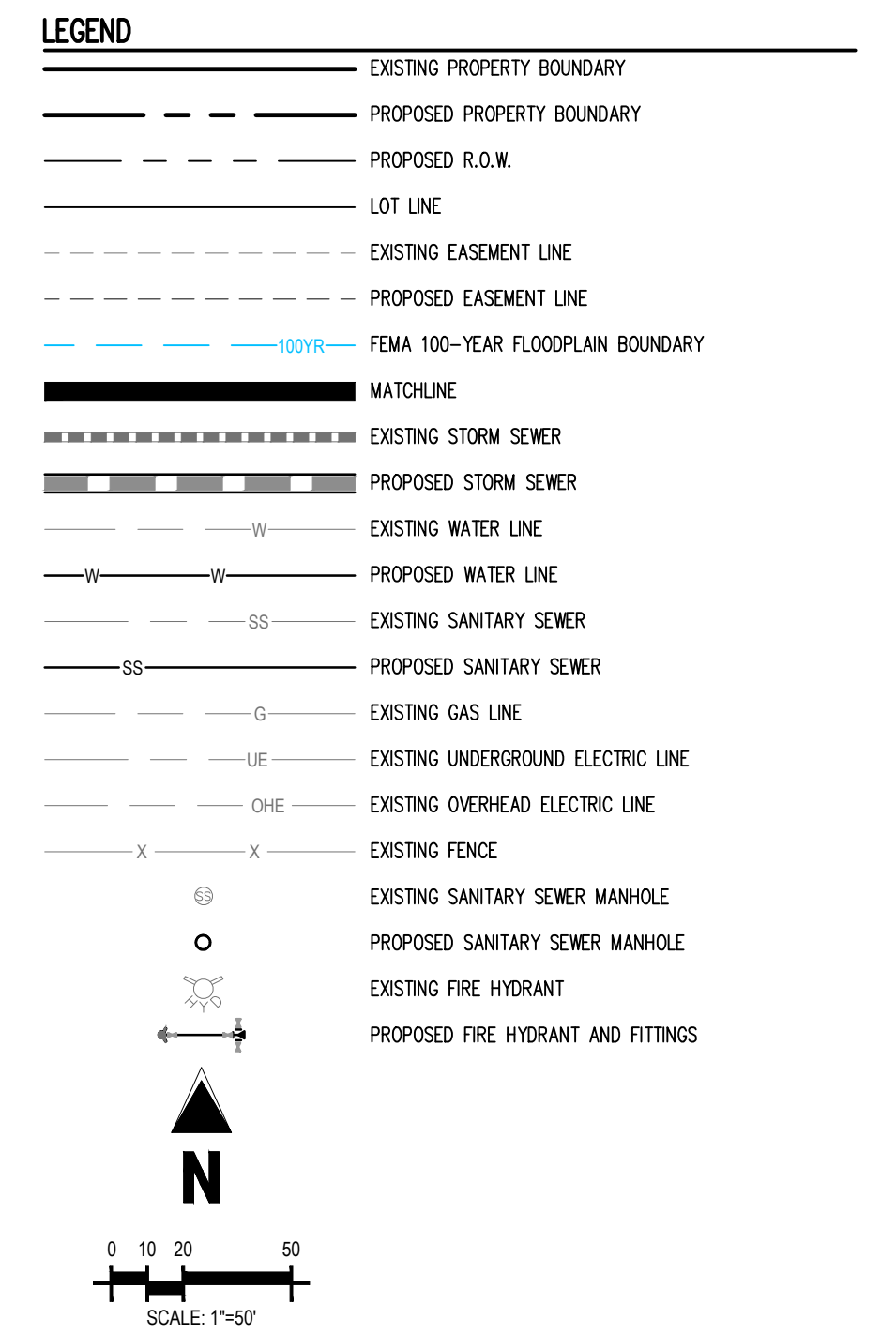
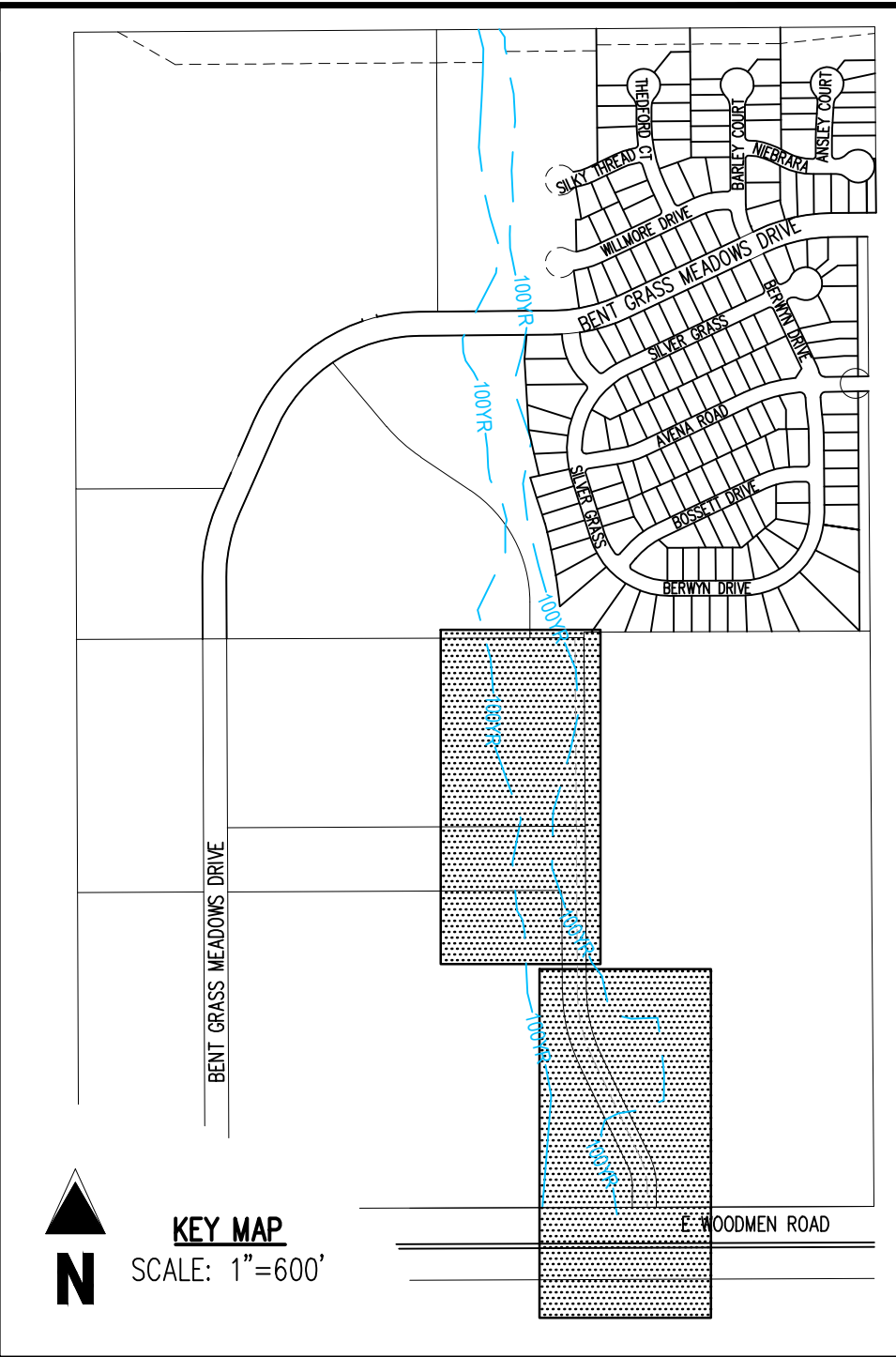
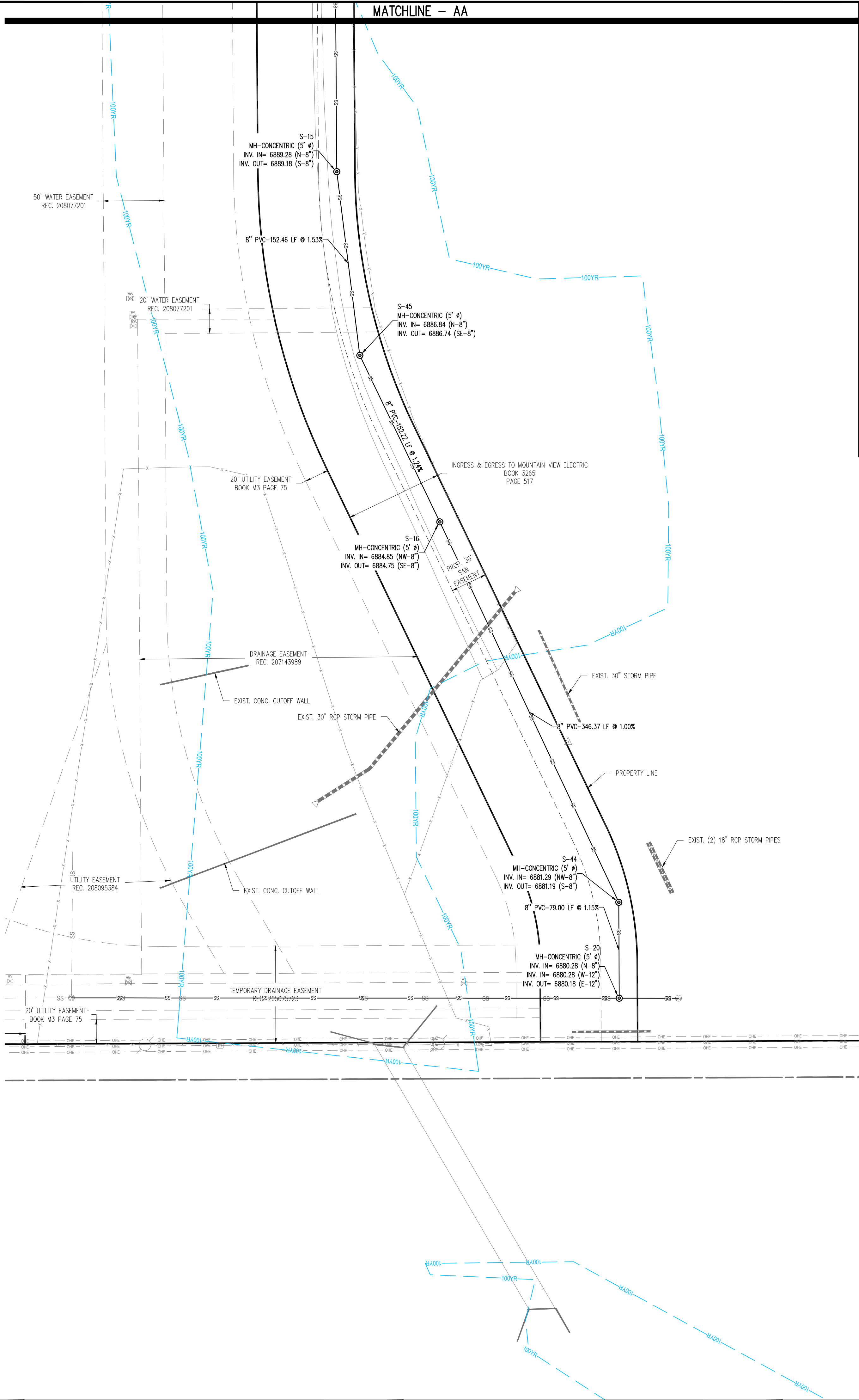
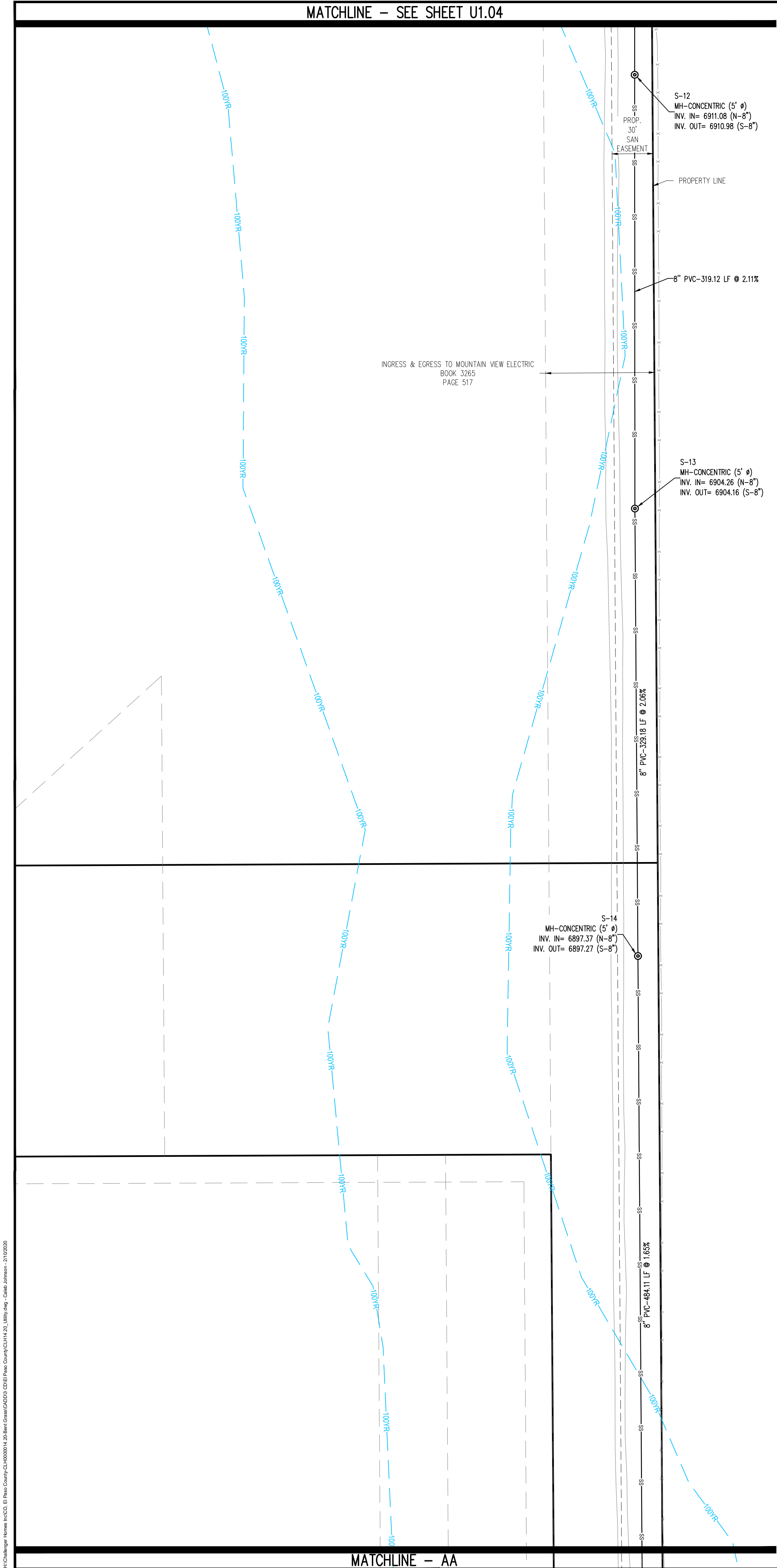
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

SANITARY LAYOUT PLAN

U1.04
Sheet 7 of 32



NOTES

- SEE SHEETS U3.01 - U3.15 FOR ALL SANITARY CROSSINGS

LEGAL DESCRIPTION

BENT GRASS RESIDENTIAL FILING NO. 2, LOCATED IN THE NORTHWEST QUARTER AND SOUTHWEST QUARTER OF SECTION 1 T13S R65W OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF EL PASO, STATE OF COLORADO

BASIS OF BEARINGS

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BENCHMARK

THE SOUTHWESTERLY CORNER OF LOT 1 WOODMEN HILLS FILING NO. 4, MONUMENTED BY A YELLOW PLASTIC SURVEYORS CAP ON A NO. 4 REBAR L5# 24954 ELEVATION = 6947.67

CAUTION - NOTICE TO CONTRACTOR

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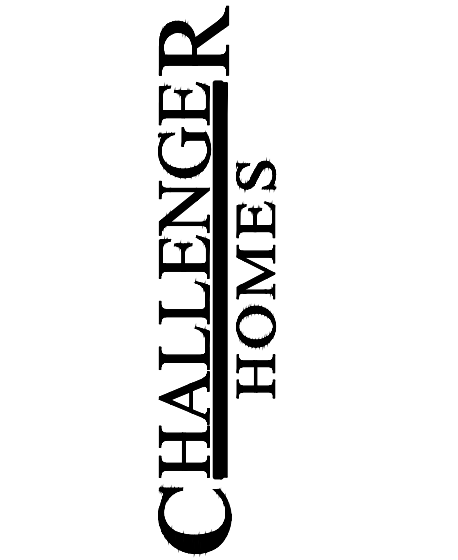
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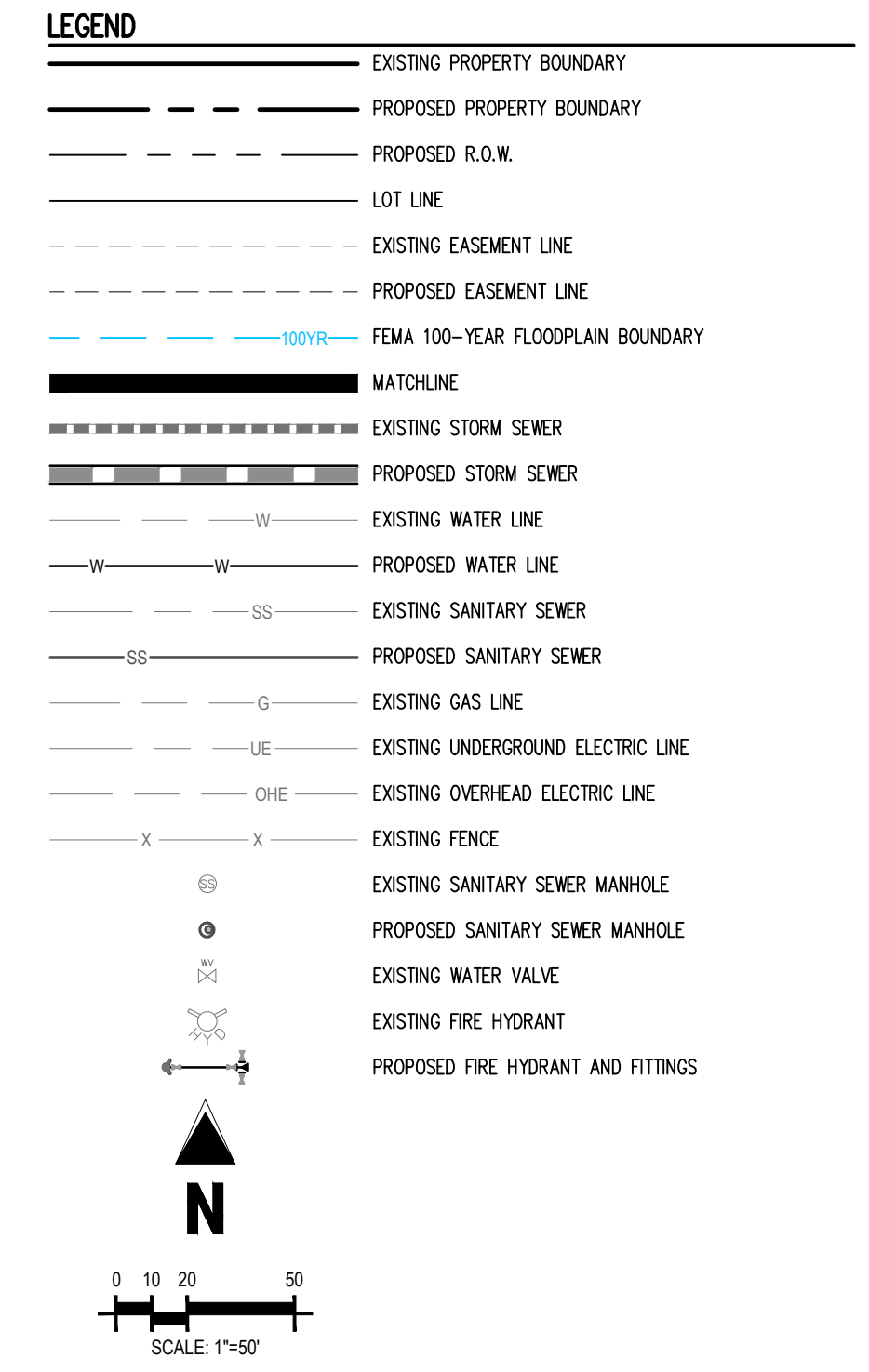
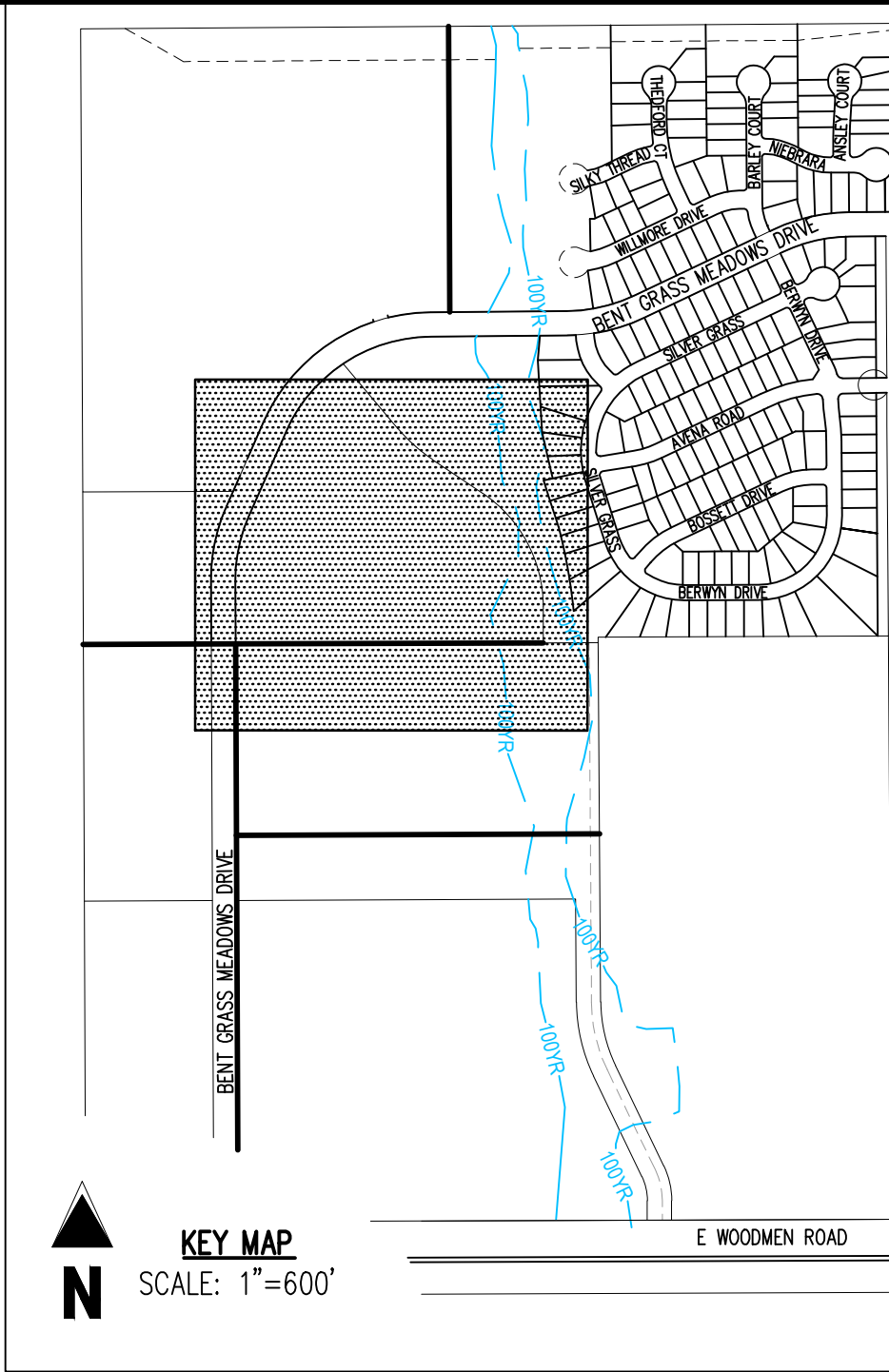
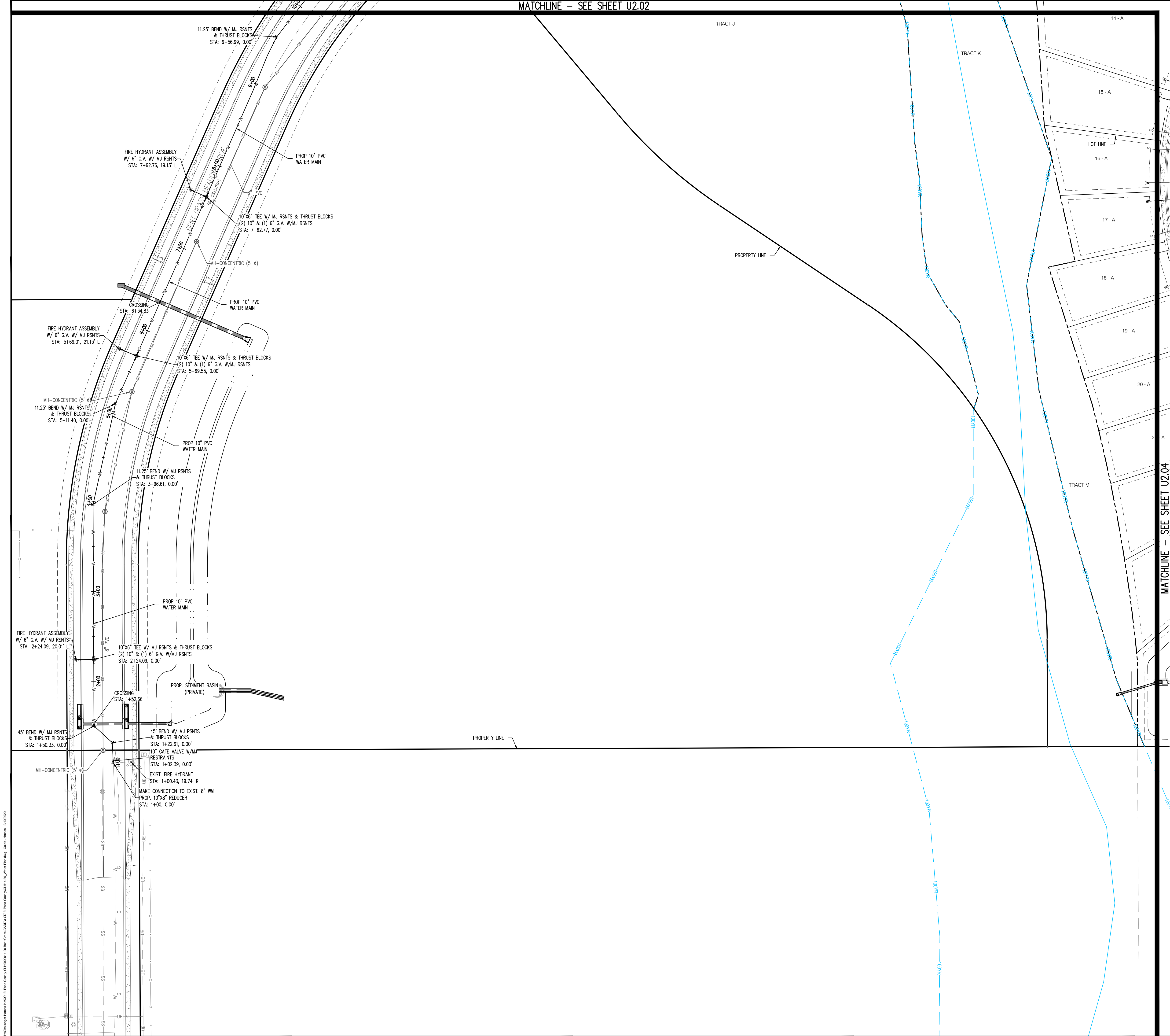


CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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SANITARY LAYOUT PLAN



LEGAL DESCRIPTION
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CHALLENGER HOMES

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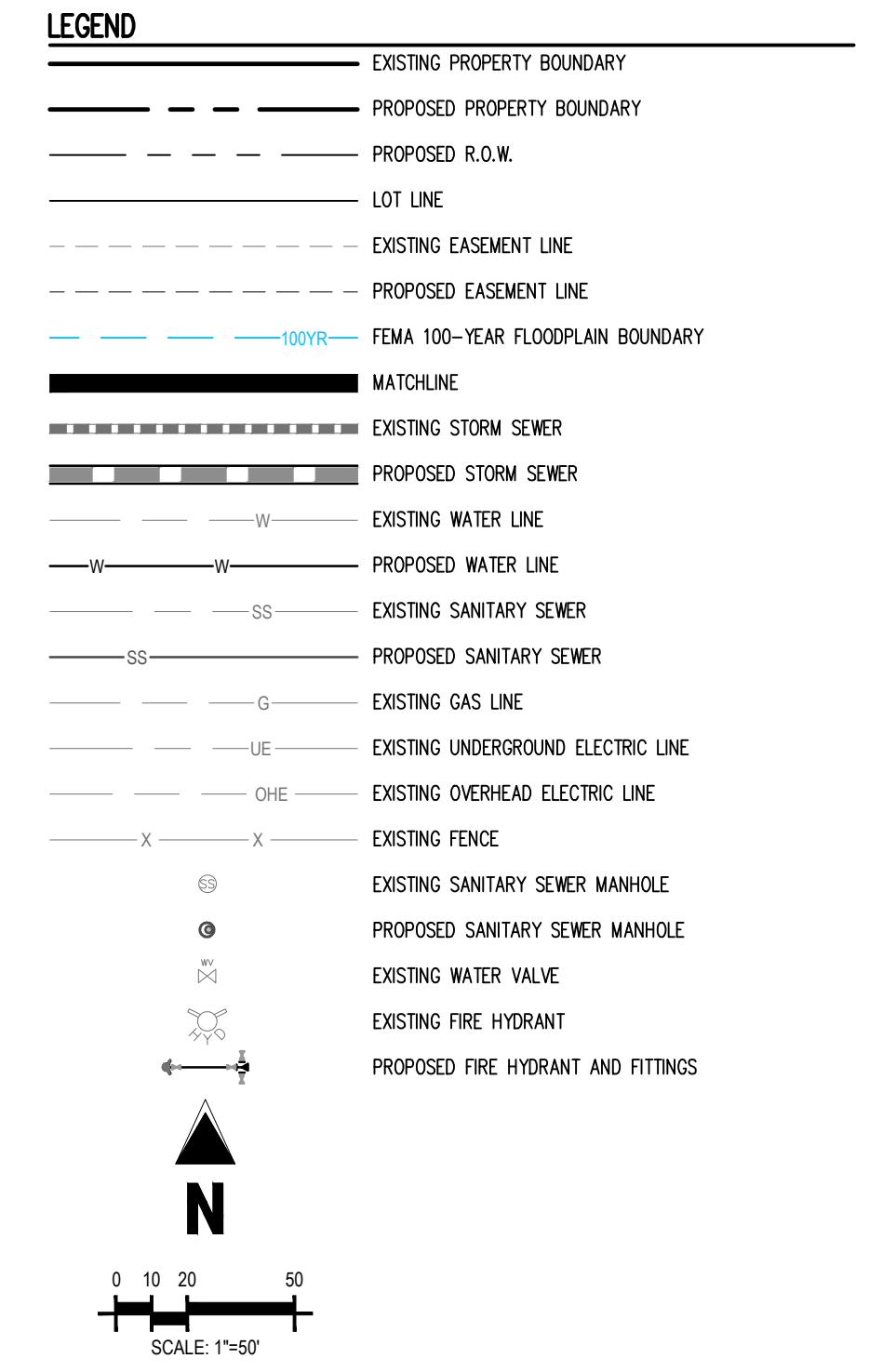
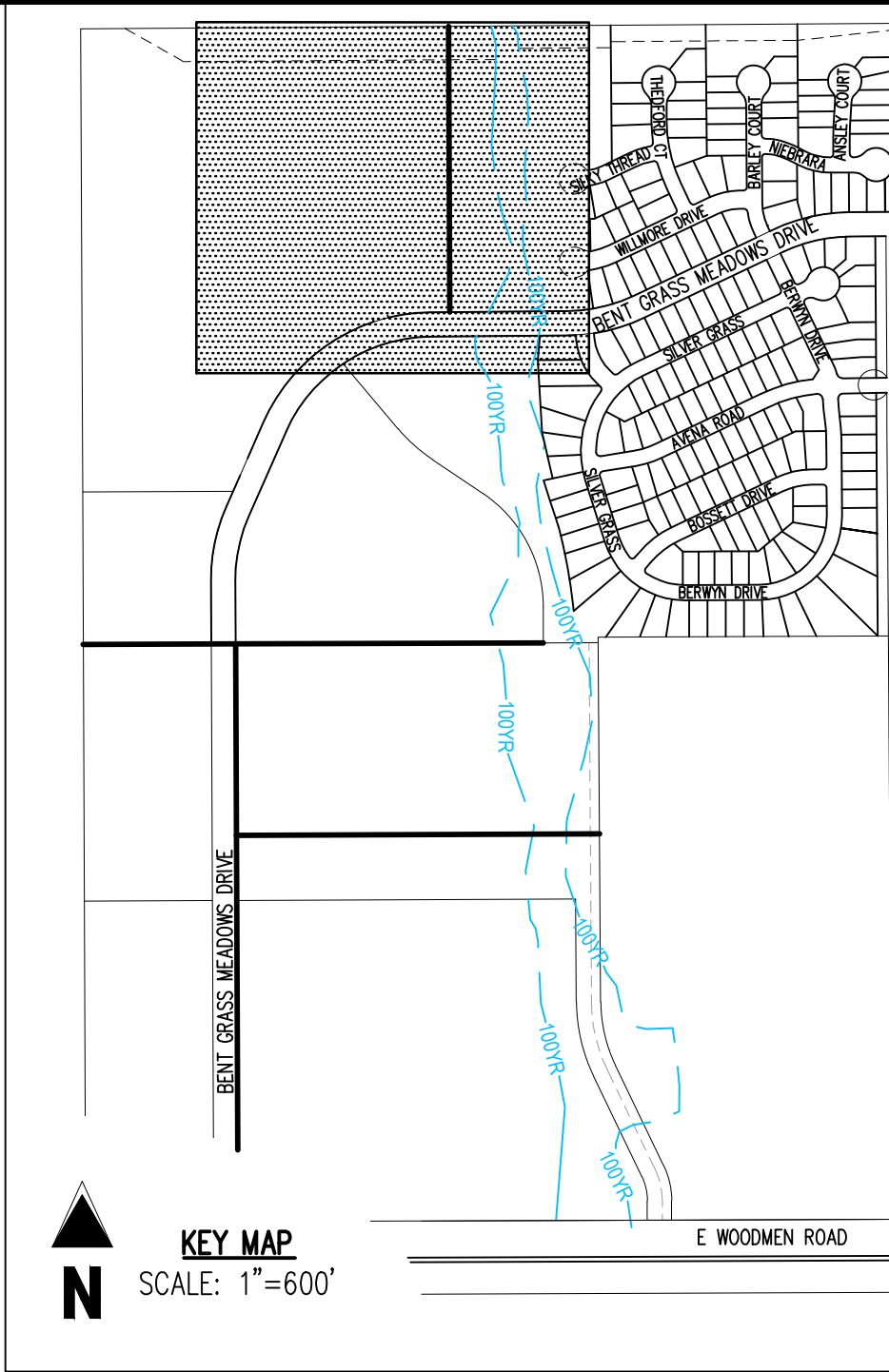
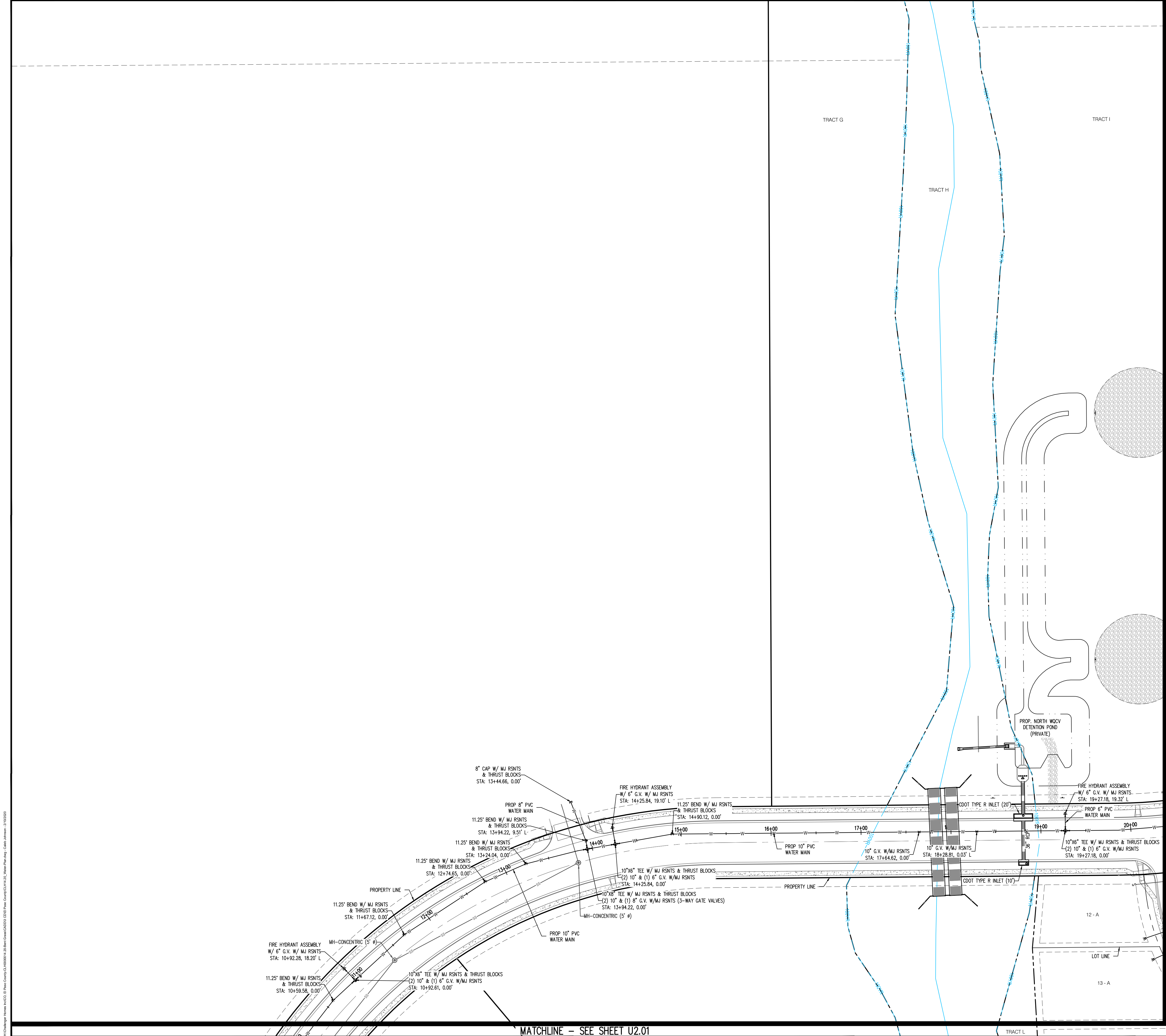
Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

WATER LAYOUT PLAN

U2.01

Sheet 9 of 32

C:\Projects\Bent Grass\Bent Grass Residential Filing No. 2\Drawings\U2.02\Bent Grass Residential Filing No. 2 - Water Layout.dwg, 02/10/2020, 11:10:20, JDP



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CHALLENGER HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

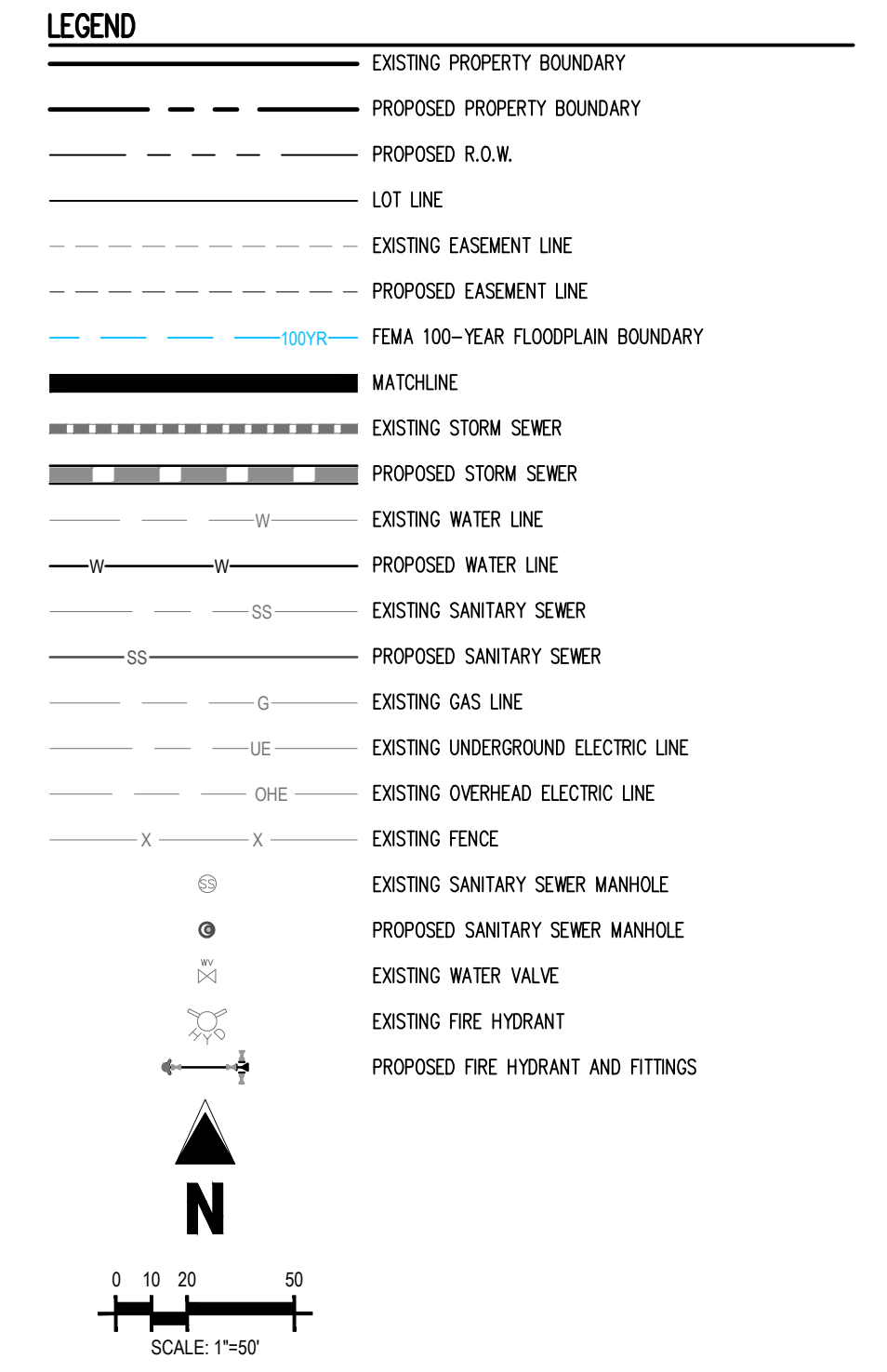
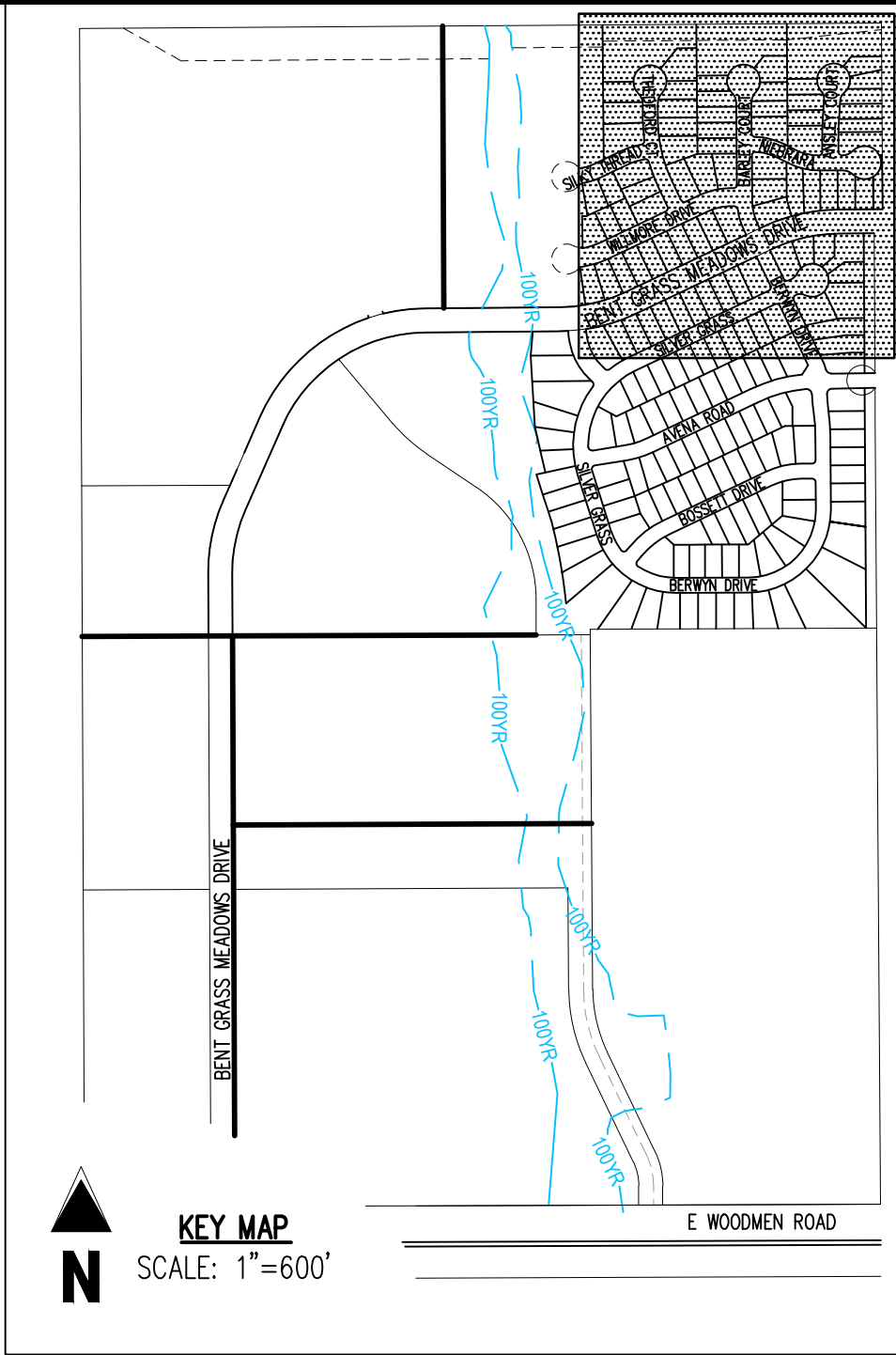
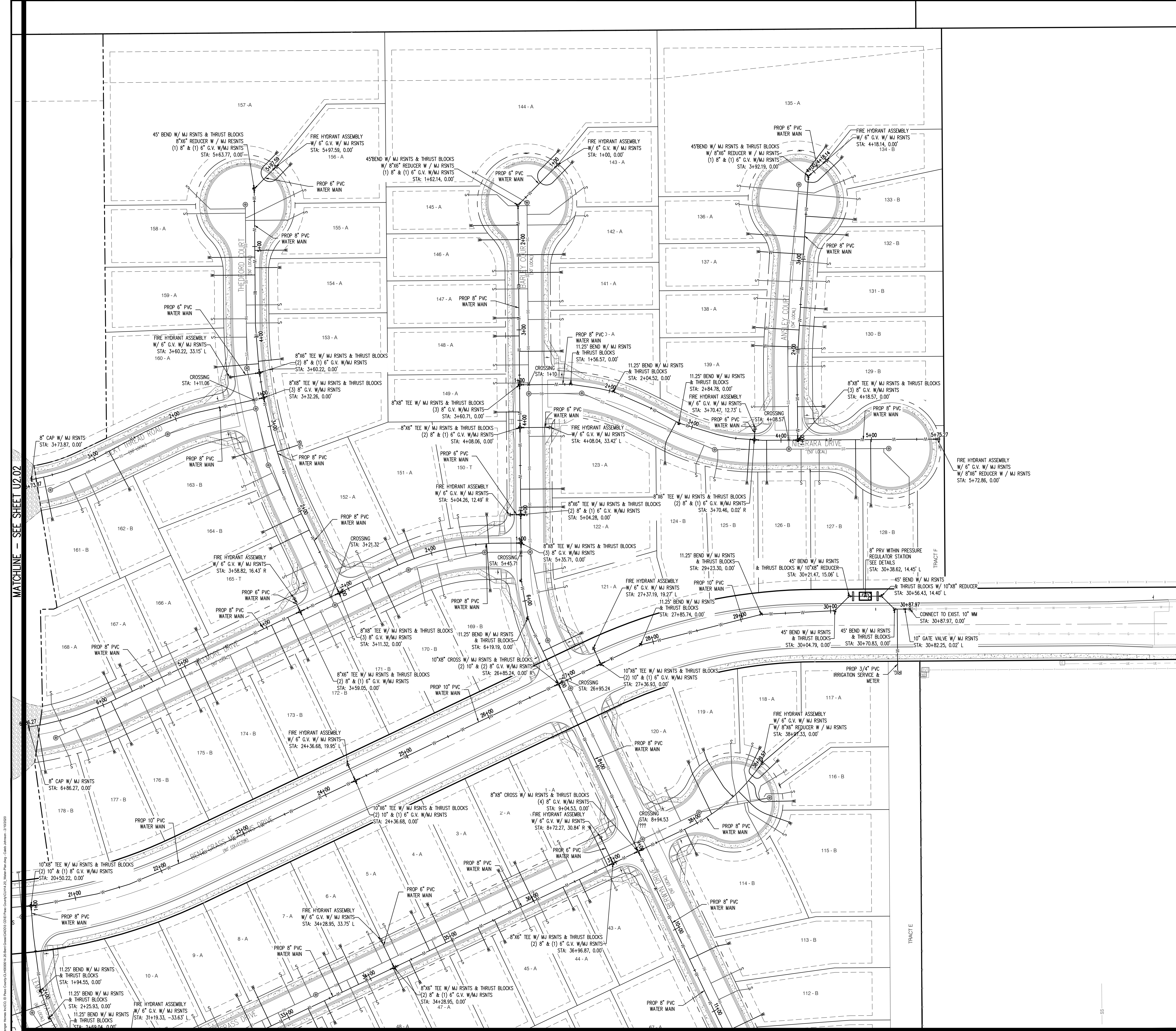
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Project No: CLH000014.20
Drawn By: JDP
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Date: 2/10/2020

