#### DEVELOPER

VERDAD REAL ESTATE 1211 SOUTH WHITE CHAPEL BLVD. SOUTHLAKE, TX 76092 (817) 912-5372

#### CIVIL ENGINEER

STERLING DESIGN ASSOCIATES, LLC 2009 W. LITTLETON BLVD. #300 LITTLETON, CO 80120 (303) 794-4727 CONTACT: JOSEPH G. SCHIEL, PE

#### **ARCHITECT**

ROGUE ARCHITECTS 513 MAIN STREET SUITE 300 FORT WORTH, TX 76102 (817) 820-0433 CONTACT: TIFFANY CHALUPA

#### LANDSCAPE **ARCHITECT**

STERLING DESIGN ASSOCIATES, LLC 2009 W. LITTLETON BLVD. #300 LITTLETON, CO 80120 (303) 794-4727 CONTACT: JONATHAN D. SPENCER, PLA

#### **AGENCY REFERRALS**

PLANNING & COMMUNITY DEVELOPMENT 2880 INTERNATIONAL CIRCLE, SUITE 110 COLORADO SPRINGS, CO 80910 (719) 520-6306, (719) 520-6300 CONTACT: KARI PARSONS

PIKES PEAK REGIONAL BUILDING DEPARTMENT 2880 INTERNATIONAL CIRCLE COLORADO SPRINGS, CO 80910 CONTACT: DAN McCULLY (719) 327-2880

EL PASO ENVIRONMENTAL HEALTH DEPARTMENT 1675 W GARDEN OF THE GODS ROAD, SUITE 2044 COLORADO SPRINGS, CO 80907

CONTACT: PEGGY RIVERA

(719) 578-3199

EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS 3275 AKERS DRIVE

CONTACT: JENNIFER IRVINE, COUNTY ENGINEER

COLORADO SPRINGS, CO 80922

COLORADO SPRINGS UTILITIES
111 S. CASCADE AVE. COLORADO SPRINGS, CO 80903 CONTACT: ZACK SHEARER (719) 668-8111, (719) 448-4800 (MAIN)

MOUNTAIN VIEW ELECTRIC ASSOCIATION, INC 11140 E. WOODMAN RD. FALCON, CO 80831-8127 CONTACT: KATHY HANSEN-LEE OR LYNN HERTEL (719) 495-2283

DONALD WESCOTT FIRE PROTECTION 15415 GLENEAGLE DRIVE COLORADO SPRINGS, CO 80921 CONTACT: CHIEF VINCENT "VINNY" BURNS

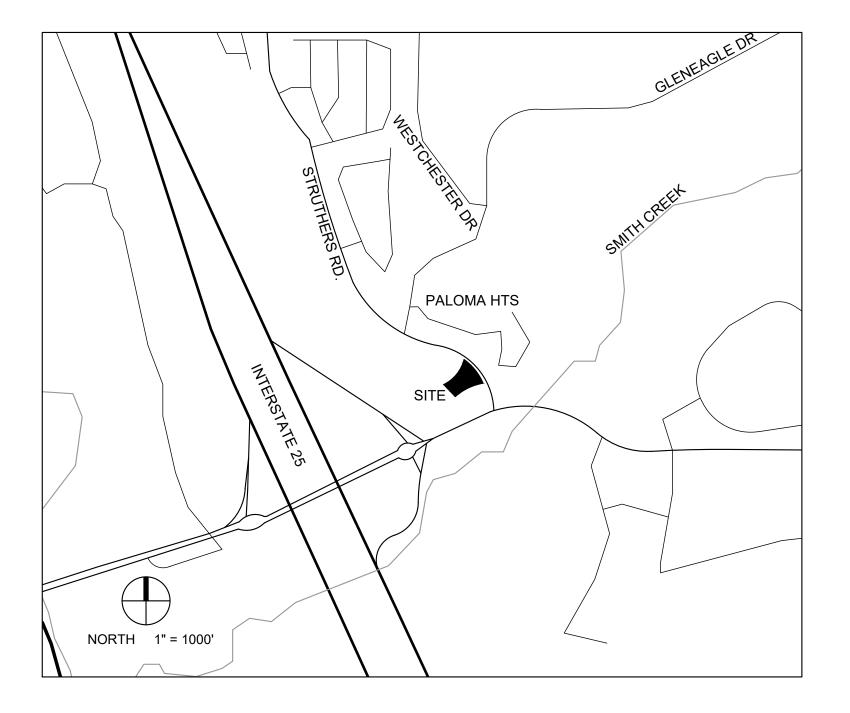
DONALA WATER &SANITATION AREA A 15850 HOLBEIN DRIVE COLORADO SPRINGS, CO 80921 CONTACT: ROBERT HULL (719) 488-3603

# ACADEMY GATEWAY SUBDIVISION FILING NO. I LOT 2

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION 1, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

> MERIDIAN, EL PASO COUNTY, COLORADO MAJOR SITE DEVELOPMENT PLAN

### VICINITY MAP



### **SUBMITTALS**

INITIAL: MAY 16, 2017 REVISED: AUGUST 25, 2017

## **LEGAL DESCRIPTION**

LOT 2, ACADEMY GATEWAY SUBDIVISION FILING NO. 1 EL PASO COUNTY, COLORADO

#### BASIS OF BEARINGS

BEARINGS ARE BASED ON A PORTION OF THE EASTERLY BOUNDARY OF THE UNITED STATES AIR FORCE ACADEMY AS RECORDED IN PLAT BOOK O-2 AT PAGE 84, RECORDS OF EL PASO COUNTY, COLORADO, BEING MONUMENTED AT THE NORTHERLY END BY A3.5" BRASS CAP STAMPED "BDY 43" IS ASSUMED TO BEAR N 56°38'25"W, A DISTANCE OF 630.29 FEET.

#### BENCHMARK

A CHISELED "+" ON THE WEST END OF THE CONCRETE MEDIAN LOCATED IN STRUTHERS ROAD EAST OF GLENEAGLE DRIVE ELEVATION = 6770.99 (NAVD 88)

CHISELED "+" ON THE EAST END OF THE CONCRETE MEDIAN LOCATED IN STRUTHERS ROAD EAST OF GLENEAGLE DRIVE ELEVATION = 6747.41 (NAVD 88)

# SHEET INDEX

1	(C100)	COVER SHEET
2	(C100) (C101)	GENERAL NOTES
3	(C201)	SITE PLAN
4	(C210)	SITE DETAILS
5	(C211)	SITE DETAILS
6	(C301)	GRADING PLAN
7	(C302)	EROSION AND STORMWATER QUALITY CONTROL PLAN
8	(C303)	EROSION AND STORMWATER QUALITY CONTROL DETAILS
9	(C304)	EROSION AND STORMWATER QUALITY CONTROL DETAILS
10	(C305)	EROSION AND STORMWATER QUALITY CONTROL DETAILS
П	(C401)	UTILITY PLAN
12	(C402)	UTILITY DETAILS
13	(P101)	PHOTOMETRIC PLAN AND DETAILS
14	(L101)	LANDSCAPE PLAN
15	(L102)	LANDSCAPE DETAILS
16	(C701)	SIGNAGE DETAILS
17	(C702)	SIGNAGE DETAILS
18	(SD2)	BUILDING ELEVATIONS
19	(SD3)	BUILDING ELEVATIONS

# CIVIL ENGINEERS - LANDSCAPE ARCHITECTS

2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE

COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

**NOT FOR CONSTRUCTION** 

STERLING DESIGN ASSOCIATES, LLC

NO.: I	DATE: 5/16/2017	BY: JDS	
DESCRIPT	TION: 1ST MAJOR SDP SUBMITTAL		
NO.: 2	DATE: 8/24/2017	BY: JDS	
DESCRIPT	TION: 2ND MAJOR SDP SUBMITTAL		
NO.: 3	DATE: -	BY: -	
DESCRIPT	ΓΙΟΝ: -		
NO.: 4	DATE: -	BY: -	
DESCRIPTION: -			
NO.: 5	DATE: -	BY: -	
DESCRIPTION: -			

DATE: <b>8/24/2017</b>	SCALE: 1" = 20'
PROJECT MANAGER:	PROJECT NO.:
JDS	-
DRAWN BY:	DRAWING FILE:

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

**COVER SHEET** 

SHEET NUMBER:

1 OF 19

COUNTY PROJECT NUMBER PPR-17-030

CAUTION - NOTICE TO CONTRACTOR

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER (1-800-245-4545) AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING IMPROVEMENTS AND UTILITIES AND SHALL REPAIR ANY DAMAGE AT HIS EXPENSE.



A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

MERIDIAN, EL PASO COUNTY, COLORADO MAJOR SITE DEVELOPMENT PLAN

#### **GENERAL NOTES**

- 1. ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION OF IMPROVEMENTS SHALL MEET OR EXCEED THE LOCAL AND APPLICABLE UTILITY DISTRICT STANDARDS AND SPECIFICATIONS, AND APPLICABLE STATE AND FEDERAL REGULATIONS. WHERE THERE IS CONFLICT BETWEEN THESE PLANS AND ANY APPLICABLE STANDARDS, THE HIGHER QUALITY STANDARD SHALL APPLY. ALL WORK SHALL BE INSPECTED AND APPROVED BY THE AUTHORITY HAVING JURISDICTION AND APPLICABLE UTILITY DISTRICT(S).
- 2. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE APPROVED PLANS, ONE (1) COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND A COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED FOR THE JOB ON SITE AT ALL TIMES.
- 3. ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARDS, UNLESS SPECIFICALLY STATED OTHERWISE.
- 4. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF THE INTENDED IMPROVEMENTS SHOWN ON THESE DRAWINGS OR DESIGNATED TO BE PROVIDED, INSTALLED, CONSTRUCTED, REMOVED, AND RELOCATED UNLESS SPECIFICALLY NOTED OTHERWISE.
- 5. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES, AS SHOWN ON THESE PLANS, IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL VERIFY PERTINENT LOCATIONS AND ELEVATIONS, ESPECIALLY AT THE CONNECTION POINTS AND AT POTENTIAL UTILITY CONFLICTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS.
- 6. THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH THE AUTHORITY HAVING JURISDICTION AND ALL UTILITY COMPANIES/DISTRICTS INVOLVED WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION AND TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION AND WITH A MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY UTILITY SERVICE.
- 7. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE ALL UTILITY RELOCATIONS, WHETHER SHOWN OR NOT SHOWN ON THE PLANS.
- 8. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH UTILITY COMPANIES/DISTIRCTS TO OBTAIN TEMPORARY POWER, TELEPHONE AND WATER SERVICE DURING CONSTRUCTION. ALL COST FOR TEMPORARY SERVICES SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
- 9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND LICENSES FOR WORK INCLUDING, BUT NOT LIMITED TO DEMOLITION, STREET CUTS, UTILITY INTERFERENCES, TRAFFIC CONTROL. GRADING. AND UTILITY FROM ALL APPLICABLE AGENCIES.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF PROJECT SAFETY INCLUDING, BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.
- 11. THE CONTRACTOR SHALL INSTALL AND MAINTAIN CONSTRUCTION FENCE PRIOR TO, AND THROUGHOUT CONSTRUCTION.
- 12. UTILITY TRENCHES ARE TO BE SLOPED OR BRACED AND SHEETED AS NECESSARY TO FURNISH SAFE WORKING CONDITIONS FOR THE WORKMEN AND THE PROTECTION OF OTHER UTILITIES IN COMPLIANCE WITH APPLICABLE LOCAL, STATE, AND FEDERAL HEALTH AND SAFETY RULES AND REGULATIONS.
- 13. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN, IN ACCORDANCE WITH THE M.U.T.C.D. TO THE APPROPRIATE RIGHT-OF-WAY AUTHORITY (CITY, COUNTY, OR STATE) FOR APPROVAL, PRIOR TO ANY CONSTRUCTION ACTIVITIES WITHIN, OR AFFECTING, THE RIGHT-OF-WAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY AND ALL TRAFFIC CONTROL DEVICES AS MAY BE REQUIRED BY THE CONSTRUCTION ACTIVITIES.
- 14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL WORK AND INSPECTIONS AS REQUIRED BY THE CITY, STATE, OR LOCAL DISTRICTS. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED SHALL BE PERFORMED PRIOR TO ANNOUNCED BUILDING POSSESSION AND THE FINAL CONNECTION OF SERVICES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COST ASSOCIATED WITH INSPECTION.
- 15. THE CONTRACTOR SHALL NOTIFY THE LOCAL AUTHORITY (CITY, COUNTY, OR STATE) AND LOCAL UTILITY COMPANIES/DISTRICTS AT LEAST 48 HOURS PRIOR TO THE START OF ANY CONSTRUCTION.
- 16. THE CONTRACTOR SHALL NOTIFY THE LOCAL AUTHORITY (CITY, COUNTY, OR STATE) AND LOCAL UTILITY COMPANIES/DITRICTS AT LEAST 48 HOURS PRIOR TO THE START OF ANY EARTH DISTURBING ACTIVITY.
- 17. ALL STRUCTURAL EROSION CONTROL MEASURES SHALL BE INSTALLED AT THE LIMITS OF CONSTRUCTION, PRIOR TO ANY GROUND-DISTURBING ACTIVITY. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED IN GOOD REPAIR BY THE CONTRACTOR UNTIL SUCH TIME AS THE ENTIRE DISTURBED AREA IS STABILIZED WITH HARD SURFACE OR PERMANENT LANDSCAPING.
- 18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.
- 19. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE RESTORATION OF ANY EXISTING IMPROVEMENTS INCLUDING, BUT NOT LIMITED TO STREET PAVEMENT, FENCES, SOD, LANDSCAPING, SPRINKLER SYSTEMS, AND UTILITIES DISTURBED DURING CONSTRUCTION TO THEIR ORIGINAL LOCATION AND CONDITION.
- 20. INCLUDED IN THIS PACKAGE IS THE DEMOLITION AND DISPOSAL OF ALL EXISTING UTILITIES, SITE IMPROVEMENTS AND SITE FURNISHINGS NEEDED FOR CONSTRUCTION OF THE IMPROVEMENTS SHOWN IN THIS SET OF CONSTRUCTION DRAWINGS. ALL QUESTIONS IN REGARD TO DEMOLITION SHALL BE SUBMITTED TO THE OWNER IN WRITING PRIOR TO BID.
- 21. ALL DEMOLITION, REMOVAL, DISPOSAL AND ABANDONMENT OF UTILITIES, STRUCTURES, SITE IMPROVEMENTS AND SITE FURNISHINGS SHALL BE DONE IN ACCORDANCE WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL STANDARDS AND SPECIFICATIONS.
- 22. DIMENSIONS FOR LAYOUT AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWING. IF PERTINENT DIMENSIONS ARE NOT SHOWN, CONTACT THE CONSULTING ENGINEER FOR CLARIFICATION, AND ANNOTATE THE DIMENSION ON THE AS-BUILT RECORD DRAWINGS.

#### **GENERAL NOTES (CONT.)**

- 23. RIM ELEVATIONS SHOWN ARE APPROXIMATE ONLY AND ARE NOT TO BE TAKEN AS FINAL ELEVATIONS. THE CONTRACTOR SHALL USE PRECAST CONCRETE ADJUSTMENT RINGS, GROUT, AND STEEL SHIMS TO ADJUST THE MANHOLE FRAMES TO THE REQUIRED FINAL GRADE IN CONFORMANCE WITH THE CITY OR UTILITY COMPANY/DISTIRCT STANDARD SPECIFICATIONS. ALL FRAMES SHALL BE ADJUSTED TO FINAL GRADE PRIOR TO THE FINAL LIFT OF ASPHALT
- 24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING AS-BUILT INFORMATION ON A SET OF RECORD DRAWINGS KEPT AT THE CONSTRUCTION SITE, AND AVAILABLE TO THE CITY INSPECTOR AT ALL TIMES.
- 25. AS-BUILT PLANS AND FIELD NOTES SHALL BE PROVIDED BY THE CONTRACTOR TO THE CITY, COUNTY, STATE, FEDERAL OR UTILITY DISTRICT; OR TO THE OWNER/DEVELOPER AS REQUIRED BY AND IN ACCORDANCE WITH THE GOVERNING AUTHORITY'S OR OWNER/DEVELOPER'S CRITERIA.
- 26 NO REVISIONS SHALL BE MADE TO THESE PLANS WITHOUT THE APPROVAL OF THE ACADEMY GATEWAY ARC, COUNTY, STATE OR LOCAL DISTRICT AND THE ENGINEER OF RECORD.
- 27. IF, DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED BY THE CONTRACTOR, HIS SUBCONTRACTORS, OR OTHER AFFECTED PARTIES, WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE PROJECT MANAGER FOR THE OWNER/DEVELOPER AND THE ENGINEER IMMEDIATELY.
- 28. <u>BENCHMARK VERIFICATION</u>: THE CONTRACTOR SHALL USE BENCHMARKS AND DATUMS SHOWN HEREON TO SET PROJECT BENCHMARK(S), BY RUNNING A LEVEL LOOP BETWEEN AT LEAST TWO BENCHMARKS, AND SHALL PROVIDE SURVEY NOTES OF SUCH TO PROJECT ENGINEER PRIOR TO COMMENCING CONSTRUCTION.
- 29. ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH THE GOVERNING AUTHORITY'S STANDARDS AND SPECIFICATIONS.
- 30. THE SITE CONSTRUCTION PLANS INDICATE THE WORK TO BE COMPLETED FOR THE PROJECT. THE CONTRACTOR SHALL COORDINATE SAID WORK WITH THE OWNER AND THE TENANT CONSTRUCTION MANAGER(S) TO CONFIRM THE LIMITS AND DETAILS OF THE WORK TO BE PROVIDED AND COMPLETED.
- 31. THE SITE CONSTRUCTION PLANS LISTED WITHIN THIS PLAN SET ARE NOT FOR CONSTRUCTION UNLESS APPROVED BY THE APPROPRIATE GOVERNING JURISDICTION. THE CONTRACTOR SHALL CONFIRM SAID APPROVAL PRIOR TO CONSTRUCTION.



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC



STERLING DESIGN ASSOCIATES, LLC

NO.: 1	DATE: 5/16/2017	BY: JDS
DESCRIPT	TION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: 8/24/2017	BY: JDS
DESCRIPT	TION: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	DATE: -	BY: -
DESCRIPT	TION: -	
NO.: 4	DATE: -	BY: -
DESCRIPT	TION: -	
NO.: 5	DATE: -	BY: -
DESCRIPT	TION: -	

DATE: 8/24/2017	SCALE: 1" = 20'
PROJECT MANAGER:  JDS	PROJECT NO.:
DRAWN BY:	DRAWING FILE:
	-

PROJECT:

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

CLIENT:

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

**GENERAL NOTES** 

SHEET NUMBER:

2 OF 19 (C10

#### ACADEMY GATEWAY SUBDIVISION FILING NO. I LOT 2 A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 Sidewalk should extend to the edge of the TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL property. Show the PED Provide fence locations as shown in MERIDIAN, EL PASO COUNTY, COLORADO improvement on your property and it is MAJOR SITE DEVELOPMENT PLAN required with development of this property GLENEAGLE GATE VIEW (50' PRIVATE R.O.W.) -5.0' PUBLIC UTILITY AND DRAINAGE EASEMENT R = 325.00'40.0' PUBLIC UTILITY $1 = 94.31^{\circ}$ DRAINAGE AND 10.0' LANDSCAPE SETBACK TRACT C ACCESS DRIVE OTHERS) ACADEMY GATEWAY SUBDIVISION FILING NO. 2.221 ACRES 2,200 S.F. PATIO **RESTAURANT WITH** DRIVE-THRU ATE (BY ROAD ROW.) 2 feet of overhang is required with an 8' x 40.0' PUBLIC **LANDSCAPE** 15' space. A SETBACK ACĂDĘMY ĠATEŴAY ▽ ▽ ▽ **EASEMENT** transformer is shown SUBDIVISION FILING NO. 1 here; that may be a R=3.0'\ 30/0' PUBL'IC safety concern. UTILITM Remove parking EASEMENT/ 25.0 | SETBACK ⊢R=50.0' / ~R=20.0' 10:0' INTERNAL ∕—{10}—√ \-R=12.0' LANDSCAPE SETBACK R=12.0'¬ Dumpster screenihal must be 100% AND ACC-EASEMENT opaque; concrete, cm/u, stuc/co, not/mesh screening/1// / IMPROV#MENTS EASEME∕NT $\pm$ 7.0' DRA $\mathbb N$ AGE EASEME/NT 20.0' PUBLIC ACADEMY GATEWAY LEGAL DESCRIPTION UTILITY AND SUBDIVISION FILING NO. 1 DRAINAGE 1.310 ACRES LOT 2, ACADEMY GATEWAY SUBDIVISION FILING NO. 1 EASEMENT. EL PASO COUNTY. COLORADO **LEGEND** BASIS OF BEARINGS BEARINGS ARE BASED ON A PORTION OF THE EASTERLY BOUNDARY OF THE UNITED STATES ─ ─ ─ PROPERTY LINE AIR FORCE ACADEMY AS RECORDED IN PLAT BOOK O-2 AT PAGE 84, RECORDS OF EL PASO EXIST. EASEMENT LINE COUNTY, COLORADO, BEING MONUMENTED AT THE NORTHERLY END BY A3.5" BRASS CAP STAMPED "BDY 43" IS ASSUMED TO BEAR N 56°38'25"W, A DISTANCE OF 630.29 FEET. **PROPOSED** BY OTHERS PAVING NOTE EXIST. SIGN ALL PAVING, INCLUDING CURB, GUTTER, AND SIDEWALK, TO BE INSTALLED OUTSIDE OF THE LANDSCAPE AREA RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL REPORT PAVEMENT PROP. RETAINING WALL RECOMMENDATIONS INCLUDING, BUT NOT LIMITED TO, THE PAVEMENT SECTION, MATERIALS SELECTION, AND SUBGRADE PREPARATION. COORDINATE ANY DESIRED MODIFICATIONS WITH FIRE HYDRANT BY OTHERS THE DEVELOPER'S CONSTRUCTION MANAGER FOR APPROVAL PRIOR TO CONSTRUCTION. FOR 8 GEOTECHNICAL REPORTS WHICH DO NOT SPECIFICALLY ADDRESS CURB, GUTTER, SIDEWALK PARKING COUNT AND SIMILAR IMPROVEMENTS <u>NOT SUBJECT TO VEHICULAR LOADS</u>, CONCRETE SHALL BE COMPACT PARKING SPACE CDOT CLASS B, IN ACCORDANCE WITH CDOT STANDARDS AND SPECIFICATIONS. CONCRETE PAVING BY OTHERS 10 PROPOSED CONCRETE PAVING **CAUTION - NOTICE TO CONTRACTOR** SCALE: 1"= 20' PROPOSED STORM INLET THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS SITE DATA **CONSTRUCTION MANAGER** UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR COORDINATION NOTE MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY LOT AREA 0.985 ACRES EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. IT SHALL BE THE (CS) COMMERCIAL SERVICES ZONING RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH THE CONTRACTOR IS TO COORDINATE THE MATERIALS CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. BUILDING SQUARE FOOT 2,200 (SF) PROVISION, PLACEMENT, AND INSTALLATION OF ANY STARBUCKS PROTOTYPICAL SITE APPURTENANCES OVERALL LOT COVERAGE 26,052 SF (60.72%) THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING IMPROVEMENTS 12. MAXIMUM JOINT SPACING IN PEDESTRIAN AREAS (I.E. SIDEWALKS AND PATIOS) SHALL BE 5' UNLESS OTHERWISE INDICATED. WITH THE OWNER PRIOR TO CONSTRUCTION. THIS AND UTILITIES AND SHALL REPAIR ANY DAMAGE AT HIS EXPENSE. LANDSCAPE COVERAGE (EXCLUDING BUFFER) 7,324 (SF) INCLUDES, BUT IS NOT LIMITED TO, SITE DIRECTIONAL/RESTRICTION/COMPLIMENTARY SIGNAGE. LANDSCAPE BUFFER AREA 9,530 (SF) CLEARANCE BAR, PRE-MENU BOARD, ORDER POINT

22 SPOTS 1PER 100 SF BUILDING

23 TOTAL PARKING SPOTS

20 REGULAR, 2 COMPACT, 1 HANDICAP SPOT

3 BIKE RACKS EQUALS 6 BIKE PARKING SPOTS

PARKING REQUIRED

PARKING PROVIDED

BIKE RACKS PROVIDED

SCHEDULE NOTES

- PROPOSED 2,200 SQ. FT. RESTAURANT WITH DRIVE-THRU. SEE ARCHITECTURAL ELEVATIONS
- 2 > PROPOSED ORDER POINT CANOPY WITH DIGITAL ORDER SCREEN AND MENU BOARD TO BE INSTALLED PER TENANT STANDARDS. SEE SIGNAGE DETAILS.
- $\langle$   $_3$  angle PROPOSED PRE-MENU BOARD TO BE INSTALLED PER TENANT STANDARDS. SEE SIGNAGE

  - 4 PROPOSED 2.0-FOOT MAXIMUM HEIGHT RETAINING WALL. REFER TO GRADING PLAN FOR CONTRACTOR. CONTRACTOR TO SUBMIT TO THE ARCHITECT AND OWNER/DEVELOPER A COLOR. AND TEXTURE SAMPLE OF THE WALL MATERIAL TO BE USED PRIOR TO INSTALLATION.
  - $\langle$  5  $\rangle$  PROPOSED CLEARANCE BAR TO BE INSTALLED PER TENANT STANDARDS. SEE SIGNAGE
  - $\langle$  6  $\rangle$  CONTRACTOR TO PAINT OR INSTALL WHITE (OUTGOING) AND GREEN (INCOMING) VEHICLE DIRECTIONAL ARROWS. SEE SITE DETAILS
  - $\langle 7 \rangle$  NOT USED.
  - $\langle 8 \rangle$  NOT USED.
  - $\langle$  9  $\rangle$  PROPOSED R1-1 "STOP" SIGN ON STEEL POST. SEE SITE DETAILS.
  - $\langle 10 \rangle$  PROPOSED ASPHALT PAVING (SEE PAVING NOTE, THIS SHEET)
  - (11) PROPOSED CONCRETE PAVING (SEE PAVING NOTE, THIS SHEET)
  - (12) PROPOSED CONCRETE SIDEWALK (SEE PAVING NOTE, THIS SHEET). SEE SITE DETAILS
  - $\langle 13 \rangle$  NOT USED.
  - (14) PROPOSED SITE LIGHT. SEE PHOTOMETRIC PLAN.
  - $\langle 15 \rangle$  PROPOSED TRASH ENCLOSURE. SEE ARCHITECTURAL PLANS.
  - $\langle$ 16 $\rangle$ PROPOSED OUTDOOR PATIO AREA. SEE ARCHITECTURAL PLANS.
  - $\langle$ 17 $\rangle$  PROPOSED ADA PARKING AREA, STRIPING AND SYMBOLS. SEE SITE DETAILS.
  - $\langle$   $_{18}
    angle$  PROPOSED ADA PARKING SIGNAGE ON STEEL POST. SEE SITE DETAILS
  - (19) PROPOSED 6.0-FOOT WIDE CONCRETE CROSSPAN. SEE SITE DETAILS
  - WIDE BY 6-FEET LONG WHITE PEDESTRIAN CROSSWALK BARS 4-FEET ON CENTER
  - (21) PROPOSED 6" CURB AND 12" GUTTER. SEE SITE DETAILS.
  - 22 PROPOSED PIPE BOLLARD. SEE SITE DETAILS.
  - (23) PROPOSED PRIVATE ADA-ACCESSIBLE CURB RAMP. SEE SITE DETAILS.
  - $\langle 24 \rangle$  FIRE HYDRANT BY OTHERS TO REMAIN.
  - $\langle 25 \rangle$  CURB AND GUTTER BY OTHERS
  - $\langle$ 26angle PROPOSED 9.0' X 19.0' REGULAR PARKING STALL WITH 4" WIDE, WHITE PARKING STRIPING.
  - $\langle 27 
    angle$  PROPOSED 8.0' X 15.0' COMPACT PARKING STALL WITH 4" WIDE, WHITE PARKING STRIPING AND "COMPACT CARS ONLY" SIGNAGE ON STEEL POST. SEE SITE DETAILS
  - (28) PROPOSED 2.0-FOOT WIDE BY 10.0-FOOT LONG WHITE STOP BAR STRIPING
  - $\langle 29 
    angle$  PROPOSED DETECTOR LOOP CENTERED ON ORDER POINT AND/OR DRIVE-THROUGH WINDOW 2-INCHES BELOW FINISHED GRADE. CONTRACTOR TO EXTEND CONDUIT TO DRIVE-THROUGH. ORDER POINT / MENU BOARD / ELECTRICAL PANEL. CONTRACTOR SHALL INSTALL ALL WORK IN ACCORDANCE WITH TENANT STANDARDS
  - (30) PROPOSED TRANSFORMER LOCATION. SEE UTILITY PLAN.
  - (31) PROPOSED BIKE RACK (SEE SITE DETAILS)
  - (32) PROPOSED 6" VERTICAL CONCRETE CURB. CONTRACTOR MAY ELECT TO POUR MONOLITHICAL TO THE ADJOINING CONCRETE PAVING. SEE SITE DETAILS.
  - (33) PROPOSED PATIO RAILING / SCREEN FENCE. SEE ARCHITECTURAL PLANS.
  - $\langle 34 \rangle$  PROPOSED MONUMENT SIGN. SEE SIGNAGE DETAILS.
  - $\langle 35 \rangle$  APPROXIMATE LOCATION OF PROPOSED PIPE BOLLARD. CONTRACTOR TO COORDINATE LOCATION WITH ELECTRIC SERVICE PROVIDER.\
  - (36) PROPOSED COMBINATION TYPE 13 INLET. SEE UTILITY DETAILS.
  - $\langle 37 \rangle$  SITE DISTANCE TRIANGLE

### **GENERAL NOTES**

CANOPY, DIGITAL ORDER SCREEN, MENU BOARD.

AND THE ORDER POINT STAR PATTERN PAVEMENT

MARKINGS.

DETECTOR LOOPS, DIRECTIONAL PAVEMENT MARKINGS,

- 1. THE CONTRACTOR SHALL REFER TO THE GRADING PLAN FOR CURB TYPES AND FLOWLINE ELEVATIONS
- 2. THE CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF EXIT PORCHES, RAMPS, STAIRS, PRECISE BUILDING DIMENSIONS, AND BUILDING ENTRANCES.
- 3. ALL SIGNING AND STRIPING WILL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND LOCAL CODES.
- 4. REFER TO ARCHITECTURAL PLANS FOR BUILDING SIGNAGE LOCATIONS.
- REFER TO ARCHITECTURAL PLANS FOR SITE ELECTRICAL INSTALLATION REQUIREMENTS INCLUDING, BUT NOT LIMITED TO LOCATION, TYPE, SIZE AND MATERIAL OF WIRING, CONDUIT, TRENCH, AND CONNECTION POINTS FOR SITE LIGHTING, SITE SIGNAGE, AND OTHER SITE IMPROVEMENTS.
- 6. SIGNAGE TO BE ON TIME CLOCK SYNCHRONIZED WITH EXTERIOR SITE LIGHTING.
- 7. CONTRACTOR SHALL COORDINATE SIGNAGE PERMIT, INSTALLATION, AND SPECIFICATIONS WITH OWNER.
- CONTRACTOR SHALL COORDINATE WIRELESS COMMUNICATIONS PACKAGE, INCLUDING VEHICLE DETECTION LOOP, WIRELESS COMMUNICATION, MONITORS, ETC. WITH OWNER OR THEIR REPRESENTATIVE. CONTRACTOR SHALL VERIFY REQUIREMENTS WITH OWNER OR THEIR REPRESENTATIVE.
- 9. SIGNAGE CONTRACTOR SHALL VERIFY SIZE AND LOCATION OF ANY AND ALL ALLOWABLE MONUMENT OR POLE SIGNAGE WITH OWNER AND PROVIDE SHOP DRAWINGS PRIOR TO FABRICATION TO OWNER FOR APPROVAL.
- 10. ALL RADIUS DIMENSIONS SHOWN ARE TO FLOWLINES UNLESS OTHERWISE NOTED. ALL RADII ARE 3 FEET UNLESS OTHERWISE
- 11. PROVIDE JOINTS AT 30 X DEPTH OF CONCRETE MAXIMUM SPACING IN VEHICULAR AREAS UNLESS OTHERWISE INDICATED.
- JOINTS SHOULD EXTEND THROUGH THE ADJACENT CURB.
- CONTROL JOINTS THEREIN SHALL BE TOOL FINISHED.
- 13. THE CONTRACTOR SHALL SELECT AND USE A BOND BREAKER AT INLETS, MANHOLES, AND SIMILAR SIZE STRUCTURES. SMALLER STRUCTURES SUCH AS VALVE COVERS AND MONUMENT BOXES SHALL NOT REQUIRE A BOND BREAKER, UNLESS SPECIFIED OTHERWISE.
- 14. PROVIDE A CONTINUOUS ISOLATION JOINT AT THE BUILDING FOUNDATION. REFERENCE THE ARCHITECTURAL PLANS FOR ADDITIONAL INFORMATION.



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC



STERLING DESIGN ASSOCIATES. LLC

/	ISSUES & REVISIONS	
N	NO.: 1 DATE: 5/16/2017	BY: JDS
	DESCRIPTION: 1ST MAJOR SDP SUBMITTAL	
	NO.: <b>2</b> DATE: 8/24/2017	BY: JDS
	DESCRIPTION: 2ND MAJOR SDP SUBMITTAL	
	NO.: 3 DATE: -	BY: -
LLY	DESCRIPTION: -	
	NO.: 4 DATE: -	BY: -
	DESCRIPTION: -	
	NO.: 5 DATE: -	BY: -
	DESCRIPTION: -	
	NO.: 6 DATE: -	BY: -
	DESCRIPTION: -	

DATE: <b>8/24/2017</b>	SCALE: 1" = 20'
PROJECT MANAGER:  JDS	PROJECT NO.:
DRAWN BY:	DRAWING FILE:
	-

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

SITE PLAN

SHEET NUMBER:

3 OF 19

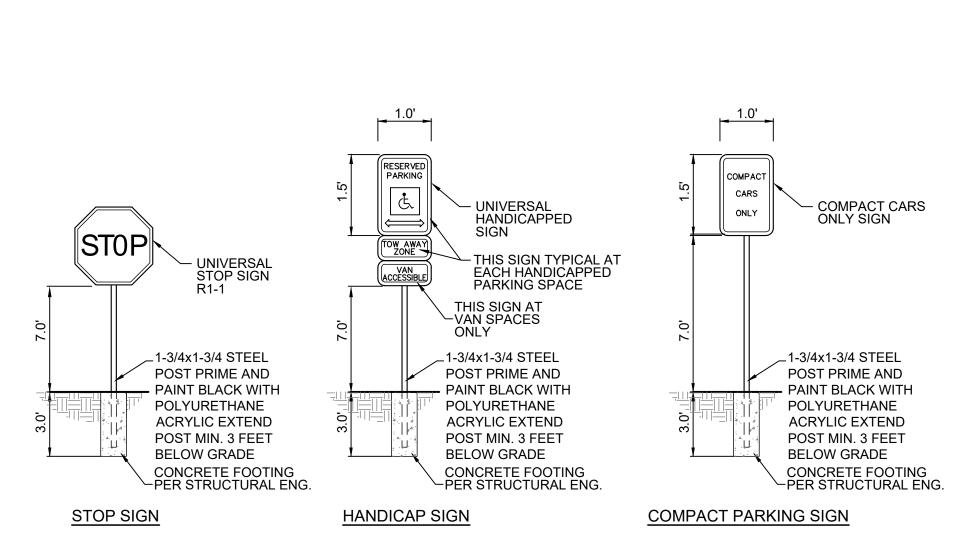
**COUNTY PROJECT NUMBER PPR-17-030** 

C201

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

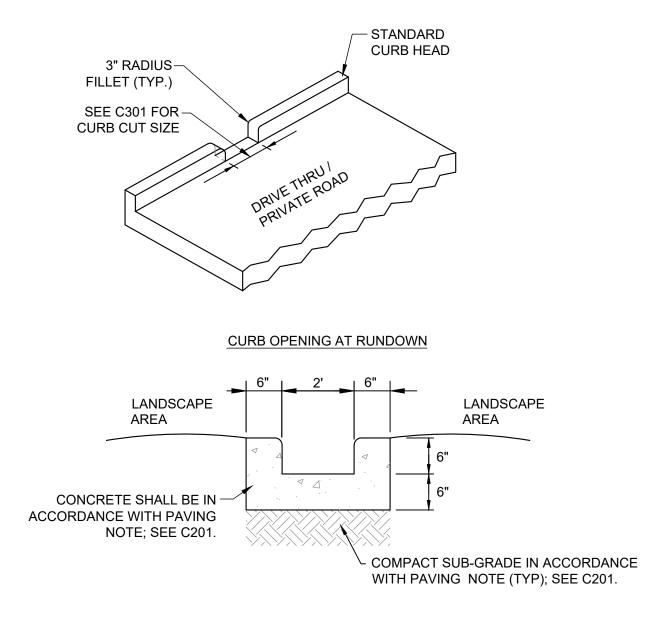
MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN



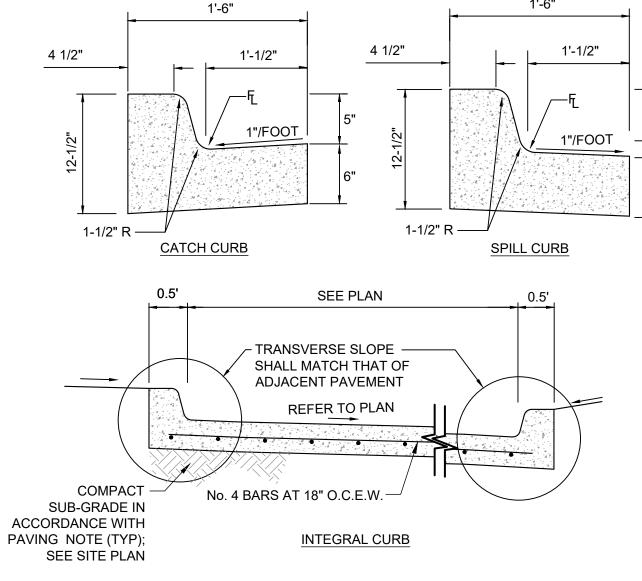
PROVIDE TRAFFIC CONTROL SIGNS COMPLYING WITH U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION'S MANUAL "UNIFORM TRAFFIC CONTROL DEVICES", LOCAL CODES, AND AS SPECIFIED.

ON-SITE TRAFFIC SIGNAGE DETAIL



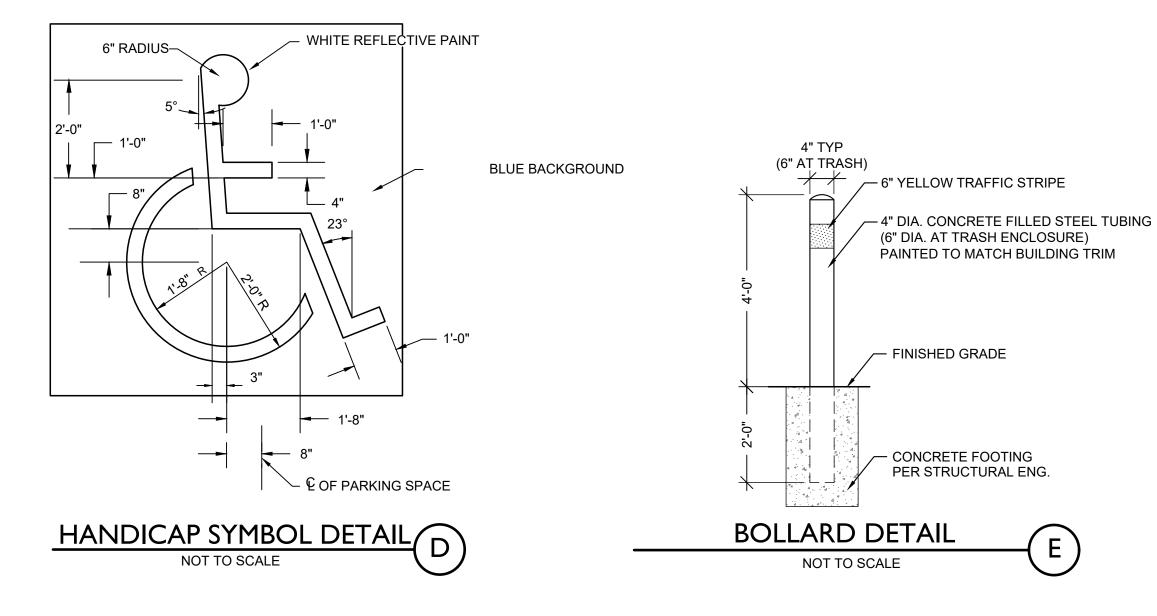
CONCRETE RUNDOWN

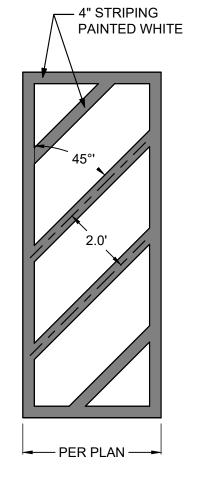
CONCRETE RUNDOWN (PRIVATE) B



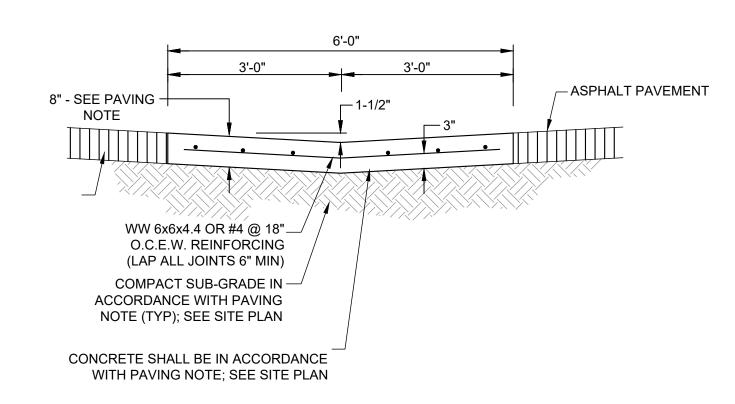
1'-6"

CONCRETE FOR CURB & GUTTER SHALL BE IN ACCORDANCE WITH PAVING NOTE; SEE SITE PLAN UNLESS CONCRETE FOR INTEGRAL VEHICULAR PAVEMENT IS MORE STRINGENT





STRIPING REQUIREMENTS NOT TO SCALE



6' CONCRETE CROSSPAN DETAIL (PRIVATE) (H



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www. Sterling Design Associates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

NOT FOR CONSTRUCTION

STERLING DESIGN ASSOCIATES, LLC

NO.: 1	DATE: 5/16/2017	BY: JDS
DESCRIPTION	ON: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: 8/24/2017	BY: JD:
DESCRIPTION	ON: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	DATE: -	BY: -
DESCRIPTION	ON: -	
NO.: 4	DATE: -	BY: -
DESCRIPTION	ON: -	
NO.: 5	DATE: -	BY: -
DESCRIPTION	ON: -	

DATE: 8/24/2017	SCALE: 1" = 20'
PROJECT MANAGER:  JDS	PROJECT NO.:
DRAWN BY:	DRAWING FILE:
	-

PROPOSED COFFEE SHOP STRUTHERS RD. **COLORADO SPRINGS, CO** 

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

SITE DETAILS

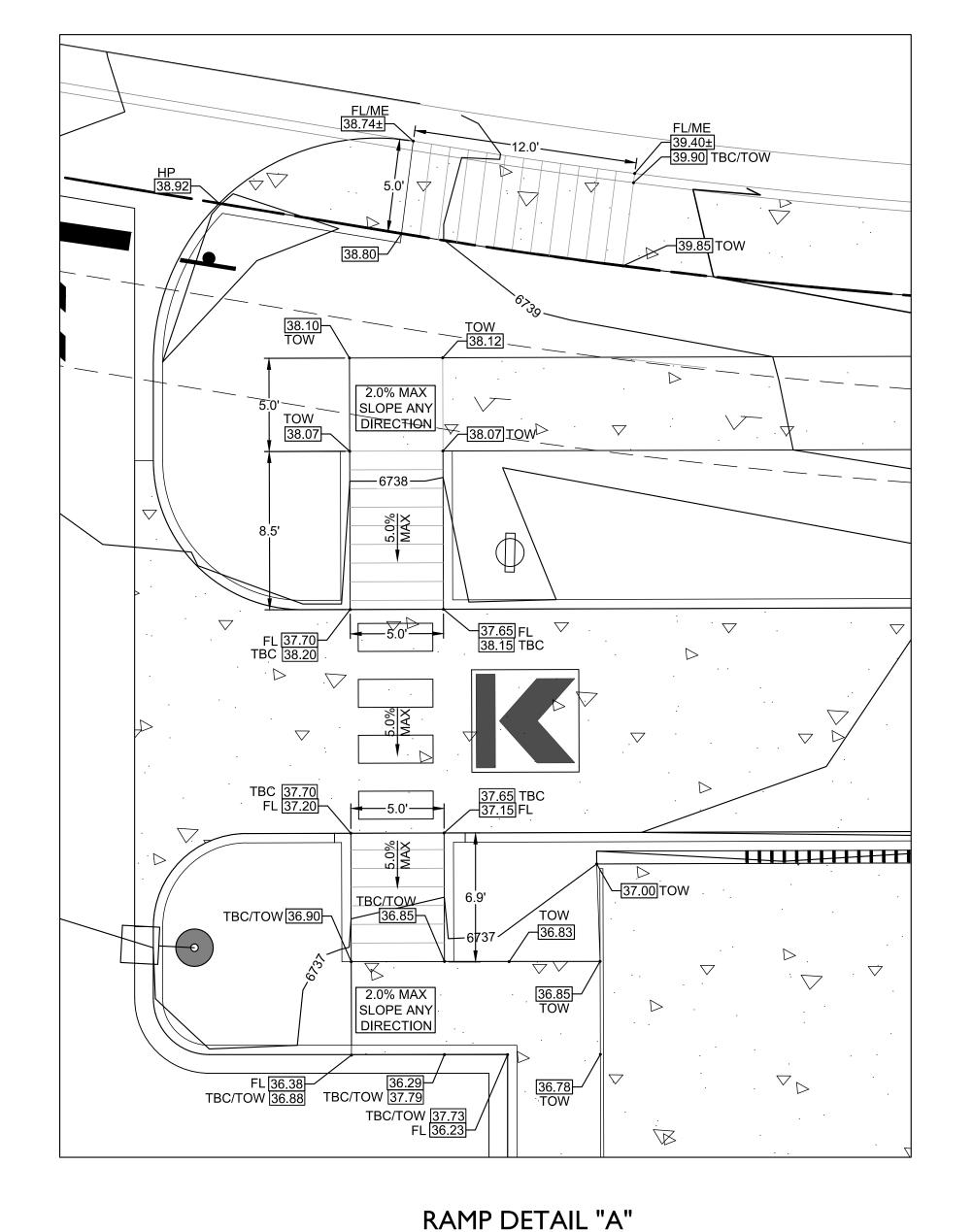
SHEET NUMBER:

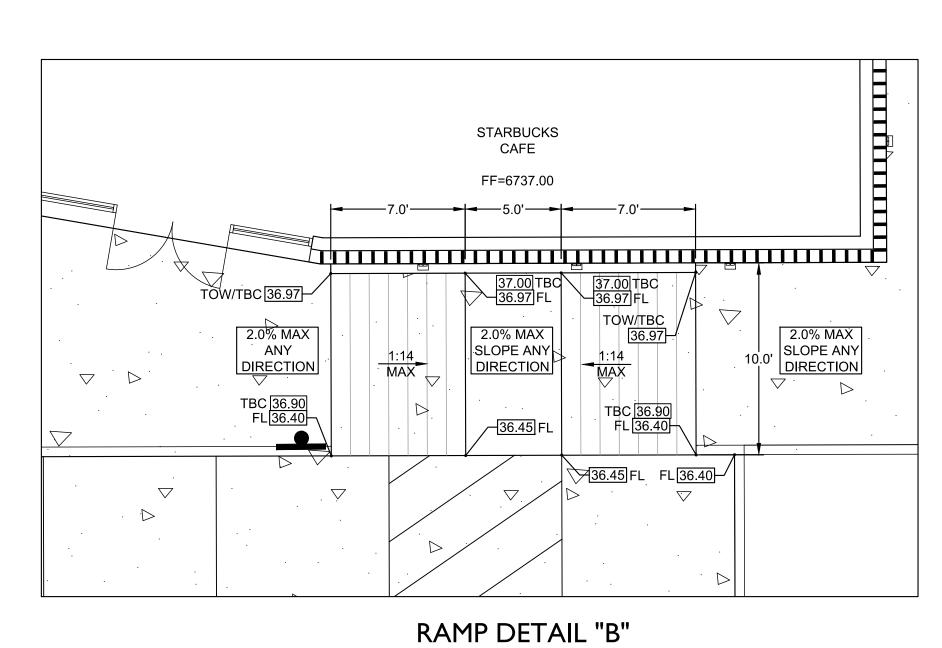
5 OF 19 C211

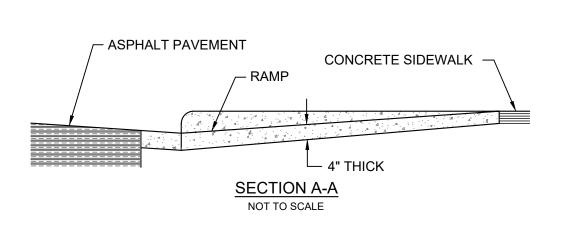
A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN





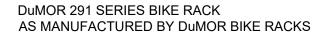


### NOTES:

SCALE: 1" = 5'

- 1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH 4. MINIMUM WIDTH OF RAMPS SHALL BE 4 FEET AND
- LOCAL STANDARDS AND SPECIFICATIONS. 2. CONCRETE FOR SIDEWALK RAMPS SHALL BE CDOT 5.
- 3. RAMP AND WINGS SHALL BE POURED MONOLITHICALLY.
- RAMP SLOPES SHALL NOT BE STEEPER THAN 12:1. MAINTAIN BACK OF WALK ELEVATION AT 2.0%
- ABOVE TOP OF CURB. NORMAL GUTTER FLOW LINE AND PROFILE SHALL BE MAINTAINED THROUGH THE RAMP AREA.
- RAMP SURFACE SHALL HAVE A COARSE BROOM FINISH WITH STRIATIONS PERPENDICULAR TO THE DIRECTION OF RAMP SLOPE.
- 8. A 1/2" EXPANSION JOINT SHALL BE REQUIRED WHERE THE CONCRETE RAMP JOINS ANY RIGID PAVEMENT OR STRUCTURE.

CURB RAMP DETAIL (PRIVATE) NOT TO SCALE



**BIKES PARKED PER UNIT:2** 

MATERIALS: 2.0" X 1/4" STEEL TUBE

**FINISHES** 

AN AFTER FABRICATION ZINC RICH EPOXY WITH EITHER HOT DIP GALVANIZE OR POLYESTER POWDER COATINGS IS STANDARD.

POWDER COAT FINISH ASSURES A HIGH LEVEL OF ADHESION AND DURABILITY BY FOLLOWING THESE STEPS

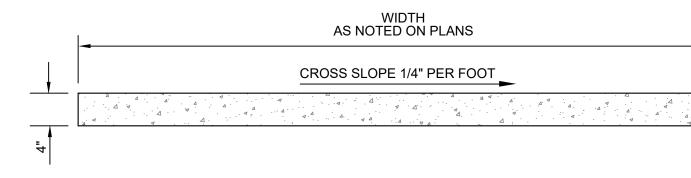
- 2. IRON PHOSPHATE PRETREATMENT
- 3. EPOXY PRIMER ELECTROSTATICALLY APPLIED 4. FINAL THICK TGIC POLYESTER POWDER COAT

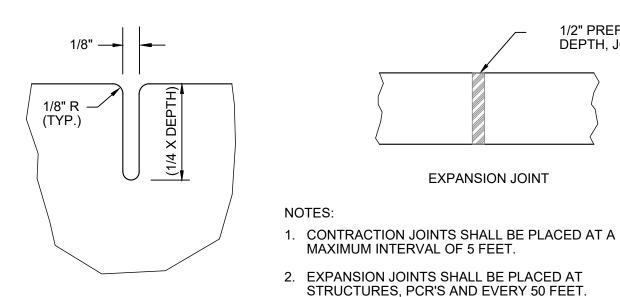
#### NOTES:

1. SANDBLAST

- 1. INSTALL RACK PARALLEL TO WALKWAY AS SHOWN SPACED 30" FROM EDGE. 2. INSTALL WITH IG (IN-GROUND) MOUNT PER MANUFACTURER'S RECOMMENDATIONS.
- 3. BLACK COLOR, POWDERCOAT







**CONTRACTION OR** WEAKENED PLANE



3. EXPANSION JOINT FILLER SHALL MEET AASHTO M 213 CRITERIA AND BE APPLIED IN ACCORDANCE WITH

MANUFACTURER'S RECOMMENDATIONS.