WATER LAYOUT PLAN

U2.02

Sheet 10 of 32



LEGAL DESCRIPTION
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CHALLENGER HOMES

CONSTRUCTION DOCUMENTS
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FOR
CHALLENGER COMMUNITIES, LLC

BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

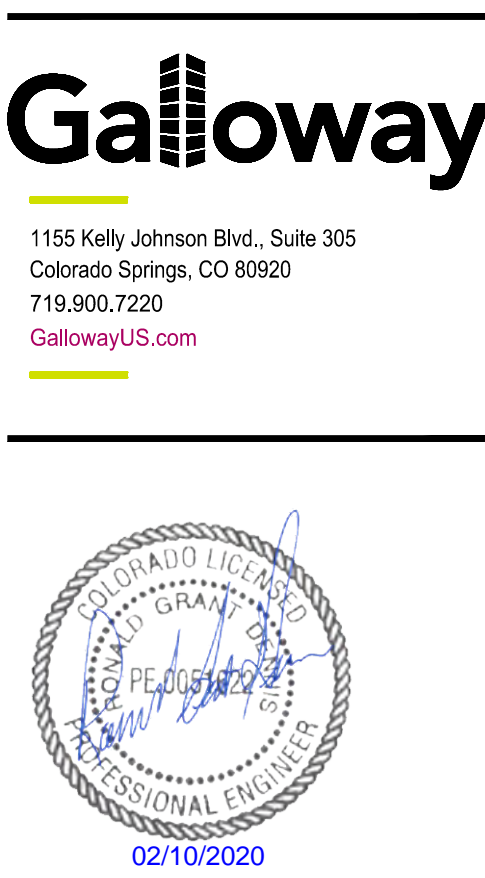
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Project No: CLH000014.20
Drawn By: JDP
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WATER LAYOUT PLAN

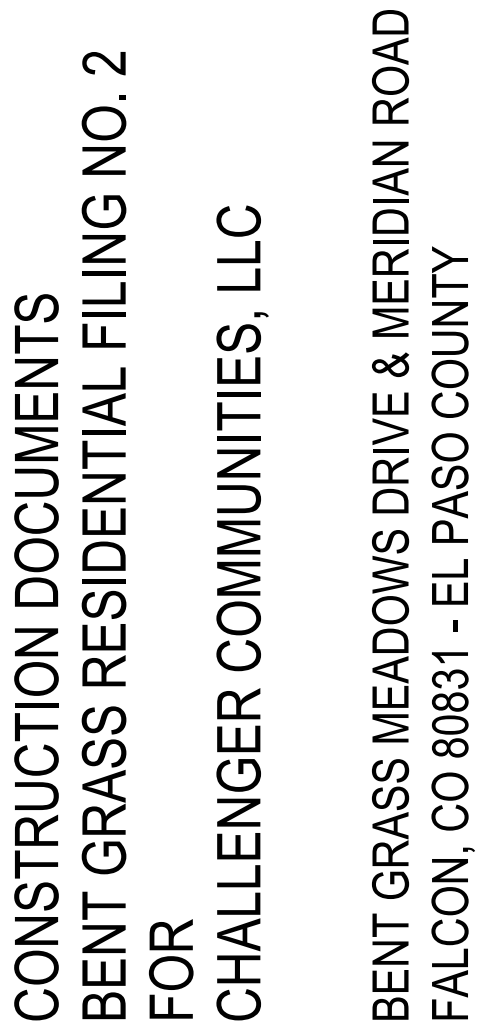
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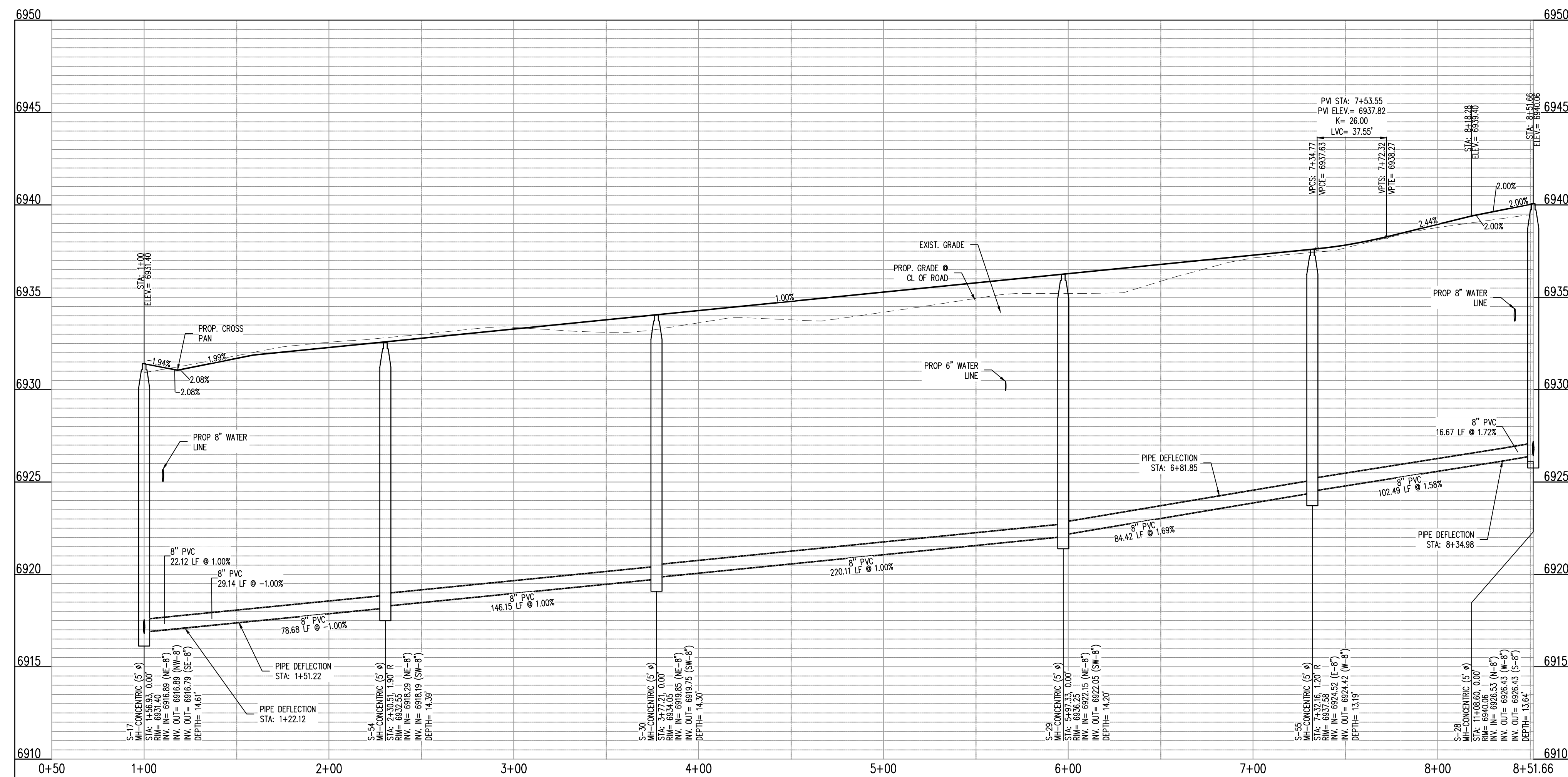
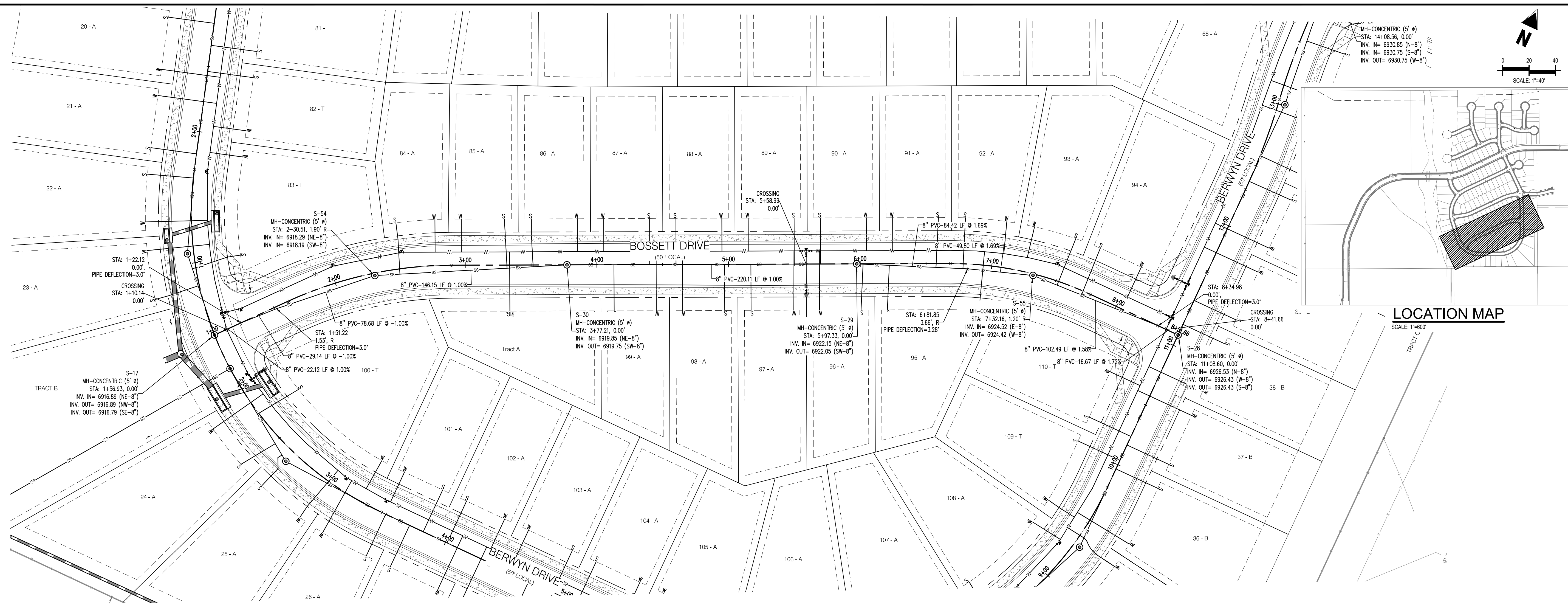
CHALLENGER HOMES

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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

AVENA ROAD SANITARY PLAN & PROFILE

U3.01
Sheet 13 of 32



BOSSETT DR - CL - 50' ROW
STA: 0+50 - 8+51.66
SCALE: H: 1"=40' V: 1"=10'



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CHALLENGER HOMES

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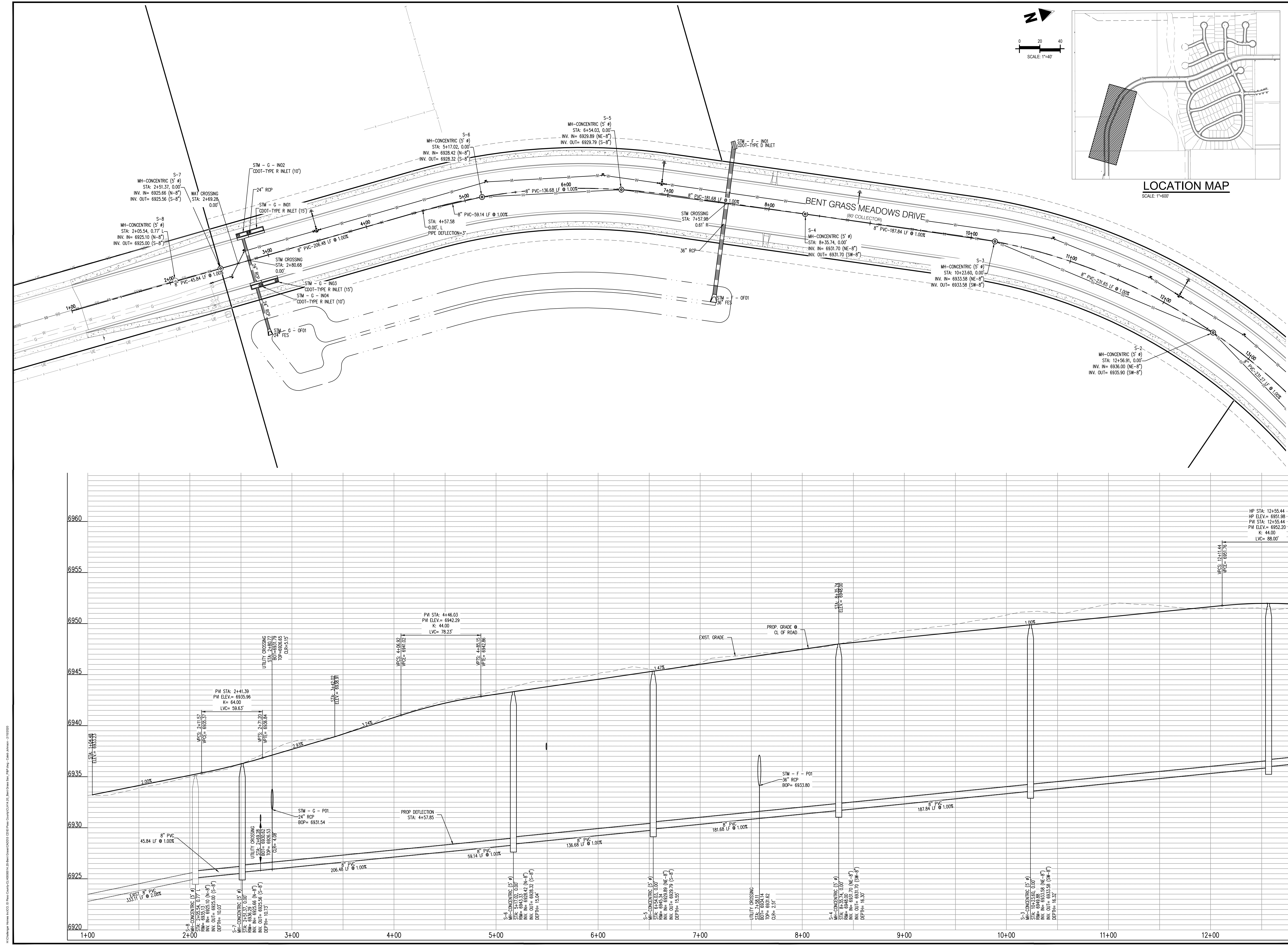
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No:	CLH000014.20
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Checked By:	RGD
Date:	2/10/2020

**BOSSETT DRIVE SANITARY
PLAN & PROFILE**

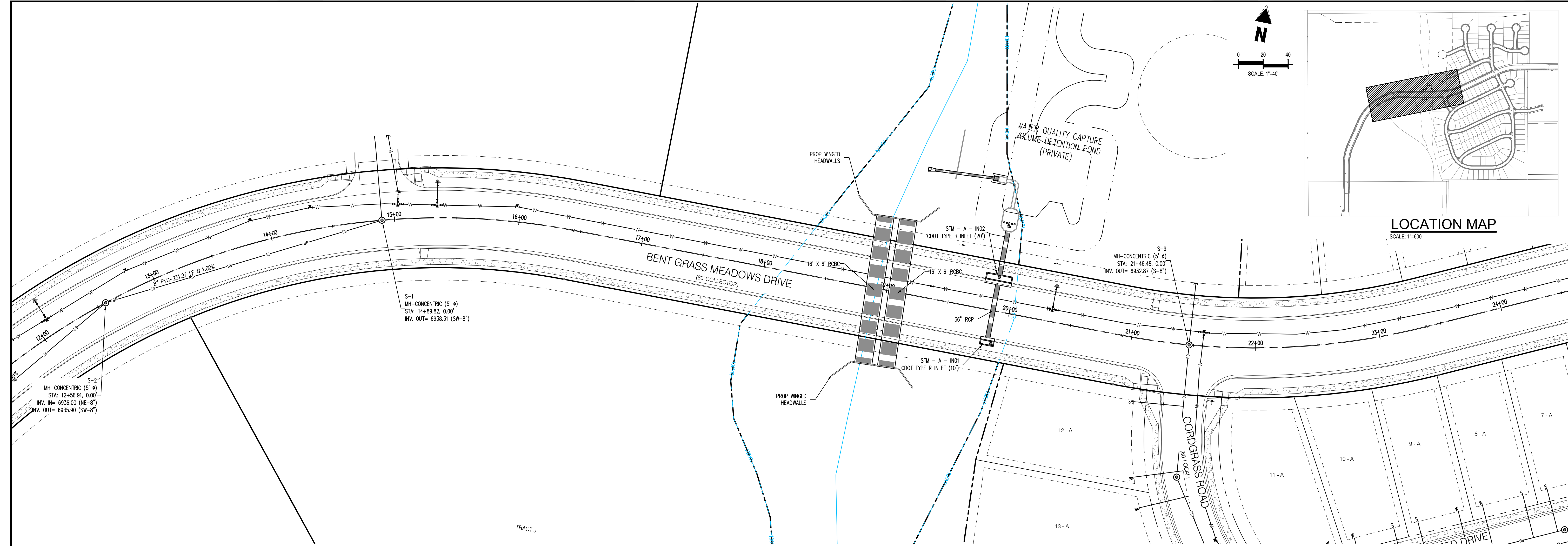
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BENT GRASS MEADOWS
DRIVE SANITARY PLAN &
PROFILE



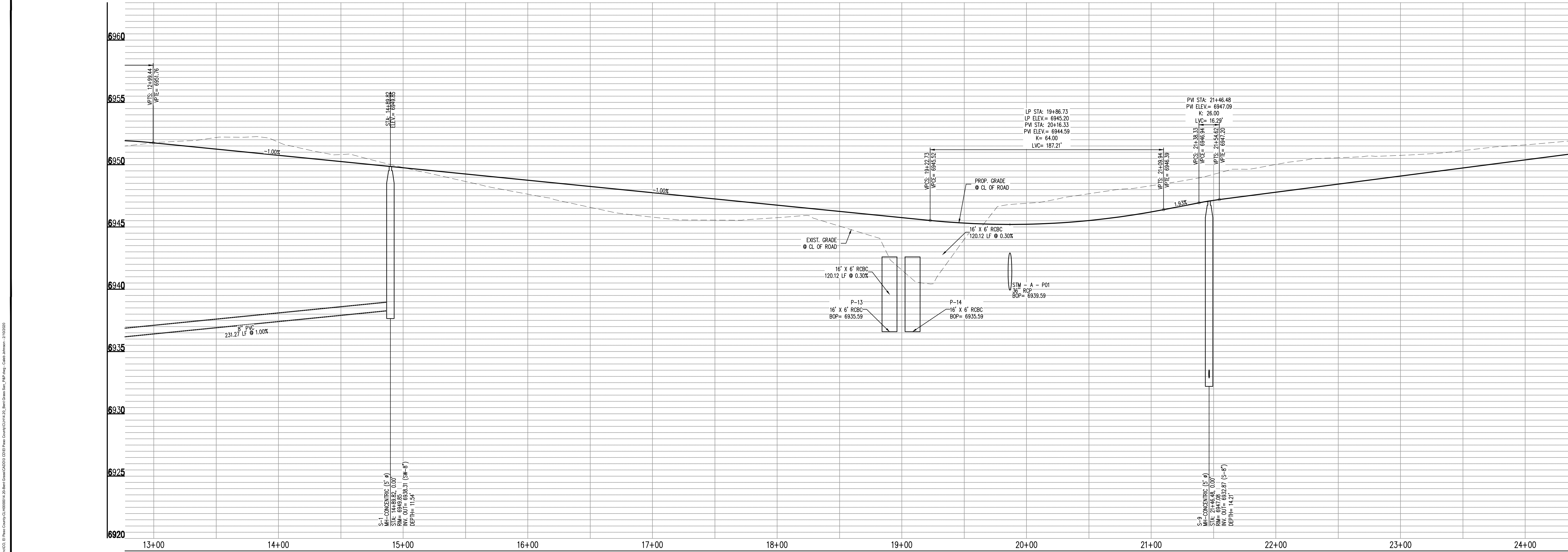
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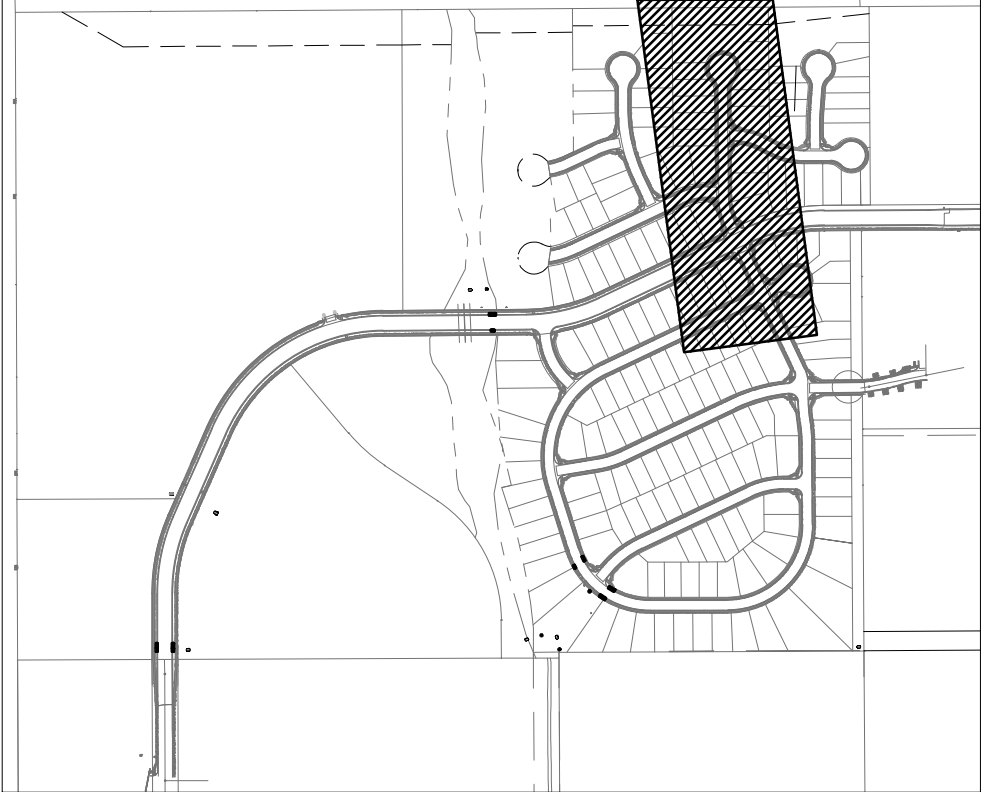
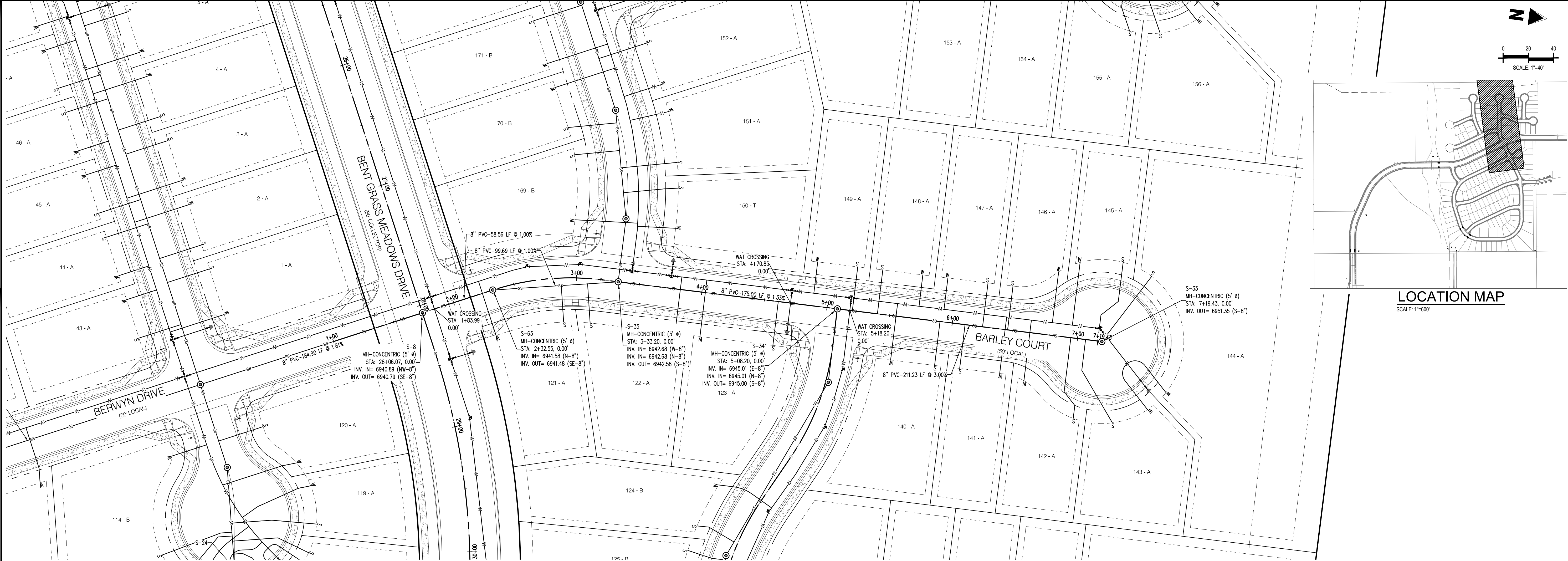