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

PRELIMINARY **NOT FOR CONSTRUCTION** 

STERLING DESIGN ASSOCIATES, LLC

-	ISSUES & REVISIONS	
	NO.: 1 DATE: 5/16/2017	BY: JDS
<u> </u>	DESCRIPTION: 1ST MAJOR SDP SUBMITTAL	
4	NO.: 2 DATE: 8/24/2017	BY: JDS
	DESCRIPTION: 2ND MAJOR SDP SUBMITTAL	
	NO.: 3 DATE: -	BY: -
	DESCRIPTION: -	
1/2" PREFORMED, FULL DEPTH, JOINT MATERIAL	NO.: 4 DATE: -	BY: -
	DESCRIPTION: -	
	NO.: 5 DATE: -	BY: -
<	DESCRIPTION: -	
<i>&gt;</i>	NO.: 6 DATE: -	BY: -
	DESCRIPTION: -	

DATE: 8/24/2017	SCALE: <b>1" = 20'</b>	
PROJECT MANAGER:  JDS	PROJECT NO.:	
DRAWN BY:	DRAWING FILE:	

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

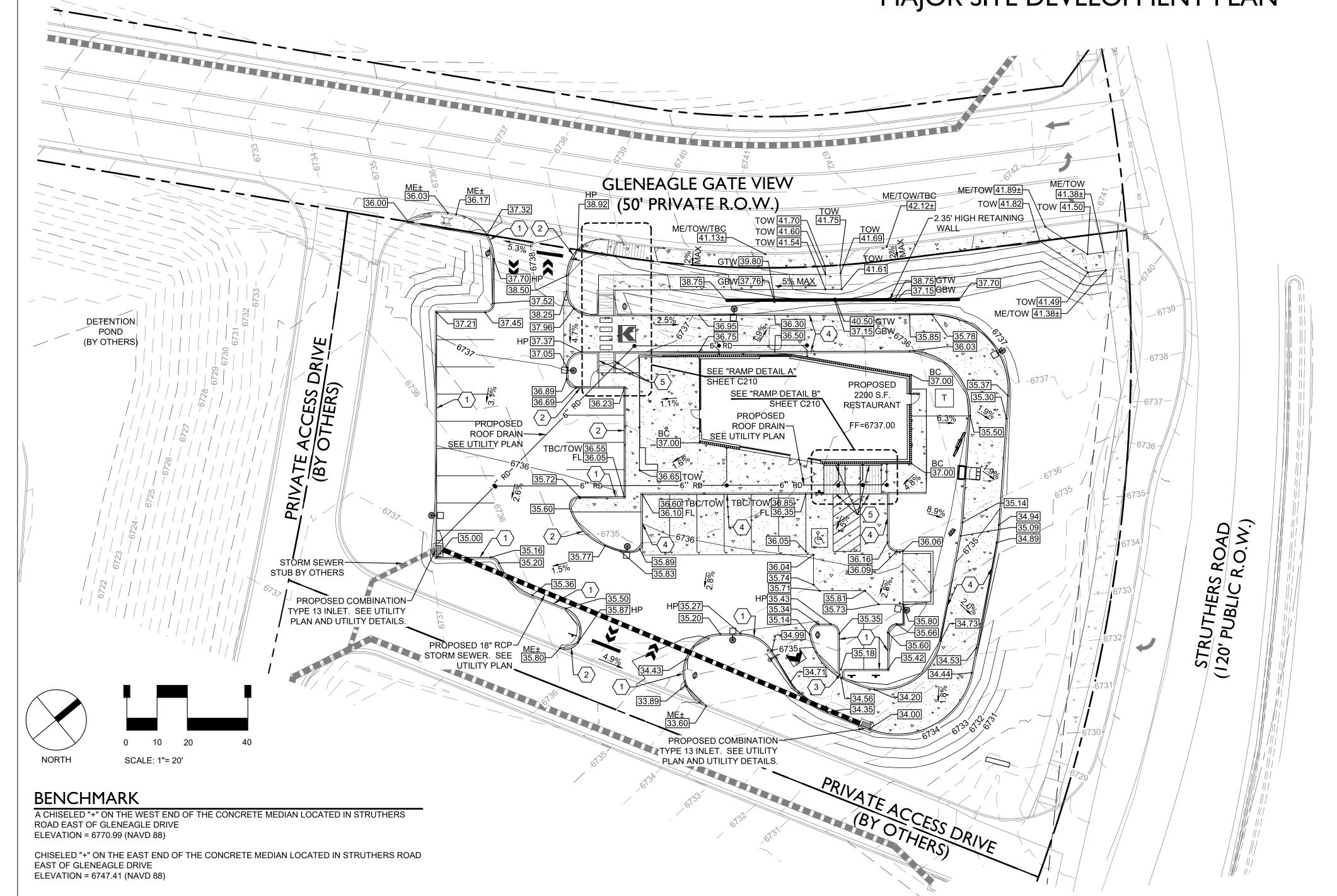
SITE DETAILS

SHEET NUMBER:

4 OF 19 C210

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION 1, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

MERIDIAN, EL PASO COUNTY, COLORADO MAJOR SITE DEVELOPMENT PLAN



## PROPOSED WITHIN DISTURBED AREA

TOTAL LAND DISTURBANCE FOR PROJECT: 37,794 SQ. FT.± 0.87 ACRES±

## **CAUTION - NOTICE TO CONTRACTOR**

THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING IMPROVEMENTS

# AND UTILITIES AND SHALL REPAIR ANY DAMAGE AT HIS EXPENSE.

### **GRADING LEGEND**

0.58%

	R.O.W. / PROPERTY LINE	(\$)	EXIST. MANHOLE
	— ADJOINING PROPERTY		EXIST. FIRE HYDRANT
	EASEMENT LINE EXISTING TO REMAIN	WV 	EXIST. WATER VALVE
	PROP. CURB AND GUTTER	TBC	TOP BACK OF CURB
	■ PROPOSED STORM SEWER	HP	HIGH POINT
		TOW	TOP OF WALK
6" RD	PROPOSED UNDERGROUND ROOF DRAIN LINE	ME	MATCH EXISTING
	PROPOSED STORM INLET	EC	EDGE OF CONCRETE
7540	EVIST CONTOLID	GTW	GRADE AT TOP OF WALL
- — — 7512— -	EXIST. CONTOUR	GBW	GRADE AT BOTTOM OF WA
7512	— PROP. CONTOUR	FL	FLOW LINE
7.53	PROP. SPOT ELEVATION	GB	GRADE BREAK
76		ВС	BUILDING CORNER

EXIST. SPOT ELEVATION

PROP. SLOPE

### **SCHEDULE NOTES**

- $\langle 1 \rangle$  CONSTRUCT 6" VERTICAL CONCRETE CURB AND GUTTER WITH 1-FOOT CATCH PAN. SEE SITE
- (2) CONSTRUCT 6" VERTICAL CONCRETE CURB AND GUTTER WITH 1-FOOT SPILL PAN. SEE SITE
- (3) CONSTRUCT 1.0-FOOT WIDE CONCRETE RUNDOWN. SEE SITE DETAILS.
- $\langle$  4 angle CONSTRUCT 6-INCH VERTICAL CONCRETE CURB. CONTRACTOR MAY ELECT TO CONSTRUCT  $^{\prime}$  MONOLITHICALLY TO THE ADJOINING CONCRETE PAVING. SEE SITE DETAILS.
- $\overline{\langle \, 5 \, \rangle}$  CURB VARIES IN HEIGHT FROM 0-INCHES TO 6-INCHES.
- $\langle$  6 angle CONSTRUCT HANDICAP PARKING AREA. NO GRADES GREATER THAN 2.0% IN ANY DIRECTION. CONTRACTOR TO VERIFY WITH CONSTRUCTION MANAGER AT FORM SET OR REMAIN SOLELY RESPONSIBLE FOR REMOVAL AND REPLACEMENT OF ANYU NON-ADA CONFORMING INSTALLATION.

#### **GENERAL GRADING NOTES**

- 1. REFER TO SHEET C100, C101, C304, AND C402 FOR ADDITIONAL PROJECT GENERAL
- 2. IF. DURING THE OVERLOT GRADING PROCESS, CONDITIONS ARE ENCOUNTERED BY THE CONTRACTOR, HIS SUBCONTRACTORS, OR OTHER AFFECTED PARTIES, WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE GEOTECHNICAL ENGINEER IMMEDIATELY FOR RECOMMENDATIONS.
- 3. ALL EARTHWORK, GRADING, OVERLOT GRADING, BACKFILLING, FILLING, EXCAVATION, COMPACTION, PAVEMENT, AND FLATWORK CONSTRUCTION WILL BE IN ACCORDANCE WITH THE RECOMMENDATIONS FROM THE GEOTECHNICAL INVESTIGATION PREPARED SPECIFICALLY FOR THIS SITE.
- 4. ALL CONCRETE PAVEMENT, CONCRETE FLATWORK, CONCRETE STRUCTURES AND CONCRETE UTILITIES SHALL BE IN ACCORDANCE WITH THE MATERIAL RECOMMENDATIONS FROM THE GEOTECHNICAL INVESTIGATION PREPARED SPECIFICALLY FOR THIS SITE.
- 5. THE EARTHWORK FOR ALL BUILDING FOUNDATIONS AND SLABS SHALL BE IN ACCORDANCE WITH ARCHITECTURAL AND STRUCTURAL PLANS AND SPECIFICATIONS, AND THE GEOTECHNICAL INVESTIGATION.
- 6. SPOT ELEVATIONS SHALL TAKE PRECEDENCE OVER CONTOURS AND SLOPES SHOWN. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF SPOT ELEVATIONS WHICH DO NOT APPEAR TO BE CONSISTENT WITH THE CONTOURS AND SLOPES. SPOT ELEVATIONS AND SPECIFIC PROFILE DESIGN SHALL BE USED FOR SETTING ELEVATIONS OF CURB AND GUTTER AND UTILITIES.
- 7. SPOT ELEVATIONS REPRESENT FLOWLINE (BOTTOM FACE OF CURB) WHERE SHOWN AT CURB AND GUTTER UNLESS OTHERWISE NOTED.
- 8. CONTOURS SHOWN ARE FOR FINISHED PAVING, SIDEWALK, SLAB, OR GROUND. ADJUSTMENT TO SUBGRADE IS THE CONTRACTOR'S RESPONSIBILITY
- 9. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING HIS OWN ESTIMATE OF EARTHWORK QUANTITIES.
- 10. REFER TO SITE PLAN FOR EXTENT OF PAVEMENT AND PAVEMENT SECTIONS
- 11. GRADES WITHIN ASPHALT PAVING AREAS SHALL BE CONSTRUCTED TO WITHIN 0.10 FEET OF THE DESIGN GRADE. HOWEVER, THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE IN ALL PAVEMENT AREAS AND ALONG CURBS. ALL CURBS SHALL BE BUILT IN ACCORDANCE WITH THE PLAN. CURBS OR PAVEMENT AREAS WHICH DO NOT PROVIDE PROPER DRAINAGE MUST BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
- 12. THE CONTRACTOR SHALL HAVE FORMS CHECKED BY A SURVEYOR FOR CURB AND GUTTER ADJACENT TO EXISTING ASPHALT OR CONCRETE. THE CROSS SLOPE SHALL NOT BE LESS THAN 2% OR GREATER THAN 4% FROM THE EXISTING SAWCUT LINE TO THE PROPOSED LIP OF GUTTER. DO NOT PLACE CONCRETE IN FORMS THAT HAVE BEEN CHECKED TO BE OR APPEAR IN ANY WAY INCORRECT. CONTACT THE ENGINEER IMMEDIATELY IF A PROBLEM SHOULD ARISE
- 13. THE CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- 14. THE CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL LANDSCAPED AND PAVED AREA.
- 15. ALL DISTURBED AREAS THAT ARE NOT DESIGNATED TO BE PAVED SHALL BE LANDSCAPED OR SEEDED, ACCORDING TO THE LANDSCAPE PLAN.
- 16. EXISTING DRAINAGE STRUCTURES SHALL BE INSPECTED AND REPAIRED AS NEEDED. AND EXISTING PIPES CLEANED OUT TO REMOVE ALL SILT AND DEBRIS.
- 17. IF ANY EXISTING STRUCTURES, SIDEWALK, AND/OR CURB AND GUTTER MODIFIED OR TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER PRIOR TO PROJECT CLOSEOUT.
- 18. ALL GRATES, MANHOLE RIMS, VALVE BOXES, VALVE COVERS, CLEANOUTS, AND VAULT OR BOX COVERS SHALL BE ADJUSTED TO "AS CONSTRUCTED" FINISHED GRADE PRIOR TO THE FINAL LIFT OF ASPHALT.
- 19. NO PROPOSED SLOPE IN LANDSCAPED AREAS OR OPEN SPACE SHALL EXCEED THREE (3) HORIZONTAL FEET TO ONE (1) VERTICAL FOOT, OR AS OTHERWISE SPECIFIED BY LOCAL CRITERIA.
- 20. THE CONTRACTOR SHALL PROTECT THE PROJECT BENCHMARK THROUGHOUT CONSTRUCTION AND SET ADDITIONAL PROJECT BENCHMARKS AS NECESSARY TO MAINTAIN VERTICAL CONTROL THROUGHOUT THE DURATION OF THE PROJECT.
- 21. THE CONTRACTOR SHALL FILL AND COMPACT BASEMENTS, CESSPOOLS, AND OTHER LARGE EXCAVATED AREAS WITH CLEAN FILL SUITABLE TO THE OWNER, AND IN ACCORDANCE WITH RECOMMENDATIONS OBTAINED FROM THE GEOTECHNICAL INVESTIGATION OR GEOTECHNICAL ENGINEER AND GRADE TO MATCH EXISTING OR PROPOSED FINISH GRADE; OR CONFIRM SUCH WORK HAS BEEN PERFORMED PRIOR TO CONSTRUCTION.

#### **ENGINEER'S STATEMENT**

THIS GRADING AND EROSION CONTROL PLAN WAS PREPARED UNDER MY DIRECTION AND SUPERVISION AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. SAID PLAN HAS BEEN PREPARED TO THE CRITERIA ESTABLISHED BY THE COUNTY FOR GRADING AND EROSION CONTROL PLANS. I ACCEPT RESPONSIBILITY FOR ANY LIABILITY CAUSED BY ANY NEGLIGENT ACTS, ERRORS, OR OMISSIONS ON MY PART IN PREPARING THIS REPORT.

### **OWNER'S STATEMENT**

THE OWNER WILL COMPLY WITH THE REQUIREMENTS OF THE GRADING AND EROSION CONTROL PLAN.

DATE

DATE

## **EL PASO COUNTY**

COUNTY PLAN REVIEW IS PROVIDED ONLY FOR GENERAL CONFORMANCE WITH COUNTY DESIGN CRITERIA. THE COUNTY IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND/OR ELEVATIONS WHICH SHALL BE CONFIRMED AT THE JOB SITE. THE COUNTY THROUGH THE APPROVAL OF THIS DOCUMENT ASSUMES NO RESPONSIBILITY FOR COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT.

FILED IN ACCORDANCE WITH THE REQUIREMENTS OF THE EL PASO COUNTY LAND DEVELOPMENT CODE, DRAINAGE CRITERIA AND ENGINEERING CRITERIA MANUAL AS AMENDED.

COUNTY ENGINEER / DIRECTOR

DATE



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC



STERLING DESIGN ASSOCIATES, LLC

NO.: 1	DATE: 5/16/2017	BY: JDS
DESCRIP	TION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: 8/24/2017	BY: JD
DESCRIP	TION: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	DATE: -	BY: -
DESCRIP	TION: -	
NO.: 4	DATE: -	BY: -
NO.: 4 DESCRIP		BY: -
DESCRIP		BY: -
DESCRIP	TION: - DATE: -	

PROJECT NO.:
-
DRAWING FILE:

PROPOSED COFFEE SHOP STRUTHERS RD. **COLORADO SPRINGS, CO** 

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

**GRADING PLAN** 

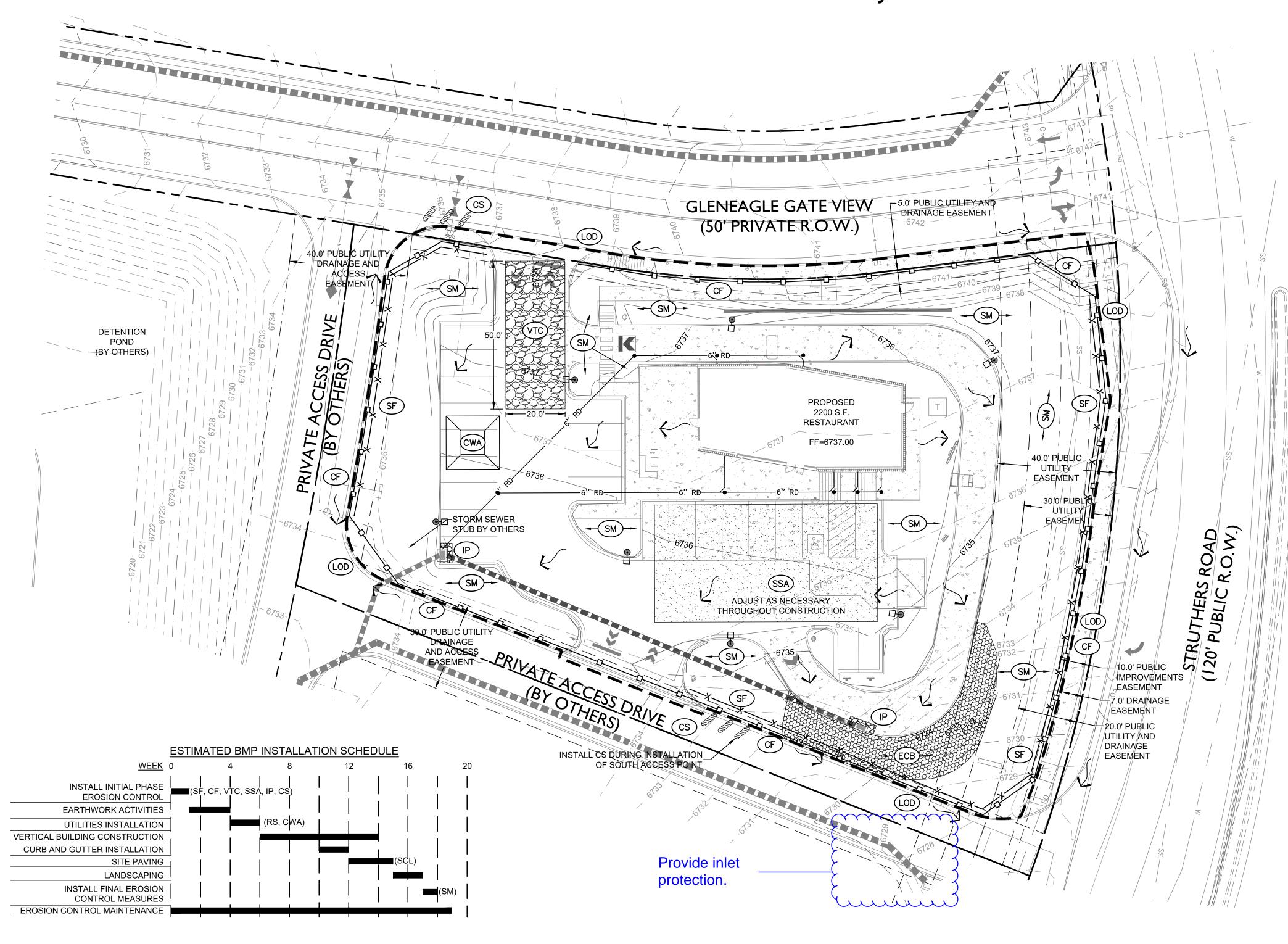
SHEET NUMBER:

6 OF 19 C301

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN



### **CAUTION - NOTICE TO CONTRACTOR**

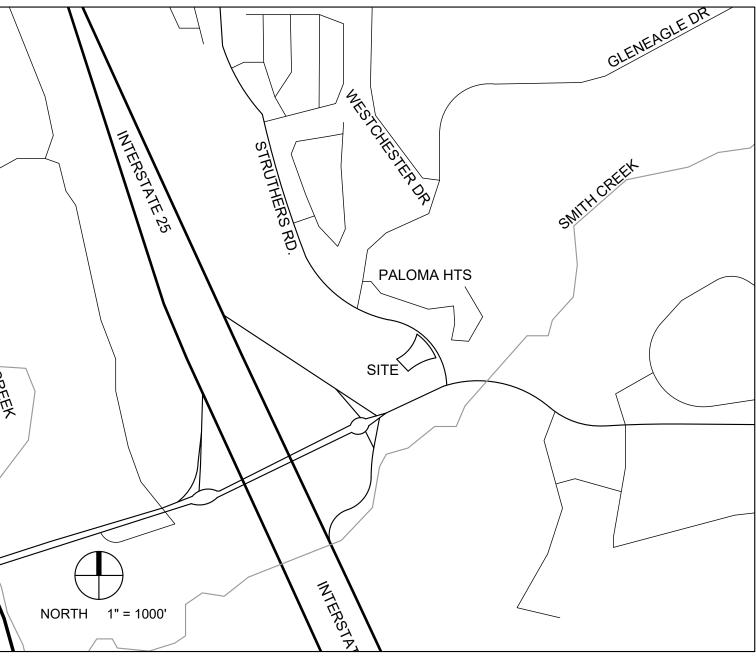
THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING IMPROVEMENTS AND UTILITIES AND SHALL REPAIR ANY DAMAGE AT HIS EXPENSE.

# **GRADING LEGEND**

	R.O.W. / PROPERTY LINE		<u>S</u>	EXIST. MANHOLE
	ADJOINING PROPERTY		***	EXIST. FIRE HYDRANT
	EASEMENT LINE			EXISTING WATER LINE
	EXISTING TO REMAIN	— 8" SS — —		EXISTING SANITARY SEWER
	PROP. CURB AND GUTTER	G		EXISTING GAS LINE
—  —  — 7512—  —	EXIST. CONTOUR	E		EXISTING ELECTRIC LINE
	PROP. CONTOUR	STS		EXISTING STORM SEWER
	PROPOSED STORM SEWER	FO		EXISTING FIBER OPTIC LINE





# PROPOSED WITHIN DISTURBED AREA

TOTAL LAND DISTURBANCE FOR PROJECT: 37,794 SQ. FT.± 0.87 ACRES±

#### **BASIS OF BEARINGS**

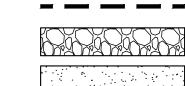
BEARINGS ARE BASED ON A PORTION OF THE EASTERLY BOUNDARY OF THE UNITED STATES COUNTY, COLORADO, BEING MONUMENTED AT THE NORTHERLY END BY A3.5" BRASS CAP STAMPED "BDY 43" IS ASSUMED TO BEAR N 56°38'25"W, A DISTANCE OF 630.29 FEET.

# **BENCHMARK**

A CHISELED "+" ON THE WEST END OF THE CONCRETE MEDIAN LOCATED IN STRUTHERS ROAD EAST OF GLENEAGLE DRIVE ELEVATION = 6770.99 (NAVD 88)

CHISELED "+" ON THE EAST END OF THE CONCRETE MEDIAN LOCATED IN STRUTHERS ROAD EAST OF GLENEAGLE DRIVE ELEVATION = 6747.41 (NAVD 88)

### **BMP LEGEND**



LIMITS OF DISTURBANCE

VEHICLE TRACKING PAD

STABILIZED STAGING AREA (MATERIAL, HAZARDOUS MATERIAL, EQUIPMENT, FUEL, LUBRICANT, CHEMICAL, WASTE & SANITARY FACILITY STORAGE

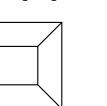
AND MAINTENANCE AREA)

(AAA)



**INLET PROTECTION** 

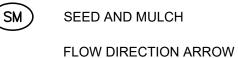
**CURB SOCKS** 







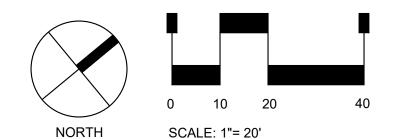














# Sterling Design Associates, Ilc

CIVIL ENGINEERS - LANDSCAPE ARCHITECTS

Littleton, CO 80120 www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332

FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

PRELIMINARY **NOT FOR CONSTRUCTION** 

STERLING DESIGN ASSOCIATES. LLC

NO . 1	& REVISIONS	DV: ID0
NO.: 1	DATE: 5/16/2017	BY: JDS
DESCRI	PTION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: 8/24/2017	BY: JD
DESCRI	PTION: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	DATE: -	BY: -
DESCRI	PTION: -	
NO.: 4	DATE: -	BY: -
DESCRI	PTION: -	
	DATE: -	BY: -
NO.: 5		
-	PTION: -	

DATE:	SCALE:
8/24/2017	1" = 20'
PROJECT MANAGER:	PROJECT NO.:
JDS	-

DESCRIPTION: -

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

**EROSION AND STORMWATER** QUALITY CONTROL PLAN

SHEET NUMBER:

7 OF 19 C302





#### **GENERAL NOTES**

- 1. REFER TO SHEET C100, C101, C304, AND C402 FOR ADDITIONAL PROJECT NOTES
- THE OWNER, SITE DEVELOPER, CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS SHALL REMOVE ALL SEDIMENT, MUD, CONSTRUCTION DEBRIS, OR OTHER POTENTIAL POLLUTANTS THAT MAY HAVE BEEN DISCHARGED TO OR, ACCUMULATE IN, THE FLOWLINES AND PUBLIC RIGHTS OF WAYS OF THE CITY AS A RESULT OF CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS SITE DEVELOPMENT OR CONSTRUCTION PROJECT. SAID REMOVAL SHALL BE CONDUCTED IN A TIMELY MANNER.
- ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES MAY BE REQUIRED OF THE OWNER AND HIS OR HER AGENTS DUE TO UNFORESEEN EROSION PROBLEMS OR IF THE PLAN AS SUBMITTED AND APPROVED. DOES NOT FUNCTION AS INTENDED. THE REQUIREMENTS OF THIS PLAN SHALL BE THE OBLIGATION OF THE LAND OWNER AND/ OR HIS SUCCESSORS OR HEIRS; UNTIL SUCH TIME AS THE PLAN IS PROPERLY COMPLETED. MODIFIED, OR VOIDED.
- 4. THE CONTRACTOR SHALL PREVENT SEDIMENT, DEBRIS AND ALL OTHER POLLUTANTS FROM ENTERING THE STORM SEWER SYSTEM DURING ALL DEMOLITION, EXCAVATION, TRENCHING, BORING, GRADING OR OTHER CONSTRUCTION OPERATIONS THAT ARE PART OF THIS PROJECT. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR REMEDIATION OF ANY ADVERSE IMPACTS TO ADJACENT WATERWAYS, WETLANDS, ETC. RESULTING FROM WORK DONE AS PART OF THIS PROJECT.
- 5. THE CONTRACTOR SHALL LOCATE, INSTALL, AND MAINTAIN ALL EROSION CONTROL AND WATER QUALITY "BEST MANAGEMENT PRACTICES" AS INDICATED ON THE APPROVED PLAN.
- 6. THE DEVELOPER, GENERAL CONTRACTOR, GRADING CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS SHALL INSURE THAT ALL LOADS OF CUT AND FILL MATERIAL IMPORTED TO OR EXPORTED FROM THIS SITE SHALL BE PROPERLY COVERED TO PREVENT LOSS OF THE MATERIAL DURING TRANSPORT ON PUBLIC RIGHTS OF WAYS.
- THE USE OF REBAR, STEEL STAKES, OR STEEL FENCE POSTS TO STAKE DOWN STRAW OR HAY BALES; OR TO SUPPORT SILT FENCING USED AS AN EROSION CONTROL MEASURE; IS PROHIBITED. THE USE OF OSHA APPROVED COLORED WARNING CAPS ON REBAR OR FENCE POSTS USED WITH EROSION CONTROL MEASURES IS NOT ACCEPTABLE.
- APPROVED EROSION AND SEDIMENT CONTROL "BEST MANAGEMENT PRACTICES" (BMP) SHALL BE MAINTAINED AND KEPT IN GOOD REPAIR FOR THE DURATION OF THIS PROJECT. AT A MINIMUM, THE CONTRACTOR OR HIS AGENT SHALL INSPECT ALL BMPS WEEKLY AND WITHIN 24 HOURS AFTER SIGNIFICANT PRECIPITATION EVENTS. ALL NECESSARY MAINTENANCE AND REPAIR SHALL BE COMPLETED IN A TIMELY MANNER. SEDIMENT WILL BE REMOVED AND DEVICES REPAIRED AS SOON AS PRACTICABLE. ACCUMULATED SEDIMENT AND DEBRIS SHALL BE REMOVED FROM A BMP WHEN THE SEDIMENT LEVEL REACHES ONE HALF THE HEIGHT OF THE BMP, AT ANY TIME THAT SEDIMENT OR DEBRIS ADVERSELY IMPACTS THE FUNCTIONING OF THE BMP, OR IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS. ALL INSPECTION, MAINTENANCE, AND REPAIR PRACTICES SHALL MEET APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS.
- 9. THE CONTRACTOR SHALL PROVIDE CONTROL MEASURES TO PREVENT OR MINIMIZE THE IMPACT TO RECIEVEING WATERS AS REQUIRED BY THE PLANS. THE CONTRACTOR SHALL EFFECTIVELY PREVENT AND CONTROL EROSION AND SEDIMENTATION ON CONSTRUCTION SITES AT THE EARLIEST PRACTICABLE TIME. IN GENERAL, CONTROL MEASURES WILL BE IMPLEMENTED PRIOR TO THE COMMENCEMENT OF EACH CONSTRUCTION OPERATION OR IMMEDIATELY AFTER THE AREA HAS BEEN DISTURBED. CONSTRUCTION STAGING AREAS AND VEHICLE MAINTENANCE ACTIVITIES SHALL BE MANAGED AND CONTROLLED SO AS TO MINIMIZE THE RUNOFF OF POLLUTANTS. DISTURBANCE OF VEGETATION SHALL BE MINIMIZED AND LIMITED TO ONLY WHAT IS SHOWN ON THE CONSTRUCTION PLANS. ALL EROSION, SEDIMENT AND WATER POLLUTION CONTROLS WILL BE MAINTAINED IN GOOD WORKING ORDER.
- 10. WATER USED IN THE CLEANING OF CEMENT TRUCK DELIVERY CHUTES SHALL BE DISCHARGED INTO A PREDEFINED, CONTAINMENT AREA ON THE JOB SITE. THE DISCHARGE OF WATER CONTAINING CEMENT TO THE STORM SEWER SYSTEM IS PROHIBITED.
- 11. THE CONTRACTOR SHALL PROTECT ALL STORM SEWER FACILITIES ADJACENT TO ANY LOCATION WHERE PAVEMENT CUTTING OPERATIONS INVOLVING WHEEL CUTTING, SAW CUTTING OR ABRASIVE WATER JET CUTTING ARE TO TAKE PLACE. THE CONTRACTOR SHALL REMOVE AND PROPERLY DISPOSE OF ALL WASTE PRODUCTS GENERATED BY SAID CUTTING OPERATIONS ON A DAILY BASIS. THE DISCHARGE OF ANY WATER CONTAMINATED BY WASTE PRODUCTS FROM CUTTING OPERATIONS TO THE STORM SEWER SYSTEM IS PROHIBITED.
- 12. PAVED SURFACES WHICH ARE ADJACENT TO CONSTRUCTION SITES SHALL BE SWEPT IN A TIMELY MANNER WHEN SEDIMENT AND OTHER MATERIALS ARE TRACKED OR DISCHARGED ON TO THEM. EITHER SWEEPING BY HAND OR USE OF STREET SWEEPERS IS ACCEPTABLE. STREET SWEEPERS USING WATER WHILE SWEEPING IS PREFERRED IN ORDER TO MINIMIZE DUST. FLUSHING OFF PAVED SURFACES WITH WATER IS PROHIBITED.
- 13. THE CONTRACTOR SHALL PROVIDE A DETAILED CONSTRUCTION SCHEDULE. THE SCHEDULE SHALL INCLUDE ALL MAJOR CONSTRUCTION ACTIVITIES AS WELL AS THE INSTALLATION OF PROPOSED EROSION CONTROL MEASURES, SEDIMENT/POLLUTANT CONTROL MEASURES, INTERIM AND FINAL SITE STABILIZATION MEASURES, PERMANENT EROSION PREVENTION MEASURES, AND PERMANENT WATER QUALITY ENHANCEMENT FACILITIES.
- 14. THE CONTRACTOR SHALL SUBMIT A SPILL PREVENTION AND MANAGEMENT PLAN. THE PLAN SHALL IDENTIFY THE MEASURES PROPOSED TO PREVENT THE DISCHARGE OF POLLUTANTS RESULTING FROM SPILLS AND EXPEDITE THE CLEANUP AND PROPER DISPOSAL OF SOILS CONTAMINATED BY CHEMICAL, PETROLEUM OR HAZARDOUS MATERIALS.
- 15. EXPOSURE OF SPOIL PILES CREATED DURING UTILITY TRENCHING SHOULD BE MINIMIZED. LOCATION OF SPOIL PILES SHOULD BE ON THE UPHILL SIDE OF THE TRENCH AND OUT OF THE WAY OF VISIBLE DRAINAGE PATHS.
- 16. STOCKPILES AND AREAS DISTURBED BY GRADING OPERATIONS SHALL BE STABILIZED FROM WIND AND WATER EROSION THROUGH THE USE OF A LAYER OF SUITABLE MULCH, TEMPORARY REVEGETATION, OR EROSION CONTROL MATTING/GEOTEXTILES AT A MINIMUM. ANY OF THESE METHODS SHALL BE APPLIED AT TIMES AND FREQUENCIES IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS: (1) TO DISTURBED AREAS AND STOCKPILES AFTER FINAL GRADING IS REACHED. (2) TO DISTURBED AREAS WHICH MAY NOT BE AT FINAL GRADE, BUT WHICH WILL REMAIN DORMANT, AND (3) TO STOCKPILE CONSTRUCTION ON ANY STOCKPILE WHICH WILL REMAIN DORMANT . SAID STABILIZATION PRACTICES SHALL BE APPLIED PER MANUFACTURER AND/OR SUPPLIER RECOMMENDATIONS AND SHALL BE TACKED OR FASTENED BY AN APPROVED METHOD SUITABLE FOR THE TYPE OF PRODUCT USED.