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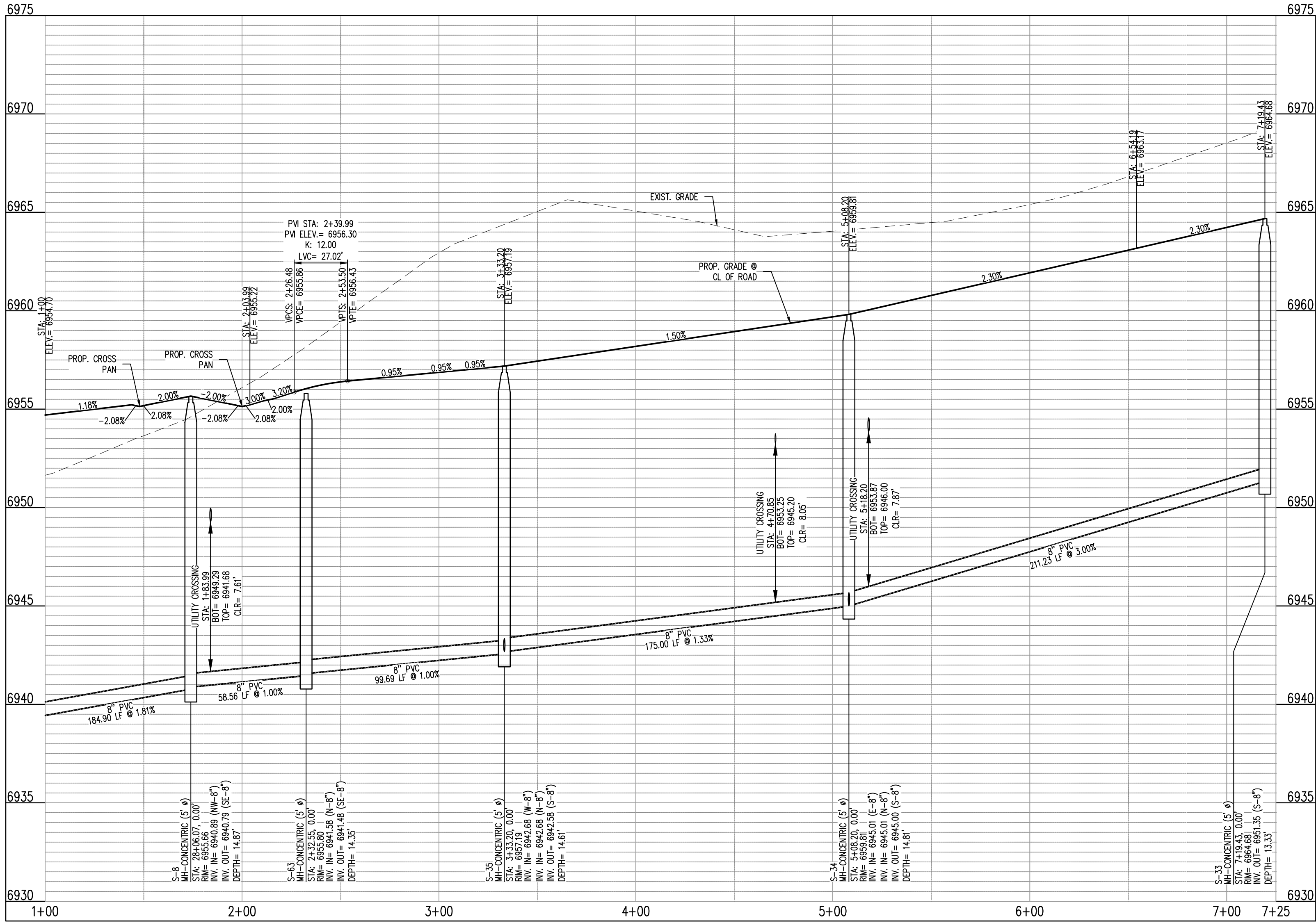
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Checked By: RGD
Date: 2/10/2020
BENT GRASS MEADOWS
DRIVE SANITARY PLAN &
PROFILE





LOCATION MAP
SCALE: 1"=600'

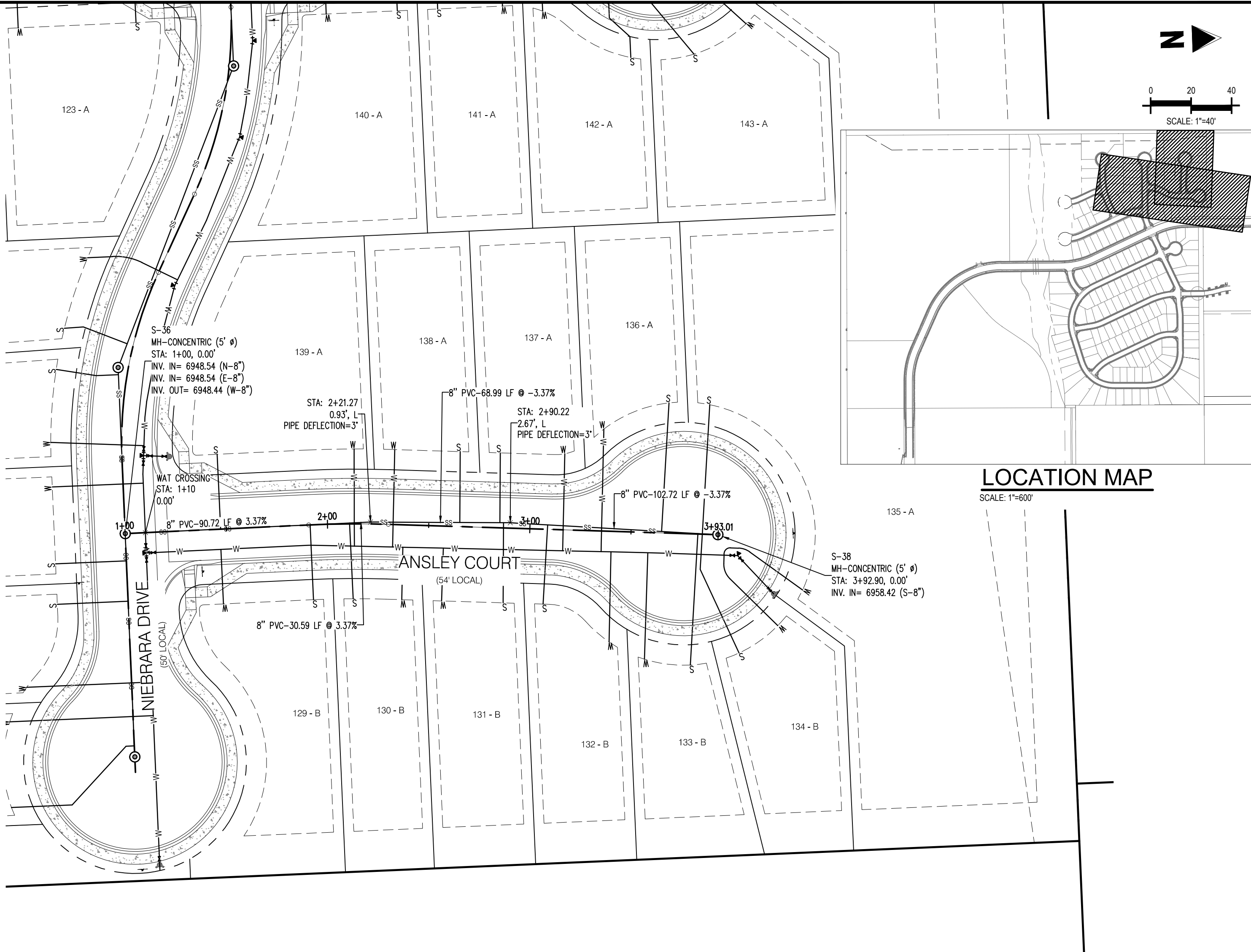
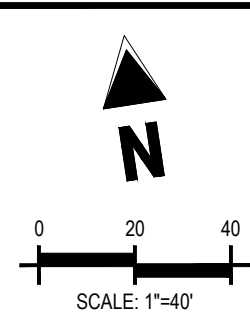
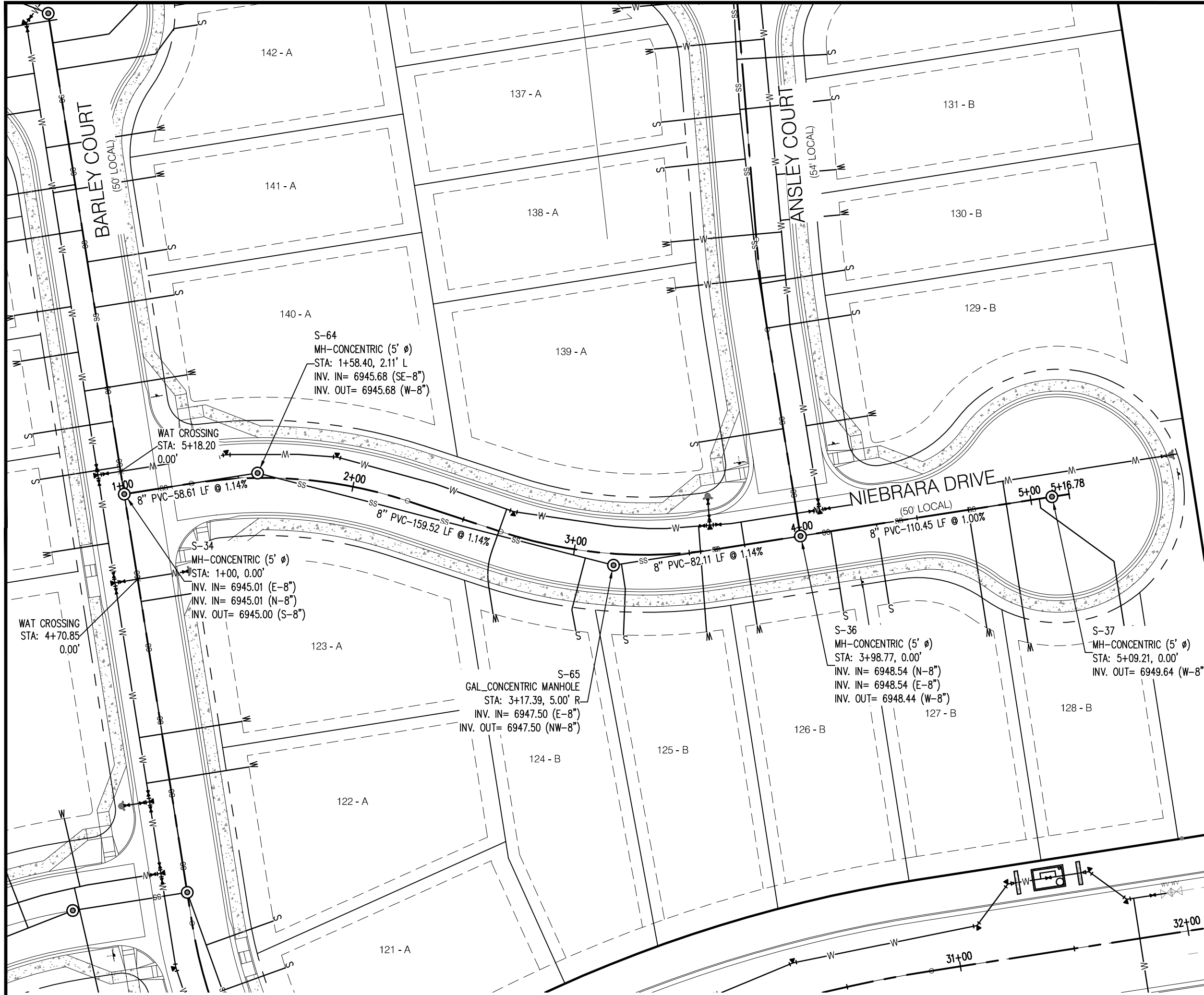


BARLEY COURT - CL - 50' ROW
STA: 1+00 - 7+25
SCALE: H: 1"=40' V: 1"=10'

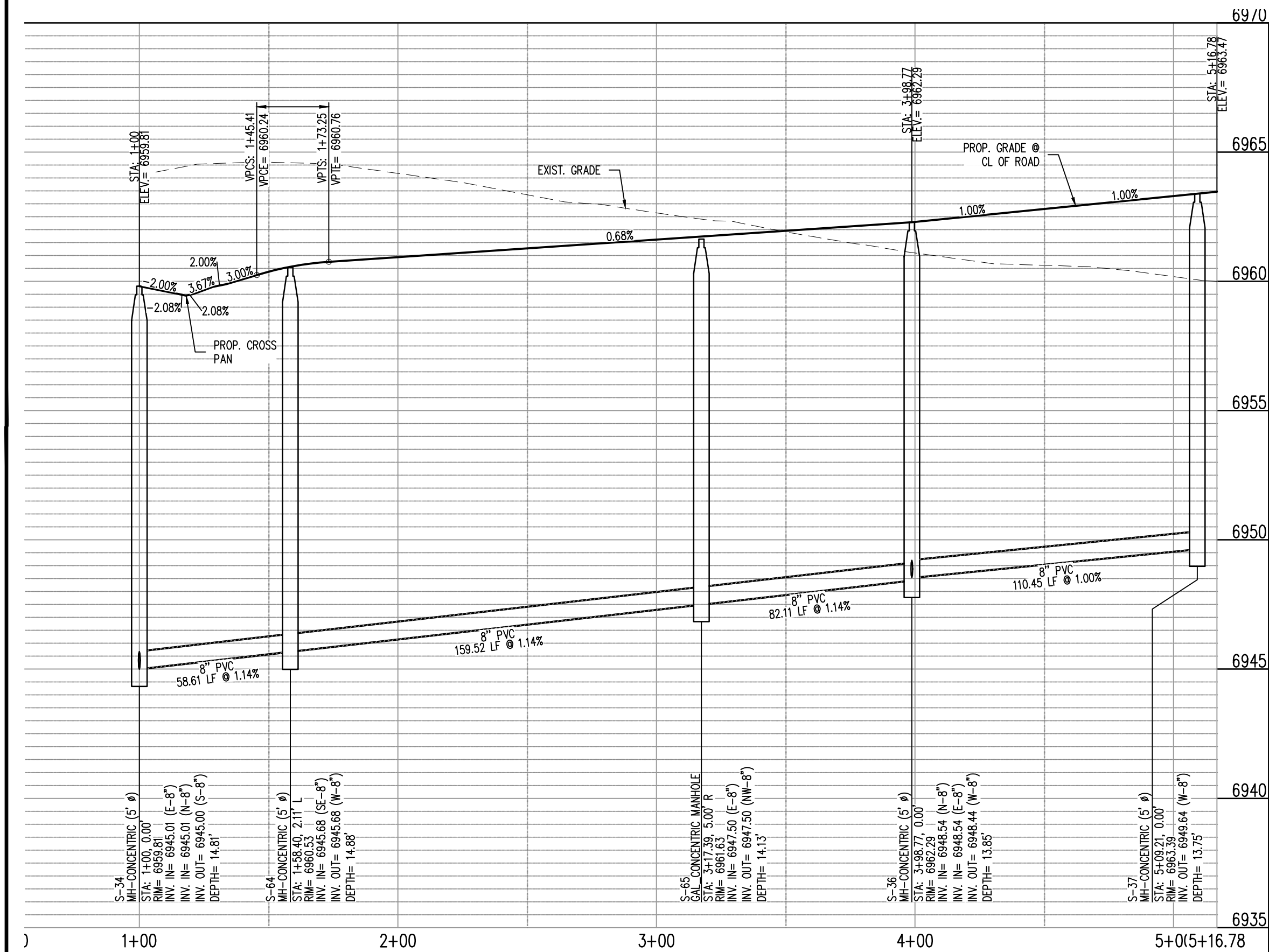
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Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

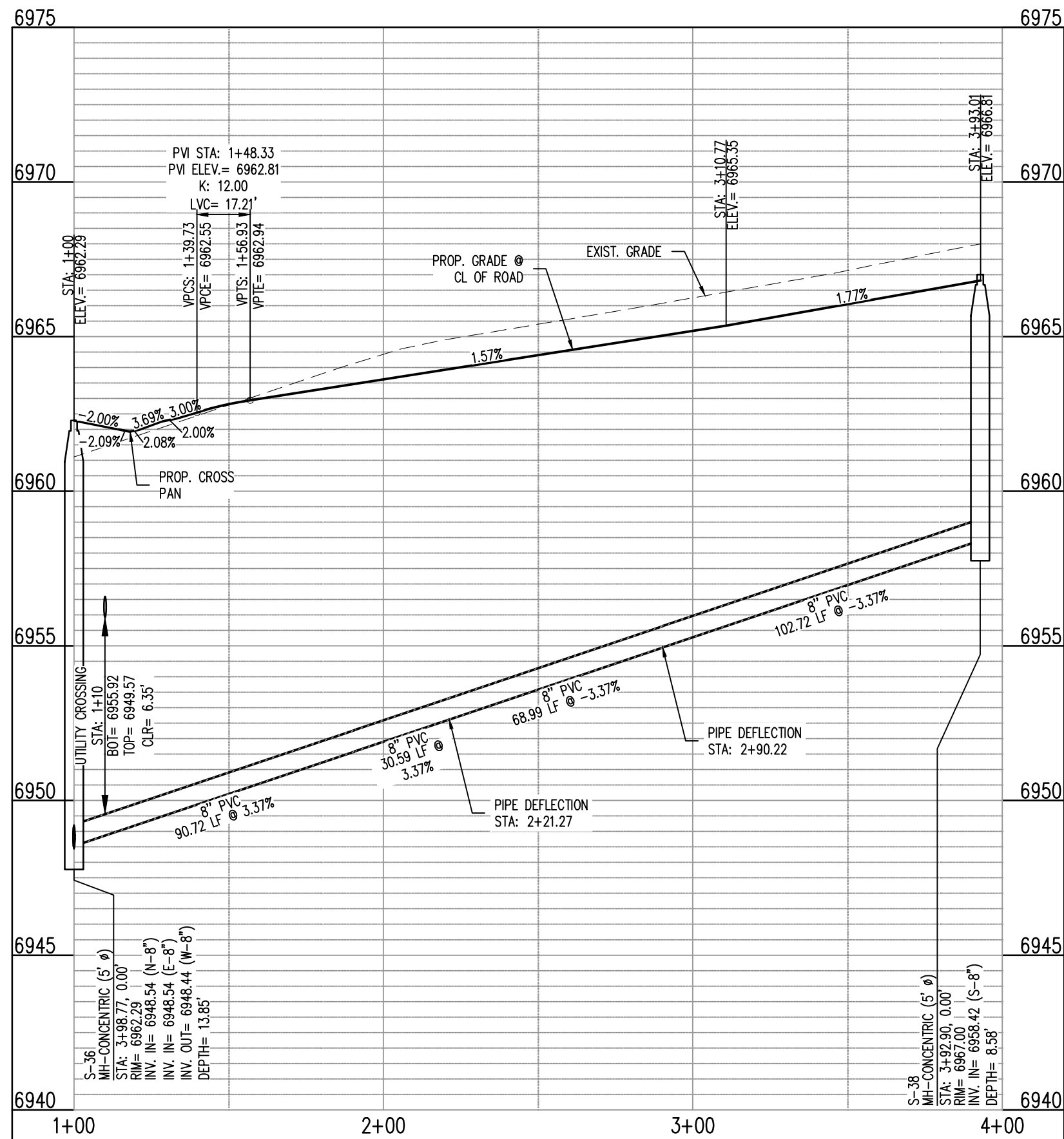
BARLEY COURT SANITARY
PLAN & PROFILE



LOCATION MAP
SCALE: 1"=600'



NIEBRARA - CL - 50' ROW
STA: 0+50 - 5+16.78
SCALE: H: 1"=40' V: 1"=10'



ANSLEY CT - CL - 50' ROW
STA: 1+00 - 4+00
SCALE: H: 1"=40' V: 1"=10'