#### BMP MAINTENANCE NOTE

ALL EROSION AND SEDIMENT CONTROL PRACTICES AND OTHER PROTECTIVE MEASURES IDENTIFIED IN THE SWMP MUST BE MAINTAINED IN EFFECTIVE OPERATING CONDITION. PROPER SELECTION AND INSTALLATION OF BMPS AND IMPLEMENTATION OF COMPREHENSIVE INSPECTION AND MAINTENANCE PROCEDURES, IN ACCORDANCE WITH THE SWMP, SHOULD BE ADEQUATE TO MEET THIS CONDITION. BMPS THAT ARE NOT ADEQUATELY MAINTAINED IN ACCORDANCE WITH GOOD ENGINEERING, HYDROLOGIC AND POLLUTION CONTROL PRACTICES, INCLUDING REMOVAL OF COLLECTED SEDIMENT OUTSIDE THE ACCEPTABLE TOLERANCES OF THE BMPS, ARE CONSIDERED TO BE NO LONGER OPERATING EFFECTIVELY AND MUST BE ADDRESSED.

# ACADEMY GATEWAY SUBDIVISION FILING NO. 1 LOT 2

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

> MERIDIAN, EL PASO COUNTY, COLORADO MAJOR SITE DEVELOPMENT PLAN

### EL PASO COUNTY STANDARD NOTES FOR GRADING AND EROSION CONTROL PLANS

CONSTRUCTION MAY NOT COMMENCE UNTIL A CONSTRUCTION PERMIT IS OBTAINED FROM PLANNING AND COMMUNITY DEVELOPMENT (PCD) AND A PRECONSTRUCTION CONFERENCE IS HELD WITH PCD INSPECTIONS.

- 2. STORMWATER DISCHARGES FROM CONSTRUCTION SITES SHALL NOT CAUSE OR THREATEN TO CAUSE POLLUTION, CONTAMINATION, OR DEGRADATION OF STATE WATERS. ALL WORK AND EARTH DISTURBANCE SHALL BE DONE IN A MANNER THAT MINIMIZES POLLUTION OF ANY ON-SITE OR OFF SITE WATERS, INCLUDING WETLANDS.
- NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS TO REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING.
- 4. A SEPARATE STORMWATER MANAGEMENT PLAN (SMWP) FOR THIS PROJECT SHALL BE COMPLETED AND AN EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQCP) ISSUED PRIOR TO COMMENCING CONSTRUCTION. DURING CONSTRUCTION THE SWMP IS THE RESPONSIBILITY OF THE DESIGNATED STORMWATER MANAGER, SHALL BE LOCATED ON SITE AT ALL TIMES AND SHALL BE KEPT UP TO DATE WITH WORK PROGRESS AND CHANGES IN THE FIELD.
- 5. ONCE THE ESQCP HAS BEEN ISSUED. THE CONTRACTOR MAY INSTALL THE INITIAL STAGE EROSION AND SEDIMENT CONTROL BMPS AS INDICATED ON THE GEC. A PRECONSTRUCTION MEETING BETWEEN THE CONTRACTOR, ENGINEER, AND EL PASO COUNTY WILL BE HELD PRIOR TO ANY CONSTRUCTION. IT IS THE RESPONSIBILITY OF THE APPLICANT TO COORDINATE THE MEETING TIME AND PLACE WITH COUNTY DS INSPECTIONS STAFF.
- 6. SOIL EROSION CONTROL MEASURES FOR ALL SLOPES, CHANNELS, DITCHES, OR ANY DISTURBED LAND AREA SHALL BE COMPLETED WITHIN 21 CALENDAR DAYS AFTER FINAL GRADING, OR FINAL EARTH DISTURBANCE, HAS BEEN COMPLETED. DISTURBED AREAS AND STOCKPILES WHICH ARE NOT AT FINAL GRADE BUT WILL REMAIN DORMANT FOR LONGER THAN 30 DAYS SHALL ALSO BE MULCHED WITHIN 21 DAYS AFTER INTERIM GRADING. AN AREA THAT IS GOING TO REMAIN IN AN INTERIM STATE FOR MORE THAN 60 DAYS SHALL ALSO BE SEEDED. ALL TEMPORARY SOIL EROSION CONTROL MEASURES AND BMPS SHALL BE MAINTAINED UNTIL PERMANENT SOIL EROSION CONTROL MEASURES ARE IMPLEMENTED AND ESTABLISHED.
- TEMPORARY SOIL EROSION CONTROL FACILITIES SHALL BE REMOVED AND EARTH DISTURBANCE AREAS GRADED AND STABILIZED WITH PERMANENT SOIL EROSION CONTROL MEASURES PURSUANT TO STANDARDS AND SPECIFICATION PRESCRIBED IN THE DCM VOLUME II AND THE ENGINEERING CRITERIA MANUAL (ECM) APPENDIX I.
- ALL PERSONS ENGAGED IN EARTH DISTURBANCE SHALL IMPLEMENT AND MAINTAIN ACCEPTABLE SOIL EROSION AND SEDIMENT CONTROL MEASURES INCLUDING BMPS IN CONFORMANCE WITH THE EROSION CONTROL TECHNICAL STANDARDS OF THE DRAINAGE CRITERIA MANUAL (DCM) VOLUME II AND IN ACCORDANCE WITH THE STORMWATER MANAGEMENT PLAN (SWMP).
- 9. ALL TEMPORARY EROSION CONTROL FACILITIES INCLUDING BMPS AND ALL PERMANENT FACILITIES INTENDED TO CONTROL EROSION OF ANY EARTH DISTURBANCE OPERATIONS, SHALL BE INSTALLED AS DEFINED IN THE APPROVED PLANS, THE SWMP AND THE DCM VOLUME II AND MAINTAINED THROUGHOUT THE DURATION OF THE EARTH DISTURBANCE OPERATION.
- 10. ANY EARTH DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER SO AS TO EFFECTIVELY REDUCE ACCELERATED SOIL EROSION AND RESULTING SEDIMENTATION. ALL DISTURBANCES SHALL BE DESIGNED. CONSTRUCTED, AND COMPLETED SO THAT THE EXPOSED AREA OF ANY DISTURBED LAND SHALL BE LIMITED TO THE SHORTEST PRACTICAL PERIOD OF TIME.
- 11. ANY TEMPORARY OR PERMANENT FACILITY DESIGNED AND CONSTRUCTED FOR THE CONVEYANCE OF STORMWATER AROUND, THROUGH, OR FROM THE EARTH DISTURBANCE AREA SHALL BE DESIGNED TO LIMIT THE DISCHARGE TO A NON-EROSIVE VELOCITY.
- 12. CONCRETE WASH WATER SHALL BE CONTAINED AND DISPOSED OF IN ACCORDANCE WITH THE SWMP. NO WASH WATER SHALL BE DISCHARGED TO OR ALLOWED TO RUNOFF TO STATE WATERS, INCLUDING ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR FACILITIES.
- 13. EROSION CONTROL BLANKETING IS TO BE USED ON SLOPES STEEPER THAN 3:1.
- BUILDING, CONSTRUCTION, EXCAVATION, OR OTHER WASTE MATERIALS SHALL NOT BE TEMPORARILY PLACED OR STORED IN THE STREET. ALLEY. OR OTHER PUBLIC WAY. UNLESS IN ACCORDANCE WITH AN APPROVED TRAFFIC CONTROL PLAN. BMP'S MAY BE REQUIRED BY EL PASO COUNTY ENGINEERING IF DEEMED NECESSARY, BASED ON SPECIFIC CONDITIONS AND CIRCUMSTANCES.
- 15. VEHICLE TRACKING OF SOILS AND CONSTRUCTION DEBRIS OFF-SITE SHALL BE MINIMIZED. MATERIALS TRACKED OFFSITE SHALL BE CLEANED UP AND PROPERLY DISPOSED OF IMMEDIATELY.
- 16. CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL WASTES FROM THE CONSTRUCTION SITE FOR DISPOSAL IN ACCORDANCE WITH LOCAL AND STATE REGULATORY REQUIREMENTS. NO CONSTRUCTION DEBRIS, TREE SLASH, BUILDING MATERIAL WASTES OR UNUSED BUILDING MATERIALS SHALL BE BURIED, DUMPED,
- 17. THE OWNER, SITE DEVELOPER, CONTRACTOR, AND/OR THEIR AUTHORIZED AGENTS SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL CONSTRUCTION DEBRIS, DIRT, TRASH, ROCK, SEDIMENT, AND SAND THAT MAY ACCUMULATE IN THE STORM SEWER OR OTHER DRAINAGE CONVEYANCE SYSTEM AND STORMWATER APPURTENANCES AS A RESULT OF SITE DEVELOPMENT.
- 18. THE QUANTITY OF MATERIALS STORED ON THE PROJECT SITE SHALL BE LIMITED, AS MUCH AS PRACTICAL, TO THAT QUANTITY REQUIRED TO PERFORM THE WORK IN AN ORDERLY SEQUENCE. ALL MATERIALS STORED ON-SITE SHALL BE STORED IN A NEAT, ORDERLY MANNER, IN THEIR ORIGINAL CONTAINERS, WITH ORIGINAL MANUFACTURER'S LABELS.
- 19. NO CHEMICALS ARE TO BE USED BY THE CONTRACTOR, WHICH HAVE THE POTENTIAL TO BE RELEASED IN STORMWATER UNLESS PERMISSION FOR THE USE OF A SPECIFIC CHEMICAL IS GRANTED IN WRITING BY THE ECM ADMINISTRATOR. IN GRANTING THE USE OF SUCH CHEMICALS, SPECIAL CONDITIONS AND MONITORING MAY BE REQUIRED.
- 20. BULK STORAGE STRUCTURES FOR PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION SO AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS, INCLUDING ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR FACILITIES.
- 21. NO PERSON SHALL CAUSE THE IMPEDIMENT OF STORMWATER FLOW IN THE FLOW LINE OF THE CURB AND GUTTER OR IN THE DITCHLINE.
- 22. INDIVIDUALS SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8, CRS), AND THE "CLEAN WATER ACT" (33 USC 1344), IN ADDITION TO THE REQUIREMENTS INCLUDED IN THE DCM VOLUME II AND THE ECM APPENDIX I. ALL APPROPRIATE PERMITS MUST BE OBTAINED BY THE CONTRACTOR PRIOR TO CONSTRUCTION (NPDES, FLOODPLAIN, 404, FUGITIVE DUST, ETC.). IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND LAWS. RULES. OR REGULATIONS OF OTHER FEDERAL. STATE. OR COUNTY AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- 23. ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE AT APPROVED CONSTRUCTION ACCESS POINTS.
- 24. PRIOR TO ACTUAL CONSTRUCTION THE PERMITEE SHALL VERIFY THE LOCATION OF EXISTING UTILITIES.

24. A WATER SOURCE SHALL BE AVAILABLE ON SITE DURING EARTHWORK OPERATIONS AND UTILIZED AS REQUIRED TO MINIMIZE DUST FROM EARTHWORK EQUIPMENT AND WIND.

25. THE SOILS REPORT FOR THIS SITE HAS BEEN PREPARED BY AND SHALL BE CONSIDERED A PART OF THESE PLANS.

26. AT LEAST TEN DAYS PRIOR TO THE ANTICIPATED START OF CONSTRUCTION, FOR PROJECTS THAT WILL DISTURB 1 ACRE OR MORE, THE OWNER OR OPERATOR OF CONSTRUCTION ACTIVITY SHALL SUBMIT A PERMIT APPLICATION FOR STORMWATER DISCHARGE TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT, WATER QUALITY DIVISION. THE APPLICATION CONTAINS CERTIFICATION OF COMPLETION OF A STORMWATER MANAGEMENT PLAN (SWMP), OF WHICH THIS GRADING AND EROSION CONTROL PLAN MAY BE A PART. FOR INFORMATION OR APPLICATION MATERIALS CONTACT:

COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT WATER QUALITY CONTROL DIVISION WQCD - PERMITS 4300 CHERRY CREEK DRIVE SOUTH

DENVER, CO 80246-1530

ATTN: PERMITS UNIT



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

# NOT FOR CONSTRUCTION

#### STERLING DESIGN ASSOCIATES. LLC

NO.: 1 DATE: 5/16/2017	BY: JDS
DESCRIPTION: 1ST MAJOR SDP SUBMITT	AL
NO.: 2 DATE: 8/24/2017	BY: JDS
DESCRIPTION: 2ND MAJOR SDP SUBMIT	TAL
NO.: 3 DATE: -	BY: -
DESCRIPTION: -	
NO.: 4 DATE: -	BY: -
DESCRIPTION: -	
NO.: 5 DATE: -	BY: -
DESCRIPTION: -	
NO.: 6 DATE: -	BY: -

DATE: <b>8/24/2017</b>	SCALE: 1" = 20'
PROJECT MANAGER:	PROJECT NO.:
DRAWN BY:	DRAWING FILE:

CWA

VEHICLE TRACKING

CONTROL (SEE

2% SLOPE

DETAIL )

VEHICLE TRACKING

CONTROL (SEE VTC -

CONCRETE WASHOUT AREA PLAN

8 X 8 MIN.

CWA-1. CONCRETE WASHOUT AREA

2. DO NOT LOCATE AN UNLINED CWA WITHIN 400' OF ANY NATURAL DRAINAGE PATHWAY OR

SITE CONSTRAINTS MAKE THIS INFEASIBLE, OR IF HIGHLY PERMEABLE SOILS EXIST ON SITE, THE CWA MUST BE INSTALLED WITH AN IMPERMEABLE LINER (16 MIL MIN. THICKNESS) OR

WATERBODY, DO NOT LOCATE WITHIN 1,000' OF ANY WELLS OR DRINKING WATER SOURCES, IF

SURFACE STORAGE ALTERNATIVES USING PREFABRICATED CONCRETE WASHOUT DEVICES OR A

4. CWA SHALL INCLUDE A FLAT SUBSURFACE PIT THAT IS AT LEAST 8' BY 8' SLOPES

7. SIGNS SHALL BE PLACED AT THE CONSTRUCTION ENTRANCE, AT THE CWA, AND

LEADING OUT OF THE SUBSURFACE PIT SHALL BE 3:1 OR FLATTER. THE PIT SHALL BE AT

5. BERM SURROUNDING SIDES AND BACK OF THE CWA SHALL HAVE MINIMUM HEIGHT OF 1'.

ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CWA TO OPERATORS

1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS

POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE

2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN

EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE

3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON

4. THE CWA SHALL BE REPAIRED, CLEANED, OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE. CONCRETE MATERIALS, ACCUMULATED IN PIT, SHALL BE

5. CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS

IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE IN A WATER-TIGHT

6. THE CWA SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.

7. WHEN THE CWA IS REMOVED, COVER THE DISTURBED AREA WITH TOP SOIL, SEED AND

MULCH OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE LOCAL JURISDICTION.

ISULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN

(DETAIL ADAPTED FROM DOUGLAS COUNTY, COLORADO AND THE CITY OF PARKER, COLORADO, NOT AVAILABLE IN AUTOCAD).

NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS.

3. THE CWA SHALL BE INSTALLED PRIOR TO CONCRETE PLACEMENT ON SITE.

6. VEHICLE TRACKING PAD SHALL BE SLOPED 2% TOWARDS THE CWA.

8. USE EXCAVATED MATERIAL FOR PERIMETER BERM CONSTRUCTION.

COMPACTED BERM AROUND

THE PERIMETER

UNDISTURBED OR ]

CWA INSTALLATION NOTES

-CWA INSTALLATION LOCATION.

OF CONCRETE TRUCKS AND PUMP RIGS.

EROSION, AND PERFORM NECESSARY MAINTENANCE

REMOVED ONCE THE MATERIALS HAVE REACHED A DEPTH OF 2'

LINED ABOVE GROUND STORAGE ARE SHOULD BE USED.

1. SEE PLAN VIEW FOR:

LEAST 3' DEEP.

CWA MAINTENANCE NOTES

DOCUMENTED THOROUGHLY.

DISCOVERY OF THE FAILURE.

CONTAINER AND DISPOSED OF PROPERLY.

COMPACTED SOIL

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

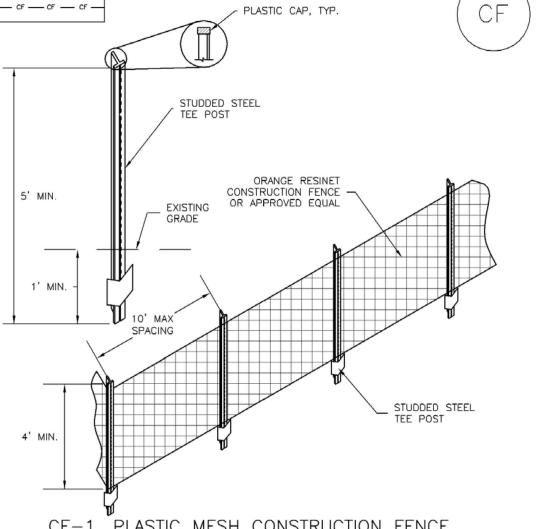
TEL: (817) 527-8421

SHEET TITLE:

EROSION AND STORMWATER QUALITY CONTROL DETAILS

SHEET NUMBER:

**COUNTY PROJECT NUMBER PPR-17-030** 



CF-1. PLASTIC MESH CONSTRUCTION FENCE

CONSTRUCTION FENCE INSTALLATION NOTES

1. SEE PLAN VIEW FOR: -LOCATION OF CONSTRUCTION FENCE.

2. CONSTRUCTION FENCE SHOWN SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING

3. CONSTRUCTION FENCE SHALL BE COMPOSED OF ORANGE, CONTRACTOR-GRADE MATERIAL THAT IS AT LEAST 4' HIGH. METAL POSTS SHOULD HAVE A PLASTIC CAP FOR SAFETY. 4. STUDDED STEEL TEE POSTS SHALL BE UTILIZED TO SUPPORT THE CONSTRUCTION FENCE. MAXIMUM SPACING FOR STEEL TEE POSTS SHALL BE 10'.

5. CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO THE TOP, MIDDLE, AND BOTTOM OF EACH POST

CONSTRUCTION FENCE MAINTENANCE NOTES

1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE

2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.

3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.

4. CONSTRUCTION FENCE SHALL BE REPAIRED OR REPLACED WHEN THERE ARE SIGNS OF DAMAGE SUCH AS RIPS OR SAGS. CONSTRUCTION FENCE IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS STABILIZED AND APPROVED BY THE LOCAL JURISDICTION. 5. WHEN CONSTRUCTION FENCES ARE REMOVED, ALL DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE FENCE SHALL BE COVERED WITH

TOPSOIL, SEEDED AND MULCHED, OR OTHERWISE STABILIZED AS APPROVED BY LOCAL

NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS. CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN DIFFERENCES ARE NOTED.

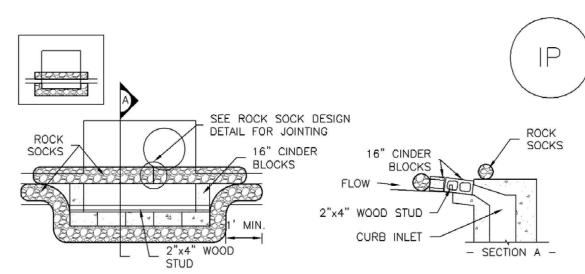
(DETAIL ADAPTED FROM TOWN OF PARKER, COLORADO, NOT AVAILABLE IN AUTOCAD)



A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN



IP-1. BLOCK AND ROCK SOCK SUMP OR ON GRADE INLET PROTECTION

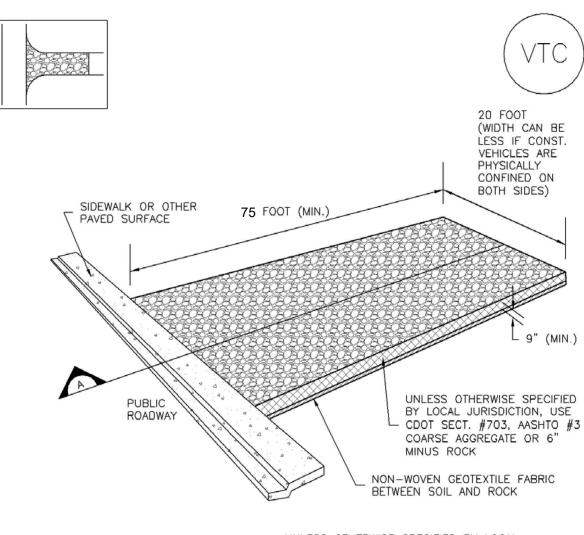
BLOCK AND CURB SOCK INLET PROTECTION INSTALLATION NOTES

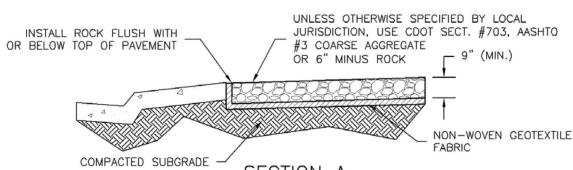
INLET PROTECTION MAINTENANCE NOTES

- 1. SEE ROCK SOCK DESIGN DETAIL FOR INSTALLATION REQUIREMENTS.
- 2. CONCRETE "CINDER" BLOCKS SHALL BE LAID ON THEIR SIDES AROUND THE INLET IN A SINGLE ROW, ABUTTING ONE ANOTHER WITH THE OPEN END FACING AWAY FROM THE CURB.

  3. GRAVEL BAGS SHALL BE PLACED AROUND CONCRETE BLOCKS, CLOSELY ABUTTING ONE ANOTHER AND JOINTED TOGETHER IN ACCORDANCE WITH ROCK SOCK DESIGN DETAIL.
- 1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- 2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE
- 3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON
- 4. SEDIMENT ACCUMULATED UPSTREAM OF INLET PROTECTION SHALL BE REMOVED AS NECESSARY TO MAINTAIN BMP EFFECTIVENESS, TYPICALLY WHEN STORAGE VOLUME REACHES 50% OF CAPACITY, A DEPTH OF 6" WHEN SILT FENCE IS USED, OR 1/4 OF THE HEIGHT FOR STRAW BALES
- 5. INLET PROTECTION IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS PERMANENTLY STABILIZED, UNLESS THE LOCAL JURISDICTION APPROVES EARLIER REMOVAL OF INLET PROTECTION IN STREETS.
- 6. WHEN INLET PROTECTION AT AREA INLETS IS REMOVED, THE DISTURBED AREA SHALL BE COVERED WITH TOP SOIL, SEEDED AND MULCHED, OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE LOCAL JURISDICTION.
- (DETAIL ADAPTED FROM TOWN OF PARKER, COLORADO AND CITY OF AURORA, COLORADO, NOT AVAILABLE IN AUTOCAD)

  NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS.
  CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN DIFFERENCES ARE NOTED.
- NOTE: THE DETAILS INCLUDED WITH THIS FACT SHEET SHOW COMMONLY USED, CONVENTIONAL METHODS OF INLET PROTECTION IN THE DENVER METROPOLITAN AREA. THERE ARE MANY PROPRIETARY INLET PROTECTION METHODS ON THE MARKET. UDFCD NEITHER ENDORSES NOR DISCOURAGES USE OF PROPRIETARY INLET PROTECTION; HOWEVER, IN THE EVENT PROPRIETARY METHODS ARE USED, THE APPROPRIATE DETAIL FROM THE MANUFACTURER MUST BE INCLUDED IN THE SWMP AND THE BMP MUST BE INSTALLED AND MAINTAINED AS SHOWN IN THE MANUFACTURER'S DETAILS.
- NOTE: SOME MUNICIPALITIES DISCOURAGE OR PROHIBIT THE USE OF STRAW BALES FOR INLET PROTECTION. CHECK WITH LOCAL JURISDICTION TO DETERMINE IF STRAW BALE INLET





#### VTC-1. AGGREGATE VEHICLE TRACKING CONTROL

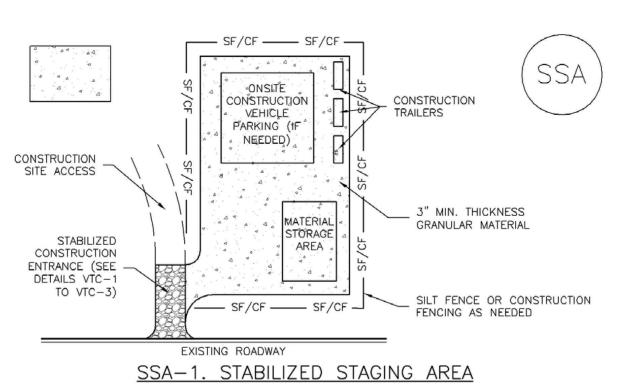
STABILIZED CONSTRUCTION ENTRANCE/EXIT INSTALLATION NOTES

- 1. SEE PLAN VIEW FOR
- -LOCATION OF CONSTRUCTION ENTRANCE(S)/EXIT(S).
  -TYPE OF CONSTRUCTION ENTRANCE(S)/EXITS(S) (WITH/WITHOUT WHEEL WASH, CONSTRUCTION MAT OR TRM).
- 2. CONSTRUCTION MAT OR TRM STABILIZED CONSTRUCTION ENTRANCES ARE ONLY TO BE USED ON SHORT DURATION PROJECTS (TYPICALLY RANGING FROM A WEEK TO A MONTH) WHERE THERE WILL BE LIMITED VEHICULAR ACCESS.
- 3. A STABILIZED CONSTRUCTION ENTRANCE/EXIT SHALL BE LOCATED AT ALL ACCESS POINTS WHERE VEHICLES ACCESS THE CONSTRUCTION SITE FROM PAVED RIGHT-OF-WAYS.