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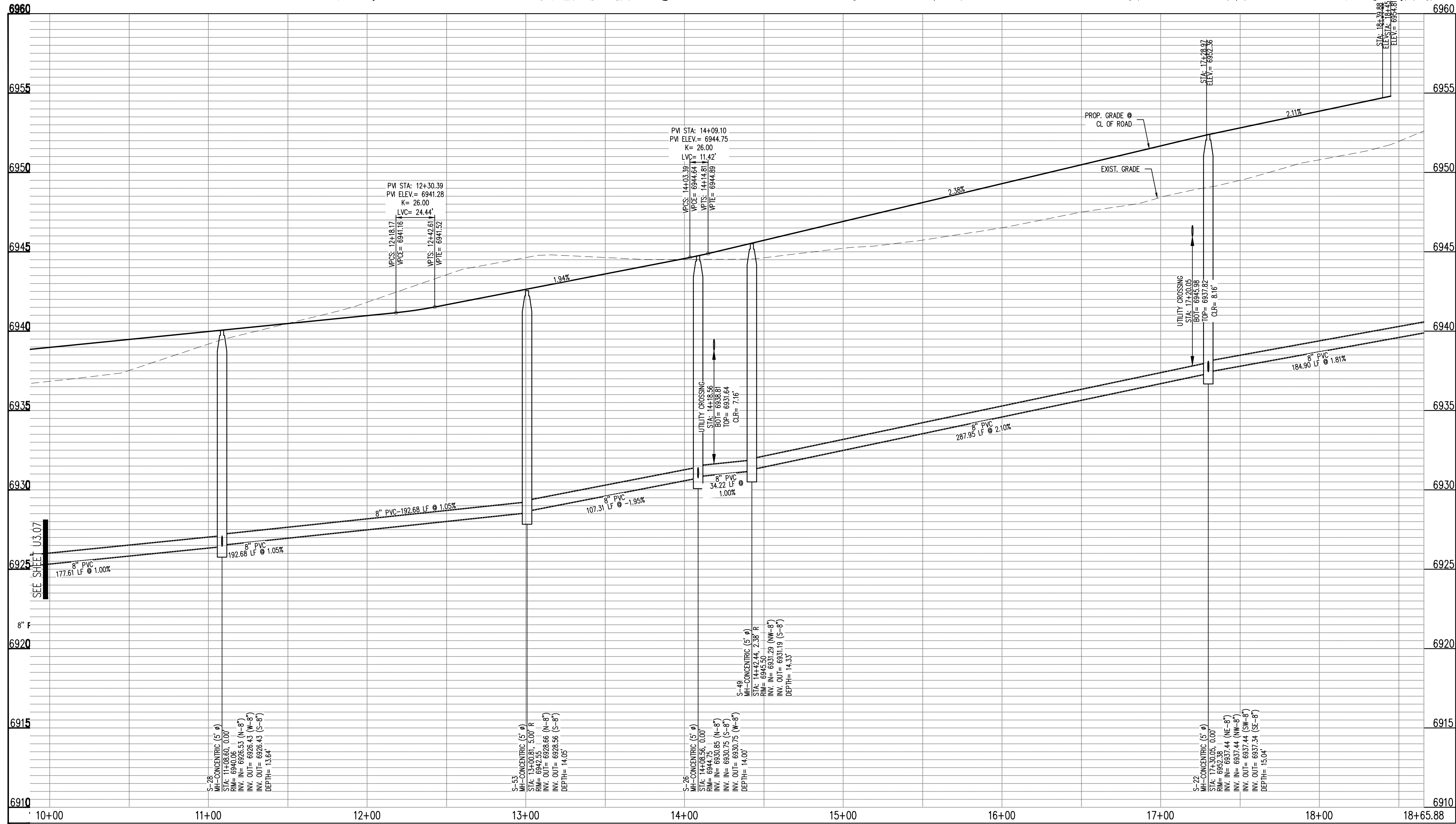
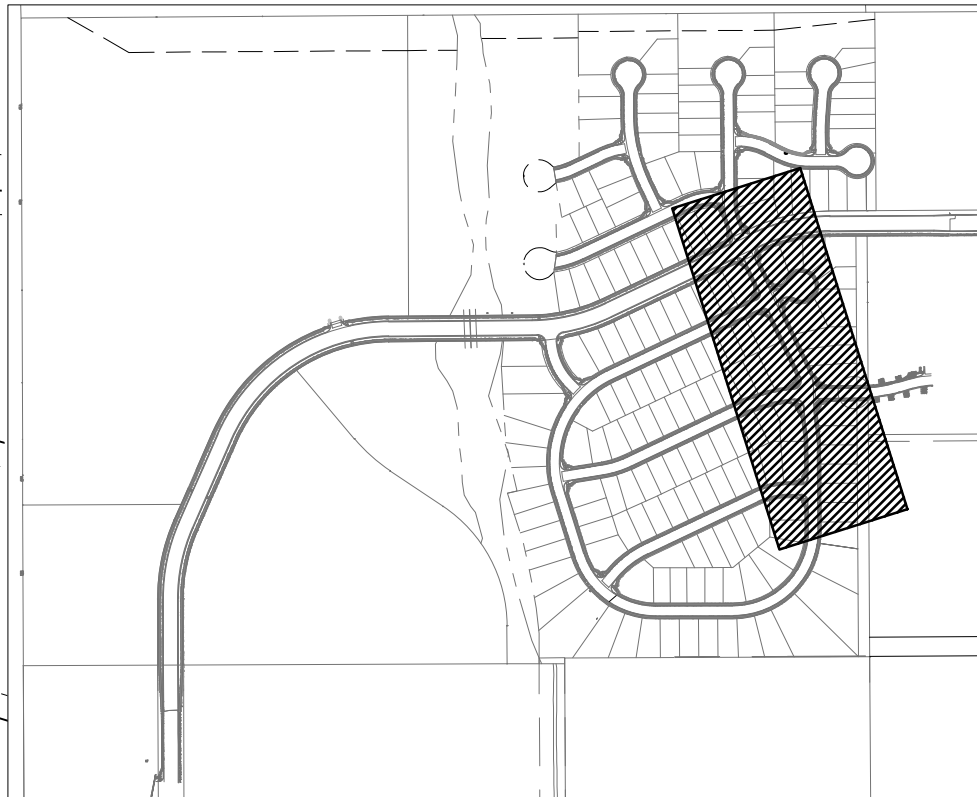
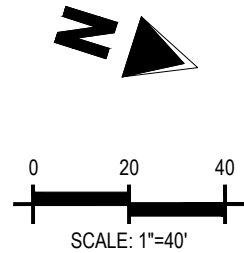
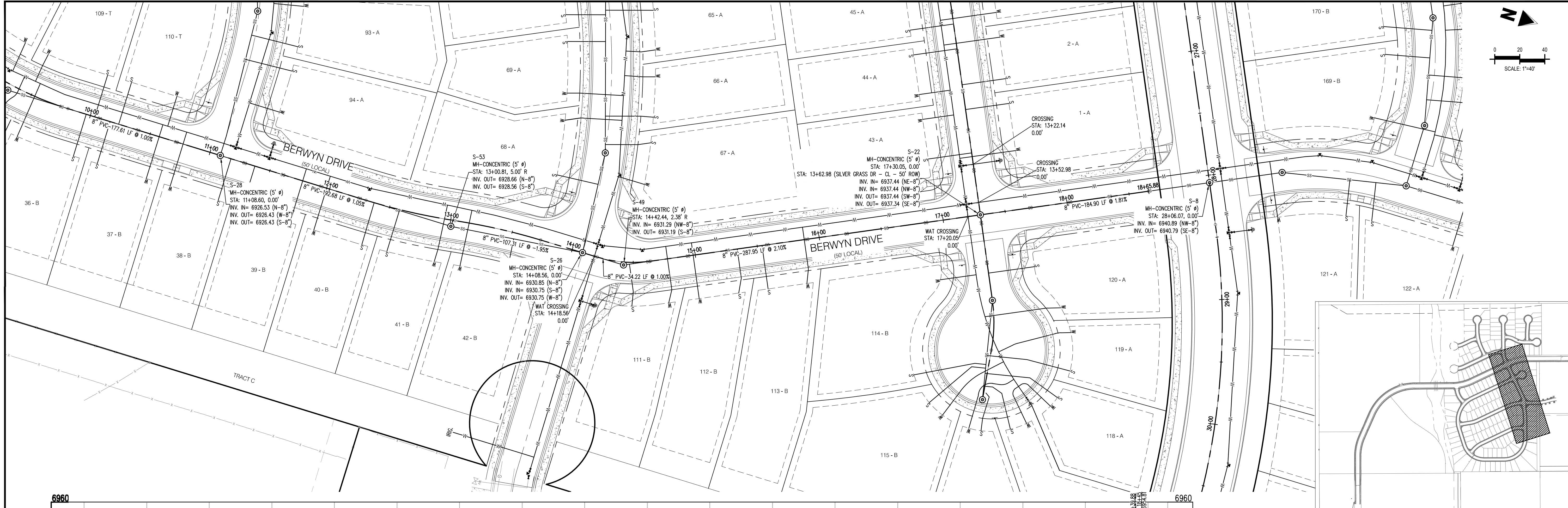
**CHALLENGER
HOMES**

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

NIEBRARA DRIVE &
ANSLEY COURT SANITARY
PLAN & PROFILE



BERWYN DR SOUTH - CL - 50' ROW
STA: 10+00 - 18+65.88
SCALE: H: 1"=40' V: 1"=10'

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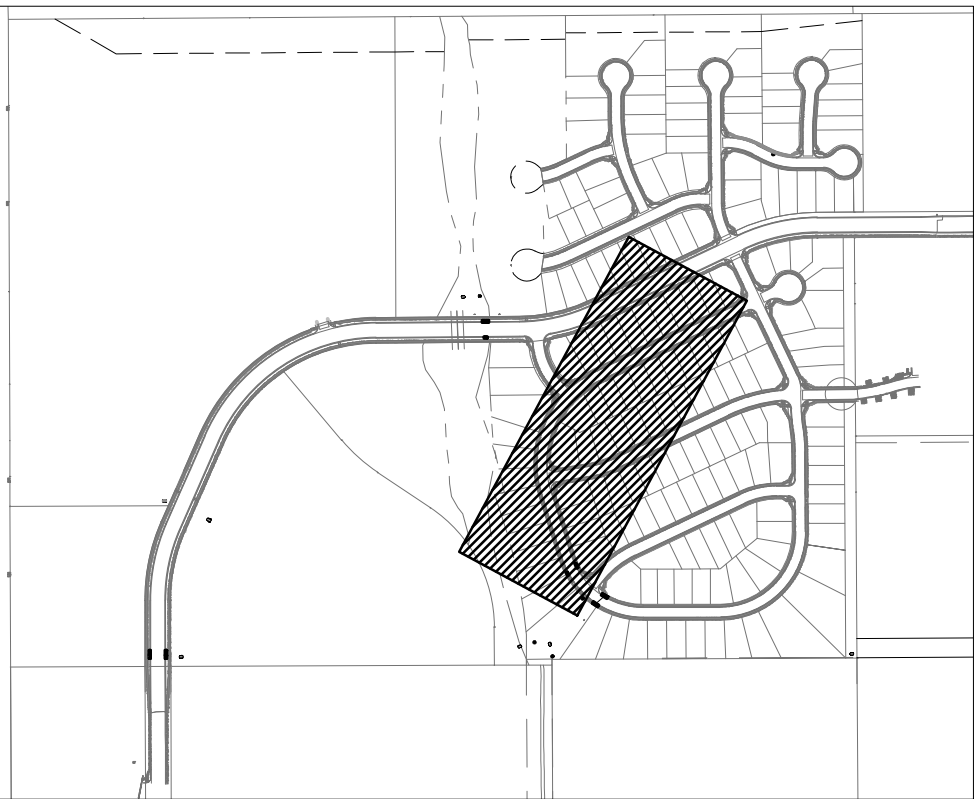
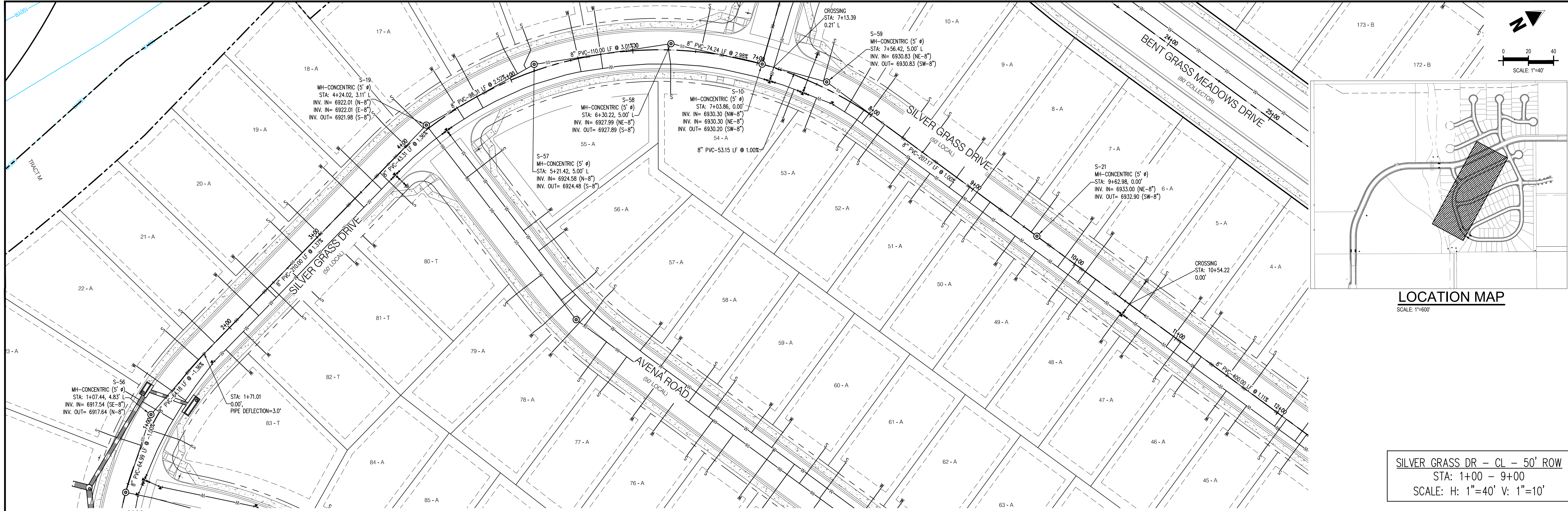
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CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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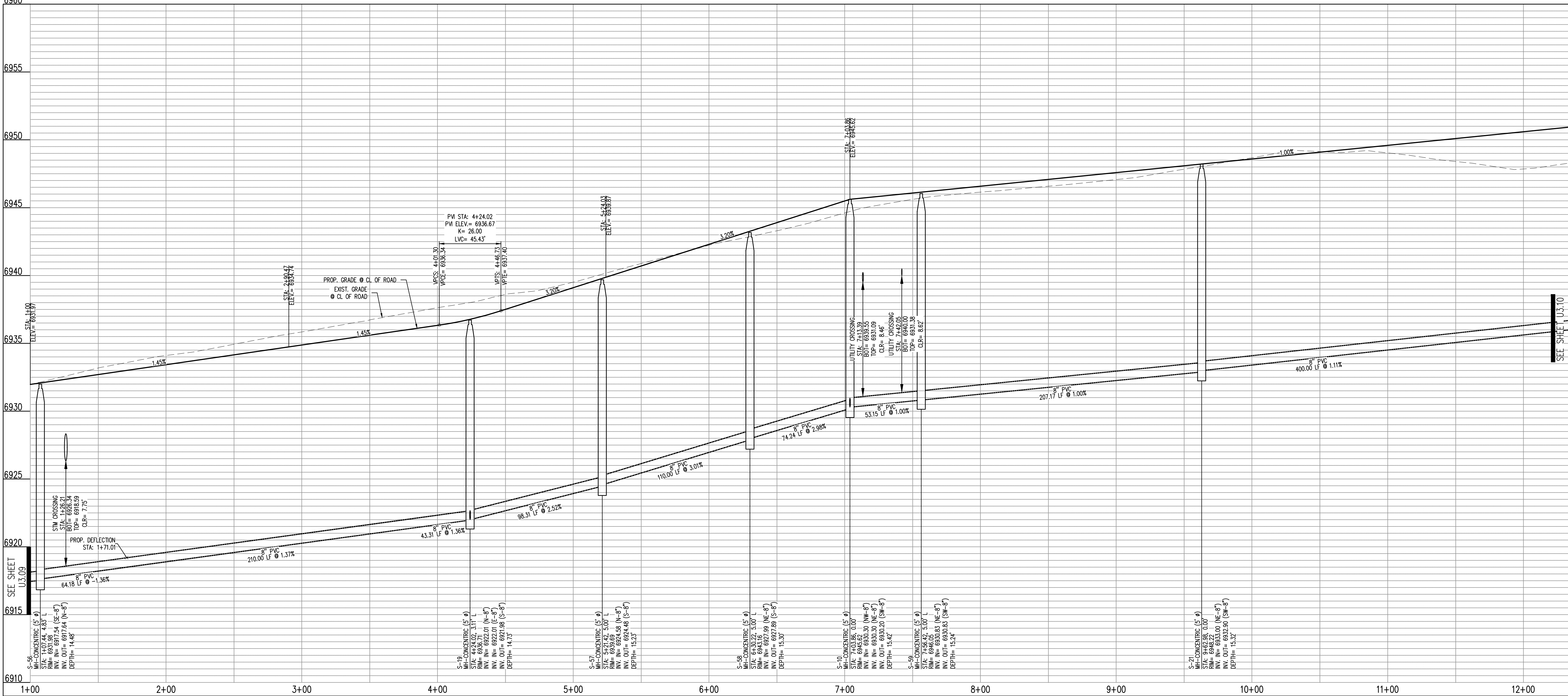
Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020
BERWYN DRIVE SANITARY
PLAN & PROFILE

U3.08



LOCATION MAP
SCALE: 1"=600'

SILVER GRASS DR - CL - 50' ROW
STA: 1+00 - 9+00
SCALE: H: 1"=40' V: 1"=10'



02/10/2020

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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

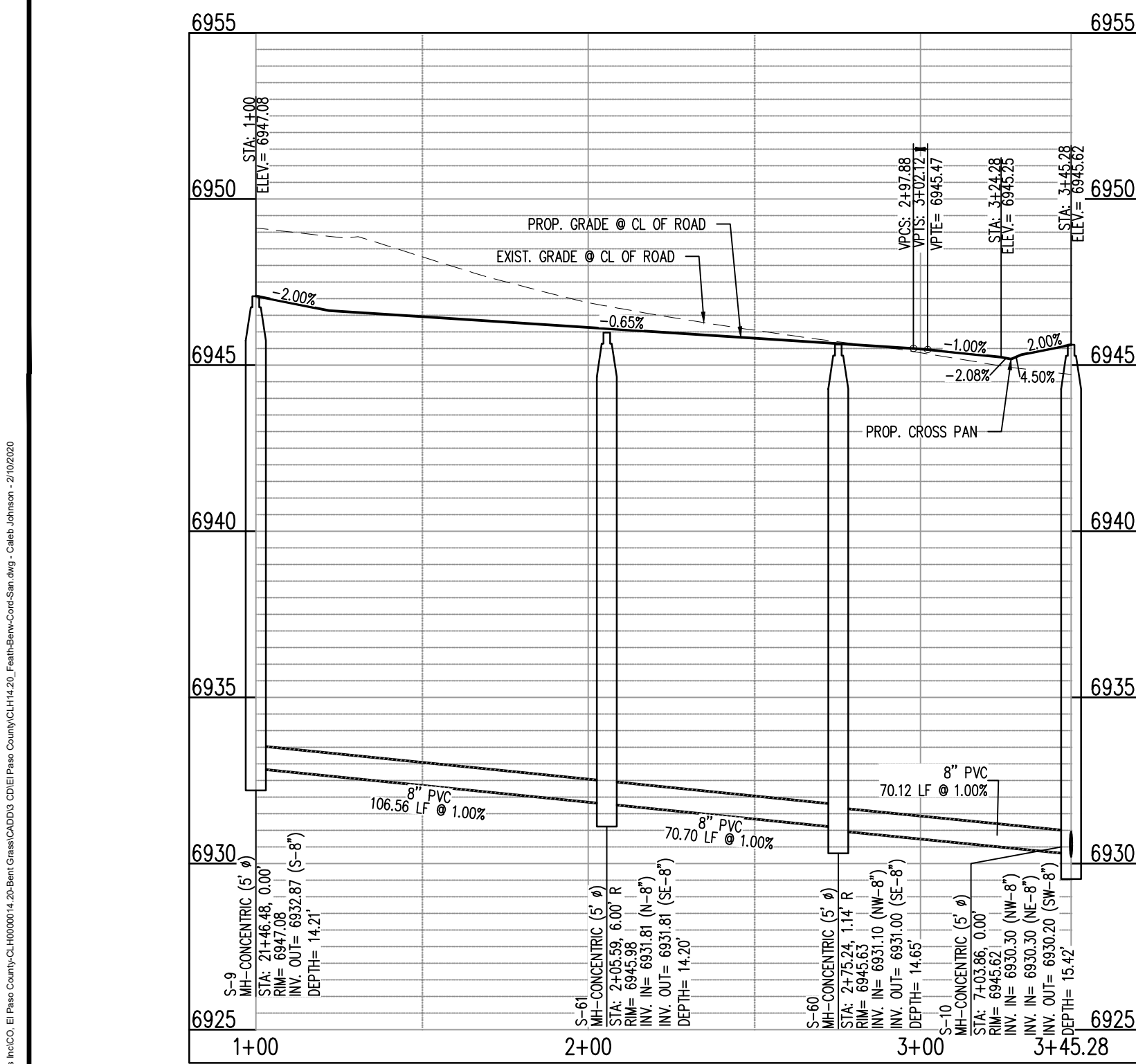
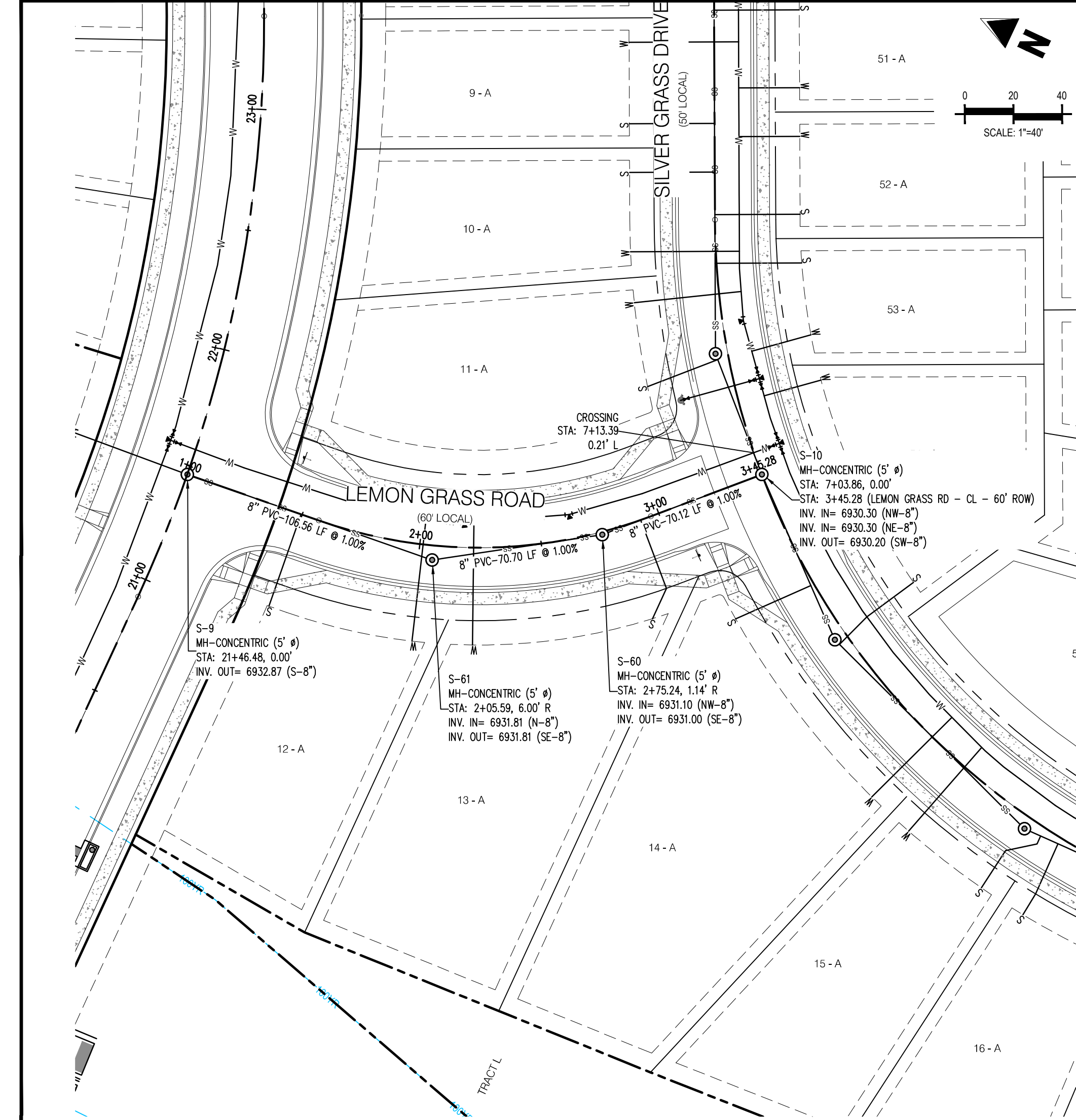
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Project No: CLH000014.20
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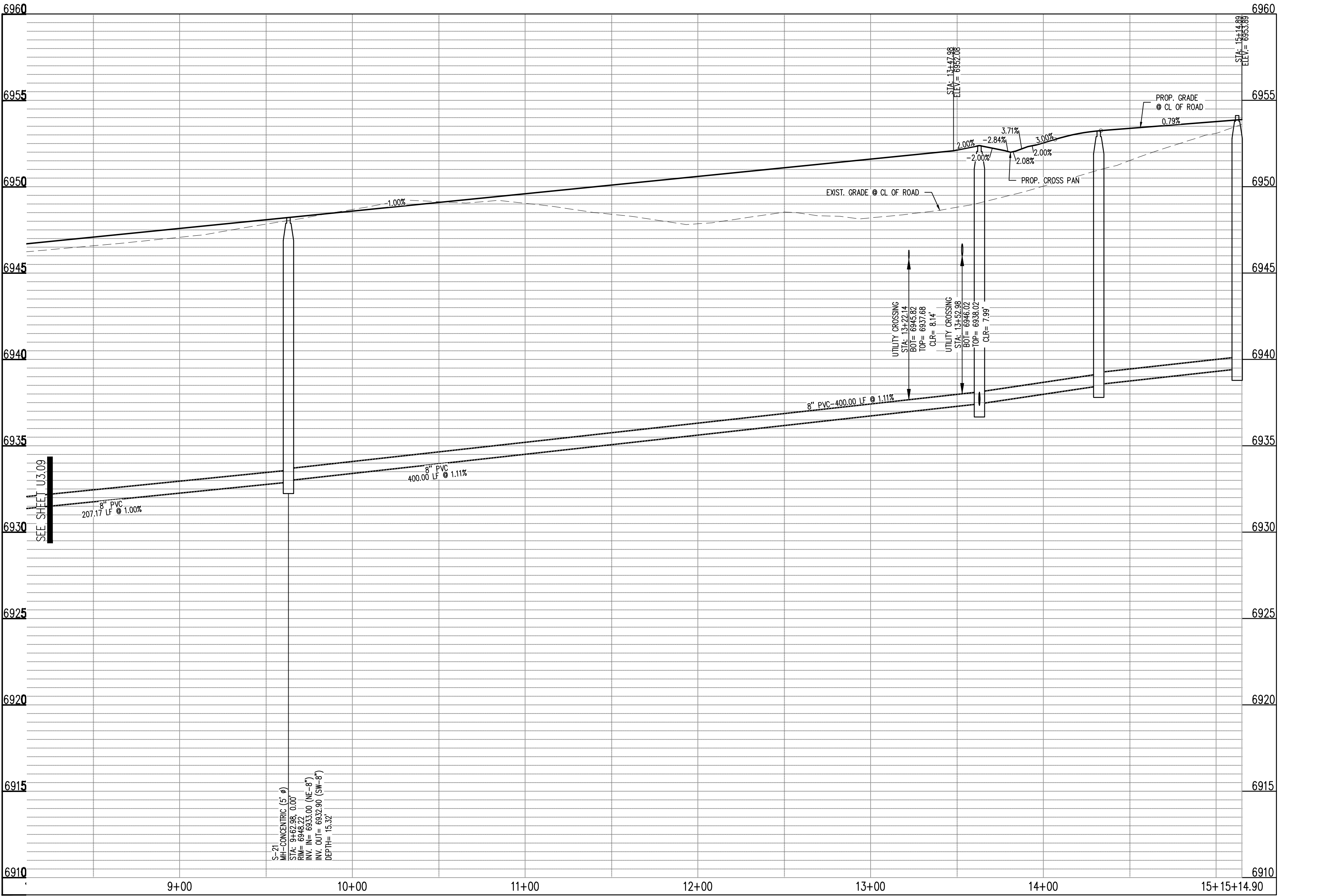
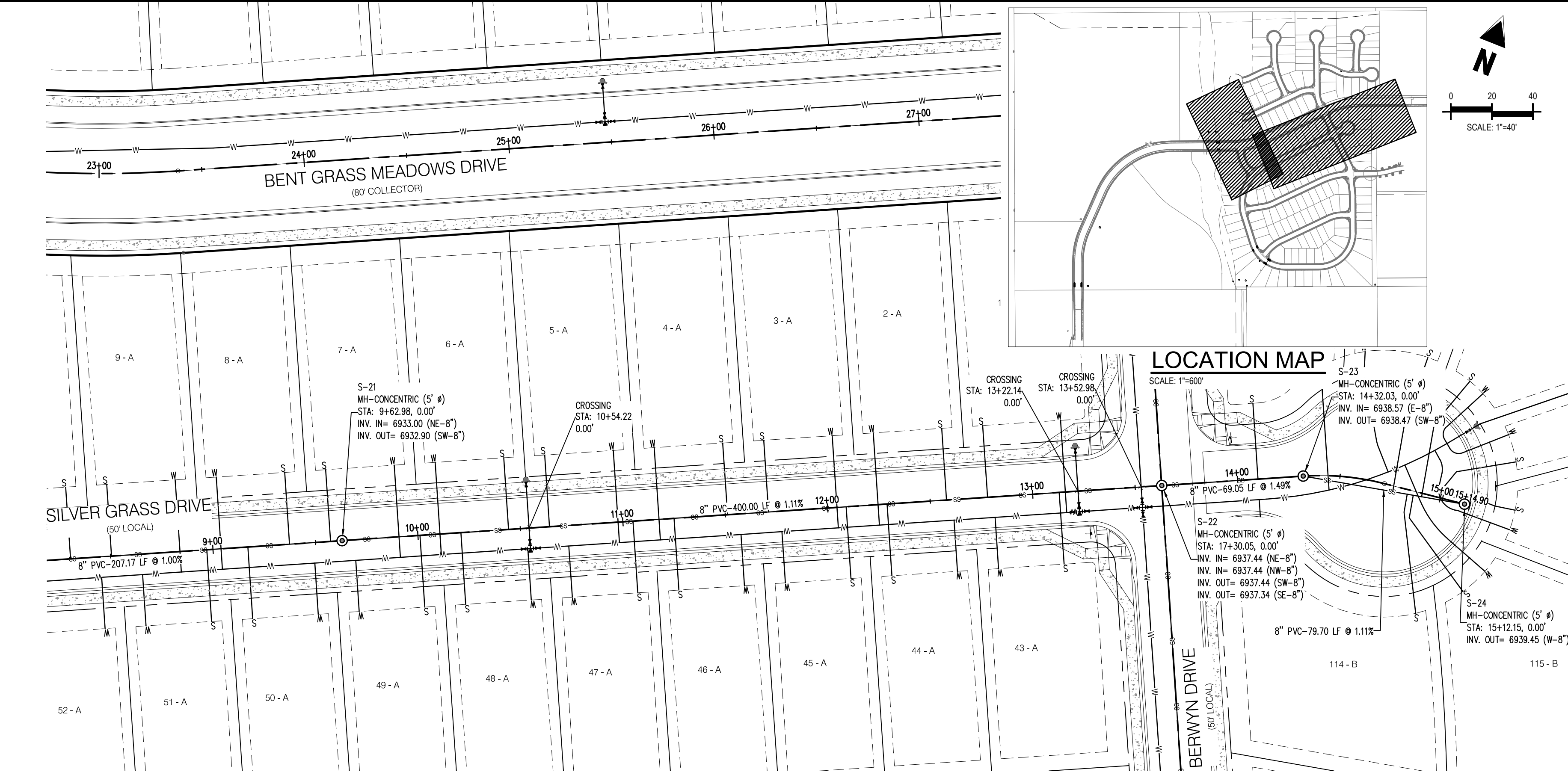
SILVER GRASS DRIVE
SANITARY PLAN & PROFILE