  4. STABILIZED CONSTRUCTION ENTRANCE/EXIT SHALL BE INSTALLED PRIOR TO ANY LAND
- DISTURBING ACTIVITIES.

  5. A NON-WOVEN GEOTEXTILE FABRIC SHALL BE PLACED UNDER THE STABILIZED
- CONSTRUCTION ENTRANCE/EXIT PRIOR TO THE PLACEMENT OF ROCK.

  6. UNLESS OTHERWISE SPECIFIED BY LOCAL JURISDICTION, ROCK SHALL CONSIST OF DOT SECT. #703, AASHTO #3 COARSE AGGREGATE OR 6" (MINUS) ROCK.
- STABILIZED CONSTRUCTION ENTRANCE/EXIT MAINTENANCE NOTES
- 1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- 2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- 3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- 4. ROCK SHALL BE REAPPLIED OR REGRADED AS NECESSARY TO THE STABILIZED ENTRANCE/EXIT TO MAINTAIN A CONSISTENT DEPTH.
- 5. SEDIMENT TRACKED ONTO PAVED ROADS IS TO BE REMOVED THROUGHOUT THE DAY AND AT THE END OF THE DAY BY SHOVELING OR SWEEPING, SEDIMENT MAY NOT BE WASHED DOWN STORM SEWER DRAINS.
- NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS. CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN DIFFERENCES ARE NOTED.
- (DETAILS ADAPTED FROM CITY OF BROOMFIELD, COLORADO, NOT AVAILABLE IN AUTOCAD)



STABILIZED STAGING AREA INSTALLATION NOTES

- SEE PLAN VIEW FOR
   -LOCATION OF STAGING AREA(S).
   -CONTRACTOR MAY ADJUST LOCATION AND SIZE OF STAGING AREA WITH APPROVAL
- 2. STABILIZED STAGING AREA SHOULD BE APPROPRIATE FOR THE NEEDS OF THE SITE. OVERSIZING RESULTS IN A LARGER AREA TO STABILIZE FOLLOWING CONSTRUCTION.
- STAGING AREA SHALL BE STABILIZED PRIOR TO OTHER OPERATIONS ON THE SITE.
   THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM 3" THICK GRANULAR
- 5. UNLESS OTHERWISE SPECIFIED BY LOCAL JURISDICTION, ROCK SHALL CONSIST OF DOT SECT. #703, AASHTO #3 COARSE AGGREGATE OR 6" (MINUS) ROCK.
- 6. ADDITIONAL PERIMETER BMPs MAY BE REQUIRED INCLUDING BUT NOT LIMITED TO SILT FENCE AND CONSTRUCTION FENCING.

#### STABILIZED STAGING AREA MAINTENANCE NOTES

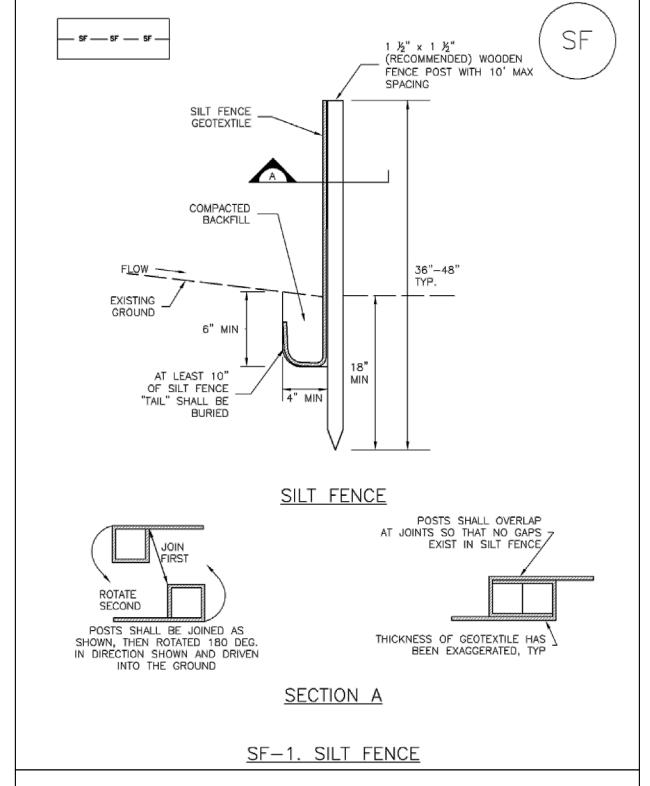
MATERIAL.

- 1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- 2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.
- 3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- 4. ROCK SHALL BE REAPPLIED OR REGRADED AS NECESSARY IF RUTTING OCCURS OR UNDERLYING SUBGRADE BECOMES EXPOSED.

#### STABILIZED STAGING AREA MAINTENANCE NOTES

- 5. STABILIZED STAGING AREA SHALL BE ENLARGED IF NECESSARY TO CONTAIN PARKING, STORAGE, AND UNLOADING/LOADING OPERATIONS.
- 6. THE STABILIZED STAGING AREA SHALL BE REMOVED AT THE END OF CONSTRUCTION. THE GRANULAR MATERIAL SHALL BE REMOVED OR, IF APPROVED BY THE LOCAL JURISDICTION, USED ON SITE, AND THE AREA COVERED WITH TOPSOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED IN A MANNER APPROVED BY LOCAL JURISDICTION.
- NOTE: MANY MUNICIPALITIES PROHIBIT THE USE OF RECYCLED CONCRETE AS GRANULAR MATERIAL FOR STABILIZED STAGING AREAS DUE TO DIFFICULTIES WITH RE-ESTABLISHMENT OF VEGETATION IN AREAS WHERE RECYCLED CONCRETE WAS PLACED.
- NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS. CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN DIFFERENCES ARE NOTED.

(DETAILS ADAPTED FROM DOUGLAS COUNTY, COLORADO, NOT AVAILABLE IN AUTOCAD)



#### SILT FENCE INSTALLATION NOTES

- SILT FENCE MUST BE PLACED AWAY FROM THE TOE OF THE SLOPE TO ALLOW FOR WATER PONDING. SILT FENCE AT THE TOE OF A SLOPE SHOULD BE INSTALLED IN A FLAT LOCATION AT LEAST SEVERAL FEET (2-5 FT) FROM THE TOE OF THE SLOPE TO ALLOW ROOM FOR PONDING AND DEPOSITION.
- 2. A UNIFORM 6" X 4" ANCHOR TRENCH SHALL BE EXCAVATED USING TRENCHER OR SILT FENCE INSTALLATION DEVICE. NO ROAD GRADERS, BACKHOES, OR SIMILAR EQUIPMENT SHALL
- 3. COMPACT ANCHOR TRENCH BY HAND WITH A "JUMPING JACK" OR BY WHEEL ROLLING. COMPACTION SHALL BE SUCH THAT SILT FENCE RESISTS BEING PULLED OUT OF ANCHOR
- TRENCH BY HAND.

  4. SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD BE NO NOTICEABLE SAG BETWEEN STAKES AFTER IT HAS BEEN ANCHORED TO THE STAKES.
- 5. SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES USING 1" HEAVY DUTY STAPLES OR NAILS WITH 1" HEADS. STAPLES AND NAILS SHOULD BE PLACED 3" ALONG THE FABRIC DOWN THE STAKE
- 6. AT THE END OF A RUN OF SILT FENCE ALONG A CONTOUR, THE SILT FENCE SHOULD BE TURNED PERPENDICULAR TO THE CONTOUR TO CREATE A "J-HOOK." THE "J-HOOK" EXTENDING PERPENDICULAR TO THE CONTOUR SHOULD BE OF SUFFICIENT LENGTH TO KEEP
- RUNOFF FROM FLOWING AROUND THE END OF THE SILT FENCE (TYPICALLY 10' 20').
- 7. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.

  SILT FENCE MAINTENANCE NOTES

SEDIMENTS IS APPROXIMATELY 6".

- 1. INSPECT BMPs EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.
- 2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE
- 3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.
- 4. SEDIMENT ACCUMULATED UPSTREAM OF THE SILT FENCE SHALL BE REMOVED AS NEEDED TO MAINTAIN THE FUNCTIONALITY OF THE BMP, TYPICALLY WHEN DEPTH OF ACCUMULATED
- 5. REPAIR OR REPLACE SILT FENCE WHEN THERE ARE SIGNS OF WEAR, SUCH AS SAGGING, TEARING, OR COLLAPSE.
- 6. SILT FENCE IS TO REMAIN IN PLACE UNTIL THE UPSTREAM DISTURBED AREA IS STABILIZED AND APPROVED BY THE LOCAL JURISDICTION, OR IS REPLACED BY AN EQUIVALENT PERIMETER SEDIMENT CONTROL BMP.
- 7. WHEN SILT FENCE IS REMOVED, ALL DISTURBED AREAS SHALL BE COVERED WITH TOPSOIL, SEEDED AND MULCHED OR OTHERWISE STABILIZED AS APPROVED BY LOCAL JURISDICTION.
- (DETAIL ADAPTED FROM TOWN OF PARKER, COLORADO AND CITY OF AURORA, NOT AVAILABLE IN AUTOCAD)

  NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS.
  CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN



PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC



STERLING DESIGN ASSOCIATES, LLC

NO.: 1	DATE: 5/16/2017	BY: JD
DESCRI	PTION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: 8/24/2017	BY: J
DESCRI	PTION: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	DATE: -	BY: -
DESCRI	PTION: -	
NO.: 4	DATE: -	BY: -
DESCRI	PTION: -	
NO · F	DATE: -	BY: -
NO 5		
DESCRI	PTION: -	

1" = 20'
PROJECT NO.:
DRAWING FILE:

PROJECT:

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

CLIENT:

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

EROSION AND STORMWATER QUALITY CONTROL DETAILS

SHEET NUMBER:

9 OF 19

C304





#### **Temporary and Permanent Seeding (TS/PS)**

Seeding dates for the highest success probability of perennial species along the Front Range are generally in the spring from April through early May and in the fall after the first of September until the ground freezes. If the area is irrigated, seeding may occur in summer months, as well. See Table TS/PS-3 for appropriate seeding dates.

#### Table TS/PS-1. Minimum Drill Seeding Rates for Various Temporary Annual Grasses

Species <sup>a</sup> (Common name)	Growth Season <sup>b</sup>	Pounds of Pure Live Seed (PLS)/acre <sup>c</sup>	Planting Depth (inches)
1. Oats	Cool	35 - 50	1 - 2
2. Spring wheat	Cool	25 - 35	1 - 2
3. Spring barley	Cool	25 - 35	1 - 2
4. Annual ryegrass	Cool	10 - 15	1/2
5. Millet	Warm	3 - 15	1/2 - 3/4
6. Sudangrass	Warm	5-10	1/2 - 3/4
7. Sorghum	Warm	5–10	1/2 - 3/4
8. Winter wheat	Cool	20–35	1 - 2
9. Winter barley	Cool	20–35	1 - 2
10. Winter rye	Cool	20–35	1 - 2
11. Triticale	Cool	25–40	1 - 2

- Successful seeding of annual grass resulting in adequate plant growth will usually produce enough dead-plant residue to provide protection from wind and water erosion for an additional year. This assumes that the cover is not disturbed or mowed closer than 8 inches.
- Hydraulic seeding may be substituted for drilling only where slopes are steeper than 3:1 or where access limitations exist. When hydraulic seeding is used, hydraulic mulching should be applied as a separate operation, when practical, to prevent the seeds from being encapsulated in
- See Table TS/PS-3 for seeding dates. Irrigation, if consistently applied, may extend the use of cool season species during the summer months. Seeding rates should be doubled if seed is broadcast, or increased by 50 percent if done using a Brillion Drill or by hydraulic seeding.

### **Temporary and Permanent Seeding (TS/PS)**

Table TS/PS-2. Minimum Drill Seeding Rates for Perennial Grasses

Common <sup>a</sup> Name	Botanical Name	Growth Season <sup>b</sup>	Growth Form	Seeds/ Pound	Pounds of PLS/acre
Alakali Soil Seed Mix	· · · · · · · · · · · · · · · · · · ·		1.1		
Alkali sacaton	Sporobolus airoides	Cool	Bunch	1,750,000	0.25
Basin wildrye	Elymus cinereus	Cool	Bunch	165,000	2.5
Sodar streambank wheatgrass	Agropyron riparium 'Sodar'	Cool	Sod	170,000	2.5
Jose tall wheatgrass	Agropyron elongatum 'Jose'	Cool	Bunch	79,000	7.0
Arriba western wheatgrass	Agropyron smithii 'Arriba'	Cool	Sod	110,000	5.5
Total					17.75
Fertile Loamy Soil Seed Mix	f)			<del>2=</del>	
Needleandthread	Stipa comata	Warm	Bunch	175,000	2.0
Blue grama	Bouteloua gracilis	Warm	Bunch	565,000	1.0
Thickspike wheatgrass	Elymus lanceolatus	Cool	Sod	130,000	3.0
Sodar streambank wheatgrass	Agropyron riparium 'Sodar'	Cool	Sod	170,000	2.5
Arriba western wheatgrass	Agropyron smithii 'Arriba'	Cool	Sod	110,000	7.0
Total					15.5
High Water Table Soil Seed Mix					
Western wheatgrass	Agropyron repens	Cool	Bunch	900,000	0.5
Redtop	Agrostis alba	Warm	Open sod	5,000,000	0.25
Prairie cordgrass	Spartina pectinata	Cool	Bunch	68,000	0.5
Alkali sacaton	Sporobolus airoides	Warm	Bunch	130,000	3.0
Pathfinder switchgrass	Panicum virgatum 'Pathfinder'	Warm	Sod	389,000	1.0
Slender wheatgrass	Elymus trachycaulus	Cool	Bunch	79,000	5.5
Total					10.75
Transition Turf Seed Mix <sup>c</sup>	2				
Sheep fescue	Festuca ovine	Cool	Bunch	2,500,000	0.5
Idaho fescue	Festuca idahoensis	Cool	Bunch	565,000	1.0
Buffalograss	Bouteloua dactyloides 'Topgun'	Warm	Sod	247,000	3.0
Blue grama	Bouteloua gracilis 'Hachita'	Warm	Bunch	130,000	3.0
Total					7.5

TS/PS-4 June 2012 Urban Drainage and Flood Control District Urban Storm Drainage Criteria Manual Volume 3

# ACADEMY GATEWAY SUBDIVISION FILING NO. I LOT 2

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION 1, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

> MERIDIAN, EL PASO COUNTY, COLORADO MAJOR SITE DEVELOPMENT PLAN

#### **Temporary and Permanent Seeding (TS/PS)**

#### Table TS/PS-2. Minimum Drill Seeding Rates for Perennial Grasses (cont.)

Common Name	Botanical Name	Growth Season <sup>b</sup>	Growth Form	Seeds/ Pound	Pounds of PLS/acr
Sandy Soil Seed Mix			200		
Blue grama	Bouteloua gracilis	Warm	Sod-forming bunchgrass	825,000	(
Camper little bluestem	Schizachyrium scoparium 'Camper'	Warm	Bunch	240,000	31
Prairie sandreed	Calamovilfa longifolia	Warm	Open sod	274,000	1
Sand dropseed	Sporobolus cryptandrus	Cool	Bunch	5,298,000	0.2
Vaughn sideoats grama	Bouteloua curtipendula 'Vaughn'	Warm	Sod	191,000	2
Arriba western wheatgrass	Agropyron smithii 'Arriba'	Cool	Sod	110,000	5
Total					10.3
Heavy Clay, Rocky Foothill See	d Mix		<del>(*)</del> (	1	
Mountain muhly	Muhlenbergia montana	Warn	Bunch	175,000	1
Arizona fescue	Festuca arizonica	Cool	Bunch	115,000	5
Vaughn sideoats grama <sup>e</sup>	Bouteloua curtipendula 'Vaughn'	Warm	Sod	191,000	2
Blue grama	Bouteloua gracilis	Warm	Bunch	130,000	3
Arriba western wheatgrass	Agropyron smithii 'Arriba'	Cool	Sod	110,000	5
Total					17
doubled if seed is broadcast an through hydraulic seeding. Hy	and rates are based on drill seeding d should be increased by 50 percen- draulic seeding may be substituted aulic mulching should be done as a dates.	t if the seeding for drilling on	is done using a E ly where slopes ar	Brillion Drill or	is applied

c If site is to be irrigated, the transition turf seed rates should be doubled

Crested wheatgrass should not be used on slopes steeper than 6H to 1V.

Can substitute 0.5 lbs PLS of blue grama for the 2.0 lbs PLS of Vaughn sideoats grama.

Urban Drainage and Flood Control District Urban Storm Drainage Criteria Manual Volume 3

Table TS/PS-3. Seeding Dates for Annual and Perennial Grasses

TS/PS-5

	(Numbers in	Grasses table reference able TS/PS-1)	Perennial Grasse	
Seeding Dates	Warm	Warm Cool W		Cool
January 1–March 15			✓	✓
March 16–April 30	4	1,2,3	✓	✓
May 1–May 15	4		✓	
May 16–June 30	4,5,6,7			
July 1–July 15	5,6,7			
July 16–August 31				
September 1–September 30		8,9,10,11		
October 1–December 31			✓	✓

Cover seeded areas with mulch or an appropriate rolled erosion control product to promote establishment of vegetation. Anchor mulch by crimping, netting or use of a non-toxic tackifier. See the Mulching BMP Fact Sheet for additional guidance.

#### **Maintenance and Removal**

Monitor and observe seeded areas to identify areas of poor growth or areas that fail to germinate. Reseed and mulch these areas, as needed.

An area that has been permanently seeded should have a good stand of vegetation within one growing season if irrigated and within three growing seasons without irrigation in Colorado. Reseed portions of the site that fail to germinate or remain bare after the first growing season.

Seeded areas may require irrigation, particularly during extended dry periods. Targeted weed control may also be necessary.

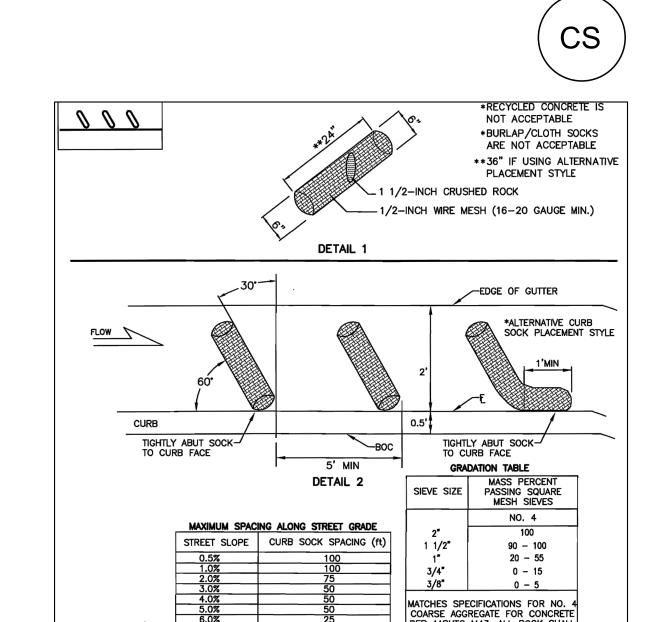
Protect seeded areas from construction equipment and vehicle access.

EC-4 Mulching (MU)

• Clean, weed-free and seed-free cereal grain straw should be applied evenly at a rate of 2 tons per acre and must be tacked or fastened by a method suitable for the condition of the site. Straw mulch must be anchored (and not merely placed) on the surface. This can be accomplished mechanically by crimping or with the aid of tackifiers or nets. Anchoring with a crimping implement is preferred, and is the recommended method for areas flatter than 3:1. Mechanical crimpers must be capable of tucking the long mulch fibers into the soil to a depth of 3 inches without cutting them. An agricultural disk, while not an ideal substitute, may work if the disk blades are dull or blunted and set vertically; however, the frame may have to be weighted to afford proper soil penetration.

#### **Maintenance and Removal**

After mulching, the bare ground surface should not be more than 10 percent exposed. Reapply mulch, as needed, to cover bare areas.



- SOCKS WILL BE USED UPGRADIENT OF INLET ANGLED AS SHOWN AND SHALL BE FLUSH WITH CURB. FOR IPS AND IPO NO LESS THAN THREE 6-INCH DIAMETER SOCKS MUST BE USED IN SEQUENCE, SPACED NO MORE THAN FIVE FEET APART, WHEN SHOWN ON PLAN. INCLINE AT 30 DEGREES FROM PERPENDICULAR, OPPOSITE THE DIRECTION OF FLOW (SEE DETAIL 2).
- A MINIMUM OF THREE CURB SOCKS SHALL BE PLACED DOWNSTREAM OF ANY DISTURBED AREA AND ALONG ALL CURBS FRONTING DISTURBED AREA, SPACED PER THE TABLE SOCKS WILL BE USED AT ANY LOCATION ALONG A CURB TO CONTROL SEDIMENT AS DIRECTED BY CITY. 16-20 GAUGE CHICKEN WIRE IS AN ACCEPTABLE ALTERNATIVE IF DOUBLE WRAPPED WITH NO MORE
- THAN 1/2 INCH OPENINGS. Curb Sock - are installed as curbside check dams and are wire mesh wrapped rock wattles installed for sediment control. Typical installation is a series of socks on a grade in the curb

## flowline upstream of a curb inlet to filter sediment-laden runoff and reduce the velocity of the Key Installation and Maintenance Requirements:

- Curb socks shall be constructed such that the final product is a cylindrical shaped filter with ½ - inch mesh filled with 1½ - inch gravel Curb socks shall not exceed 24" in length, 6" in diameter and shall be placed at 60° angles in curb drainage flow lines flush with the curb. The spacing and quantity of the
- Install delineators with the curb socks for high traffic areas where installations present

curb socks should be determined by the length and grade of the curb drainage flow

- a potential traffic hazard or where they may be damaged by snow plowing activities
- Recycled or demolished concrete material is not an acceptable substitute for rock fill. Installed features need to be monitored for correct placement or rupture and



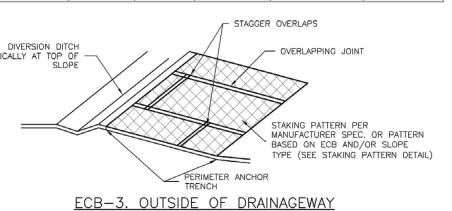
Features are not appropriately spaced in this application. Upgradient erosion control measures need to be implemented to prevent heavy sediment loading. Curb socks are not designed to manage the amount of sediment removal depicted here.

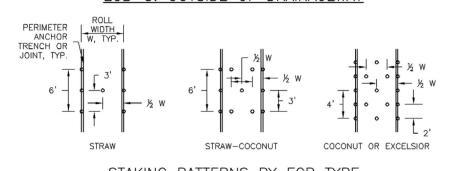
adjusted, replaced or repaired as needed

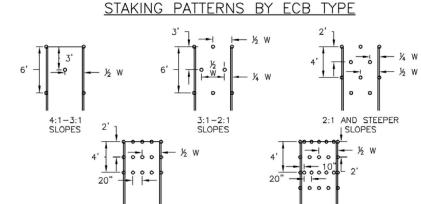


Table RECP-1. ECTC Standard Specification for Temporary Rolled Erosion Control Products (Adapted from Erosion Control Technology Council 2005)

<b>Product Description</b>	Slo Applica		Channel Applications*	Minimum Tensile Strength <sup>1</sup>	Expected Longevity	
	Maximum Gradient	C Factor <sup>2,5</sup>	Max. Shear Stress <sup>3,4,6</sup>			
Mulch Control Nets	5:1 (H:V)	≤0.10 @ 5:1	0.25 lbs/ft <sup>2</sup> (12 Pa)	5 lbs/ft (0.073 kN/m)		
Netless Rolled Erosion Control Blankets	4:1 (H:V)	≤0.10 @ 4:1	0.5 lbs/ft <sup>2</sup> (24 Pa)	5 lbs/ft (0.073 kN/m)	Up to 12	
Single-net Erosion Control Blankets & Open Weave Textiles	3:1 (H:V)	≤0.15 @ 3:1	1.5 lbs/ft <sup>2</sup> (72 Pa)	50 lbs/ft (0.73 kN/m)	months	
Double-net Erosion Control Blankets	2:1 (H:V)	≤0.20 @ 2:1	1.75 lbs/ft <sup>2</sup> (84 Pa)	75 lbs/ft (1.09 kN/m)		
Mulch Control Nets	5:1 (H:V)	≤0.10 @ 5:1	0.25 lbs/ft <sup>2</sup> (12 Pa)	25 lbs/ft (0.36 kN/m)	24 months	
Erosion Control Blankets & Open Weave Textiles (slowly degrading)	1.5:1 (H:V)	≤0.25 @ 1.5:1	2.00 lbs/ft <sup>2</sup> (96 Pa)	100 lbs/ft (1.45 kN/m)	24 months	
Erosion Control Blankets & Open Weave Textiles	1:1 (H:V)	≤0.25 @ 1:1	2.25 lbs/ft <sup>2</sup> (108 Pa)	125 lbs/ft (1.82 kN/m)	36 months	







LOW FLOW CHANNEL HIGH FLOW CHANNEL STAKING PATTERNS BY SLOPE OR CHANNEL TYPE EROSION CONTROL BLANKET INSTALLATION NOTES

-TYPE OF ECB (STRAW, STRAW-COCONUT, COCONUT, OR EXCELSIOR). -AREA, A, IN SQUARE YARDS OF EACH TYPE OF ECB. 2. 100% NATURAL AND BIODEGRADABLE MATERIALS ARE PREFERRED FOR RECPS, ALTHOUGH SOME JURISDICTIONS MAY ALLOW OTHER MATERIALS IN SOME APPLICATIONS. 3. IN AREAS WHERE ECBs ARE SHOWN ON THE PLANS, THE PERMITTEE SHALL PLACE TOPSOIL AND PERFORM FINAL GRADING, SURFACE PREPARATION, AND SEEDING AND MULCHING. SUBGRADE SHALL BE SMOOTH AND MOIST PRIOR TO ECB INSTALLATION AND THE ECB SHALL BE IN FULL CONTACT WITH SUBGRADE. NO GAPS OR VOIDS SHALL EXIST UNDER THE

4. PERIMETER ANCHOR TRENCH SHALL BE USED ALONG THE OUTSIDE PERIMETER OF ALL 5. JOINT ANCHOR TRENCH SHALL BE USED TO JOIN ROLLS OF ECBs TOGETHER (LONGITUDINALLY AND TRANSVERSELY) FOR ALL ECBs EXCEPT STRAW WHICH MAY USE AN OVERLAPPING JOINT. 6. INTERMEDIATE ANCHOR TRENCH SHALL BE USED AT SPACING OF ONE—HALF ROLL LENGTH FOR COCONUT AND EXCELSIOR ECBs. 7. OVERLAPPING JOINT DETAIL SHALL BE USED TO JOIN ROLLS OF ECBs TOGETHER FOR ECBs ON SLOPES. 8. MATERIAL SPECIFICATIONS OF ECBs SHALL CONFORM TO TABLE ECB-1. 9. ANY AREAS OF SEEDING AND MULCHING DISTURBED IN THE PROCESS OF INSTALLING ECBS SHALL BE RESEEDED AND MULCHED. 10. DETAILS ON DESIGN PLANS FOR MAJOR DRAINAGEWAY STABILIZATION WILL GOVERN IF DIFFERENT FROM THOSE SHOWN HERE.

'	ABLE ECB-1.	ECD MATERIA	AL SPECIFICAT	10143
TYPE	COCONUT CONTENT	STRAW CONTENT	EXCELSIOR CONTENT	RECOMMEND NETTING**
STRAW*	-	100%	-	DOUBLE/ NATURAL
STRAW- COCONUT	30% MIN	70% MAX	-	DOUBLE/ NATURAL
COCONUT	100%	-	-	DOUBLE/ NATURAL
EXCELSIOR	-	-	100%	DOUBLE/ NATURAL

#### EROSION CONTROL BLANKET MAINTENANCE NOTES

1. INSPECT BMP'S EACH WORKDAY, AND MAINTAIN THEM IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMP'S SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMP'S AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE

2. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE 3. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.

4. ECBs SHALL BE LEFT IN PLACE TO EVENTUALLY BIODEGRADE, UNLESS REQUESTED TO BE REMOVED BY THE LOCAL JURISDICTION. 5. ANY ECB PULLED OUT, TORN, OR OTHERWISE DAMAGED SHALL BE REPAIRED OR REINSTALLED. ANY SUBGRADE AREAS BELOW THE GEOTEXTILE THAT HAVE ERODED TO CREATED A VOID UNDER THE BLANKET, OR THAT REMAIN DEVOID OF GRASS SHALL BE REPAIRED, RESEEDED AND MULCHED AND THE ECB REINSTALLED. NOTE: MANY JURISDICTIONS HAVE BMP DETAILS THAT VARY FROM UDFCD STANDARD DETAILS. CONSULT WITH LOCAL JURISDICTIONS AS TO WHICH DETAIL SHOULD BE USED WHEN DIFFERENCES ARE NOTED.

(DETAILS ADAPTED FROM DOUGLAS COUNTY, COLORADO AND TOWN OF PARKER COLORADO, NOT AVAILABLE IN AUTOCAD)



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

# NOT FOR CONSTRUCTION

#### STERLING DESIGN ASSOCIATES, LLC

NO.: 1	DATE: 5/16/2017	BY: JDS
DESCRIPTION	N: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	ATE: 8/24/2017	BY: JD
DESCRIPTION	N: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	PATE: -	BY: -
DESCRIPTION	N: -	
NO.: 4 E	PATE: -	BY: -
DESCRIPTION	N: -	
NO.: 5 E	DATE: -	BY: -
DESCRIPTION	N: -	

DATE: 8/24/2017	SCALE: 1" = 20'
PROJECT MANAGER:  JDS	PROJECT NO.:
DRAWN BY:	DRAWING FILE:
	-

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

**EROSION AND STORMWATER** QUALITY CONTROL DETAILS

SHEET NUMBER:

10 OF 19

COUNTY PROJECT NUMBER PPR-17-030

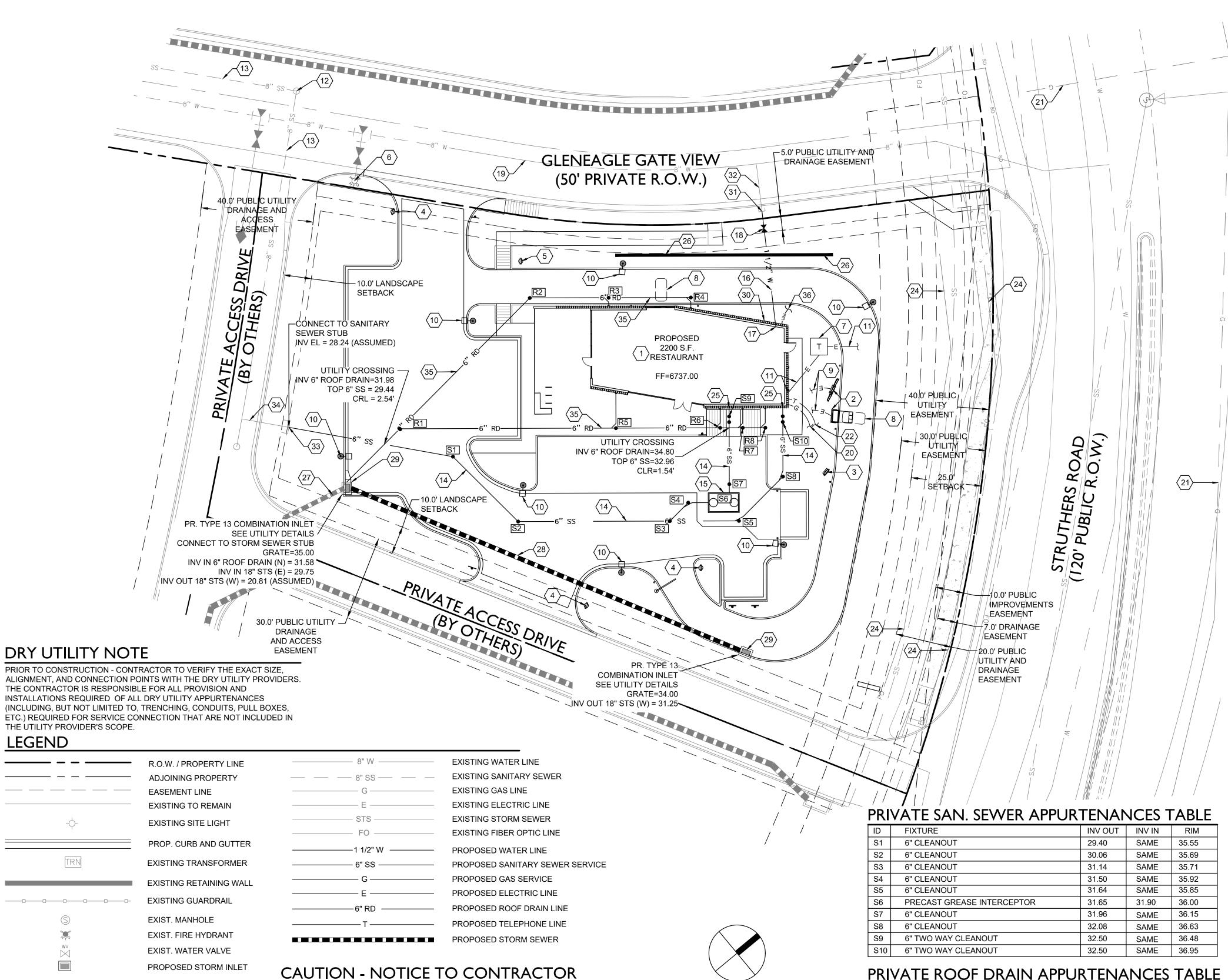
C305



A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN





SCHEDULE NOTES

- $\langle$  1  $\rangle$  PROPOSED 2,200 SQ. FT. STARBUCKS CAFE. SEE ARCHITECTURAL ELEVATIONS.
- $\langle$   $_2$  angle PROPOSED ORDER POINT CANOPY WITH DIGITAL ORDER SCREEN AND MENU BOARD TO BE INSTALLED PER STARBUCKS STANDARDS. SEE SIGNAGE DETAILS.
- $\langle$  3 angle PROPOSED PRE-MENU BOARD TO BE INSTALLED PER STARBUCKS STANDARDS. SEE
- $\langle$  4  $\rangle$  PROPOSED "DRIVE-THRU" DIRECTIONAL SIGNAGE TO BE INSTALLED PER STARBUCKS. STANDARDS. SEE SIGNAGE DETAILS.
- $\langle$  5 angle PROPOSED "THANK YOU" AND "EXIT ONLY" SIGNAGE TO BE INSTALLED PER STARBUCKS
- $\langle$   $_{6}$  angle existing fire hydrant to remain and be protected throughout construction
- $\overline{7}$  PROPOSED LOCATION OF THE TRANSFORMER PAD. CONTRACTOR TO VERIFY EXACT OCATION AND SIZE WITH UTILITY PROVIDOR PRIOR TO INSTALLATION. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF CONCRETE PAD AND CONDUIT AS REQUIRED BY THE UTILITY PROVIDER. CONTRACTOR SHALL COORDINATE SAID WORK WITH THE UTILITY
- $\langle$  8  $\rangle$  PROPOSED DETECTOR LOOP CENTERED ON ORDER POINT AND/OR DRIVE-THROUGH WINDOW 2-INCHES BELOW FINISHED GRADE. CONTRACTOR TO EXTEND CONDUIT TO DRIVE-THROUGH / ORDER POINT / MENU BOARD / ELECTRICAL PANEL. CONTRACTOR SHALL INSTALL ALL WORK IN ACCORDANCE WITH STARBUCKS STANDARDS.
- $\langle$  9 angle APPROXIMATE LOCATION FOR UNDERGROUND ELECTRICAL CONDUIT FOR DIGITAL ORDER
- (10) PROPOSED SITE LIGHT. SEE PHOTOMETRIC PLAN. CONTRACTOR TO COORDINATE CONDUIT PLACEMENT FOR SITE LIGHTING WITH SITE ELECTRICAL PLAN.
- $\langle 11 \rangle$  APPROXIMATE ALIGNMENT AND CONNECTION POINTS FOR PROPOSED ELECTRIC SERVICE.
- $\langle 12 \rangle$  EXISTING SEWER MANHOLE TO REMAIN.
- (13) EXISTING 8" SANITARY SEWER MAIN TO REMAIN AND BE PROTECTED THROUGHOUT
- $\langle$ 14angle PROPOSED 6" ASTM 3034 SDR-35 PVC SANITARY SEWER SERVICE WITH CLEANOUTS. INSTALL AT 2.0% MINIMUM SLOPE. REFER TO ARCHITECTURAL PLANS FOR DETAILS AND INFORMATION REGARDING PONT OF CONNECTION/CONTINUATION AT THE BUILDING.
- (15) PROPOSED GREASE INTERCEPTOR WITH VENT LINES TO BUILDING
- $\langle$ 16angle PROPOSED 1-1/2" TYPE K COPPER WATER SERVICE 4.5 FEET MINIMUM DEPTH BELOW GRADE. IN ACCORDANCE WITH DONALA WATER STANDARDS AND SPECIFICATIONS. CONTRACTOR TO COORDINATE INSTALLATION OF ALL APPURTENANCES NECESSARY FOR CONSTRUCTION WITH DONALA WATER, PERFORM REQUIRED WORK, AND CONFIRM PROPOSED SERVICE SIZE WITH ARCH/MEP PLANS PRIOR TO CONSTRUCTION.
- (17) DOMESTIC WATER SERVICE ENTRY. CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL LOCATION AND CONTINUATION WITH ARCH/MEP PLANS PRIOR TO CONSTRUCTION. CONTRACTOR TO REFER TO ARCH/MEP PLANS FOR WATER METER PLACEMENT (INSIDE
- $\langle 18 \rangle$  PROPOSED CURB STOP AND BOX TO BE INSTALLED IN ACCORDANCE WITH DONALA WATER STANDARDS AND SPECIFICATIONS. CONTRACTOR TO COORDINATE INSTALLATION OF ALL APPURTENANCES NECESSARY FOR CONSTRUCTION WITH DONALA WATER.
- $\langle 19 \rangle$  EXISTING 8" WATER MAIN BY OTHERS TO REMAIN AND BE PROTECTED DURING CONSTRUCTION.
- $\langle 20 \rangle$  APPROXIMATE ALIGNMENT AND CONNECTION POINTS FOR PROPOSED GAS SERVICE. SEE DRY UTILITY NOTE.
- $\langle 21 \rangle$  EXISTING GAS MAIN TO REMAIN AND BE PROTECTED THROUGHOUT CONSTRUCTION.
- $\langle 22 \rangle$  APPROXIMATE ALIGNMENT AND CONNECTION POINTS FOR PROPOSED PHONE SERVICE. SEE DRY UTILITY NOTE.
- $\langle 23 \rangle$  EXISTING UNDERGROUND ELECTRIC LINE TO REMAIN AND BE PROTECTED DURING
- $\langle 24 \rangle$  EXISTING FIBER OPTIC LINE TO REMAIN AND BE PROTECTED THROUGHOUT CONSTRUCTION.
- SANITARY SEWER SERVICE ENTRY. CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL LOCATION AND CONTINUATION WITH ARCH/MEP PLANS PRIOR TO CONSTRUCTION.
- (26) PROPOSED 2.0-FOOT MAXIMUM HEIGHT RETAINING WALL. REFER TO GRADING PLAN FOR APPROXIMATE ELEVATIONS AT TOP AND BASE OF WALL. WALL IS TO BE DESIGN / BUILD BY
- $\overline{\langle 27 \rangle}$  STORM SEWER STUB BY OTHERS.
- $\langle 28 \rangle$  PROPOSED 18" RCP STORM SEWER.
- $\langle 29 \rangle$  PROPOSED TYPE 13 COMBINATION INLET. SEE UTILITY DETAILS.
- $\langle 30 \rangle$  INSTALL STUB AFTER THE DOMESTIC METER FOR IRRIGATION. CONTRACTOR TO SHALL VERIFY HORIZONTAL AND VERTICAL LOCATION WITH ARCH/MEP PLANS PRIOR TO CONSTRUCTION. CONTRACTOR TO REFER TO IRRIGATION PLANS FOR CONTINUATION.
- $\langle 31 \rangle$  POINT OF CONNECTION TO 1-1/2" TYPE K COPPER WATER LINE STUB.
- $\langle 32 \rangle$  1-1/2" TYPE K COPPER WATER LINE STUB (BY OTHERS).
- $\langle 33 \rangle$  POINT OF CONNECTION TO 6" SDR~35 PVC SEWER LINE STUB.
- $\langle 34 \rangle$  6" SDR~35 PVC SEWER LINE STUB (BY OTHERS).
- (35) PROPOSED 6" ASTM 3034 SDR-35 PVC ROOF DRAIN WITH CLEANOUTS. INSTALL AT 2.0% MINIMUM SLOPE. REFER TO ARCHITECTURAL PLANS FOR DETAILS AND INFORMATION REGARDING POINT OF CONNECTION/CONTINUATION AT THE BUILDING.
- $\langle 36 \rangle$  PROPOSED IRRIGATION LINE TO BE INSTALLED AFTER THE METER AND BEFORE THE DOMESTIC BACKFLOW PREVENTION DEVICE. SEE IRRIGATION PLANS AND ARCHITECTURAL/MEP PLANS.



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC



STERLING DESIGN ASSOCIATES. LLC

NO.: 1	DATE: 5/16/2017	BY: JDS
DESCRIF	PTION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: 8/24/2017	BY: JD
DESCRIF	PTION: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	DATE: -	BY: -
DESCRIF	PTION: -	
NO.: 4	DATE: -	BY: -
DESCRIF	PTION: -	
NO.: 5	DATE: -	BY: -
DESCRIF	PTION: -	

DATE: <b>8/24/2017</b>	SCALE: 1" = 20'
PROJECT MANAGER:  JDS	PROJECT NO.:
DRAWN BY:	DRAWING FILE:
	-

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

**UTILITY PLAN** 

SHEET NUMBER:

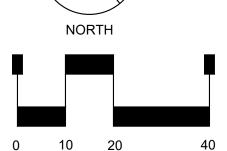
11 of 19 C401

COUNTY PROJECT NUMBER PPR-17-030



THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND. WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING IMPROVEMENTS AND UTILITIES AND SHALL REPAIR ANY DAMAGE AT HIS EXPENSE.



SCALE: 1"= 20'

PRIVATE ROOF DRAIN APPURTENANCES TABLE FIXTURE INV IN

6" CLEANOUT SAME 35.77 32.14 6" CLEANOUT SAME 34.24 37.11 6" CLEANOUT 34.80 SAME 36.65 6" CLEANOUT 34.80 SAME 36.36 6" CLEANOUT 33.68 SAME | 36.75 6" CLEANOUT SAME 36.44 34.44 6" CLEANOUT 34.62 SAME 36.53 6" CLEANOUT 34.80 SAME 36.90

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION 1, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

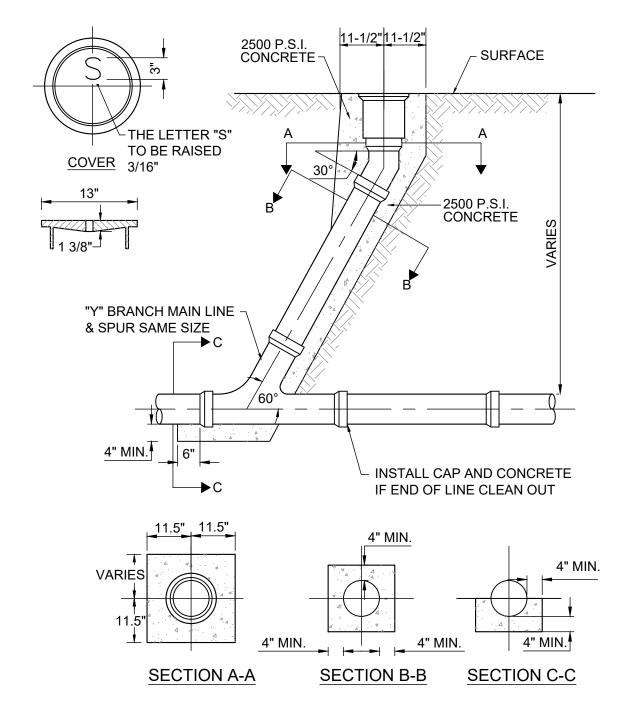
MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN

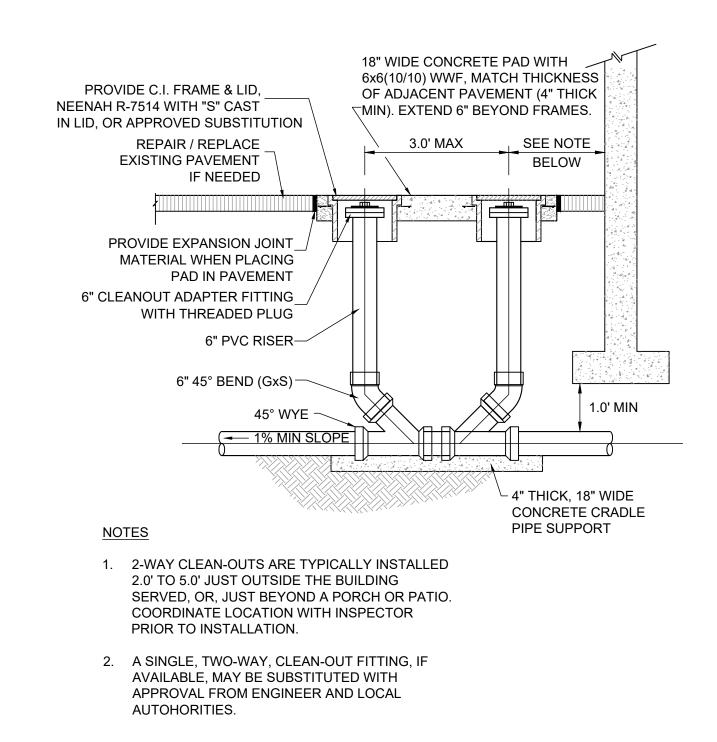
### **GENERAL UTILITY NOTES**

REFER TO SHEET C100, C101, C304, AND C402 FOR ADDITIONAL PROJECT GENERAL NOTES.

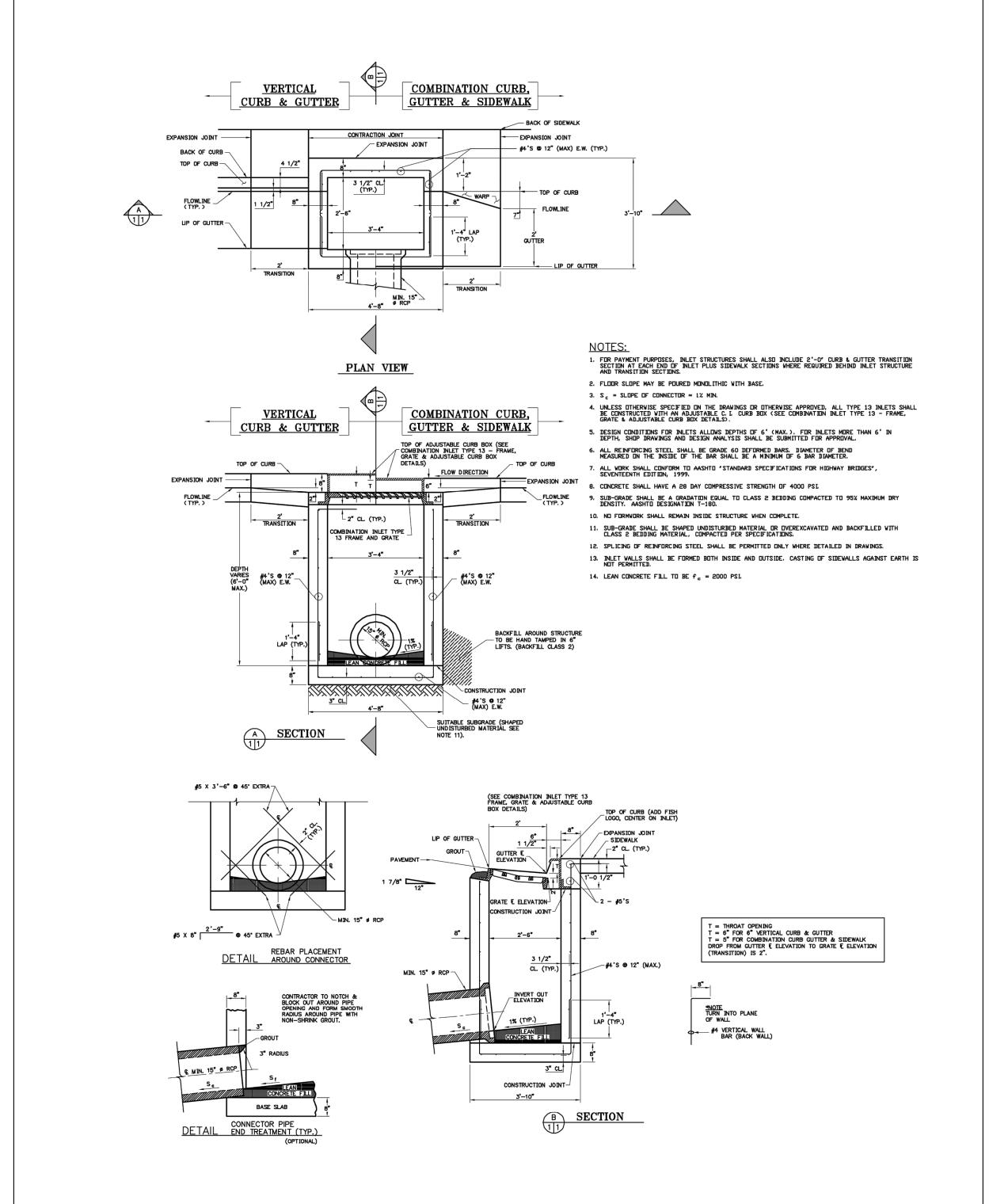
- WHEN IT IS NECESSARY TO LOWER OR RAISE WATER LINES AT STORM DRAINS AND OTHER UTILITY CROSSINGS, A MINIMUM CLEARANCE IN ACCORDANCE WITH THE GOVERNING AUTHORITY'S STANDARDS AND SPECIFICATIONS SHALL BE MAINTAINED BETWEEN OUTSIDE OF PIPES.
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED IN ACCORDANCE WITH THE GOVERNING AUTHORITIES STANDARDS AND THE RECOMMENDATIONS OF THE GEOTECHNICAL INVESTIGATION. THE CONTRACTOR SHALL SUBMIT COMPACTION TEST RESULTS TO THE GOVERNING AUTHORITY IN ACCORDANCE WITH THE GOVERNING AUTHORITY'S CRITERIA.
- 4. ALL WATER LINES SHALL HAVE A MINIMUM COVER IN ACCORDANCE WITH THE GOVERNING AUTHORITY'S STANDARDS AND SPECIFICATIONS.
- 5. THE CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF UTILITY ENTRANCES.
- 6. WATERLINES SHALL BE TESTED AND DISINFECTED IN ACCORDANCE WITH THE GOVERNING AUTHORITY'S STANDARDS AND SPECIFICATIONS.
- 7. SANITARY SEWER LINES SHALL BE TESTED IN ACCORDANCE WITH THE GOVERNING AUTHORITY'S STANDARDS AND SPECIFICATIONS.
- 8. ALL WATER, STORM SEWER, AND SANITARY SEWER MAIN AND SERVICE LINES SHALL BE BEDDED IN ACCORDANCE WITH THER GOVERNING AUTHORITY'S STANDARDS AND SPECIFICATIONS.
- 9. LENGTHS OF PIPE SHOWN ARE THE HORIZONTAL DISTANCE FROM CENTERLINE TO CENTERLINE OF FITTING, BEND, MANHOLE AND/OR INLET. THEREFORE, LENGTHS SHOWN ARE APPROXIMATE.
- 10. CONTRACTOR SHALL INSTALL CORROSION PROTECTION ON FERROUS METAL WATER LINES AND FITTINGS INSTALLED IN CORROSIVE SOILS IN ACCORDANCE WITH THE RECOMMENDATIONS OF THE GEOTECHNICAL INVESTIGATION AND IN ACCORDANCE WITH GOVERNING AUTHORITY'S STANDARDS
- 11. ALL CONCRETE STRUCTURES AND CONCRETE UTILITIES SHALL BE IN ACCORDANCE WITH THE MATERIAL RECOMMENDATIONS FROM THE GEOTECHNICAL INVESTIGATION PREPARED SPECIFICALLY
- 12. THE CONTRACTOR IS RESPONSIBLE TO COORDINATE AND PROVIDE SLEEVING OR OTHER IMPROVEMENTS AS NECESSARY FOR UTILITY PENETRATIONS THROUGH THE FOUNDATION WALLS AND SLAB IN ACCORDANCE WITH THE STRUCTURAL PLANS, MEP PLANS; AND APPLICABLE BUILDING AND PLUMBING/UTILITY CODE(S).
- 13. THE CONTRACTOR SHALL COORDINATE THE EXACT ALIGNMENT, CONNECTION POINT, AND BUILDING ENTRY LOCATION FOR TELEPHONE, DATA, ELECTRIC, CABLE, GAS, AND OTHER DRY UTILITY SERVICES WITH THE UTILITY PROVIDER AND MEP PLANS PRIOR TO CONSTRUCTION, AND INSTALL NECESSARY APPURTENANCES.