U3.09

Sheet 21 of 32



LEMON GRASS RD - CL - 60' ROW
STA: 1+00 - 3+45.28
SCALE: H: 1"=40' V: 1"=10'



SILVER GRASS DR - CL - 50' ROW, STA: 9+00 - 15+14.90, SCALE: H: 1"=40' V: 1"=10'

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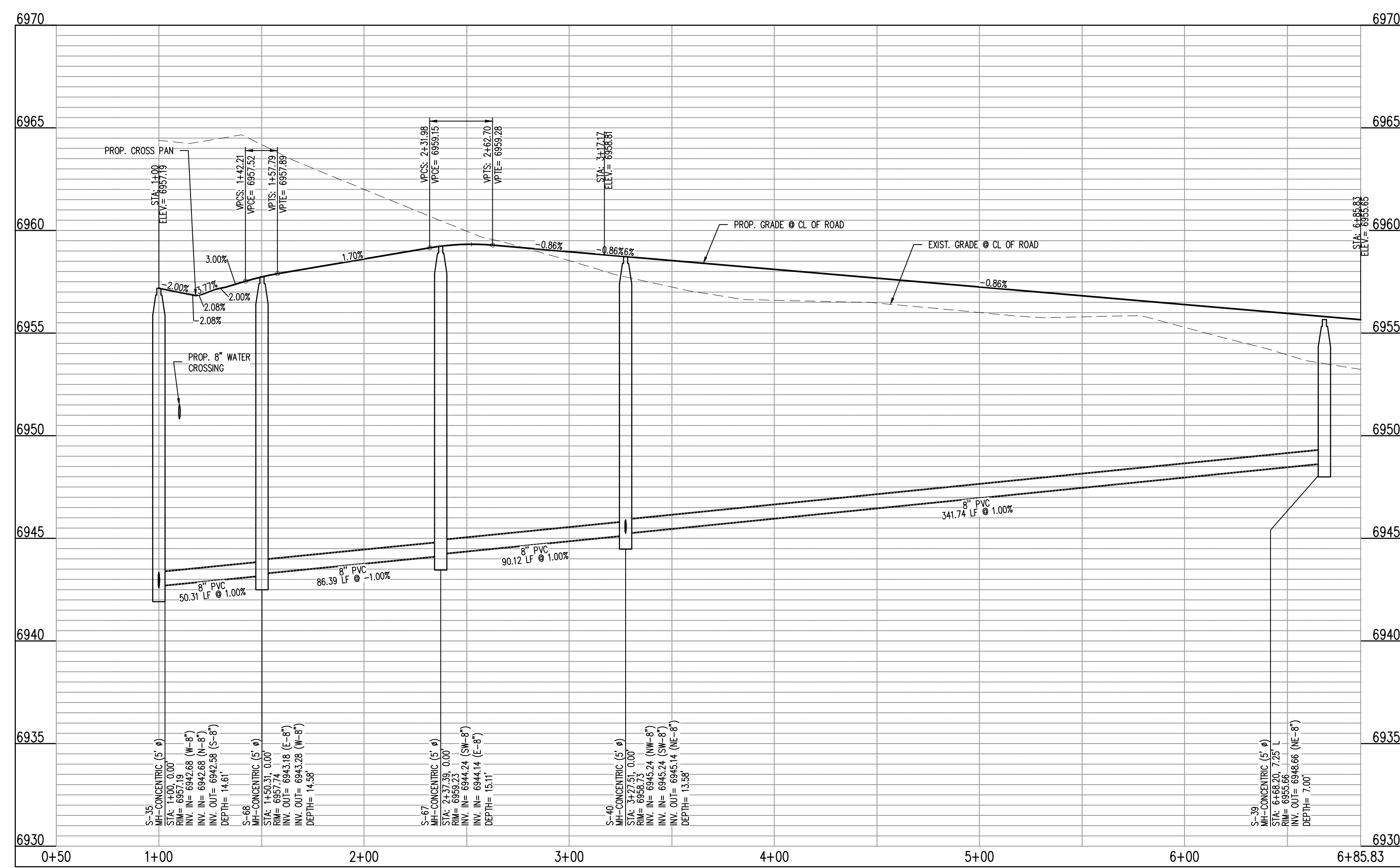
CHALLENGER HOMES

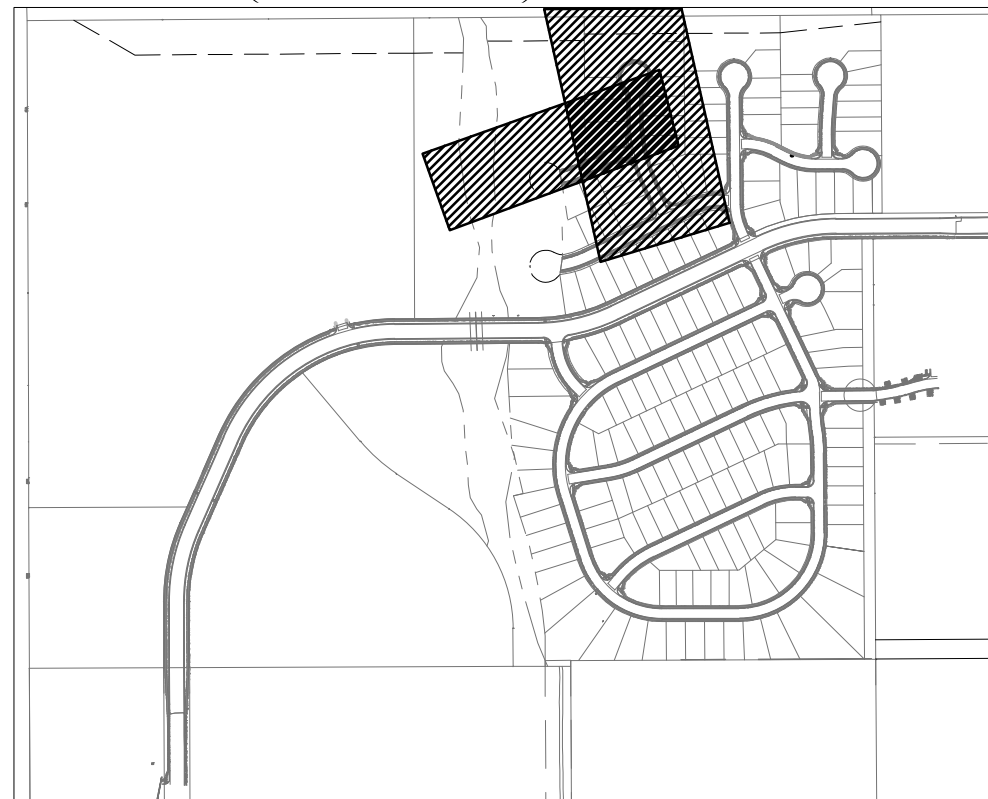
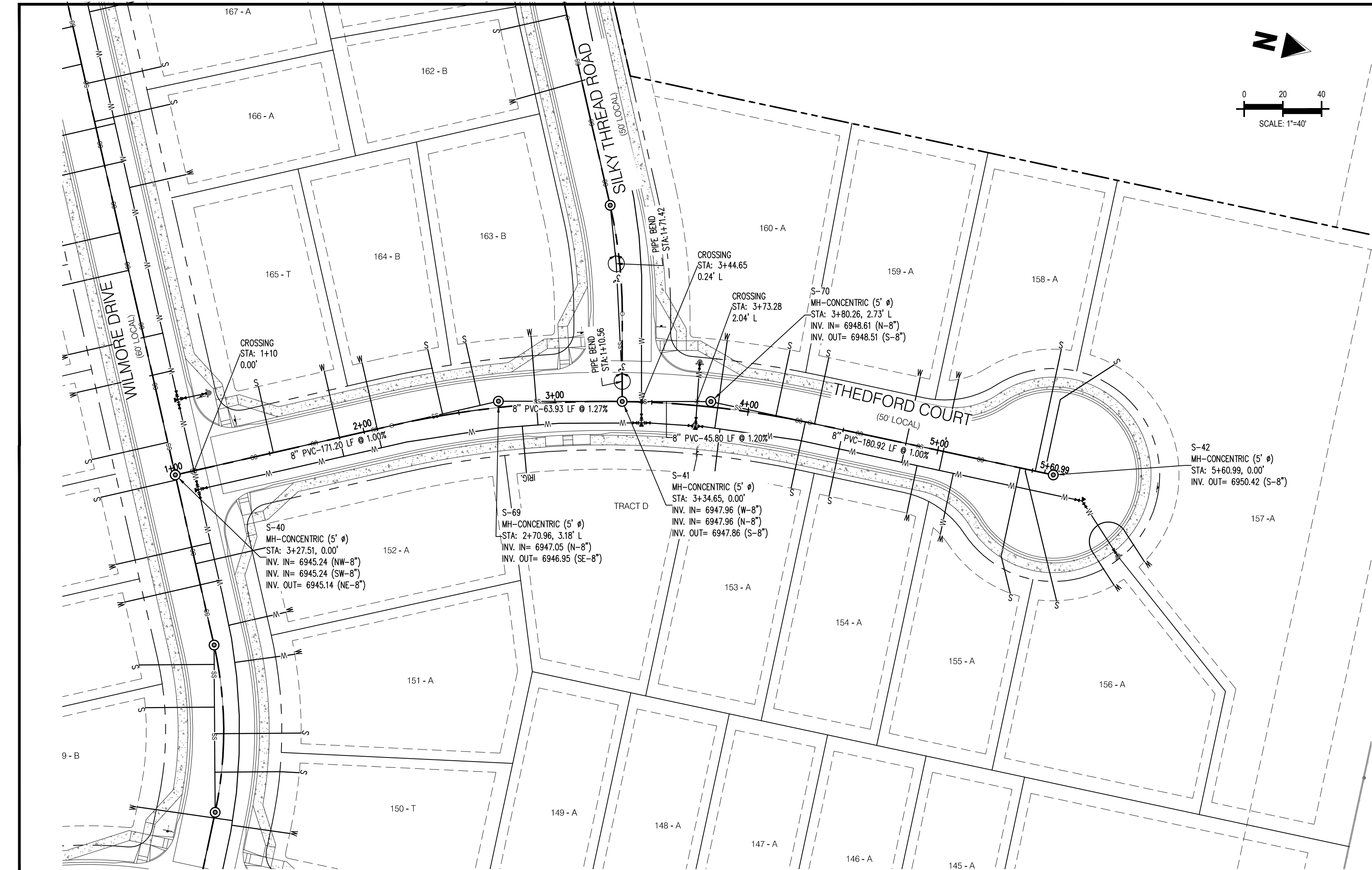
CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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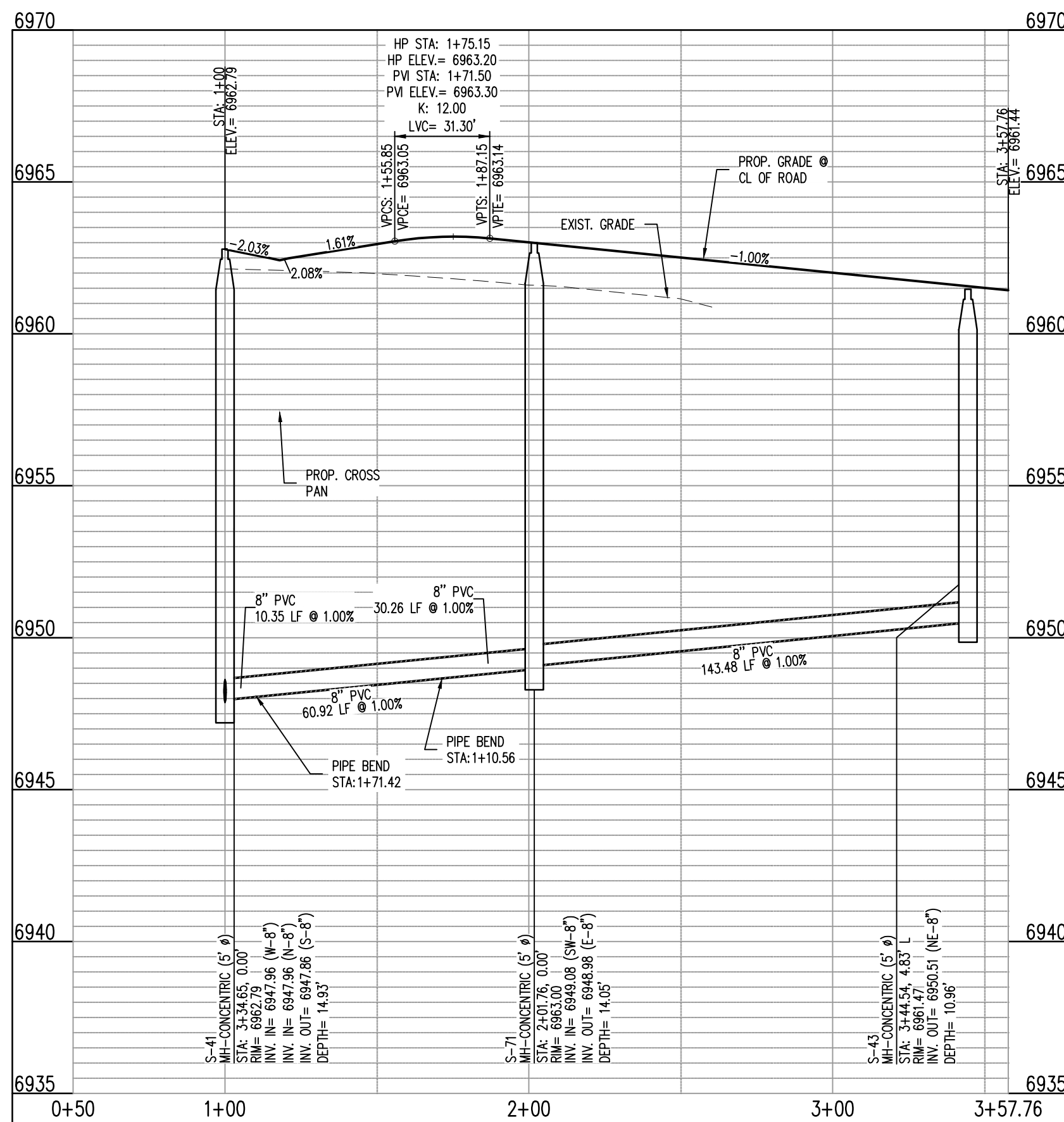
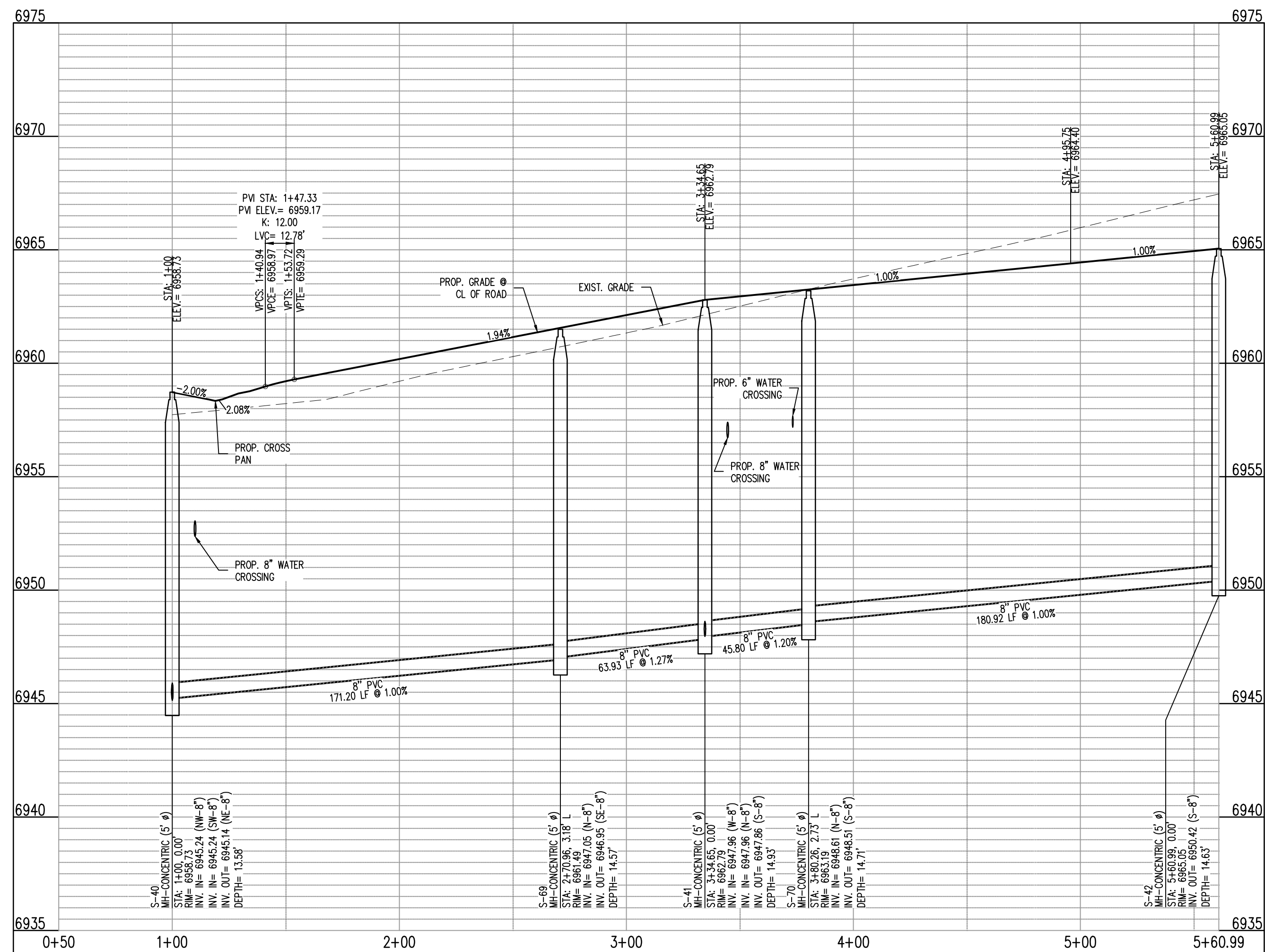
Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

SILVER GRASS DRIVE &
LEMON GRASS ROAD
SANITARY PLAN & PROFILE





LOCATION MAP
SCALE: 1"=600'



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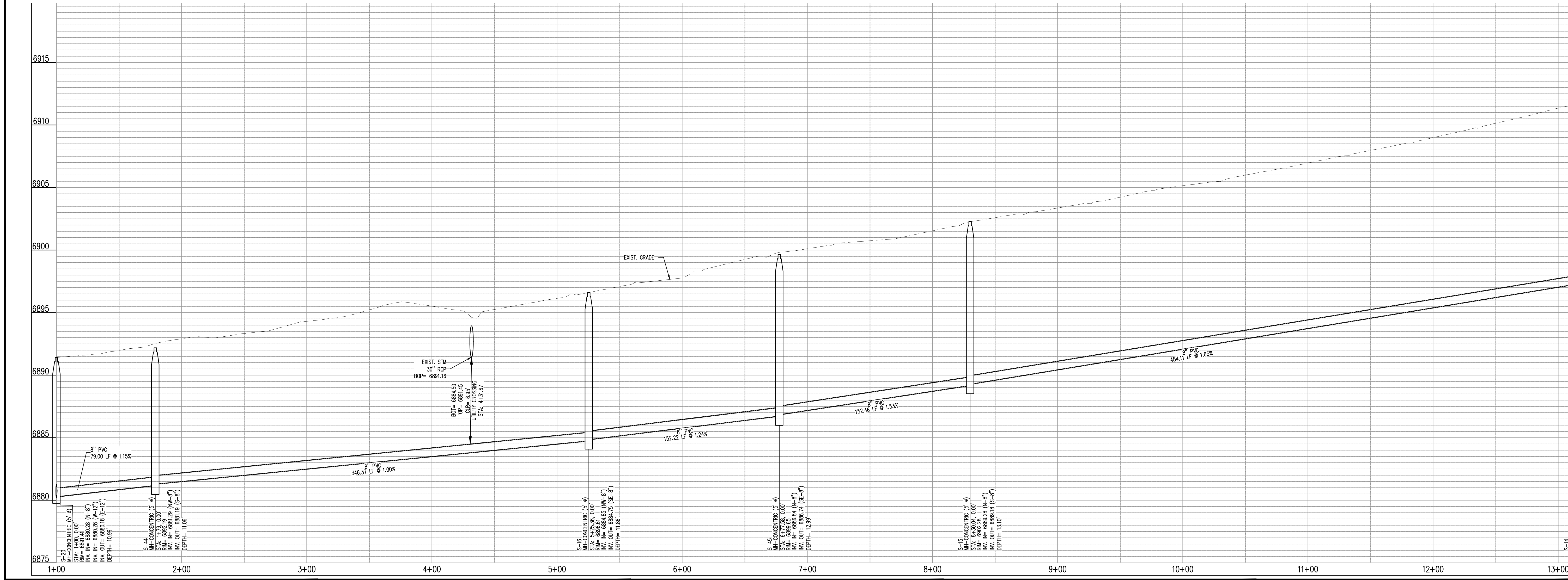
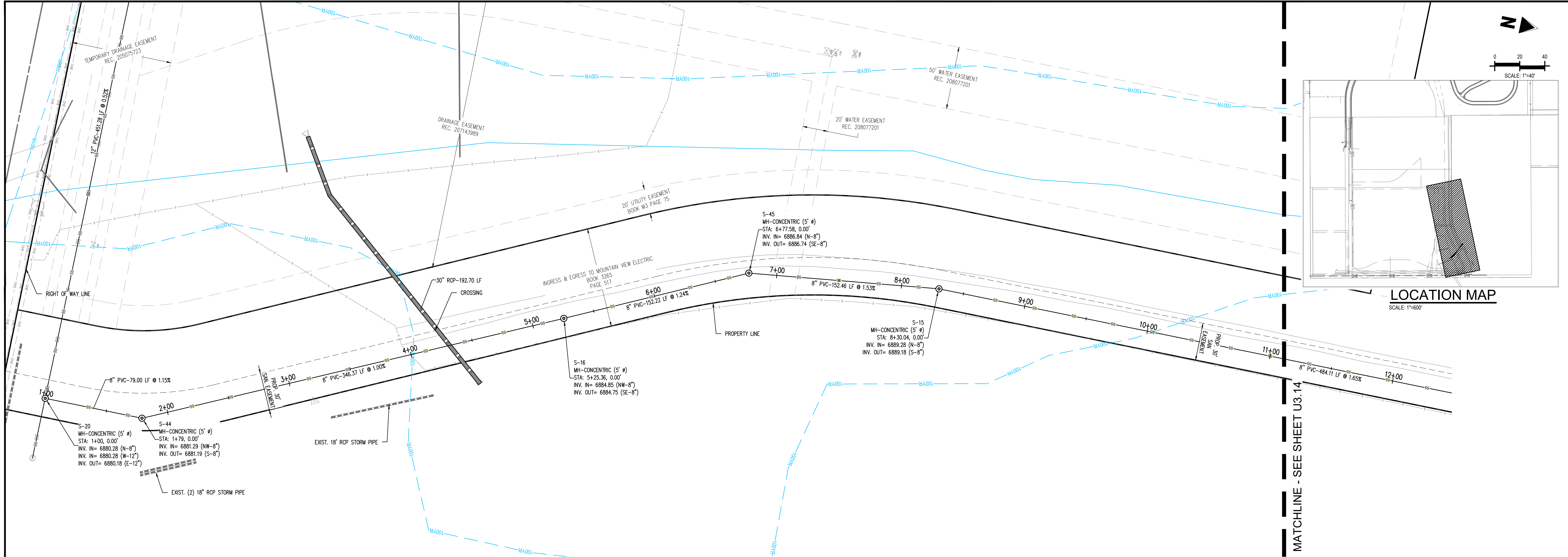
CHALLENGER
HOMES

CONSTRUCTION DOCUMENTS
BENT GRASS RESIDENTIAL FILING NO. 2
FOR
CHALLENGER COMMUNITIES, LLC
BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
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Date: 2/10/2020

THEDFORD COURT &
SILKY THREAD ROAD
SANITARY PLAN & PROFILE



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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
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Date: 2/10/2020

RIGHT OF WAY CONNECTION
SANITARY PLAN & PROFILE

U3.13
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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

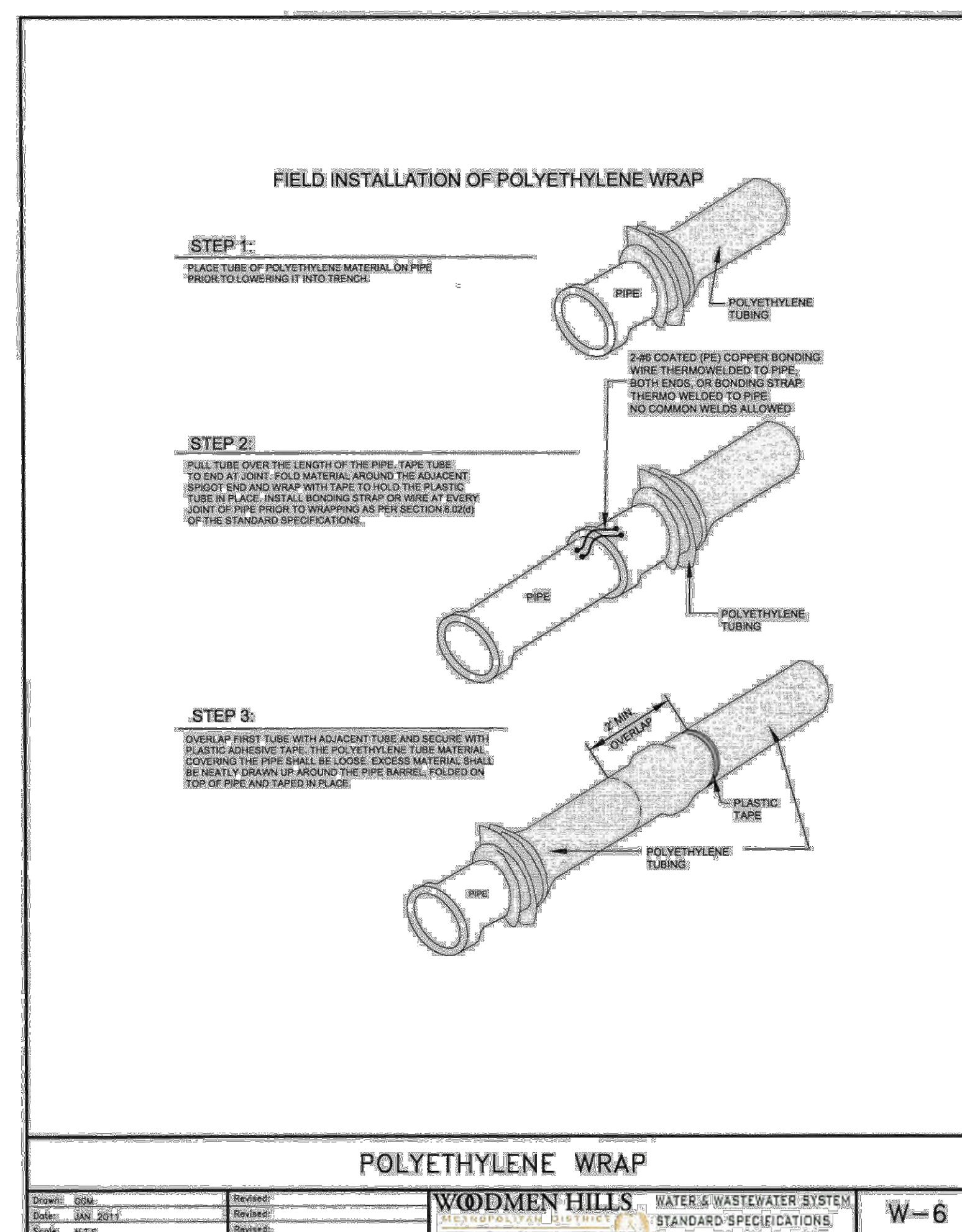
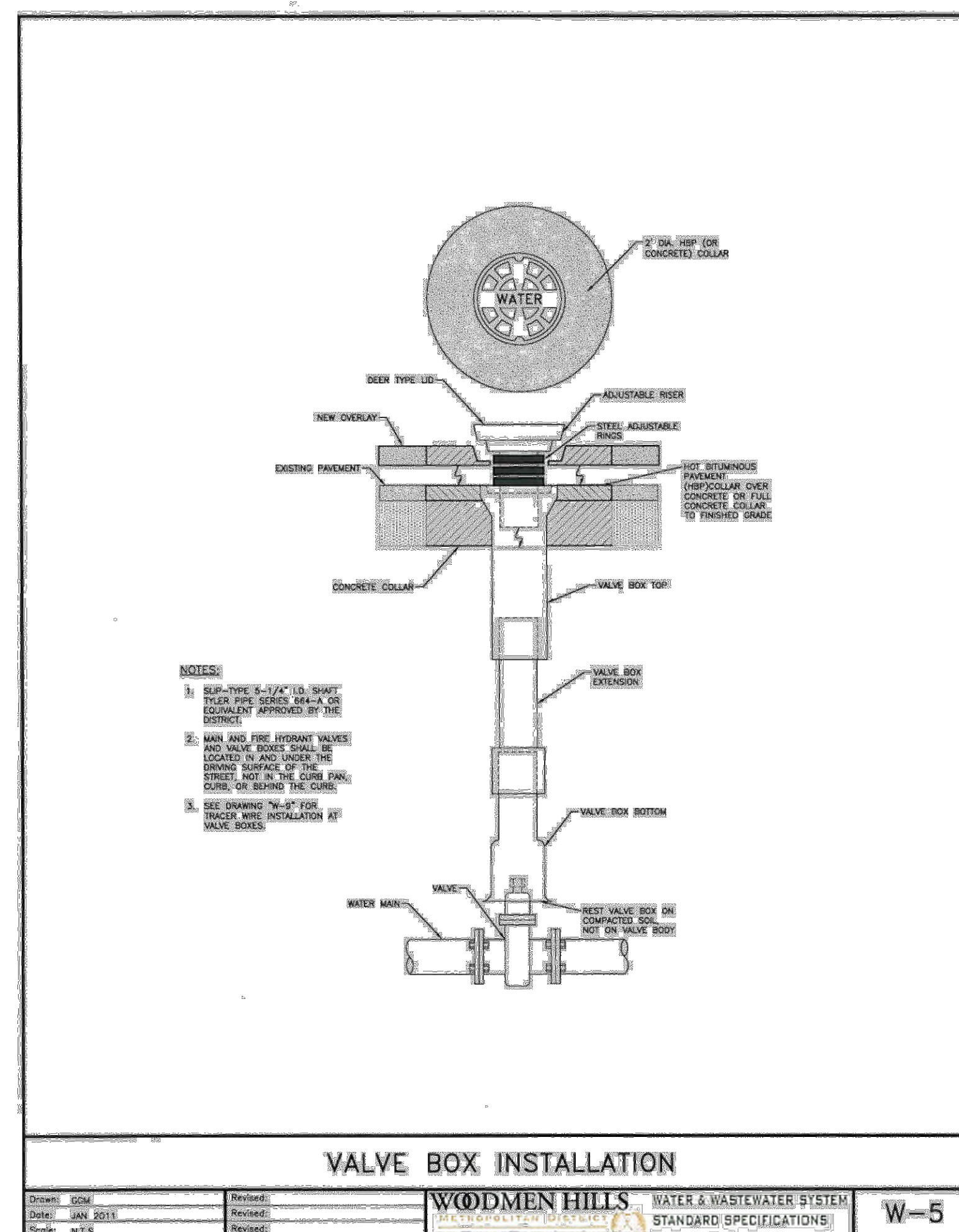
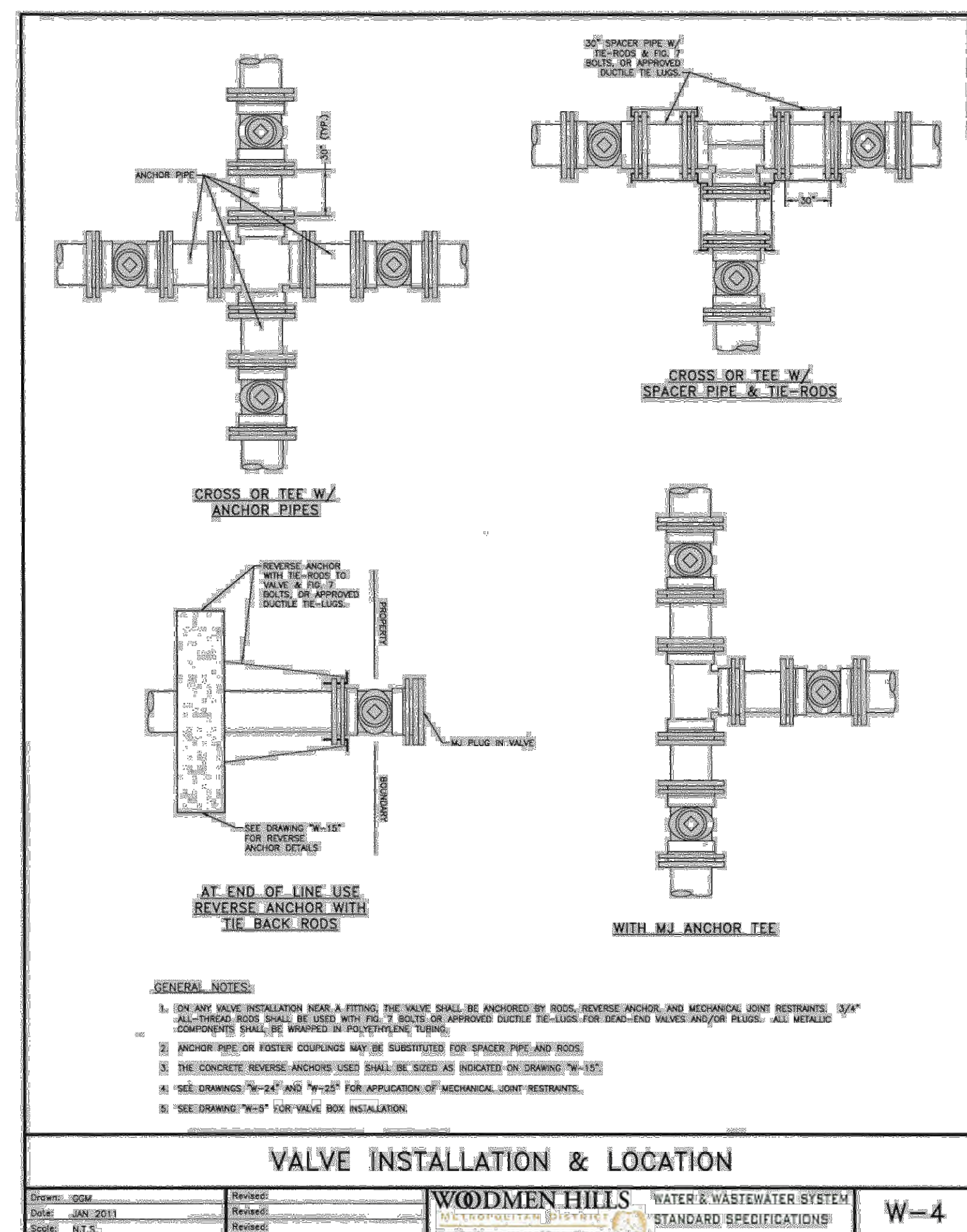
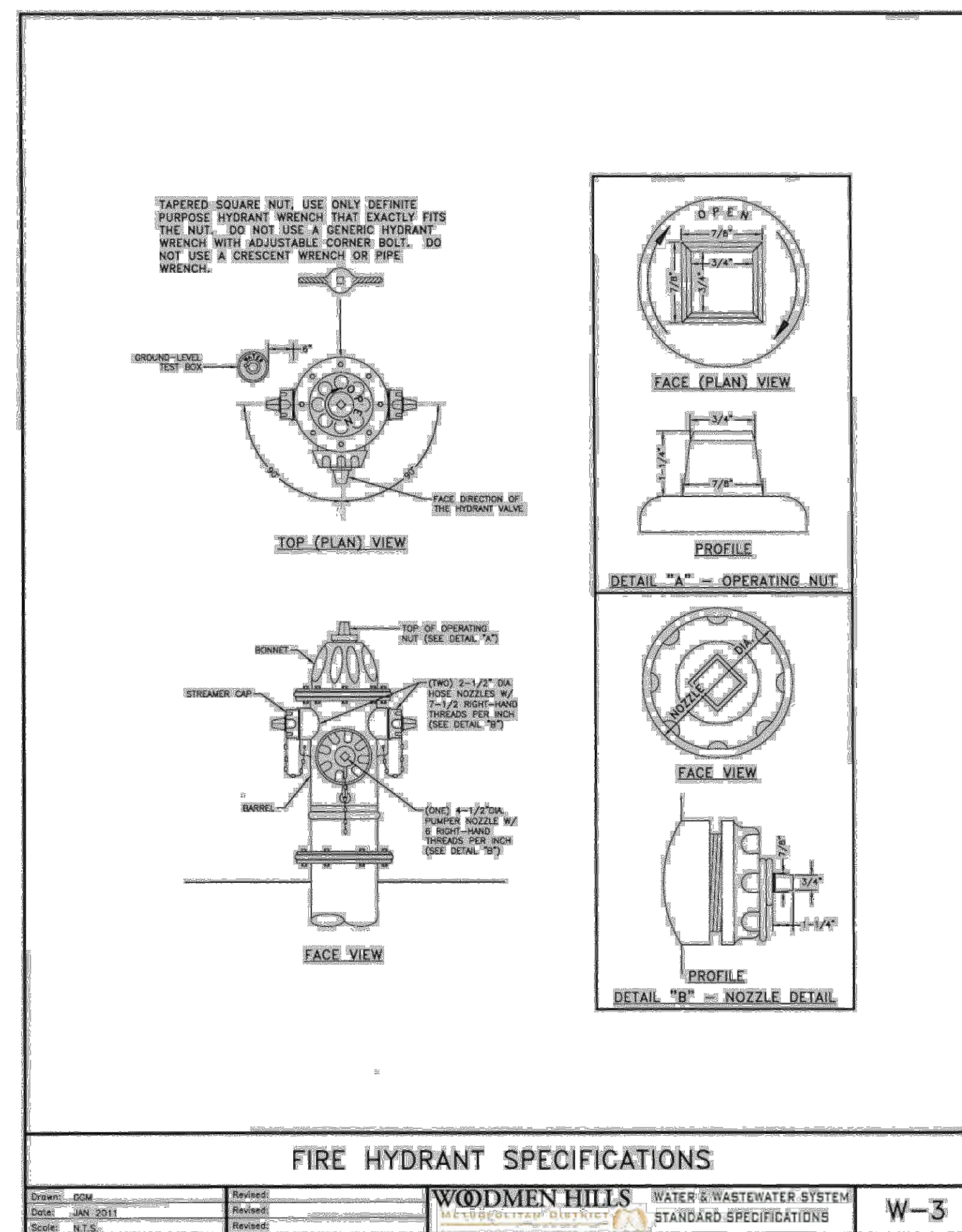
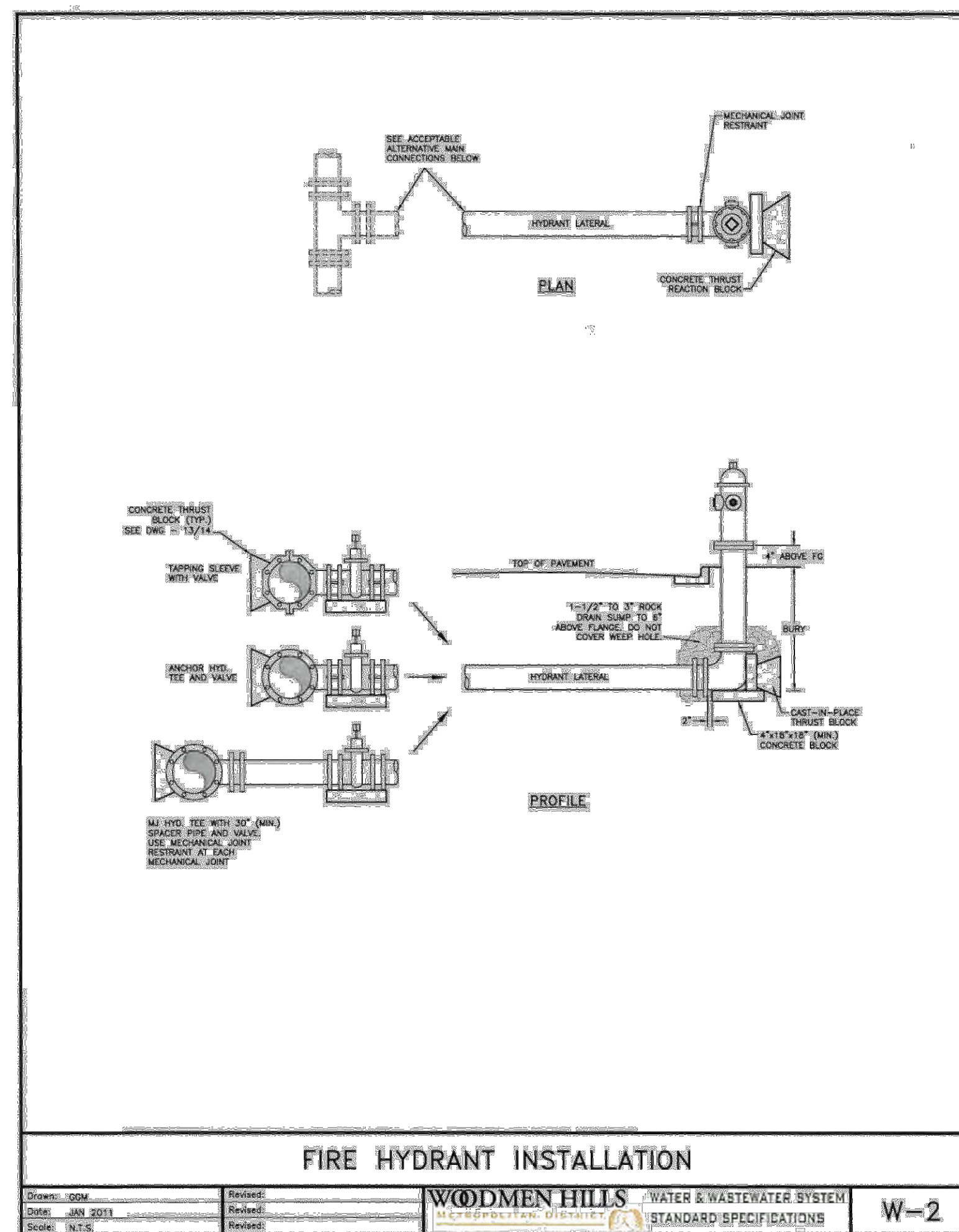
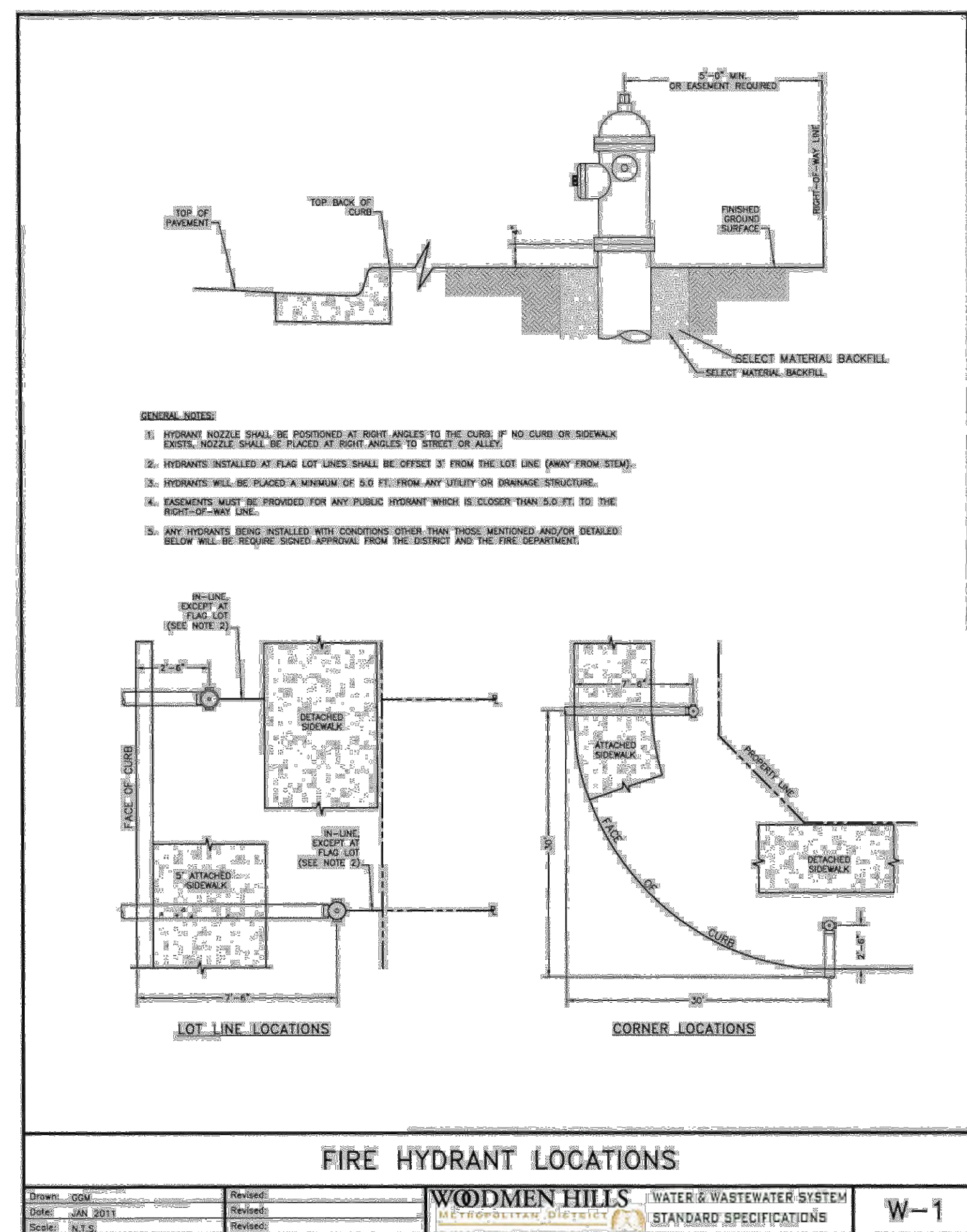
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Project No:	CLH000014.20
Drawn By:	JDP
Checked By:	RGD
Date:	2/10/2020

UTILITY DETAILS

J4.01

Sheet 27 of 32





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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

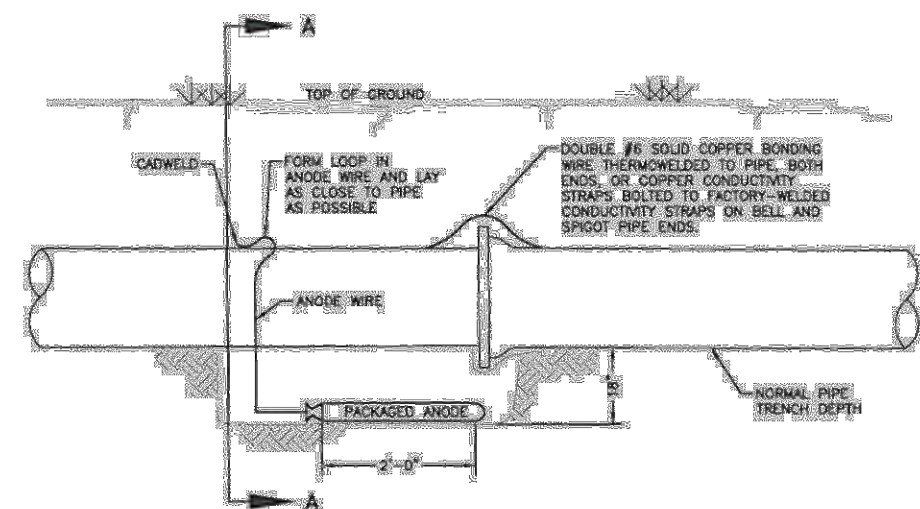
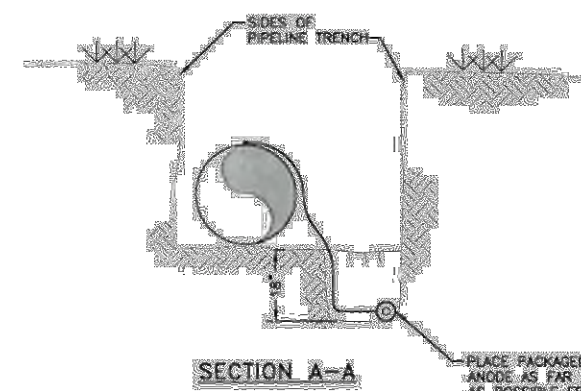
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Project No:	CLH000014.20
Drawn By:	JDP
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Date:	2/10/2020

UTILITY DETAILS

U4.02

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ELEVATION

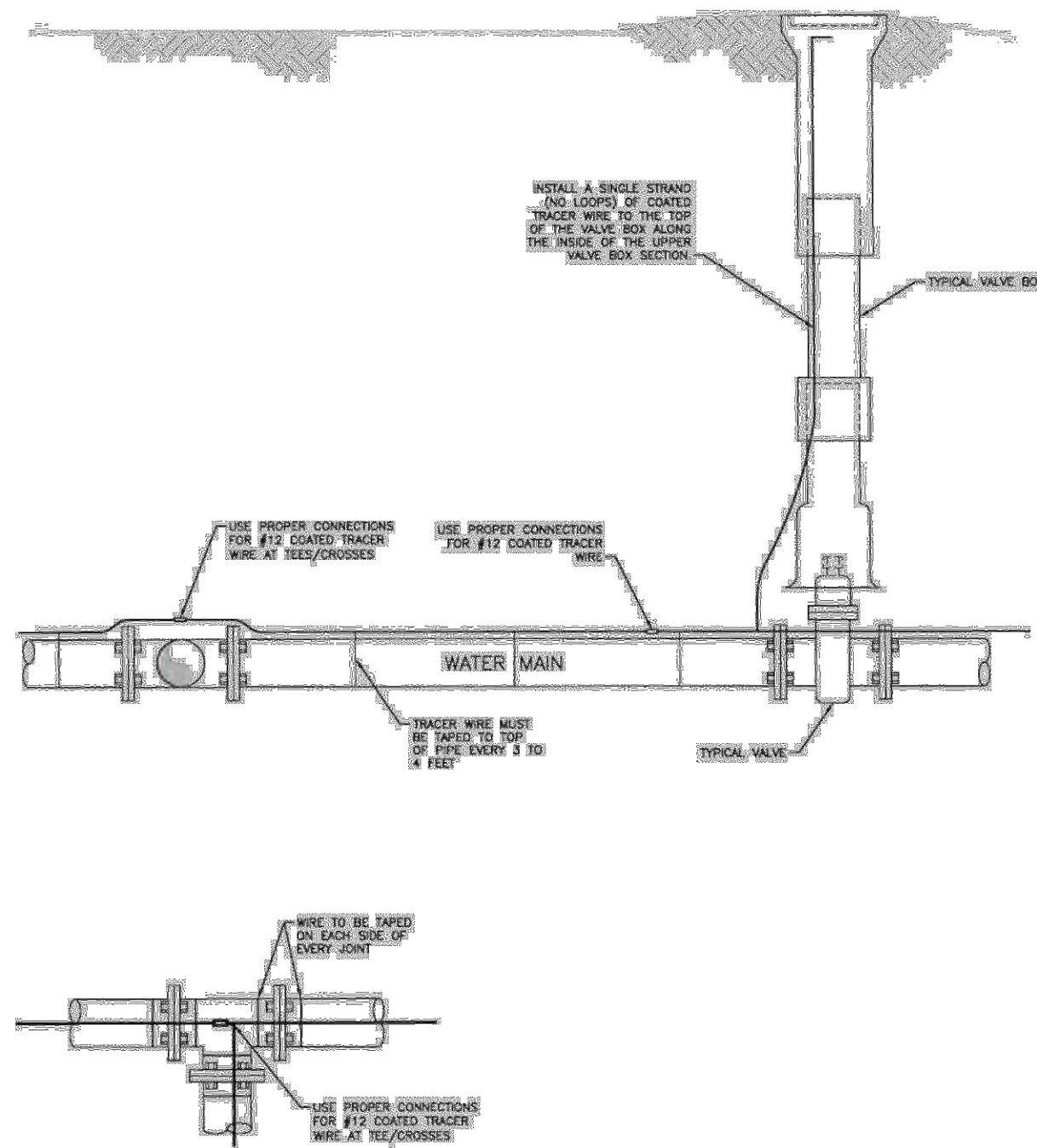
SECTION A-A

NOTES:

1. CADDIS CONNECTION TO BE PRIMED AND COATED CAREFULLY. PACKAGED ANODES SHOULD BE COVERED WITH FINE SOIL CONTAINING NO ROCKS OR DIRT CLUMPS, TAMPED.
2. WHEN ANODES ARE REQUIRED WITH METAL FITTINGS AND APPURTENANCES TOGETHER WITH PVC PIPE INSTALLATION, THE ANODES SHALL BE PLACED AND ATTACHED TO THE METAL IN SAME MANNER AS SHOWN ON THIS DRAWING. S.B.L. ANODES CAN BE USED ON METAL FITTINGS 12" AND LESS IN DIAMETER AND 17LB. ANODES FOR METAL FITTINGS GREATER THAN 12" DIAMETER WHEN USING RIG PIPE.
3. PACKAGED ANODE TO BE WETTED AND COVERED WITH SOIL PRO TO BACKFILLING.

BONDING JOINT & ANODE INSTALLATION

Drawn: GCM	Revised:	 WOODMEN HILLS METROPOLITAN DISTRICT	WATER & WASTEWATER SYSTEM	<div style="border: 2px solid black; padding: 5px; font-size: 24pt; font-weight: bold;">W-7</div>
Date: MAY 2011	Revised:		STANDARD SPECIFICATIONS	
Scale: N.T.S.	Revised:			



TRACER WIRE INSTALLATION DETAILS

Drawn: GGM	Revised:	 WOODMEN HILLS METROPOLITAN DISTRICT WATER & WASTEWATER SYSTEM STANDARD SPECIFICATIONS <small>2015 Edition</small>	W-9
Date: JAN. 2011	Revised:		
Scale: N.T.S.	Revised:		

[illegible]

THRUST BLOCK DATA

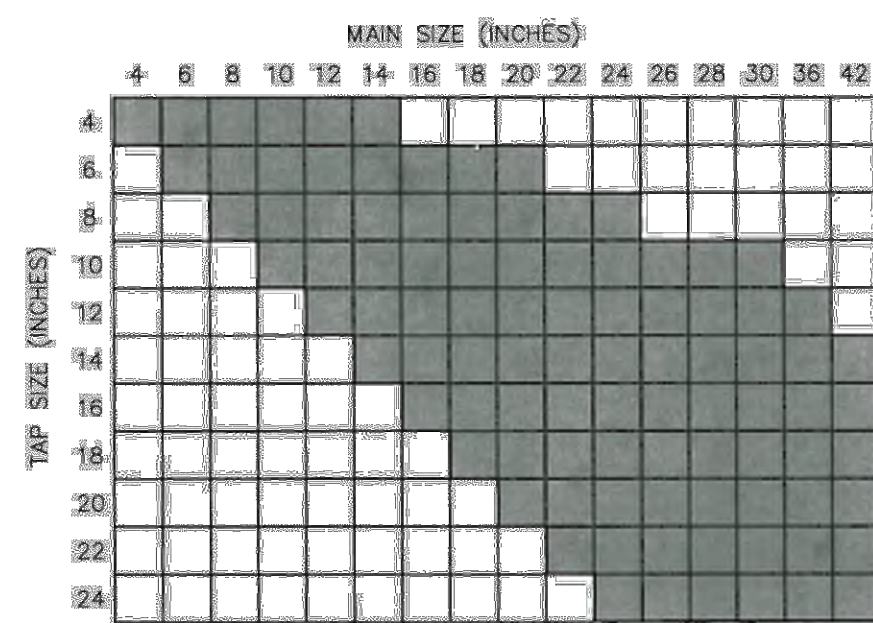
Drawn: GOM	Revised:	 WOODMEN HILLS WATER & WASTEWATER SYSTEM BETHLEHEM POLYMER DISTRICT STANDARD SPECIFICATIONS	W-10
Date: JAN 2011	Revised:		
Scale: N.T.S.	Revised:		

CONCRETE THRUST BLOCKS

WATER MAIN AND TAP SIZE COMBINATIONS WHICH REQUIRE A CONCRETE THRUST REACTION BLOCK BEHIND THE MAIN AT THE TAPPING SLEEVE OR SADDLE.

ALL WATER MAINS

INDICATES CONCRETE THRUST BLOCK REQUIRED



ANY THRUST REACTION BLOCK REQUIREMENTS FOR WATER MAIN AND TAP SIZE COMBINATIONS OTHER THAN THOSE SHOWN ABOVE WILL REQUIRE SPECIAL DESIGN APPROVAL BY THE DISTRICT.

THRUST BLOCKS REQUIRED FOR TAPS

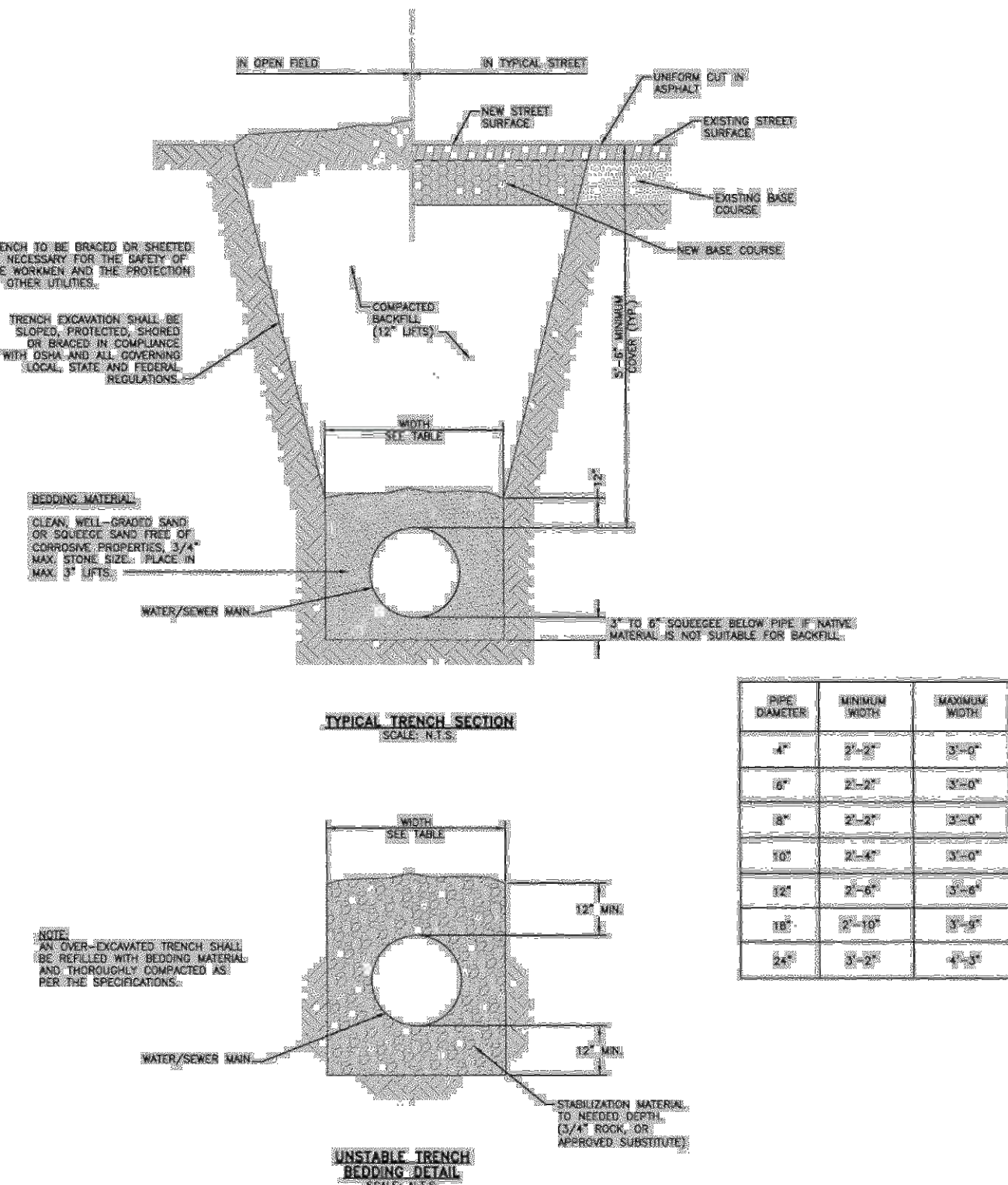
Drawn: GMM
 Date: JUN 2011
 Scale: N.T.S.

Revised:
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 Revised:

WOODMEN HILLS
 RECYCLED PAPER BUSINESS

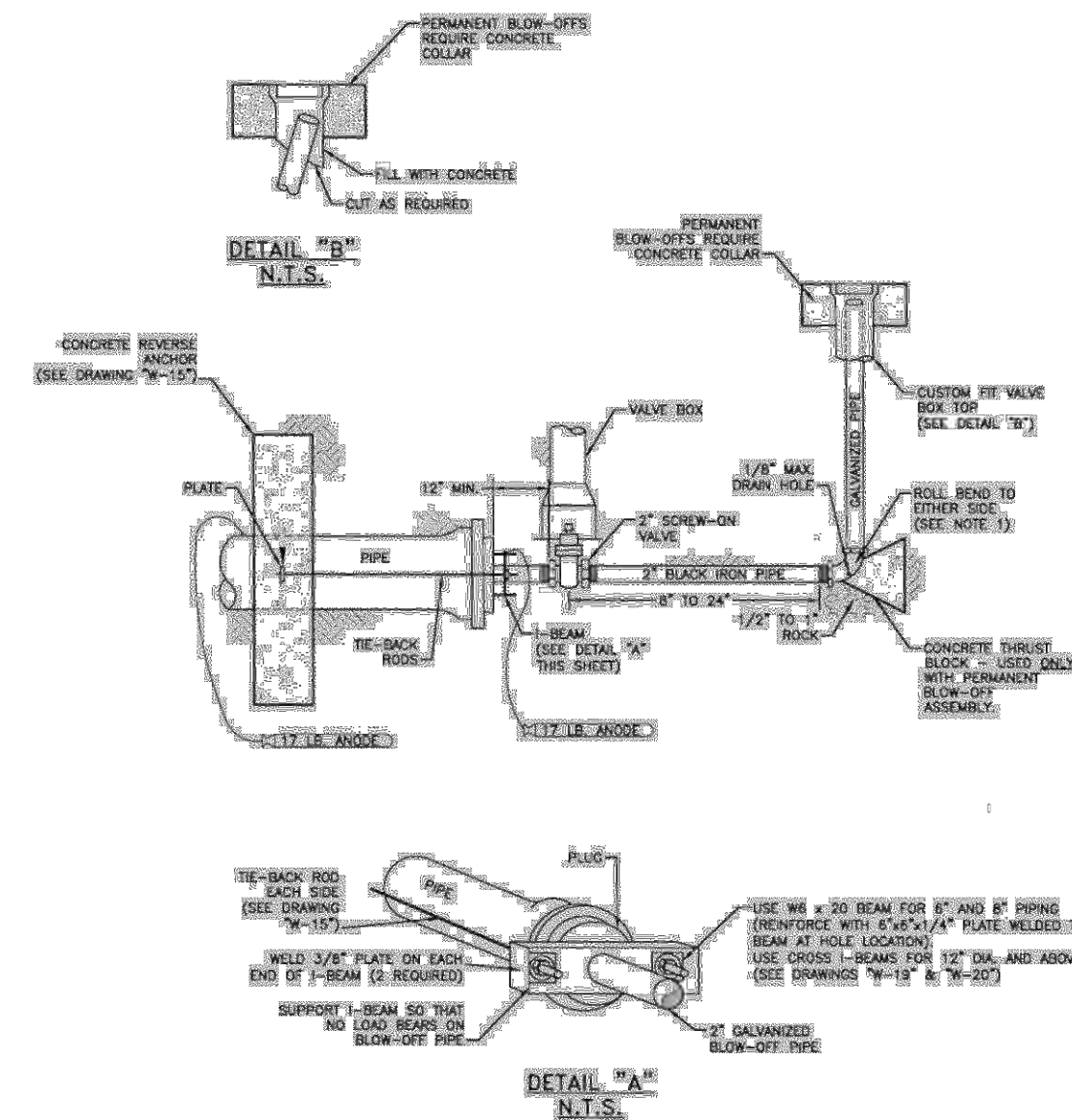
WATER & WASTEWATER SYSTEM
 STANDARD SPECIFICATIONS

W-11



TYPICAL TRENCH CROSS-SECTION

Drawn: GCM	Revised:	 WOODMEN HILLS METROPOLITAN UTILITY DISTRICT "SUSTAINING THE QUALITY OF LIFE"	WATER & WASTEWATER SYSTEM STANDARD SPECIFICATIONS	W-12
Date: JAN 2011	Revised:			
Scale: N.T.S.	Revised:			



TEMP & PERMANENT BLOW-OFF ASSEMBLIES FOR 6" & 8" MAINS

Drawn: GDM Date: JAN 2011 Scale: N.T.S.	Revised: Revised: Revised:	 WOODMEN HILLS METROPOLITAN DISTRICT	WATER & WASTEWATER SYSTEM STANDARD SPECIFICATIONS	W-18
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FOR
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BENT GRASS MEADOWS DRIVE & MERIDIAN ROAD
FALCON, CO 80831 - EL PASO COUNTY

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Project No: CLH000014.20
Drawn By: JDP
Checked By: RGD
Date: 2/10/2020

UTILITY DETAILS

CLASS "A"
(CONCRETE CRADLE)
L.F.=2.8

CLASS "B"
(GRANULAR BASE)
L.F.=1.9

CLASS "C"
(SHAPED BOTTOM)
L.F.=1.5

CLASS "D"
(FLAT BOTTOM)
L.F.=1.1

NOTES:

1. FOR ROCK OR OTHER INCOMPRESSIBLE MATERIALS THE TRENCH SHALL BE OVEREXCAVATED A MINIMUM OF 8" AND FILLED WITH GRANULAR BEDDING MATERIAL AS DEFINED BY CLASS "B" BEDDING.
2. L.F. (L) LOAD FACTOR
3. CLASS "D" BEDDING WILL NOT BE ACCEPTABLE UNDER ANY CONDITION.
4. MINIMUM DENSITY FOR CAREFULLY COMPACTED SELECT BACKFILL SHALL BE 90% OF STD. PROCTOR DENSITY OR AS SPECIFIED FOR THE TRENCH BACKFILL, WHICHEVER IS GREATER.

PIPE BEDDING

Drawn: GDM	Reviewed: [Signature]	WOODMEN HILLS WATER & WASTEWATER SYSTEM	WW-1
Date: JAN 2011	Revised: [Signature]	METROPOLITAN DISTRICT STANDARD SPECIFICATIONS	
Scale: N.T.S.	Revised: [Signature]		

TYPICAL TRENCH CROSS SECTION

PIPE DIAMETER	MINIMUM WIDTH	MAXIMUM WIDTH
4"	1'-5"	3'-9"
6"	1'-7"	3'-11"
8"	1'-9"	4'-1"
12"	2'-1"	4'-5"
15"	2'-6"	4'-9"
18"	2'-10"	5'-2"
24"	3'-2"	5'-6"

ALL PIPE EMBEDMENT SHALL BE IMPORTED CLASS B BEDDING UNLESS OTHERWISE DIRECTED.

ANY OVER EXCAVATED TRENCH SHALL BE REFILLED AND THOROUGHLY COMPACTED UNDER THE DIRECTION OF THE DISTRICT.

UNDER NO CIRCUMSTANCES WILL PIPE BE LAID IN A PROPOSED FILL AREA PRIOR TO IT BEING COMPLETELY FILLED. THE FILL WILL BE PLACED FIRST TO PROPOSED GRADE AND COMPACTED AS REQUIRED. A TRENCH THEN WILL BE EXCAVATED AND THE PIPE INSTALLED IN THE USUAL MANNER.

PIPE BEDDING

Drawn: GDM	Reviewed: [Signature]	WOODMEN HILLS WATER & WASTEWATER SYSTEM	WW-2
Date: JAN 2011	Revised: [Signature]	METROPOLITAN DISTRICT STANDARD SPECIFICATIONS	
Scale: N.T.S.	Revised: [Signature]		

PRECAST MANHOLE

PIPE I.D.	MANHOLE I.D.	WALL
8" TO 15"	4'-0"	8"
18" TO 30"	5'-0"	8"
36" TO 54"	6'-0"	8"
60" & LARGER	SPECIAL DESIGN	8"

PRECAST MANHOLE

Drawn: GDM	Reviewed: [Signature]	WOODMEN HILLS WATER & WASTEWATER SYSTEM	WW-3
Date: JAN 2011	Revised: [Signature]	METROPOLITAN DISTRICT STANDARD SPECIFICATIONS	
Scale: N.T.S.	Revised: [Signature]		

SANITARY SEWER MANHOLE

WATERPROOFING REQUIREMENTS

1. APPLY CEMENT GROUT LAYER (3/4" TO 1" THICK) TO ALL INTERIOR JOINTS ABOVE FLOW CHANNEL.
2. APPLY COAL TAR EPOXY DAMPROOFING TO ALL EXTERIOR CONCRETE SURFACES.
3. APPLY CEMENT GROUT LAYER (3/4" TO 1" THICK) TO ALL EXTERIOR CONCRETE SURFACES, AS DIRECTED BY DISTRICT, PRIOR TO DAMPROOFING.

SANITARY SEWER MANHOLE

Drawn: GDM	Reviewed: [Signature]	WOODMEN HILLS WATER & WASTEWATER SYSTEM	WW-4
Date: JAN 2011	Revised: [Signature]	METROPOLITAN DISTRICT STANDARD SPECIFICATIONS	
Scale: N.T.S.	Revised: [Signature]		

MANHOLE RING AND COVER ADJUSTMENT

MANHOLE RING AND COVER ADJUSTMENT

Drawn: GDM	Reviewed: [Signature]	WOODMEN HILLS WATER & WASTEWATER SYSTEM	WW-5
Date: JAN 2011	Revised: [Signature]	METROPOLITAN DISTRICT STANDARD SPECIFICATIONS	
Scale: N.T.S.	Revised: [Signature]		

SANITARY SEWER MAIN CLEAN-OUT

SANITARY SEWER MAIN CLEAN-OUT

Drawn: GDM	Reviewed: [Signature]	WOODMEN HILLS WATER & WASTEWATER SYSTEM	WW-6
Date: JAN 2011	Revised: [Signature]	METROPOLITAN DISTRICT STANDARD SPECIFICATIONS	
Scale: N.T.S.	Revised: [Signature]		