COMBINATION TYPE 13 INLET



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

**NOT FOR CONSTRUCTION** 

STERLING DESIGN ASSOCIATES, LLC

NO I	DATE: 5/16/2017	BY: JDS
DESCRIP	TION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: 8/24/2017	BY: JDS
DESCRIP	TION: 2ND MAJOR SDP SUBMITTAL	
NO.: 3	DATE: -	BY: -
DESCRIP	TION: -	
NO.: 4	DATE: -	BY: -
DESCRIP	TION: -	
NO.: 5	DATE: -	BY: -
DESCRIP	TION: -	

SCALE: 1" = 20' 8/24/2017 PROJECT MANAGER: PROJECT NO.: DRAWN BY: DRAWING FILE:

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

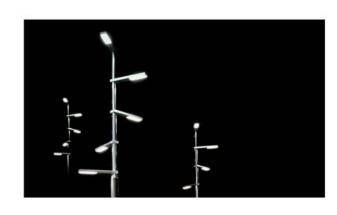
**UTILITY DETAILS** 

SHEET NUMBER:

12 OF 19 C402

landscapeforms\*

Product Data Sheet



Rama, designed by Gonzalo Milá, is a simple, innovative and versatile street lamp delivering the latest in LED technology, Directional lighting and use of white light provides a great visual experience with reduced light pollution and excellent performance with reduced energy consumption. The housing is made of aluminum to dissipate heat and increase LED longevity. Offered with 3 standard pole heights, a single luminaire or multiple luminaires can be mounted in different positions and heights. A simple clamping mechanism also allows Rama luminaires to be retrofitted to existing 5" poles, making it suitable for any

#### LED Technology

Rama employs solid state LED technology that provides exceptional energy efficiency and very long life, reducing maintenance cost. Designed to maximize this potential by managing the thermal properties of LEDs, the cast aluminum housing and anodized heat sink dissipate heat while its meticulous details and finish express quality you can see. Available in two standard color temperatures and offered with a clear or frosted glass lens.

#### Rama LED R4

Type 2 Distribution Housing: Aluminum extrusion Lamp: 24 Cree XPG2 LEDs L70: >100,000 hrs Drive Current: 500mA Optic: Polycarbonate Lens: Clear / Frosted Glass Power Supply: 120V-277V LED Driver: Philips Xitanium 75W Dimmable: 0-10V or DALI IP Rating: IP66 Weight: 20 lbs (luminaire) EPA: 1.06 ft<sup>2</sup>



Housing: Aluminum extrusion Lamp: 48 Cree XPG2 LEDs L70: >100,000 hrs Drive Current: 500mA Optic: Polycarbonate Lens: Clear / Frosted Glass Power Supply: 120V-277V LED Driver: Philips Xitanium 75V Dimmable: 0-10V or DALI IP Rating: IP66 Weight: 20 lbs (luminaire) EPA: 1.06 ft<sup>2</sup>



Rama is equipped with Constant Light Output (CLO) technology, CLO programmatically compensates for lumen depreciation by adjusting the drive current, allowing light output to remain constant throughout the luminaire maintenance cycle.

Landscape Forms, Inc. I 800.521.2546 | F 269.381.3455 | 7800 E. Michigan Ave., Kalamazoo, MI 49048

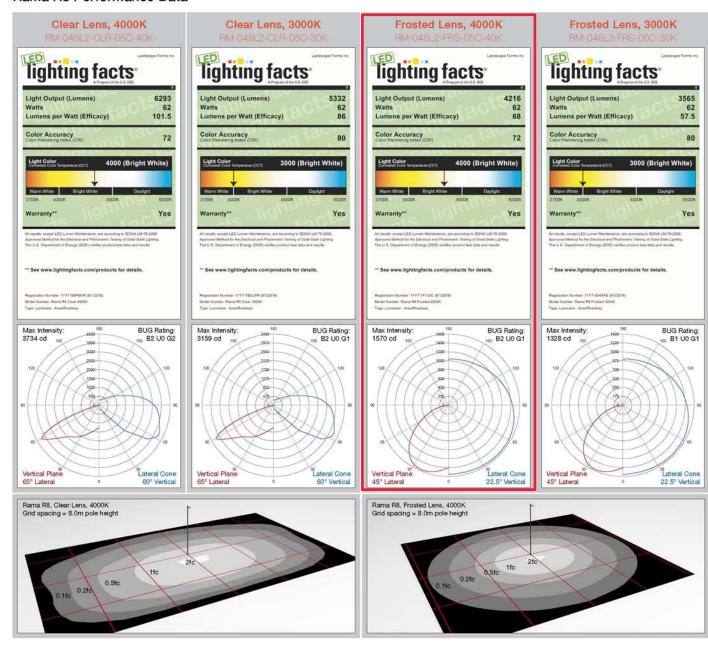
### RAMA

Product Data Sheet



landscapeforms'

#### Rama R8 Performance Data



page 1 of 6

Product Data Sheet







To Order Rama Luminaire

Product	Lamp	Lens	Drive Current	Color Temp.	Input Voltage	Mounting	Finish
	024L2 (R4, 24 LED Type 2)	CLR (Clear Glass)		40K (4000K)		5A ( w/ clamp - single mount )	AL
RM Rama	048L2	FRS	05C (500mA CLO <sup>1</sup> )	30K	UV1 (120-277VAC)	5B	( Aluminur Silver )
	( R8, 48 LED Type 2 )	(Frosted Glass)		(3000K)		( w/o clamp - double mount )	

EXAMPLE: RM - 024L2 - FRS - 05C - 30K / UV1 - 5A - AL

<sup>1</sup>CLO = Constant Light Output

#### To Order Aluminum Pole

Luminaire Configuration Twist Lock Diameter Wall Thick RM60219-01 (1) or (2) Rama luminaires at top 4.5 meters RM60219-02 (1) or (2) Rama luminaires at top .125" .188" RM60220-01 (1) or (2) Rama luminaires at top .188" RM60220-02 (1) or (2) Rama luminaires at top RM60220-03 (2) Rama luminaires at different heights .188"

Specify: Rama pole with desired mounting option, optional 7-pin (ANSI 136.41) twist lock receptacle, and Powdercoat Color.

.188" RM60220-04 (2) Rama luminaires at different heights RM60221-01 (1) or (2) Rama luminaires at top 5"/6" .188" RM60221-02 (1) or (2) Rama luminaires at top 5"/6" 5"/6" RM60221-03 (2) Rama luminaires at different heights RM60221-04 (2) Rama luminaires at different heights 5"/6" .188" .188" .188" RM60226-01 (5) Rama luminaires in spiral configuration No 5" / 7" 5" / 7" RM60226-02 (5) Rama luminaires in sprial configuration

EXAMPLE: RM60220-01 - Powdercoat Color

Landscape Forms, Inc. I 800.521.2546 | F 269.381.3455 | 7800 E. Michigan Ave., Kalamazoo, MI 49048

#### RAMA Product Data Sheet

page 4 of 6



Finish

The Rama LED body is aluminum extrusion with a natural aluminum powdercoat finish offered by Santa & Cole. The Pangard II® powdercoat finish on poles is offered exclusively by Landscape Forms.

#### Warranty

Santa & Cole warrants all metal parts and finishes for a three year period. Electrical components including LED boards and electronic drivers are warranted for five years.

UL Listed, CE, RoHS Compliant, Dark-Sky Compliant

Rama is designed by Santa & Cole

Rama design is protected by Spanish Industrial Design #147,594 and International Industrial Model #76912

Visit our landscapeforms.com for more information. Specifications are subject to change without notice. Landscape Forms supports the Landscape Architecture Foundation at the Second Century level. ©2016 Landscape Forms, Inc. Printed in U.S.A.

Landscape Forms, Inc. I 800.521.2546 | F 269.381.3455 | 7800 E. Michigan Ave., Kalamazoo, MI 49048

1.9 2.8 3.0 0.9 1.4 2.0 2.2 1.9 Perimeter 1.6 1.7 Illuminance (Fc) Average = 0.080.2 0.3 0.5 0.9 1.6 2.4 2/6 0.8 0.4 Maximum = 0.60.2 0.3 0.3 0,5 0.9 1.6 2.6 3. 0.5 Minimum = 0.0Avg/Min Ratio = N.A. °0.5 °0. Max/Min Ratio = N.A. 0.3 0.3 0.4 0.5 0.6 0.8 1.0 1.3 1.6 1.6 °0.4 % 0.2 0.4 0.6 1.1 1.6 1.9 °0.5 °0.3 0.4 0.4 0.5 0.6 0.7 0.9 1.0 1/6 0.2 0.4 0.8 1.4 2.3 28 0.5 0.6 0.6 0.7 0.9 1.3 1.5 1.5 0.4 0.8 1.4 2.3 3.1 3.0 2.1 1.3 0.8  $\sqrt[6]{0.7}$   $\sqrt[6]{0.9}$   $\sqrt[1]{0.1}$   $\sqrt[6]{1.1}$   $\sqrt[6]{1.4}$   $\sqrt[6]{2.1}$   $\sqrt[6]{2.5}$   $\sqrt[6]{2.2}$   $\sqrt[6]{15}$   $\sqrt[6]{0.9}$   $\sqrt[6]{0.6}$   $\sqrt[6]{0.4}$   $\sqrt[6]{0.9}$ 0.6 1.1 1.8 2.4 2.6 2.0 1.2 0.8 0,6 0.6 1.2 1.6 1.6 1.6 2.0 2.8 3.7 0.6 0.9 1.3 1.6 1.7 1.4 1.0 0 7 0 6 1/8 2.6 2.2 1.9 2.2 2.9 RM1 0.4 0.6 0.9 1.1 1.2 1.2 1.0 0.8 0.6 0 6 3.0 2.2 1.7 1.8 2 0.8 1.2 1.5 1.2 0.9 0.7 0.6 0.6 0.9 1.5 2.4 2.8 2 5 1.7 1.3 1.2 1.3 1.8 2.3 2.2 1.7 1.1 0.7 0 6 0.6 0.8 1.2 1.7 1.9 1.6 1.1 0.9 0.7 2.9 /3.0 2.3/ 1.4 0.8 0.6 0.5 0.6 0.8 1.0 1.0 0.9 0.7 0.5 0.5

RM-048L2-FRS-05C-40K-UV1-5A-AL (Rama R8, Frosted Lens, 4000K, w/ Clamp) --- RM60220-01 (6m Pole, no Twist Lock)

Scale: 1 inch= 20 Ft.

Luminaire Schedule

── RM1

Symbol | Label

7800 E. Michigan Ave. Kalamazoo, MI 49048 800.430.6209 landscapeforms.com

Description

Arrangement

SINGLE

Paved Surfaces

Illuminance (Fc)

Average = 1.13

Maximum = 3.2

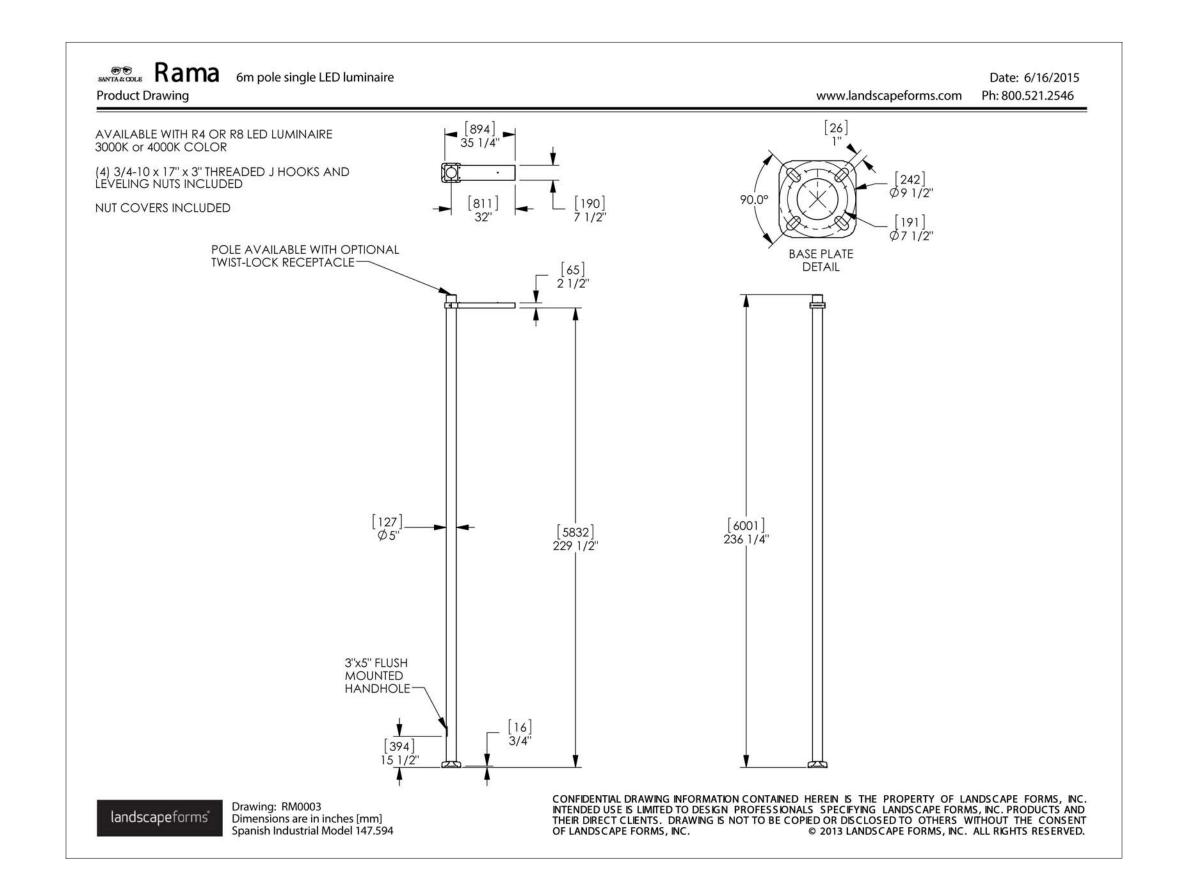
Minimum = 0.2

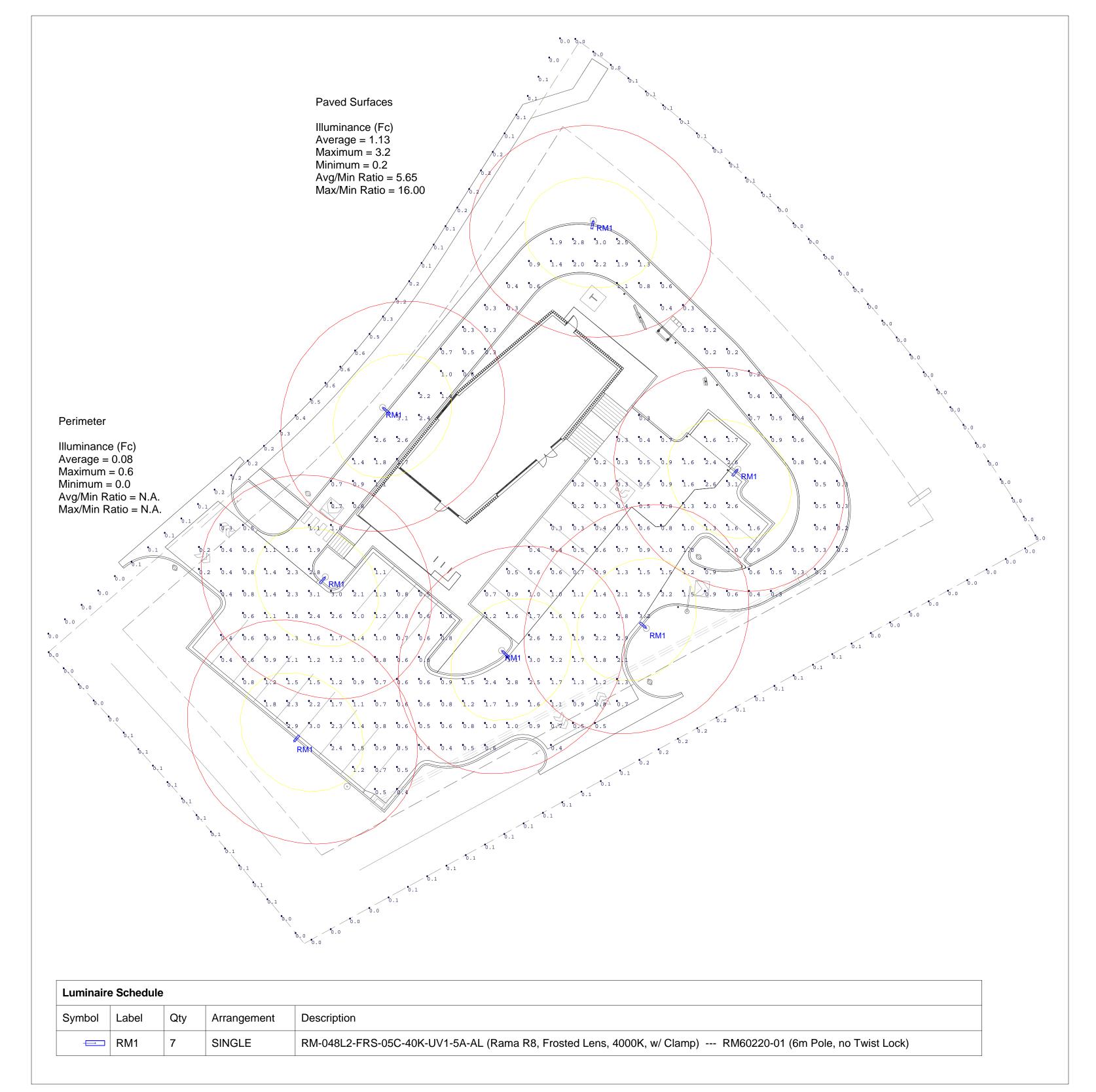
Avg/Min Ratio = 5.65 Max/Min Ratio = 16.00

> Completed By: AH Academy Gateway - Rama Lighting Checked By: CG This drawing shows suggested fixtures and mounting locations based on the information provided. Landscape Forms assumes no responsibility for variation of light levels that result from changes in parameters not reported at the time of design.

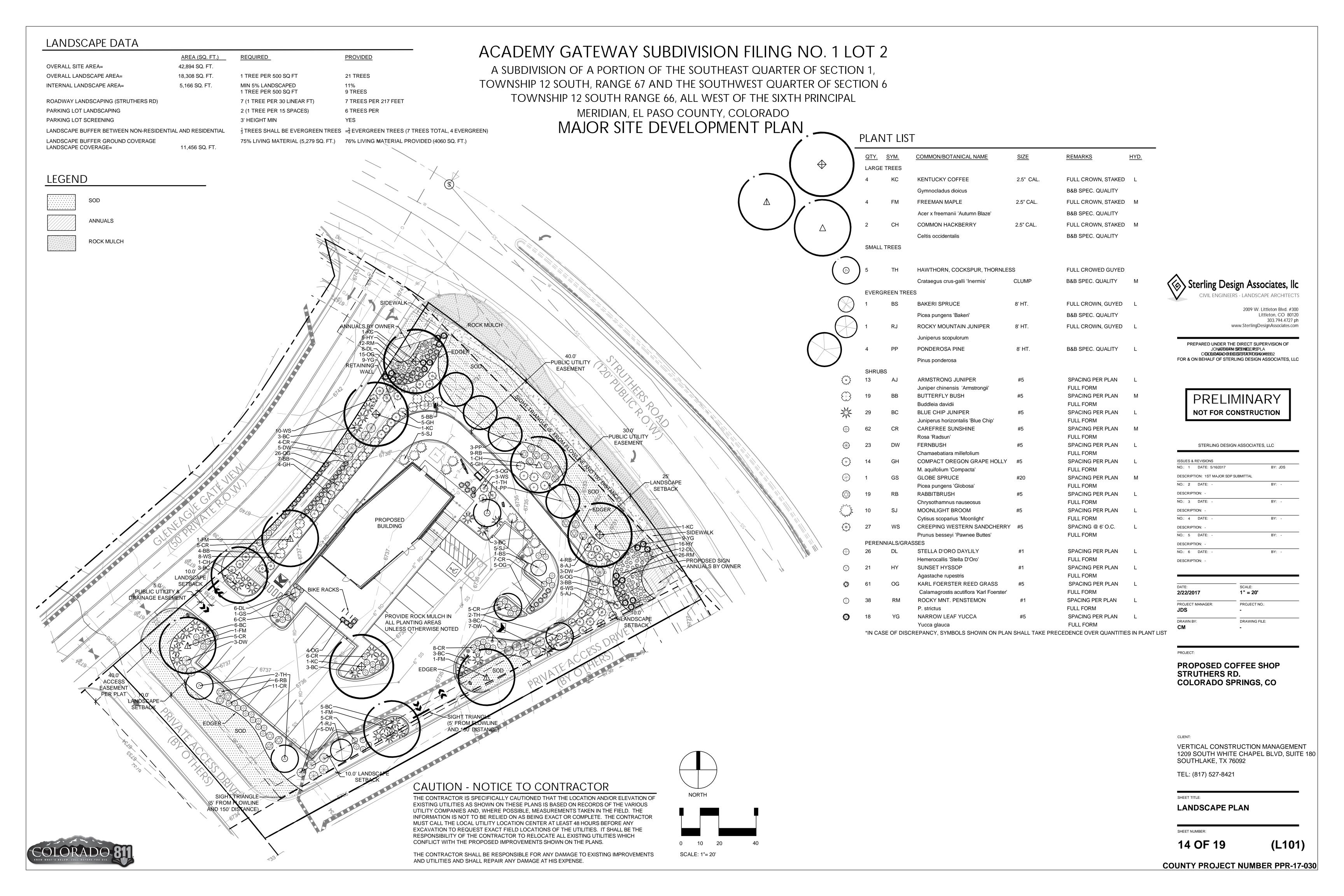
 Drawing/calculations reflect lighting from Rama fixtures only.
 Distance between readings = 7ft.
 CLO active Filename: ILP342 RevA.AGI Revision A Page 1 of 2 AGi32 Version 18.3.2

page 5 of 6 Landscape Forms, Inc. I 800.521.2546 | F 269.381.3455 | 7800 E. Michigan Ave., Kalamazoo, MI 49048





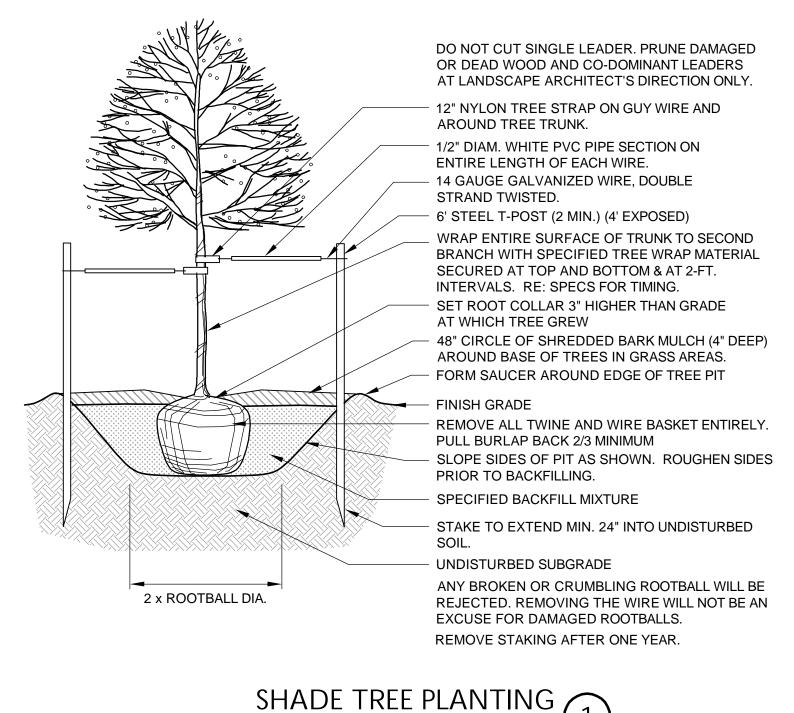
Scale: 1 inch= 20 Ft.

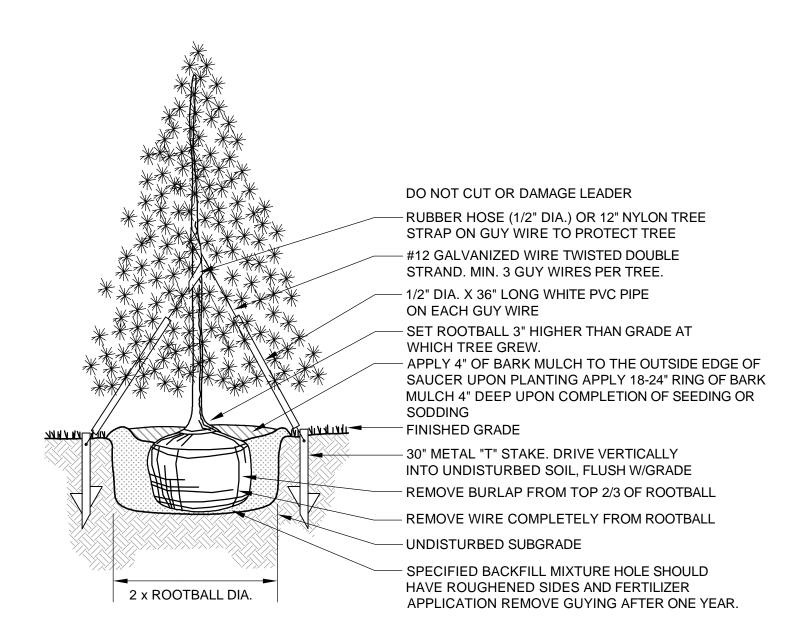


A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION 1, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

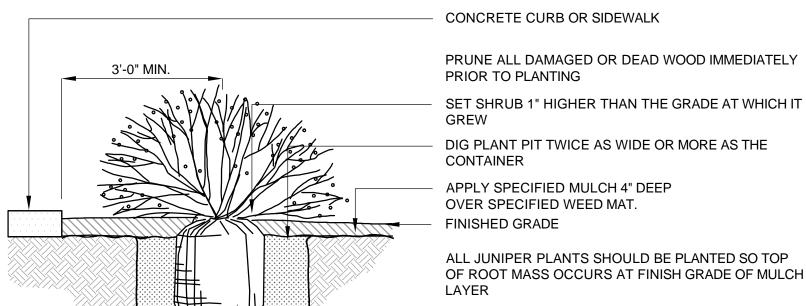
MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN









ALL JUNIPER PLANTS SHOULD BE PLANTED SO TOP OF ROOT MASS OCCURS AT FINISH GRADE OF MULCH LOOSEN SIDES OF PLANT PIT AND ROOTBALL

REMOVE CONTAINER

BACKFILL: ONE THIRD ORGANIC MATTER TWO THIRDS SITE SOIL THOROUGHLY MIXED

ANY BROKEN OR CRUMBLING ROOTBALL WILL BE REJECTED. REMOVING THE CONTAINER WILL NOT BE AN EXCUSE FOR DAMAGED ROOTBALL





NOTE: WALL SHOWN FOR GENERAL LOOK AND FEEL. \*IMAGE BY NATIVE EARTH LANDSCAPING, SANTA FE, NM LANDSCAPÉ WALL

NOT TO SCALE

DRY STACKED

- 4"-8" RED STRIP STONE

#### LANDSCAPE NOTES

THIS LANDSCAPE ARCHITECTURAL SITE PLAN IS TO BE USED IN CONJUNCTION WITH THE CIVIL. ARCHITECTURAL, MECHANICAL, ELECTRICAL, STRUCTURAL AND IRRIGATION PLANS AND SPECIFICATIONS TO FORM COMPLETE INFORMATION REGARDING THIS

CONSTRUCTION SHALL CONFORM TO ALL APPLICABLE STATE AND LOCAL CODES.

ALL MATERIALS AND WORKMANSHIP SHALL BE GUARANTEED FOR ONE YEAR, FROM DATE OF FINAL ACCEPTANCE. AT NO ADDITIONAL COST TO THE OWNER.

#### 4. COMPLETION AND MAINTENANCE:

- A. FINAL ACCEPTANCE: WITHIN TEN DAYS OF CONTRACTOR'S NOTICE THAT THE ENTIRE PROJECT IS COMPLETE, THE L.A. SHALL REVIEW INSTALLATION. IF FINAL ACCEPTANCE IS NOT GIVEN, THE L.A. SHALL PREPARE A PUNCH LIST OF PENDING ITEMS. THE PUNCH LIST ITEMS SHALL BE COMPLETED BY THE CONTRACTOR WITHIN TEN CALENDAR DAYS FROM DATE ISSUED. SUBSEQUENT REVIEW AND APPROVAL SHALL SIGNIFY ACCEPTANCE.
- B. MAINTENANCE: ALL LANDSCAPING SHALL BE MAINTAINED FROM INSTALLATION TO FINAL ACCEPTANCE. MAINTENANCE SHALL INCLUDE WATERING, FERTILIZING, WEEDING, MOWING, TRIMMING, ROLLING, REGRADING, REPLANTING, DISEASE AND INSECT PROTECTION.

#### 5. SITE CONDITIONS:

LANDSCAPE CONTRACTOR SHALL EXAMINE THE SITE CONDITIONS UNDER WHICH THE WORK IS TO BE PERFORMED AND NOTIFY THE GENERAL CONTRACTOR IN WRITING OF UNSATISFACTORY CONDITIONS. DO NOT PROCEED UNTIL CONDITIONS ARE CORRECTED. LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR FINISHED GRADES AND POSITIVE DRAINAGE IN LANDSCAPE AREAS.

#### DAMAGE AND CLEANING:

- A. CONTRACTOR SHALL REPAIR OR REPLACE ALL ITEMS DAMAGED DUE TO THIS WORK AT NO ADDITIONAL COST TO THE OWNER.
- B. CONTRACTOR SHALL CLEAN ALL AREAS DUE TO THIS WORK AND PROPERLY REMOVE ALL UNUSED MATERIALS FROM SITE.

#### 7. RIGHT OF REJECTION:

OWNER/LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY MATERIALS AT ANY TIME. ALL PLANT MATERIALS SHALL MEET THE MINIMUM CURRENT "AMERICAN STANDARDS FOR NURSERY STOCK" BY THE AMERICAN ASSOCIATION OF NURSERYMEN. CONTRACTOR SHALL PROVIDE SAMPLES, SPECS., AND OR TAGS FOR ALL MATERIALS.

#### 8. SOIL PREPARATION (AS NEEDED):

SOD AND SHRUB BED AREAS SHALL RECEIVE SIX INCHES OF APPROVED TOPSOIL AMEND SOIL WITH FOUR CUBIC YARDS PER THOUSAND SQUARE FEET OF ORGANIC COMPOST. COMPOST TO HAVE A C:N RATIO BETWEEN 15:1 AND 30:1, LESS THAN 6 mmhos/cm SALT CONTENT, A pH BETWEEN 6.5 AND 8.5, AND OVER 30% ORGANIC MATTER. APPLY 5 LBS. OF 20-10-5 GRANULAR FERTILIZER PER THOUSAND SQ. FEET. ROTOTILL AMENDMENTS AND TOPSOIL TO A DEPTH OF SIX INCHES. VERIFY SOIL PREP SPECIFICATIONS WITH WATER PROVIDER STANDARDS. PROVIDE EVIDENCE OF APPLICATION AS REQUIRED. TEST SOIL IN EXISTING ASPHALT AREAS FOR STERILANT USE. IF STERILANTS ARE FOUND PRESENT, REMOVE ALL CONTAMINATED SOIL AND REPLACE WITH NEW TOPSOIL.

9. PLANTING: TREES AND SHRUBS SHALL BE PLANTED PER DETAILS. STAKE AND GUY TREES PER DETAILS. TREE AND SHRUB BED LOCATIONS SHALL BE APPROVED PRIOR TO PLANTING. BACKFILL WITH 1/3 SOIL AMENDMENT AND 2/3 SITE SOIL, THOROUGHLY MIXED. FERTILIZE AS RECOMMENDED WITH AGRIFORM TABLETS.

SOD SHALL BE "RTF" FESCUE BY GREEN VALLEY TURF (GVT) (303-798-6764) OR APPROVED EQUAL. SOIL PREP, INSTALL AND WATER ACCORDING TO GVT GUIDELINES.

#### 11. EDGER (AS NEEDED):

INSTALL 4" ROLL-TOP RYERSON STEEL EDGING (14 GAUGE WITH GREEN FINISH) OR EQUAL. INSTALL WITH STAKES AND SPLICERS PER MANUFACTURER'S RECOMMENDATIONS. INSTALL BETWEEN ALL SOD OR SEEDED AREAS AND MULCHED

#### 12. MULCH:

MULCH SHALL BE 1  $\frac{1}{2}$ "-2" CRUSHED ANGULAR MOUNTAIN GRANITE, PLACED OVER DEWITT PRO 5 FILTER FABRIC, 3" DEEP IN ALL PLANTING AREAS. WOOD MULCH SHALL BE PROVIDED IN ANY PERENNIAL OR ANNUAL AREAS, AROUND SHRUBS AND TREES WITHIN ROCK MULCH AREAS. WOOD MULCH SHALL BE SHREDDED WESTERN RED CEDAR (NOT COLORED) AND SHALL BE PLACED AS FOLLOWS; 3' DIA. RINGS AROUND TREES AND 1 ½ THE WIDTH OF THE ROOT BALL AROUND SHRUBS, PER THE ACADEMY GATEWAY DESIGN GUIDELINES. AN 18" WIDE PERIMETER OF ROCK MULCH SHALL BE PLACED AROUND THE FOUNDATION OF THE BUILDING WERE ADJACENT TO LANDSCAPE AREAS. COBBLE IN R.O.W. SHALL BE 2"-4" ROUNDED GRANITE COBBLE OVER FILTER FABRIC, MIN 4" DEPTH.

#### 13. MAINTENANCE:

THE PROPERTY OWNER OF RECORD, THE OWNER'S AGENT OR TENANT SHALL KEEP ALL LANDSCAPING IN A WELL MAINTAINED AND HEALTHY GROWING CONDITION, AND IN ACCORDANCE WITH ACADEMY GATEWAY DESIGN GUIDELINES.

#### 14. IRRIGATION:

ALL LANDSCAPE AREAS SHALL BE WATERED BY AN AUTOMATIC IRRIGATION SYSTEM TO INCLUDE IRRIGATION TO ALL PROPOSED PLANT MATERIAL. ALL SHRUB BEDS SHALL BE IRRIGATED USING A UNDERGROUND DRIP IRRIGATION AND ALL SOD AREAS SHALL BE IRRIGATED WITH OVERHEAD POP-UP ROTARY NOZZLE HEADS.



2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JON**JATSHEARINI SSIRJENJE**JE IR EPLA COLODRADO OR ENGISTER ATMONOMENTABBB32 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

**PRELIMINARY** NOT FOR CONSTRUCTION

STERLING DESIGN ASSOCIATES, LLC

NO.: 1	DATE:	5/16/2017	BY: JD
DESCRI	PTION: 1S	T MAJOR SDP SUBMITTAL	
NO.: 2	DATE:	-	BY: -
DESCRI	PTION: -		
NO.: 3	DATE:	-	BY: -
DESCRI	PTION: -		
NO.: 4	DATE:	-	BY: -
DESCRI	PTION: -		
NO.: 5	DATE:	-	BY: -
DESCRI	PTION: -		
NO · 6	DATE:	-	BY: -

DATE: <b>2/22/2017</b>	SCALE: 1" = 20'
PROJECT MANAGER:	PROJECT NO.:
DRAWN BY:	DRAWING FILE:

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

LANDSCAPE NOTES AND DETAILS

SHEET NUMBER:

15 OF 19

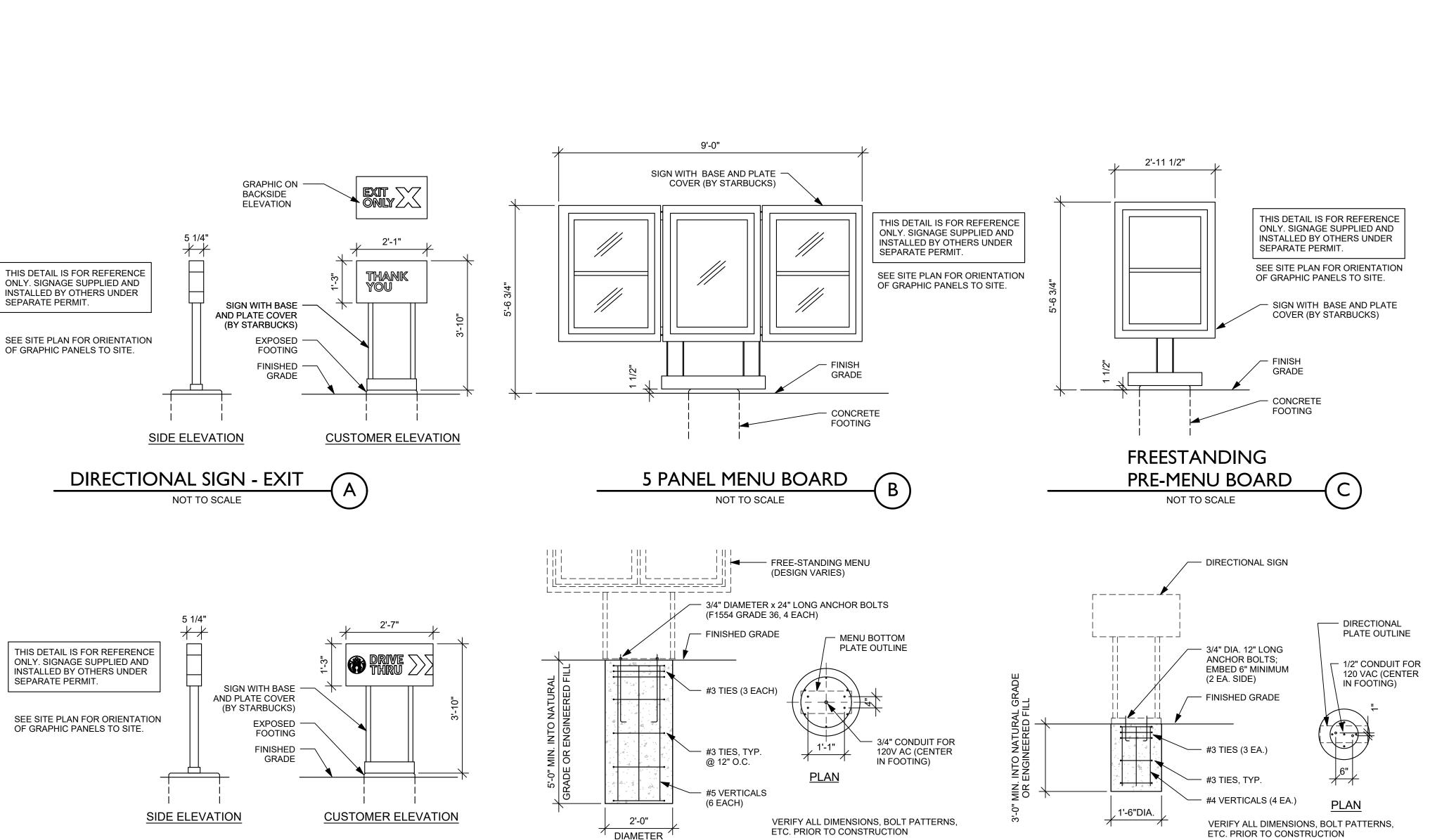
COUNTY PROJECT NUMBER PPR-17-030

(L102)

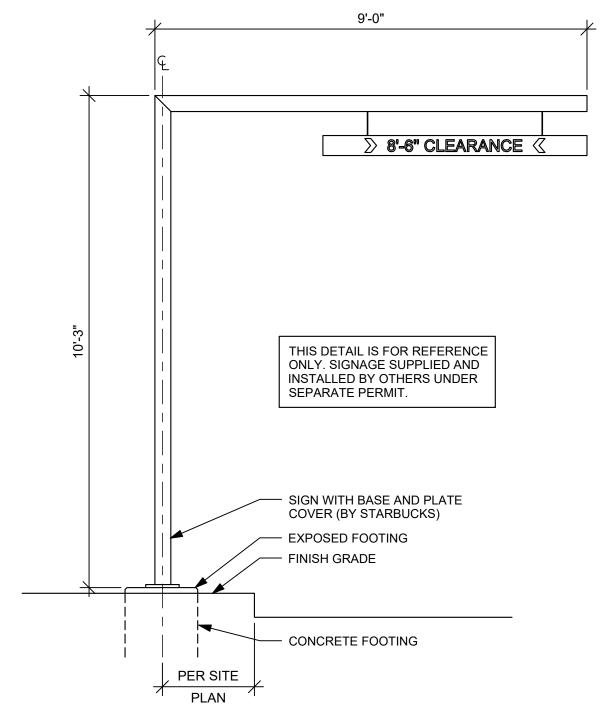
A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I,
TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6
TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL
MERIDIAN, EL PASO COUNTY, COLORADO

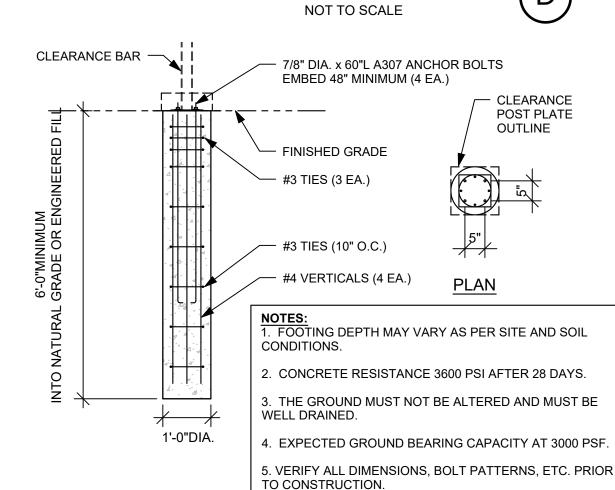
MAJOR SITE DEVELOPMENT PLAN

DIRECTIONAL SIGN FOOTING



MENU BOARD FOOTING





**CLEARANCE BAR** 





2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

PRELIMINARY NOT FOR CONSTRUCTION

STERLING DESIGN ASSOCIATES, LLC

NO.: 1	DATE: 5/16/2017	BY: JDS
DESCRIP	TION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: -	BY: -
DESCRIP	PTION: -	
NO.: 3	DATE: -	BY: -
DESCRIP	PTION: -	
NO.: 4	DATE: -	BY: -
DESCRIP	PTION: -	
NO.: 5	DATE: -	BY: -
DESCRIP	PTION: -	
NO.: 6	DATE: -	BY: -

DATE: <b>2/22/2017</b>	SCALE: 1" = 20'
PROJECT MANAGER:  JDS	PROJECT NO.:
DRAWN BY:	DRAWING FILE:

PROJEC<sup>-</sup>

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

CLIENT:

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

SIGNAGE DETAILS

SHEET NUMBER:

16 OF 19 (C701)



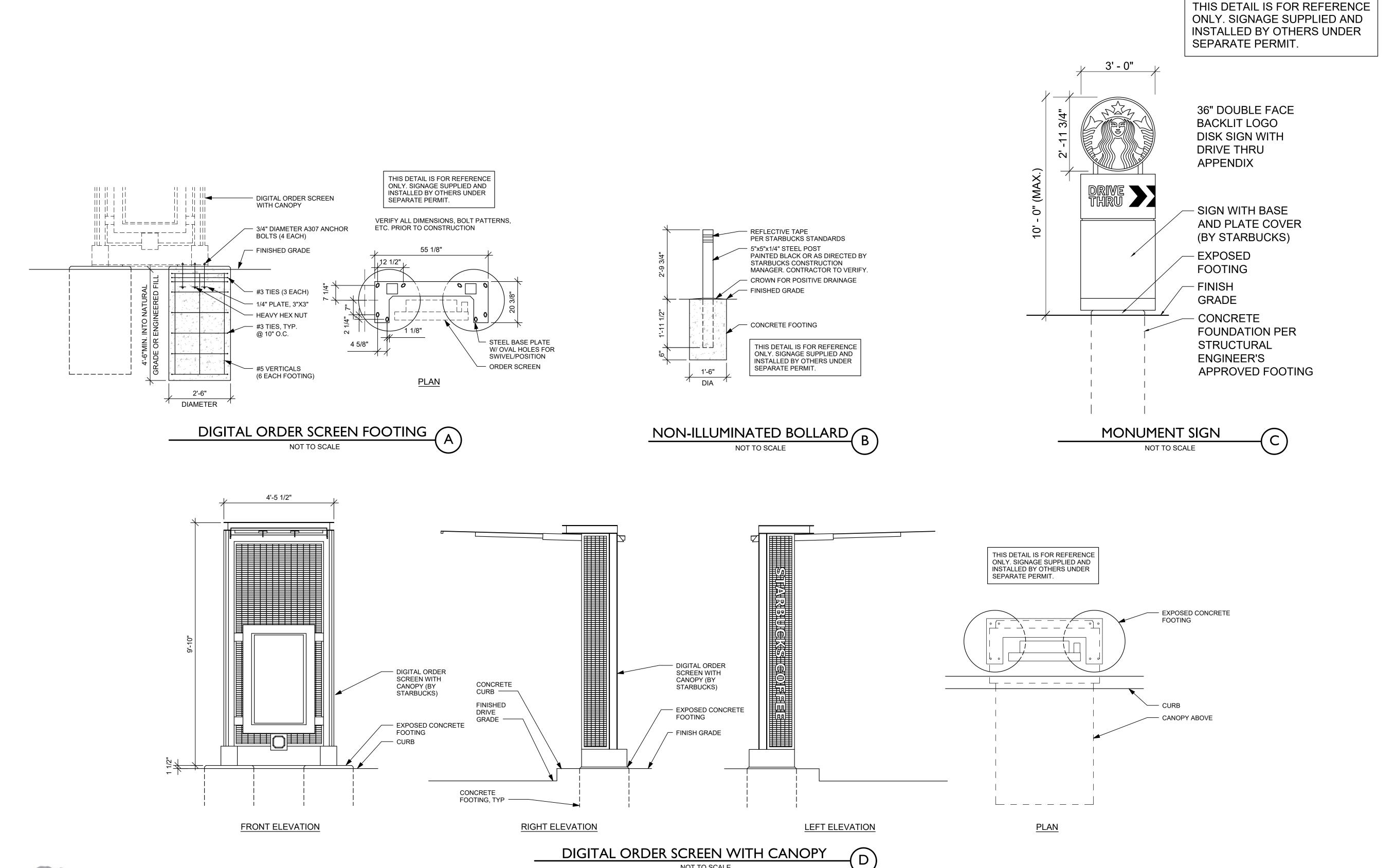


DIRECTIONAL SIGN - ENTRY (E)

A SUBDIVISION OF A PORTION OF THE SOUTHEAST QUARTER OF SECTION I, TOWNSHIP 12 SOUTH, RANGE 67 AND THE SOUTHWEST QUARTER OF SECTION 6 TOWNSHIP 12 SOUTH RANGE 66, ALL WEST OF THE SIXTH PRINCIPAL

MERIDIAN, EL PASO COUNTY, COLORADO

# MAJOR SITE DEVELOPMENT PLAN





2009 W. Littleton Blvd. #300 Littleton, CO 80120 303.794.4727 ph www.SterlingDesignAssociates.com

PREPARED UNDER THE DIRECT SUPERVISION OF JOSEPH SCHIEL, PE COLORADO REGISTRATION 0048332 FOR & ON BEHALF OF STERLING DESIGN ASSOCIATES, LLC

PRELIMINARY NOT FOR CONSTRUCTION

STERLING DESIGN ASSOCIATES, LLC

NO I	DATE: 5/16/2017	BY: JDS
DESCRIP	TION: 1ST MAJOR SDP SUBMITTAL	
NO.: 2	DATE: -	BY: -
DESCRIP	TION: -	
NO.: 3	DATE: -	BY: -
DESCRIP	TION: -	
NO.: 4	DATE: -	BY: -
DESCRIP	TION: -	
	DATE: -	BY: -
NO.: 5		
NO.: 5 DESCRIP	TION: -	

DATE:	SCALE:
2/22/2017	1" = 20'
PROJECT MANAGER:	PROJECT NO.:
JDS	-
DRAWN BY:	DRAWING FILE:
	_

PROJEC<sup>-</sup>

PROPOSED COFFEE SHOP STRUTHERS RD. COLORADO SPRINGS, CO

CLIENT:

VERTICAL CONSTRUCTION MANAGEMENT 1209 SOUTH WHITE CHAPEL BLVD, SUITE 180 SOUTHLAKE, TX 76092

TEL: (817) 527-8421

SHEET TITLE:

SIGNAGE DETAILS

SHEET NUMBER:

17 OF 19 (C702)



TRUTHERS

PROJECT ADDRESS:
209 GLENEAGLE GATE \
COLORADO SPRINGS, C REVISION SCHEDULE DESCRIPTIONS REV DATE BY

16-0349 PROJECT #:

ORIGINAL ISSUE DATE: 08-07-2017

PRODUCTION DESIGNER: R. ORELLANA J. PETTY CHECKED BY:

SHEET TITLE:

SCALE:

**ELEVATIONS** 

1/4" = 1'-0"

SHEET NUMBER:

DRYVIT - COMMERCIAL PLASTER - SAND PEBBLE FINE -PAINT SW6991: BLACK MAGIC

METAL PREFINISHED DARK BRONZE



METAL HOT ROLLED STEEL FINISH BAND





RTU SCREEN WALL

FINISH SW2927

WEATHERVANE

**DECORATIVE MESH-**



5 SOUTH ELEVATION

Scale: 1/4" = 1'-0"



6 NORTH ELEVATION

Scale: 1/4" = 1'-0"

TRUTHERS

PRUJEUL AUDALOG.
209 GLENEAGLE GATE \
COLORADO SPRINGS, C

REVISION SCHEDULE REV DATE BY DESCRIPTIONS

16-0349 PROJECT #:

ORIGINAL ISSUE DATE: 08-07-2017

PRODUCTION DESIGNER: R. ORELLANA J. PETTY CHECKED BY:

SHEET TITLE:

**ELEVATIONS** 

SCALE:

SHEET NUMBER:

DRYVIT - COMMERCIAL PLASTER - SAND PEBBLE FINE -PAINT SW6991: BLACK MAGIC



METAL PREFINISHED DARK BRONZE









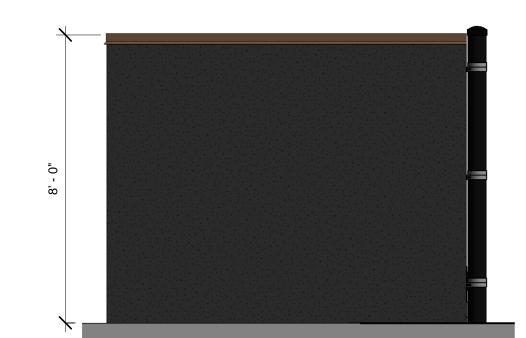
**WEST ELEVATION** 

Scale: 1/4" = 1'-0"

Scale: 1/4" = 1'-0"

DUMPSTER ENCLOSURE - WEST ELEVATION

Scale: 3/8" = 1'-0"



RTU SCREEN WALL

FINISH SW2927

WEATHERVANE

**DECORATIVE MESH-**

Scale: 3/8" = 1'-0"

DUMPSTER ENCLOSURE - NORTH ELEVATION

mesh is not 100% opaque please revise material to stucco,

CMU, decorative

concrete, etc....





DUMPSTER ENCLOSURE - EAST ELEVATION Scale: 3/8" = 1'-0"



DUMPSTER ENCLOSURE - SOUTH ELEVATION Scale: 3/8" = 1'-0"

18

As indicated

#### dsdrice (2)



Subject: Cloud+

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdrice

Date: 9/8/2017 10:10:25 AM

Color:

Provide inlet protection.



Subject: Callout

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdrice

Date: 9/8/2017 10:06:54 AM

Color:

fill in

#### dsdparsons (6)



Subject: Callout

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdparsons Date: 9/11/2017 8:47:24 AM

Color:

Sidewalk should extend to the edge of the property. Show the PED ramps. The improvement

on your property and it is required with

development of this property



Subject: Callout

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdparsons Date: 9/11/2017 8:52:25 AM

Color:

Dumpster screening must be 100% opaque; concrete, cmu, stucco, not mesh screening fencing



Subject: Callout

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdparsons Date: 9/11/2017 8:27:06 AM

Color:

2 feet of overhang is required with an 8' x 15' space. A transformer is shown here; that may be a safety concern. Remove parking space.



Subject: Callout

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdparsons Date: 9/11/2017 8:58:49 AM

Color:

Provide fence locations as shown in details



Subject: Callout

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdparsons Date: 9/11/2017 8:56:48 AM

Color:

is this a 12' tall fence? show fencing on site plan sheet- fencing over 7' tall must meet structural

setbacks.

86.48'

N61°15'39"E 266.55'



Subject: Callout

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: dsdparsons Date: 9/11/2017 8:54:36 AM

Color:

mesh is not 100% opaque please revise material to stucco, CMU, decorative concrete, etc....

#### AutoCAD SHX Text (107)



Subject:

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: AutoCAD SHX Text

Date: Color:



Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: AutoCAD SHX Text

Date: Color:



Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: AutoCAD SHX Text

Date: Color:



Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: AutoCAD SHX Text

Date: Color: D

**************************************	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	H
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	D
70	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Y
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	S
N49'04'22" 86 48'	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	N49°04'22"E
>	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	N32°26'48"E78.69'

Δ=16'37'33" R=325.00' L=94.31	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Δ=16°37'33" R=325.00' L=94.31'
- 277.12° - 277.12° - 277.13°	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Δ=24°17'19" L=217.45'
L=123,40°	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Δ=04°55'38" L=123.40'
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Н
COMPACT	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	COMPACT
ONLY	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	ONLY

VAN CCESSIBI	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	VAN
OW AWA	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	ZONE
VAN ACCESSIBLE	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	ACCESSIBLE
TOW AWAY ZONE	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	TOW AWAY
RESERVED PARKING	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	RESERVED
CARS	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CARS

RESERVED PARKING	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	PARKING
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	S
EC	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	EC
TBC	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	TBC
FL	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	FL
GBW	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	GBW

	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Н
GB	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	GB
-7512-	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	7512
HP	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	HP
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	D
GTW	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	GTW

SYO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	D
TOW	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	TOW
AYO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Υ
ВС	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status:	BC

0.58%

Subject:

Date: Color:

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: AutoCAD SHX Text

Checkmark: Unchecked Author: AutoCAD SHX Text

Date: Color:

Subject:

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: AutoCAD SHX Text

Date: Color:

0.58%%%

WV N	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	WV
S	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	S
TO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Y
ME	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	ME
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Н

(SF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SF
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
—7512 <del>—</del>	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	7512
(cs)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CS
TYPO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Y
LOD	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	LOD

SSA	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SSA
(SF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SF
(CF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CF
SF	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SF
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
(CF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CF

LOD	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	LOD
LOD	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	LOD
LOD	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	LOD
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	IP
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	IP
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	s

(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
(CF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CF
(CF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CF
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
HYC	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Н

(SF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SF
(cs)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	cs
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	IP
LOD	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	LOD
(SF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SF
(ECB)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	ECB

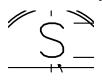
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
ECB	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	ECB
CWA	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CWA
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
(CF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CF
4Y0	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	D

(CF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CF
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
S	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	S
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
70	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	D
(CF)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CF

LOD	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	LOD
(cs)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CS
VTC	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	VTC
TO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Y
(SM)	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SM
SSA	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	SSA

VTC	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	VTC
CWA	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	CWA
SYO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	D
S	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	S
140 ·	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	D
TYC	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Н

TRN	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	TRN
	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	S
TYO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Y
THO	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Y
WV N 1	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	WV
TYC	Subject: Page Label: [1] 1 VCM-Northgate C100Cover_page - C100 Lock: Unlocked Status: Checkmark: Unchecked Author: AutoCAD SHX Text Date: Color:	Н



Subject:

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Unlocked

Status:

Checkmark: Unchecked Author: AutoCAD SHX Text

Date: Color:

#### ajh (12)



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:19:41 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:16:37 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:19:00 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:19:54 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:17:09 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:20:12 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:18:21 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:18:39 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:17:38 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: ajh

Date: 8/25/2017 7:18:10 AM

Color:



Subject: Rectangle

Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100

Lock: Locked Status:

Checkmark: Unchecked

Author: aih

Date: 8/25/2017 7:19:10 AM

Color:

40K (4000K) 30K

Subject: Rectangle
Page Label: [1] 1 VCM-Northgate C100Cover\_page - C100
Lock: Locked

Status:

Checkmark: Unchecked

Author: ajh
Date: 8/25/2017 7:18:29 AM
Color: