

OWL MARKETPLACE FILING NO. 1

EL PASO COUNTY, COLORADO

CIVIL CONSTRUCTION PLANS

AGENCY CONTACTS:

COUNTY: EL PASO COUNTY PLANNING & COMMUNITY DEVELOPMENT
RYAN HOWSER, PLANNER
2880 INTERNATIONAL CIRCLE, SUITE 110
COLORADO SPRINGS, CO 80910
(719) 520-6300

FIRE: FALCON FIRE DEPARTMENT
TRENT HARWIG, FIRE CHIEF
7030 OLD MERIDIAN ROAD
FALCON, CO 80831
(719)495-4050

WATER: WOODMEN HILLS METROPOLITAN DISTRICT
8046 EASTONVILLE ROAD
FALCON, CO 80831
(719) 495-2500

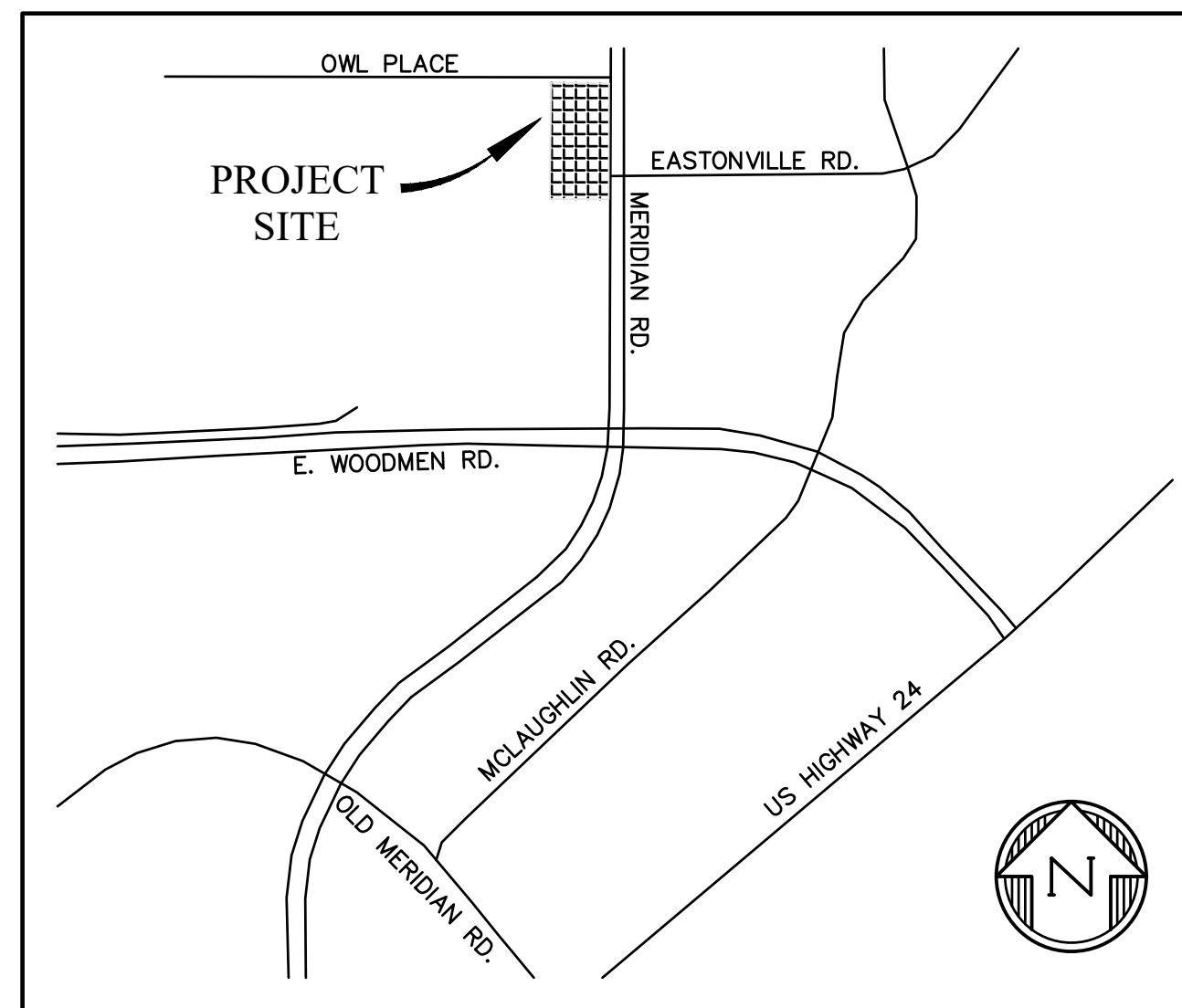
WASTEWATER: WOODMEN HILLS METROPOLITAN DISTRICT
8046 EASTONVILLE ROAD
FALCON, CO 80831
(719) 495-2500

ELECTRIC: MOUNTAIN VIEW ELECTRIC ASSOCIATION
LES ULFERS
11140 E. WOODMEN ROAD
FALCON, CO 80831
(719) 495-2283

GAS: COLORADO SPRINGS UTILITIES
TODD STURTEVANT
1521 HANCOCK EXPRESSWAY
COLORADO SPRINGS, CO 80947
(719) 668-3556

TELEPHONE: CENTURY LINK
SALLY KLEIN
(719) 636-4329
(LOCATORS) (719) 597-8418
AT&T
(LOCATORS) (719) 635-3674

CABLE: COMCAST
DALE STEWART
213 N. UNION BLVD
COLORADO SPRINGS, CO 80909
(719) 442-4733



VICINITY MAP
NTS

LEGAL DESCRIPTION:

LOT 14 & 15, FALCON RANCHETTES, ACCORDING TO THE PLAT AS RECORDED OCTOBER 21, 1986 AT RECEPTION NO. 1470182, COUNTY OF EL PASO, STATE OF COLORADO.

FLOODPLAIN STATEMENT:

THE EFFECTIVE FLOODPLAIN ZONE A LIMITS ARE DEFINED ON THE FIRM FOR EL PASO COUNTY, COLORADO AND UNINCORPORATED AREAS, MAP NUMBER 08041C0553G, EFFECTIVE DATE DECEMBER 7, 2018.

A CLOMR TO MODIFY THE FLOODPLAIN WAS APPROVED BY FEMA CASE NO. 22-08-0669R (DEC 21, 2022).

BENCHMARK:

ELEVATIONS ARE BASED ON DREXEL, BARRELL & CO. "CONTROL POINT #300", A 2" ALUMINUM CAP LOCATED 30' NORTH OF THE SE CORNER OF LOT 2 OWL MARKETPLACE, WITH AN ELEVATION OF 6912.90 (NAVD 88).

TIMING:

ANTICIPATED STARTING AND COMPLETION TIME PERIOD OF SITE GRADING: SUMMER 2024 -SPRING 2025

AREAS:

TOTAL AREA OF THE SITE TO BE CLEARED, EXCAVATED OR GRADED: APPROXIMATELY 4.61 ACRES

RECEIVING WATERS:

FALCON WATERSHED - UNNAMED TRIBUTARY TO UPPER BLACK SQUIRREL CREEK

SOILS:

HYDROLOGIC TYPE A: COLUMBINE GRAVELLY SANDY LOAM (NO. 19)

DESIGN ENGINEER'S STATEMENT:

THESE DETAILED PLANS AND SPECIFICATIONS WERE PREPARED UNDER MY DIRECTION AND SUPERVISION. SAID PLANS AND SPECIFICATIONS HAVE BEEN PREPARED ACCORDING TO CRITERIA ESTABLISHED BY THE COUNTY FOR THE DETAILED ROADWAY, DRAINAGE, GRADING AND EROSION CONTROL PLANS AND SPECIFICATIONS, AND SAID PLANS AND SPECIFICATIONS ARE IN CONFORMITY WITH APPLICABLE MASTER DRAINAGE PLANS AND MASTER TRANSPORTATION PLANS. SAID PLANS AND SPECIFICATIONS MEET THE PURPOSES FOR WHICH THE PARTICULAR ROADWAY AND DRAINAGE FACILITIES ARE DESIGNED AND ARE CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. I ACCEPT RESPONSIBILITY FOR ANY LIABILITY CAUSED BY ANY NEGLIGENT ACTS, ERRORS OR OMISSIONS ON MY PART IN PREPARATION OF THESE DETAILED PLANS AND SPECIFICATIONS.

KATHERINE G. VARNUM _____ DATE
P.E. # 53459

OWNER'S STATEMENT:

I, THE OWNER/DEVELOPER HAVE READ AND WILL COMPLY WITH THE REQUIREMENTS OF THE GRADING AND EROSION CONTROL PLAN AND ALL OF THE REQUIREMENTS SPECIFIED IN THESE DETAILED PLANS AND SPECIFICATIONS.

BRIAN ZUREK _____ DATE

EL PASO COUNTY:

COUNTY PLAN REVIEW IS PROVIDED ONLY FOR GENERAL CONFORMANCE WITH COUNTY DESIGN CRITERIA. THE COUNTY IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND/OR ELEVATIONS WHICH SHALL BE CONFIRMED AT THE JOB SITE. THE COUNTY THROUGH THE APPROVAL OF THIS DOCUMENT ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT.

FILED IN ACCORDANCE WITH THE REQUIREMENTS OF THE EL PASO COUNTY LAND DEVELOPMENT CODE, DRAINAGE CRITERIA MANUAL, AND ENGINEERING CRITERIA MANUAL AS AMENDED.

IN ACCORDANCE WITH ECM SECTION 1.12, THESE CONSTRUCTION DOCUMENTS WILL BE VALID FOR CONSTRUCTION FOR A PERIOD OF 2 YEARS FROM THE DATE SIGNED BY THE EL PASO COUNTY ENGINEER. IF CONSTRUCTION HAS NOT STARTED WITHIN THOSE 2 YEARS, THE PLANS WILL NEED TO BE RESUBMITTED FOR APPROVAL, INCLUDING PAYMENT OF REVIEW FEES AT THE PLANNING AND COMMUNITY DEVELOPMENT DIRECTOR'S DISCRETION.

JOSHUA PALMER, P.E. _____ DATE
COUNTY ENGINEER

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| SHT3 | C3 | SITE DEMO |
| SHT4 | C4 | OWL PLACE PLAN & PROFILE |
| SHT5 | C5 | MERIDIAN PARK DRIVE PLAN & PROFILE |
| SHT6 | C6 | MERIDIAN PARK DRIVE FLOWLINE PLAN & PROFILE |
| SHT7 | C7 | EASTONVILLE ROUNDABOUT |
| SHT8 | C8 | MERIDIAN ROAD IMPROVEMENTS |
| SHT9 | C9 | SITE PAVING & CONCRETE PLAN |
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811 Know what's below.
Call before you dig.

CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.
PCD FILE: VR2321

PREPARED BY:



DREXEL, BARRELL & CO.
Engineers • Surveyors
101 SAWATCH ST., STE #100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719)260-0887
COLORADO SPRINGS • LAFAYETTE

CLIENT:

BH RE INVESTMENTS, LLC
450 N MCCLINTOCK DRIVE
CHANDLER, AZ 85226
(480) 313-2724

CONSTRUCTION PLANS FOR:
OWL MARKETPLACE
EL PASO COUNTY, COLORADO

| ISSUE | DATE |
|---------------|------------|
| INITIAL ISSUE | 08/04/2023 |
| RESUBMITTAL | 04/12/2024 |
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| | |
| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-CVR |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.
DRAWING SCALE:
HORIZONTAL: N/A
VERTICAL: N/A

COVER SHEET

PROJECT NO. 21611-01CSCV
DRAWING NO.

C1

SHEET: 1 OF 18

STANDARD NOTES FOR EL PASO COUNTY GRADING AND EROSION CONTROL PLANS

- STORMWATER DISCHARGES FROM CONSTRUCTION SITES SHALL NOT CAUSE OR THREATEN TO CAUSE POLLUTION, CONTAMINATION, OR DEGRADATION OF STATE WATERS. ALL WORK AND EARTH DISTURBANCE SHALL BE DONE IN A MANNER THAT MINIMIZES POLLUTION OF ANY ON-SITE OR OFF-SITE WATERS, INCLUDING WETLANDS.
- NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS FROM REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING.
- A SEPARATE STORMWATER MANAGEMENT PLAN (SWMP) FOR THIS PROJECT SHALL BE COMPLETED AND AN EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQCP) ISSUED PRIOR TO COMMENCING CONSTRUCTION. MANAGEMENT OF THE SWMP DURING CONSTRUCTION IS THE RESPONSIBILITY OF THE DESIGNATED QUALIFIED STORMWATER MANAGER OR CERTIFIED EROSION CONTROL INSPECTOR. THE SWMP SHALL BE LOCATED ON SITE AT ALL TIMES DURING CONSTRUCTION AND SHALL BE KEPT UP TO DATE WITH WORK PROGRESS AND CHANGES IN THE FIELD.
- ONCE THE ESQCP IS APPROVED AND A "NOTICE TO PROCEED" HAS BEEN ISSUED, THE CONTRACTOR MAY INSTALL THE INITIAL STAGE EROSION AND SEDIMENT CONTROL MEASURES AS INDICATED ON THE APPROVED GEC. A PRECONSTRUCTION MEETING BETWEEN THE CONTRACTOR, ENGINEER, AND EL PASO COUNTY WILL BE HELD PRIOR TO ANY CONSTRUCTION. IT IS THE RESPONSIBILITY OF THE APPLICANT TO COORDINATE THE MEETING TIME AND PLACE WITH COUNTY STAFF.
- CONTROL MEASURES MUST BE INSTALLED PRIOR TO COMMENCEMENT OF ACTIVITIES THAT COULD CONTRIBUTE POLLUTANTS TO STORMWATER. CONTROL MEASURES FOR ALL SLOPES, CHANNELS, DITCHES, AND DISTURBED LAND AREAS SHALL BE INSTALLED IMMEDIATELY UPON COMPLETION OF THE DISTURBANCE.
- ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE MAINTAINED AND REMAIN IN EFFECTIVE OPERATING CONDITION UNTIL PERMANENT SOIL EROSION CONTROL MEASURES ARE IMPLEMENTED AND FINAL STABILIZATION IS ESTABLISHED. ALL PERSONS ENGAGED IN LAND DISTURBANCE ACTIVITIES SHALL ASSESS THE ADEQUACY OF CONTROL MEASURES AT THE SITE AND IDENTIFY IF CHANGES TO THOSE CONTROL MEASURES ARE NEEDED TO ENSURE THE CONTINUED EFFECTIVE PERFORMANCE OF THE CONTROL MEASURES. ALL CHANGES TO TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES MUST BE INCORPORATED INTO THE STORMWATER MANAGEMENT PLAN.
- TEMPORARY STABILIZATION SHALL BE IMPLEMENTED ON DISTURBED AREAS AND STOCKPILES WHERE GROUND DISTURBING CONSTRUCTION ACTIVITY HAS PERMANENTLY CEASED OR TEMPORARILY CEASED FOR LONGER THAN 14 DAYS.
- FINAL STABILIZATION MUST BE IMPLEMENTED AT ALL APPLICABLE CONSTRUCTION SITES. FINAL STABILIZATION IS ACHIEVED WHEN ALL GROUND DISTURBING ACTIVITIES ARE COMPLETE AND ALL DISTURBED AREAS EITHER HAVE A UNIFORM VEGETATIVE COVER WITH INDIVIDUAL PLANT DENSITY OF 70 PERCENT OF PRE-DISTURBANCE LEVELS ESTABLISHED OR EQUIVALENT PERMANENT ALTERNATIVE STABILIZATION METHOD IS IMPLEMENTED. ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE REMOVED UPON FINAL STABILIZATION AND BEFORE PERMIT CLOSURE.
- ALL PERMANENT STORMWATER MANAGEMENT FACILITIES SHALL BE INSTALLED AS DESIGNED IN THE APPROVED PLANS. ANY PROPOSED CHANGES THAT AFFECT THE DESIGN OR FUNCTION OF PERMANENT STORMWATER MANAGEMENT STRUCTURES MUST BE APPROVED BY THE ECM ADMINISTRATOR PRIOR TO IMPLEMENTATION.
- EARTH DISTURBANCES SHALL BE CONDUCTED IN SUCH A MANNER SO AS TO EFFECTIVELY MINIMIZE ACCELERATED SOIL EROSION AND RESULTING SEDIMENTATION. ALL DISTURBANCES SHALL BE DESIGNED, CONSTRUCTED, AND COMPLETED SO THAT THE EXPOSED AREA OF ANY DISTURBED LAND SHALL BE LIMITED TO THE SHORTEST PRACTICAL PERIOD OF TIME. PRE-EXISTING VEGETATION SHALL BE PROTECTED AND MAINTAINED WITHIN 50 HORIZONTAL FEET OF A WATERS OF THE STATE UNLESS SHOWN TO BE INFEASIBLE AND SPECIFICALLY REQUESTED AND APPROVED.
- COMPACTION OF SOIL MUST BE PREVENTED IN AREAS DESIGNATED FOR INFILTRATION CONTROL MEASURES OR WHERE FINAL STABILIZATION WILL BE ACHIEVED BY VEGETATIVE COVER. AREAS DESIGNATED FOR INFILTRATION CONTROL MEASURES SHALL ALSO BE PROTECTED FROM SEDIMENTATION DURING CONSTRUCTION UNTIL FINAL STABILIZATION IS ACHIEVED. IF COMPACTION PREVENTION IS NOT FEASIBLE DUE TO SITE CONSTRAINTS, ALL AREAS DESIGNATED FOR INFILTRATION AND VEGETATION CONTROL MEASURES MUST BE LOOSENEED PRIOR TO INSTALLATION OF THE CONTROL MEASURE(S).
- ANY TEMPORARY OR PERMANENT FACILITY DESIGNED AND CONSTRUCTED FOR THE CONVEYANCE OF STORMWATER AROUND, THROUGH, OR FROM THE EARTH DISTURBANCE AREA SHALL BE A STABILIZED CONVEYANCE DESIGNED TO MINIMIZE EROSION AND THE DISCHARGE OF SEDIMENT OFF SITE.
- CONCRETE WASH WATER SHALL BE CONTAINED AND DISPOSED OF IN ACCORDANCE WITH THE SWMP. NO WASH WATER SHALL BE DISCHARGED TO OR ALLOWED TO ENTER STATE WATERS, INCLUDING ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR FACILITIES. CONCRETE WASHOUTS SHALL NOT BE LOCATED IN AN AREA WHERE SHALLOW GROUNDWATER MAY BE PRESENT, OR WITHIN 50 FEET OF A SURFACE WATER BODY, CREEK OR STREAM.
- DURING DEWATERING OPERATIONS OF UNCONTAMINATED GROUND WATER MAY BE DISCHARGED ON SITE, BUT SHALL NOT LEAVE THE SITE IN THE FORM OF SURFACE RUNOFF UNLESS AN APPROVED STATE DEWATERING PERMIT IS IN PLACE.
- EROSION CONTROL BLANKETING OR OTHER PROTECTIVE COVERING SHALL BE USED ON SLOPES STEEPER THAN 3:1.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL WASTES FROM THE CONSTRUCTION SITE FOR DISPOSAL IN ACCORDANCE WITH LOCAL AND STATE REGULATORY REQUIREMENTS. NO CONSTRUCTION DEBRIS, TREE SLASH, BUILDING MATERIAL WASTES OR UNUSED BUILDING MATERIALS SHALL BE BURIED, DUMPED, OR DISCHARGED AT THE SITE.
- WASTE MATERIALS SHALL NOT BE TEMPORARILY PLACED OR STORED IN THE STREET, ALLEY, OR OTHER PUBLIC WAY, UNLESS IN ACCORDANCE WITH AN APPROVED TRAFFIC CONTROL PLAN. CONTROL MEASURES MAY BE REQUIRED BY EL PASO COUNTY ENGINEERING IF DEEMED NECESSARY, BASED ON SPECIFIC CONDITIONS AND CIRCUMSTANCES.
- TRACKING OF SOILS AND CONSTRUCTION DEBRIS OFF-SITE SHALL BE MINIMIZED. MATERIALS TRACKED OFF-SITE SHALL BE CLEANED UP AND PROPERLY DISPOSED OF IMMEDIATELY.
- THE OWNER/DEVELOPER SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL CONSTRUCTION DEBRIS, DIRT, TRASH, ROCK, SEDIMENT, SOIL, AND SAND THAT MAY ACCUMULATE IN ROADS, STORM DRAINS AND OTHER DRAINAGE CONVEYANCE SYSTEMS AND STORMWATER APPURTENANCES AS A RESULT OF SITE DEVELOPMENT.
- THE QUANTITY OF MATERIALS STORED ON THE PROJECT SITE SHALL BE LIMITED, AS MUCH AS PRACTICAL, TO THAT QUANTITY REQUIRED TO PERFORM THE WORK IN AN ORDERLY SEQUENCE. ALL MATERIALS STORED ON-SITE SHALL BE STORED IN A NEAT, ORDERLY MANNER, IN THEIR ORIGINAL CONTAINERS, WITH ORIGINAL MANUFACTURER'S LABELS.
- NO CHEMICAL(S) HAVING THE POTENTIAL TO BE RELEASED IN STORMWATER ARE TO BE STORED OR USED ONSITE UNLESS PERMISSION FOR THE USE OF SUCH CHEMICAL(S) IS GRANTED IN WRITING BY THE ECM ADMINISTRATOR. IN GRANTING APPROVAL FOR THE USE OF SUCH CHEMICAL(S), SPECIAL CONDITIONS AND MONITORING MAY BE REQUIRED.
- BULK STORAGE OF ALLOWED PETROLEUM PRODUCTS OR OTHER ALLOWED LIQUID CHEMICALS IN EXCESS OF 55 GALLONS SHALL REQUIRE ADEQUATE SECONDARY CONTAINMENT PROTECTION TO CONTAIN ALL SPILLS ONSITE AND TO PREVENT ANY SPILLED MATERIALS FROM ENTERING STATE WATERS, ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR OTHER FACILITIES.
- NO PERSON SHALL CAUSE THE IMPEDIMENT OF STORMWATER FLOW IN THE CURB AND GUTTER OR DITCH EXCEPT WITH APPROVED SEDIMENT CONTROL MEASURES.
- OWNER/DEVELOPER AND THEIR AGENTS SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8, CRS), AND THE "CLEAN WATER ACT" (33 USC 1344), IN ADDITION TO THE REQUIREMENTS OF THE LAND DEVELOPMENT CODE, DCM VOLUME II AND THE ECM APPENDIX I. ALL APPROPRIATE PERMITS MUST BE OBTAINED BY THE CONTRACTOR PRIOR TO CONSTRUCTION (1041, NPDES, FLOODPLAIN, 404, FUGITIVE DUST, ETC.). IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND OTHER LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, STATE, LOCAL, OR COUNTY AGENCIES, THE MOST RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE ONLY AT APPROVED CONSTRUCTION ACCESS POINTS.
- PRIOR TO CONSTRUCTION THE PERMITTEE SHALL VERIFY THE LOCATION OF EXISTING UTILITIES.
- A WATER SOURCE SHALL BE AVAILABLE ON SITE DURING EARTHWORK OPERATIONS AND SHALL BE UTILIZED AS REQUIRED TO MINIMIZE DUST FROM EARTHWORK EQUIPMENT AND WIND.
- THE SOILS REPORT FOR THIS SITE HAS BEEN PREPARED BY GROUND ENGINEERING, DATED AUGUST 25, 2015, WITH ADDENDA #1, DATED MARCH 17, 2017, AND SHALL BE CONSIDERED A PART OF THESE PLANS.
- AT LEAST TEN (10) DAYS PRIOR TO THE ANTICIPATED START OF CONSTRUCTION, FOR PROJECTS THAT WILL DISTURB ONE (1) ACRE OR MORE, THE OWNER OR OPERATOR OF CONSTRUCTION ACTIVITY SHALL SUBMIT A PERMIT APPLICATION FOR STORMWATER DISCHARGE TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT, WATER QUALITY DIVISION. THE APPLICATION CONTAINS CERTIFICATION OF COMPLETION OF A STORMWATER MANAGEMENT PLAN (SWMP), OF WHICH THIS GRADING AND EROSION CONTROL PLAN MAY BE A PART. FOR INFORMATION OR APPLICATION MATERIALS CONTACT:

COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT
 WATER QUALITY CONTROL DIVISION
 WOOD - PERMITS
 4300 CHERRY CREEK DRIVE SOUTH
 DENVER, CO 80246-1530
 ATTN: PERMITS UNIT

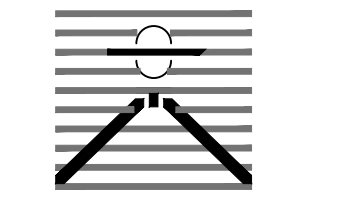
STANDARD NOTES FOR EL PASO COUNTY CONSTRUCTION PLANS

- ALL DRAINAGE AND ROADWAY CONSTRUCTION SHALL MEET THE STANDARDS AND SPECIFICATIONS OF THE CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2, AND THE EL PASO COUNTY ENGINEERING CRITERIA MANUAL.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION AND FIELD NOTIFICATION OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, BEFORE BEGINNING CONSTRUCTION. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CALL 811 TO CONTACT THE UTILITY NOTIFICATION CENTER OF COLORADO (UNCC).
- CONTRACTOR SHALL KEEP A COPY OF THESE APPROVED PLANS, THE GRADING AND EROSION CONTROL PLAN, THE STORMWATER MANAGEMENT PLAN (SWMP), THE SOILS AND GEOTECHNICAL REPORT, AND THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES, INCLUDING THE FOLLOWING:
 a. EL PASO COUNTY ENGINEERING CRITERIA MANUAL (ECM)
 b. CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2
 c. COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
 d. CDOT M & S STANDARDS
- NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS FROM REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING. ANY MODIFICATIONS NECESSARY TO MEET CRITERIA AFTER-THE-FACT WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- IT IS THE DESIGN ENGINEER'S RESPONSIBILITY TO ACCURATELY SHOW EXISTING CONDITIONS, BOTH ONSITE AND OFFSITE, ON THE CONSTRUCTION PLANS. ANY MODIFICATIONS NECESSARY DUE TO CONFLICTS, OMISSIONS, OR CHANGED CONDITIONS WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (PCD) - INSPECTIONS, PRIOR TO STARTING CONSTRUCTION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE REQUIREMENTS OF ALL JURISDICTIONAL AGENCIES AND TO OBTAIN ALL REQUIRED PERMITS, INCLUDING BUT NOT LIMITED TO EL PASO COUNTY EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQCP), REGIONAL BUILDING FLOODPLAIN DEVELOPMENT PERMIT, U.S. ARMY CORPS OF ENGINEERS-ISSUED 401 AND/OR 404 PERMITS, AND COUNTY AND STATE FUGITIVE DUST PERMITS.
- CONTRACTOR SHALL NOT DEVIATE FROM THE PLANS WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE DESIGN ENGINEER AND PCD. CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY UPON DISCOVERY OF ANY ERRORS OR INCONSISTENCIES.
- ALL STORM DRAIN PIPE SHALL BE CLASS III RCP UNLESS OTHERWISE NOTED AND APPROVED BY PCD. WATERTIGHT JOINTS SHALL BE PROVIDED ON THE SPECIFIED PIPES.
- CONTRACTOR SHALL COORDINATE GEOTECHNICAL TESTING PER ECM STANDARDS. PAVEMENT DESIGN SHALL BE APPROVED BY EL PASO COUNTY PCD PRIOR TO PLACEMENT OF CURB AND GUTTER AND PAVEMENT.
- ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE AT APPROVED CONSTRUCTION ACCESS POINTS.
- SIGHT VISIBILITY TRIANGLES AS IDENTIFIED IN THE PLANS SHALL BE PROVIDED AT ALL INTERSECTIONS. OBSTRUCTIONS GREATER THAN 18 INCHES ABOVE FLOWLINE ARE NOT ALLOWED WITHIN SIGHT TRIANGLES.
- SIGNING AND STRIPING SHALL COMPLY WITH EL PASO COUNTY DOT AND MUTCD CRITERIA. [IF APPLICABLE, ADDITIONAL SIGNING AND STRIPING NOTES WILL BE PROVIDED.]
- CONTRACTOR SHALL OBTAIN ANY PERMITS REQUIRED BY EL PASO COUNTY DOT, INCLUDING WORK WITHIN THE RIGHT-OF-WAY AND SPECIAL TRANSPORT PERMITS.
- THE LIMITS OF CONSTRUCTION SHALL REMAIN WITHIN THE PROPERTY LINE UNLESS OTHERWISE NOTED. THE OWNER/DEVELOPER SHALL OBTAIN WRITTEN PERMISSION AND EASEMENTS, WHERE REQUIRED, FROM ADJOINING PROPERTY OWNER(S) PRIOR TO ANY OFF-SITE DISTURBANCE, GRADING, OR CONSTRUCTION.

SIGNAGE AND STRIPING NOTES

- SIGNING AND STRIPING NOTES:
- ALL SIGNS AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
 - REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY A METHOD THAT DOES NOT MATERIALLY DAMAGE THE PAVEMENT. THE PAVEMENT MARKINGS SHALL BE REMOVED TO THE EXTENT THAT THEY WILL NOT BE VISIBLE UNDER DAY OR NIGHT CONDITIONS. AT NO TIME WILL IT BE ACCEPTABLE TO PAINT OVER EXISTING PAVEMENT MARKINGS.
 - ANY DEVIATION FROM THE STRIPING AND SIGNING PLAN SHALL BE APPROVED BY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT.
 - ALL SIGNS SHOWN ON THE SIGNING AND STRIPING PLAN SHALL BE NEW SIGNS. EXISTING SIGNS MAY REMAIN OR BE REUSED IF THEY MEET CURRENT EL PASO COUNTY AND MUTCD STANDARDS.
 - STREET NAME AND REGULATORY STOP SIGNS SHALL BE ON THE SAME POST AT INTERSECTIONS.
 - ALL REMOVED SIGNS SHALL BE DISPOSED OF IN A PROPER MANNER BY THE CONTRACTOR.
 - ALL STREET NAME SIGNS SHALL HAVE "D" SERIES LETTERS, WITH LOCAL ROADWAY SIGNS BEING 4" UPPER-LOWER CASE LETTERING ON 8" BLANK AND NON-LOCAL ROADWAY SIGNS BEING 6" LETTERING, UPPER-LOWER CASE ON 12" BLANK, WITH A WHITE BORDER THAT IS NOT RECESSED. MULTI-LANE ROADWAYS WITH SPEED LIMITS OF 35 MPH OR HIGHER SHALL HAVE 8" UPPER-LOWER CASE LETTERING ON 12" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. THE WIDTH OF THE NON-RECESSED WHITE BORDERS SHALL MATCH PAGE 255 OF THE 2012 MUTCD "STANDARD HIGHWAY SIGNS", SIGNAL POLE MOUNTED AND OVERHEAD STREET NAME SIGNS SHALL BE SIZED TO "MUTCD" SIZE STANDARDS.
 - ALL TRAFFIC SIGNS SHALL HAVE A MINIMUM HIGH INTENSITY PRISMATIC GRADE SHEETING.
 - ALL LOCAL RESIDENTIAL STREET SIGNS SHALL BE MOUNTED ON A 1.75" X 1.75" SQUARE TUBE SIGN POST AND STUB POST BASE. FOR OTHER APPLICATIONS, REFER TO THE CDOT STANDARD S-614-8 REGARDING USE OF THE P2 TUBULAR STEEL POST SLIPBASE DESIGN.
 - ALL SIGNS SHALL BE SINGLE SHEET ALUMINUM WITH 0.100" MINIMUM THICKNESS.
 - ALL LIMIT LINES/STOP LINES, CROSSWALK LINES, PAVEMENT LEGENDS, AND ARROWS SHALL BE A MINIMUM 125 MIL THICKNESS PREFORMED THERMOPLASTIC PAVEMENT MARKINGS WITH TAPERED LEADING EDGES PER CDOT STANDARD S-627-1. STOP BARS SHALL BE 24" IN WIDTH. CROSSWALKS LINES SHALL BE 24" WIDE AND 9' LONG.
 - WORD AND SYMBOL MARKINGS SHALL BE THE NARROW TYPE.
 - ALL LONGITUDINAL LINES SHALL BE A MINIMUM 15MIL THICKNESS EPOXY PAINT. ALL NON-LOCAL RESIDENTIAL ROADWAYS SHALL INCLUDE BOTH RIGHT AND LEFT EDGE LINE STRIPING AND ANY ADDITIONAL STRIPING AS REQUIRED BY CDOT S-627-1.
 - THE CONTRACTOR SHALL NOTIFY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (719) 520-6819 PRIOR TO AND UPON COMPLETION OF SIGNING AND STRIPING.
 - THE CONTRACTOR SHALL OBTAIN A WORK IN THE RIGHT OF WAY PERMIT FROM THE EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS (DPW) PRIOR TO ANY SIGNAGE OR STRIPING WORK WITHIN AN EXISTING EL PASO COUNTY ROADWAY.

PREPARED BY:



DREXEL, BARRELL & CO.
 Engineers • Surveyors
 101 SAWATCH ST., STE #100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
 (719)260-0887
 COLORADO SPRINGS • LAFAYETTE

CLIENT:

BH RE INVESTMENTS, LLC
 450 N MCCLINTOCK DRIVE
 CHANDLER, AZ 85226
 (480) 313-2724

CONSTRUCTION PLANS FOR:

**OWL
 MARKETPLACE**
 EL PASO COUNTY, COLORADO

| ISSUE | DATE |
|---------------|------------|
| INITIAL ISSUE | 08/04/2023 |
| RESUBMITTAL | 04/12/2024 |
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| | |
| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-CVR |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: N/A
 VERTICAL: N/A

**GENERAL NOTES
 & ROAD
 CROSS SECTIONS**

PROJECT NO. 21611-01CSCV
 DRAWING NO.

C2

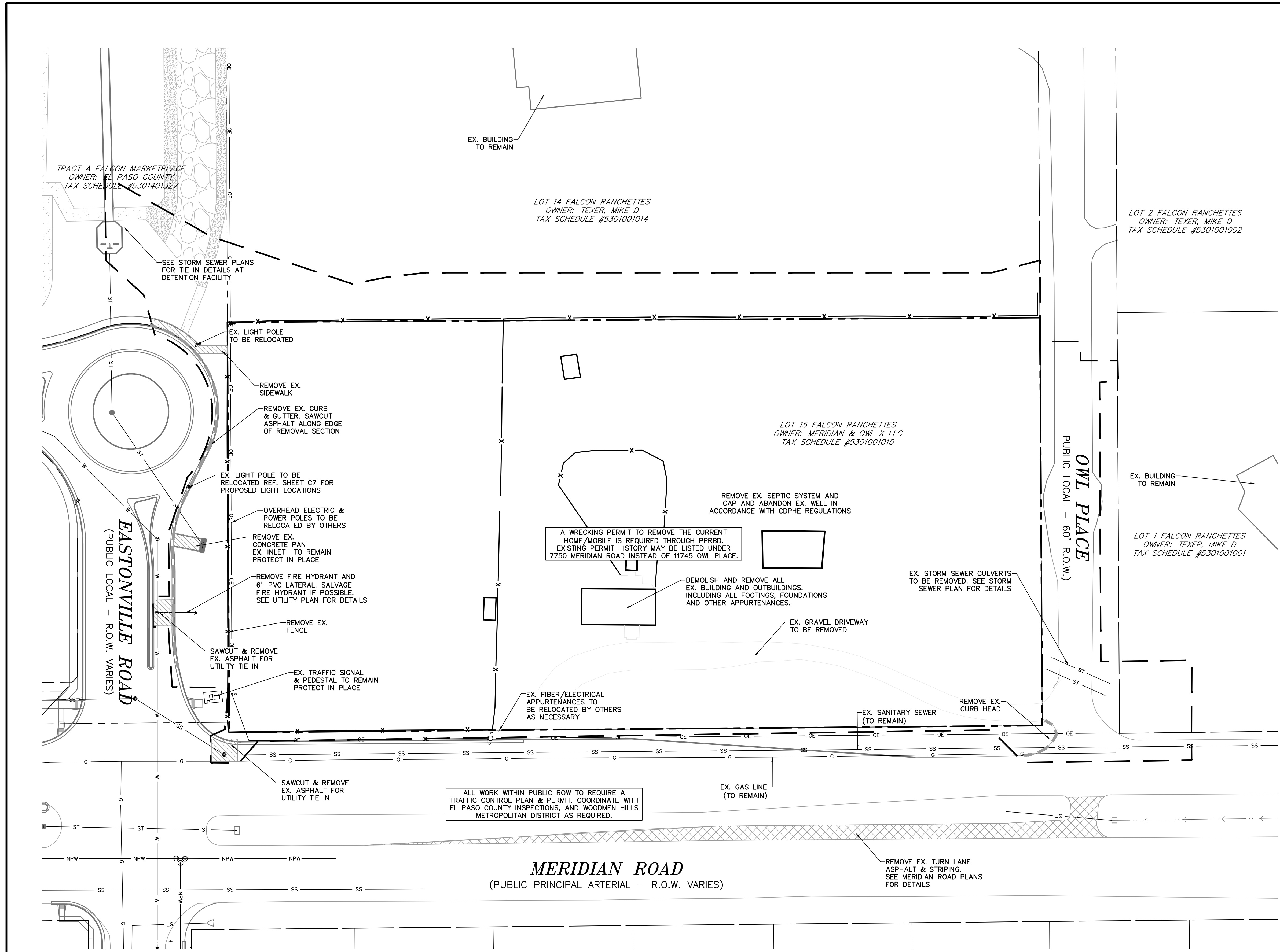
SHEET: 2 OF 18



Know what's below.
 Call before you dig.

CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

PCD FILE: VR2321



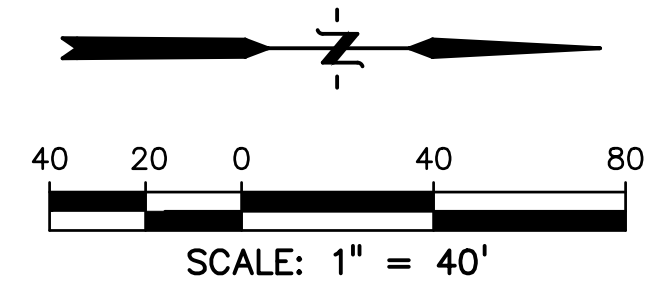
PREPARED BY:



CLIENT:

BH RE INVESTMENTS, LLC
450 N MCCLINTOCK DRIVE
CHANDLER, AZ 85226
(480) 313-2724

KEY MAP
NTS



LEGEND

| | |
|----------------------------|--------|
| PROPERTY LINE..... | --- |
| EX. OVERHEAD ELECTRIC..... | OE --- |
| EX. SANITARY SEWER..... | SS --- |
| EX. GAS..... | G --- |
| EX. WATER..... | W --- |
| EX. STORM SEWER..... | ST --- |
| LIMITS OF DISTURBANCE..... | --- |

ABBREVIATION

| | |
|----------------|---------------------------|
| EX..... | EXISTING |
| MH..... | MANHOLE |
| PP..... | PROPOSED |
| PT..... | POINT |
| SS OR SAN..... | SANITARY SEWER |
| ST..... | STORM SEWER |
| WT..... | WATER |
| XING..... | CROSSING |
| EOP..... | END OF PIPE |
| CDS..... | CUL-DE-SAC |
| BT..... | BEGIN TRANSITION |
| ET..... | END TRANSITION |
| EOR..... | EDGE OF ROAD |
| SW..... | SIDEWALK |
| C..... | CENTERLINE |
| C&G..... | CURB AND GUTTER |
| PED..... | PEDESTRIAN |
| PC..... | POINT OF CURVATURE |
| PT..... | POINT OF TANGENCY |
| ROW..... | RIGHT OF WAY |
| XWALK..... | CROSSWALK |
| XPAN..... | CROSS-PAN |
| PRC..... | POINT OF REVERSE CURVE |
| MPD..... | MERIDIAN PARK DRIVE |
| ETV..... | NORTH FL EASTONVILLE ROAD |
| OWL..... | OWL PLACE |
| FL..... | FLOWLINE |

CONSTRUCTION PLANS FOR:
OWL MARKETPLACE
EL PASO COUNTY, COLORADO

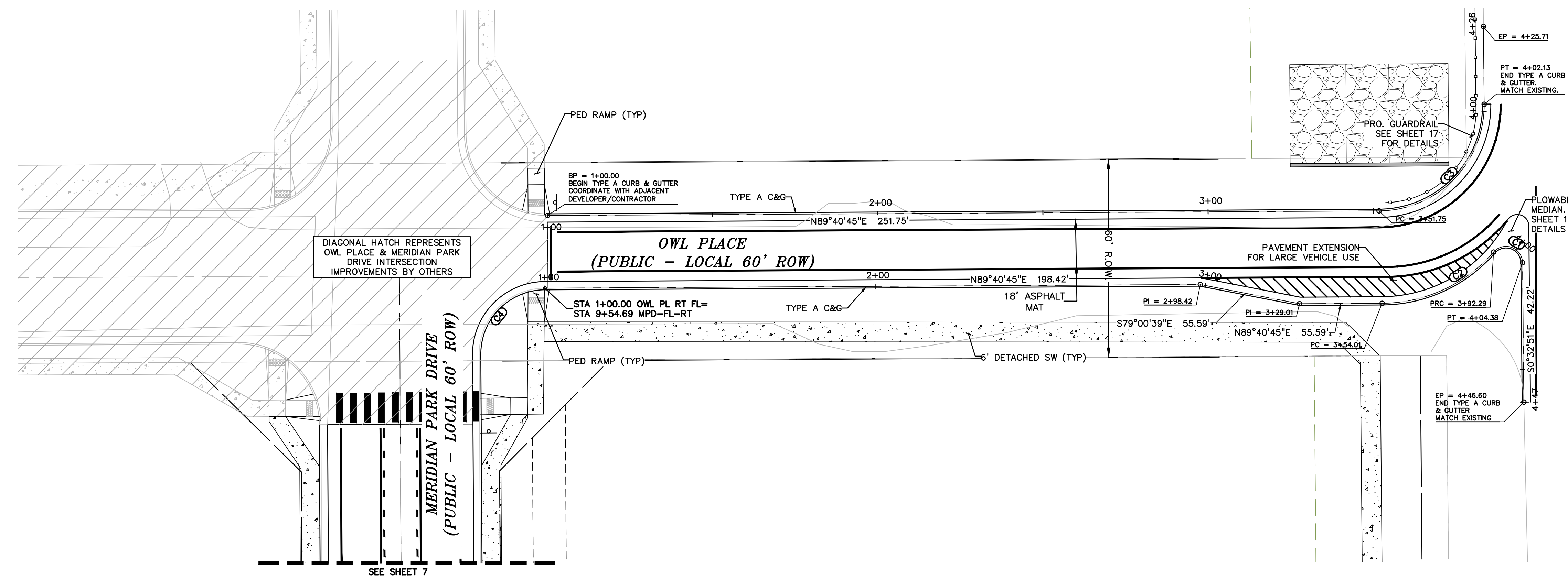
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| | |
| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-DMO |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.
DRAWING SCALE:
HORIZONTAL: 1" = 40'
VERTICAL: N/A

EXISTING CONDITIONS & SITE DEMO

PROJECT NO. 21611-01CSCV
DRAWING NO.

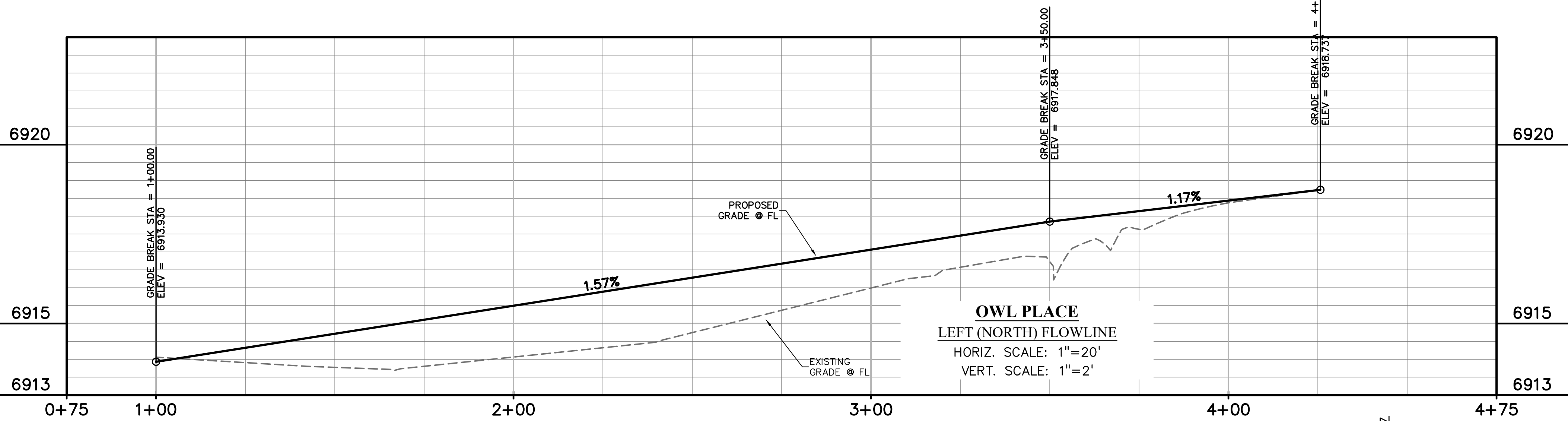
811 Know what's below.
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PCD FILE: VR2321



Previous comment was provided in error. As this is not a median, a flowable nose is not required. Please revise.

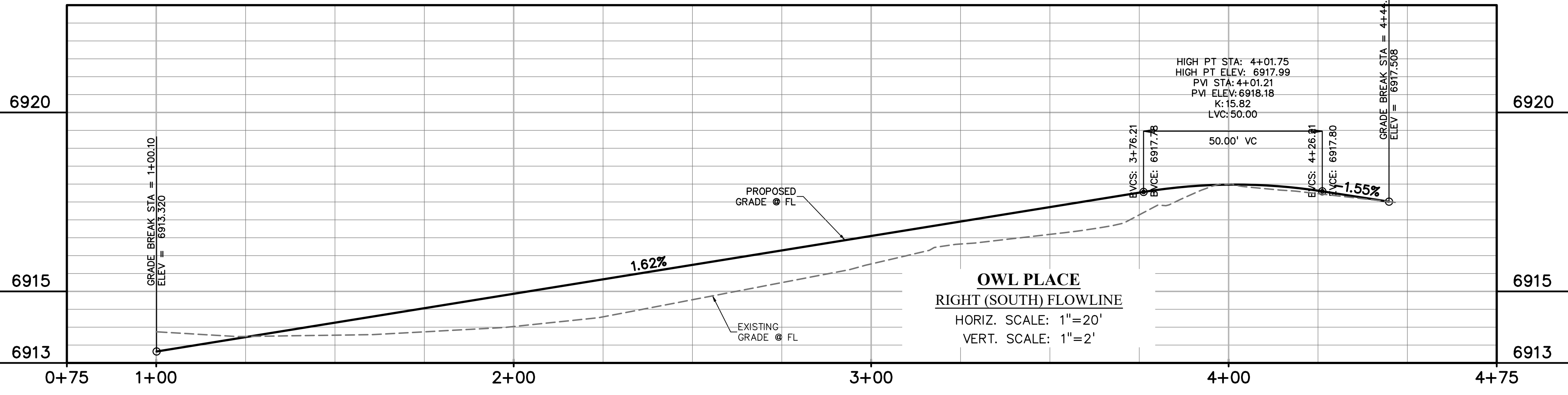
LEGEND

- PROPERTY LINE
- UTILITY EASEMENT
- PP ROW
- PP LOT LINE
- PP C&G
- PP 5' ATTACHED SW
- PP STORM INLET
- PP MH
- PP FIRE HYDRANT
- LOT NUMBER
- PP TRAIL
- PP XWALK W/ STOP BAR
- PP GRADE @
- EX WATER FIRE HYDRANT
- EX MH
- EX GROUND @



FLOWLINE CURVE TABLE

| CURVE # | LENGTH | RADIUS | DELTA | CHORD BEARING | CHORD DISTANCE |
|---------|--------|--------|------------|---------------|----------------|
| C1 | 12.10' | 4.99' | 138°50'30" | N69°48'11"W | 9.35' |
| C2 | 38.28' | 45.03' | 48°42'00" | N65°18'34"E | 37.14' |
| C3 | 46.40' | 32.25' | 82°25'34" | N44°34'51"E | 42.50' |
| C4 | 31.42' | 20.00' | 89°59'57" | S44°40'47"W | 28.28' |
| C6 | 23.56' | 15.00' | 90°00'00" | S45°19'12"E | 21.21' |



PREPARED BY:

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 450 N MCCLINTOCK DRIVE
 CHANDLER, AZ 85226
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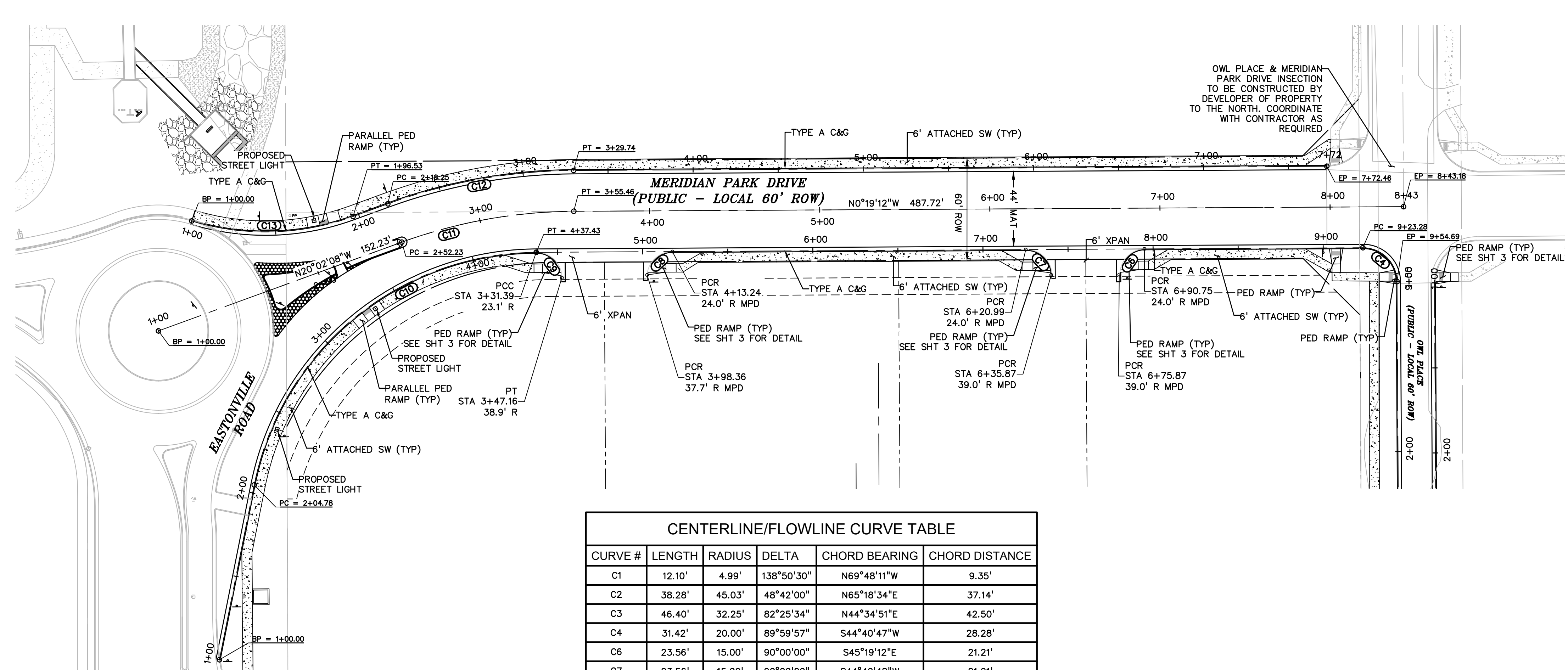
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OWL MARKETPLACE
 EL PASO COUNTY, COLORADO

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| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-RD |

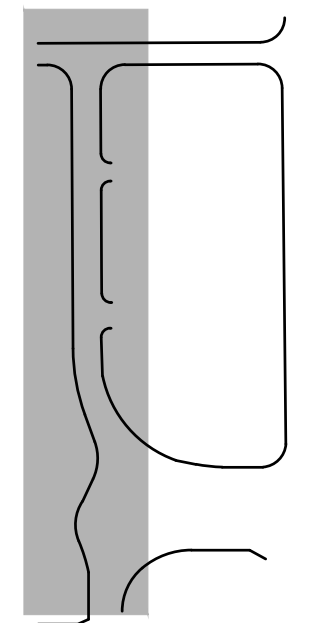
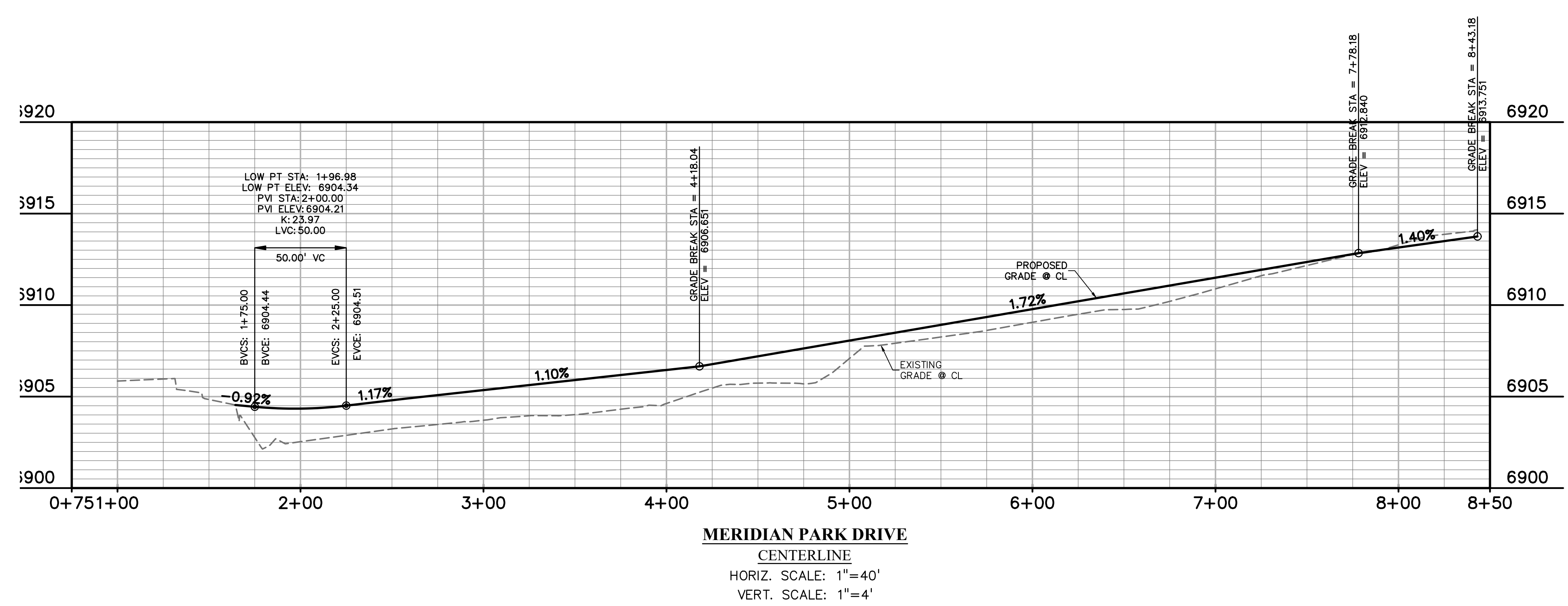
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.
 DRAWING SCALE:
 HORIZONTAL: 1"=20'
 VERTICAL: N/A

OWL PLACE PLAN & PROFILE
 PROJECT NO. 21611-01CSCV
 DRAWING NO.

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 PCD FILE: VR2321

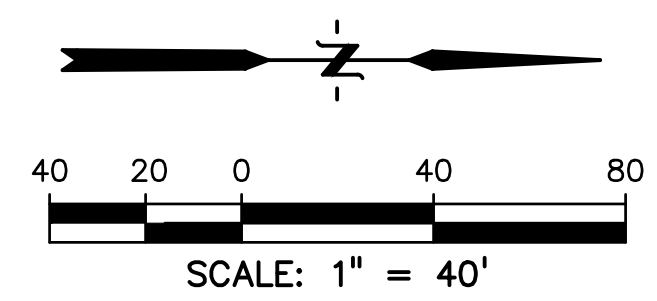


| CENTERLINE/FLOWLINE CURVE TABLE | | | | | |
|---------------------------------|---------|---------|------------|---------------|----------------|
| CURVE # | LENGTH | RADIUS | DELTA | CHORD BEARING | CHORD DISTANCE |
| C1 | 12.10' | 4.99' | 138°50'30" | N69°48'11"W | 9.35' |
| C2 | 38.28' | 45.03' | 48°42'00" | N65°18'34"E | 37.14' |
| C3 | 46.40' | 32.25' | 82°25'34" | N44°34'51"E | 42.50' |
| C4 | 31.42' | 20.00' | 89°59'57" | S44°40'47"W | 28.28' |
| C6 | 23.56' | 15.00' | 90°00'00" | S45°19'12"E | 21.21' |
| C7 | 23.56' | 15.00' | 90°00'00" | S44°40'48"W | 21.21' |
| C8 | 23.56' | 15.00' | 90°00'00" | S45°19'12"E | 21.21' |
| C9 | 23.56' | 15.00' | 90°00'00" | S44°40'48"W | 21.21' |
| C10 | 232.65' | 170.00' | 78°24'42" | S39°31'33"E | 214.92' |
| C11 | 103.21' | 307.05' | 19°15'31" | S10°10'40"E | 102.72' |
| C12 | 111.49' | 324.10' | 19°42'33" | S10°10'40"E | 110.94' |
| C13 | 96.53' | 150.00' | 36°52'12" | N01°36'03"W | 94.87' |



LEGEND

- PROPERTY LINE
- UTILITY EASEMENT
- PP ROW
- PP LOT LINE
- PP C&G
- PP 5' ATTACHED SW
- PP STORM INLET
- PP MH
- PP FIRE HYDRANT
- LOT NUMBER
- PP TRAIL
- PP XWALK W/ STOP BAR
- PP GRADE @ E
- EX WATER FIRE HYDRANT
- EX MH
- EX GROUND @ E



ABBREVIATION

- EX.....EXISTING
- MH.....MANHOLE
- PP.....PROPOSED
- PT.....POINT
- SS OR SAN.....SANITARY SEWER
- ST.....STORM SEWER
- WT.....WATER
- XING.....CROSSING
- EOP.....END OF PIPE
- CDS.....CUL-DE-SAC
- BT.....BEGIN TRANSITION
- ET.....END TRANSITION
- EOR.....EDGE OF ROAD
- SW.....SIDEWALK
- CL.....CENTERLINE
- C&G.....CURB AND GUTTER
- PED.....PEDESTRIAN
- PC.....POINT OF CURVATURE
- PT.....POINT OF TANGENCY
- ROW.....RIGHT OF WAY
- XWALK.....CROSSWALK
- XPAN.....CROSS-PAN
- PRC.....POINT OF REVERSE CURVE
- MPD.....MERIDIAN PARK DRIVE
- ETV.....NORTH FL EASTONVILLE ROAD
- OWL.....OWL PLACE
- FL.....FLOWLINE

PREPARED BY:

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CLIENT:
BH RE INVESTMENTS, LLC
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 CHANDLER, AZ 85226
 (480) 313-2724

CONSTRUCTION PLANS FOR:
OWL MARKETPLACE
 EL PASO COUNTY, COLORADO

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| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-RD |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.
 DRAWING SCALE:
 HORIZONTAL: 1"=40'
 VERTICAL: N/A

MERIDIAN PARK DRIVE PLAN & PROFILE
 PROJECT NO. 21611-01CSCV
 DRAWING NO.

811 Know what's below. Call before you dig.
 CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.
 PCD FILE: VR2321

PREPARED BY:



CLIENT:

BH RE INVESTMENTS, LLC
450 N MCCLINTOCK DRIVE
CHANDLER, AZ 85226
(480) 313-2724

CONSTRUCTION PLANS FOR:
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DESIGNED BY: KGV
DRAWN BY: KGV
CHECKED BY: TDM
FILE NAME: 21611-RD

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

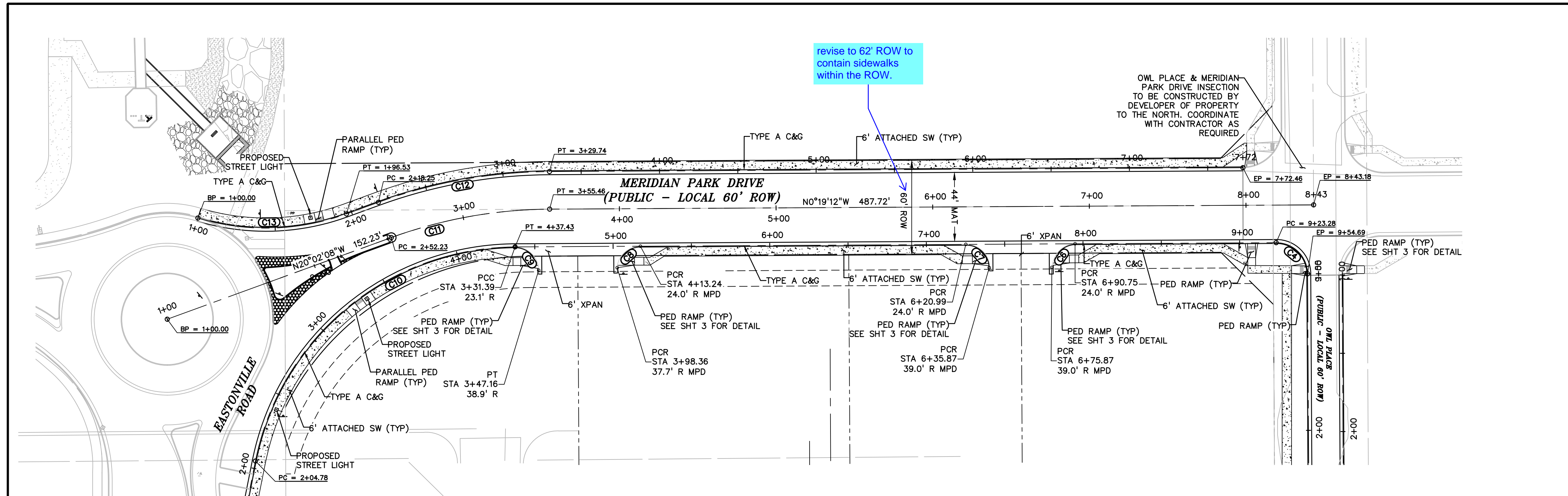
DRAWING SCALE:
HORIZONTAL: 1"=40'
VERTICAL: N/A

MERIDIAN PARK DRIVE FLOWLINE PLAN & PROFILE

PROJECT NO. 21611-01CSCV
DRAWING NO.

C6

SHEET: 6 OF 18



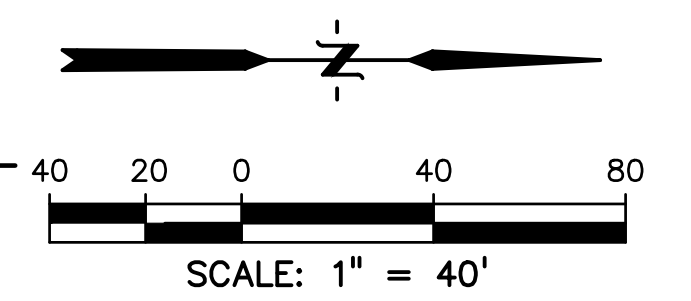
revise to 62' ROW to contain sidewalks within the ROW.

OWL PLACE & MERIDIAN PARK DRIVE INSECTION TO BE CONSTRUCTED BY DEVELOPER OF PROPERTY TO THE NORTH. COORDINATE WITH CONTRACTOR AS REQUIRED.

KEY MAP
NTS

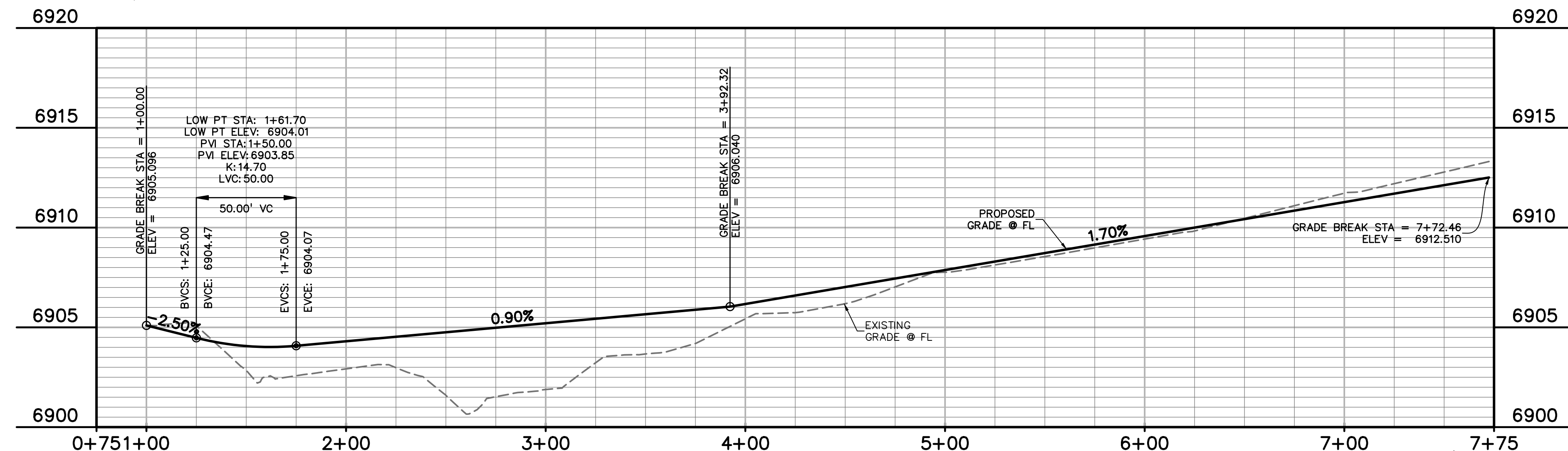
LEGEND

- PROPERTY LINE
- UTILITY EASEMENT
- PP ROW
- PP LOT LINE
- PP C&G
- PP 5' ATTACHED SW
- PP STORM INLET
- PP MH
- PP FIRE HYDRANT
- LOT NUMBER
- PP TRAIL
- PP XWALK W/ STOP BAR
- PP GRADE @ E
- EX WATER FIRE HYDRANT
- EX MH
- EX GROUND @ E

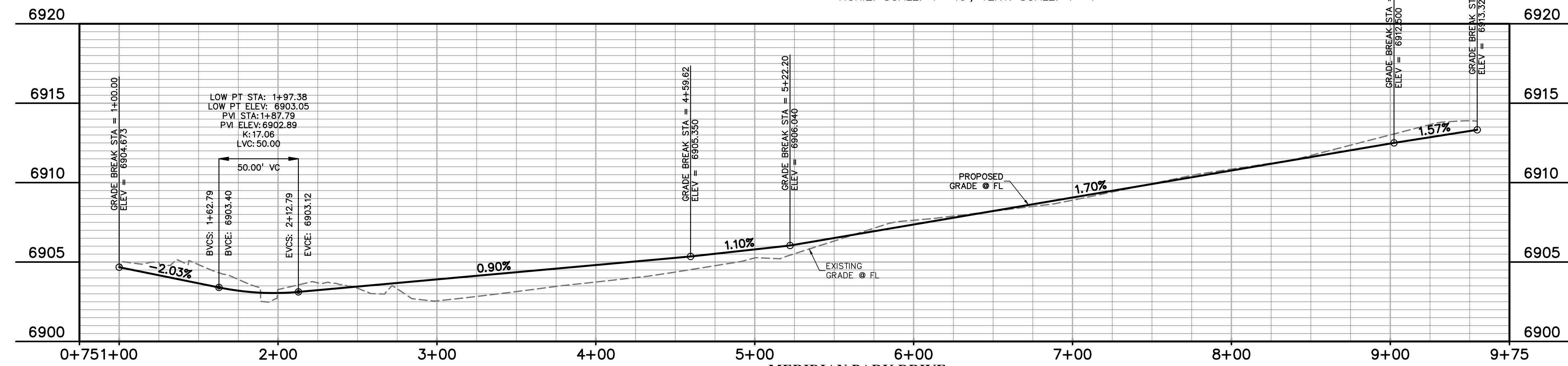


ABBREVIATION

- EX.....EXISTING
- MH.....MANHOLE
- PP.....PROPOSED
- PT.....POINT
- SS OR SAN.....SANITARY SEWER
- ST.....STORM SEWER
- WT.....WATER
- XING.....CROSSING
- EOP.....END OF PIPE
- CDS.....CUL-DE-SAC
- BT.....BEGIN TRANSITION
- ET.....END TRANSITION
- EOR.....EDGE OF ROAD
- SW.....SIDEWALK
- C&G.....CENTERLINE
- C&G.....CURB AND GUTTER
- PED.....PEDESTRIAN
- PC.....POINT OF CURVATURE
- PT.....POINT OF TANGENCY
- ROW.....RIGHT OF WAY
- XWALK.....CROSSWALK
- XPAN.....CROSS-PAN
- PRC.....POINT OF REVERSE CURVE
- MPD.....MERIDIAN PARK DRIVE
- ETV.....NORTH FL EASTONVILLE ROAD
- OWL.....OWL PLACE
- FL.....FLOWLINE

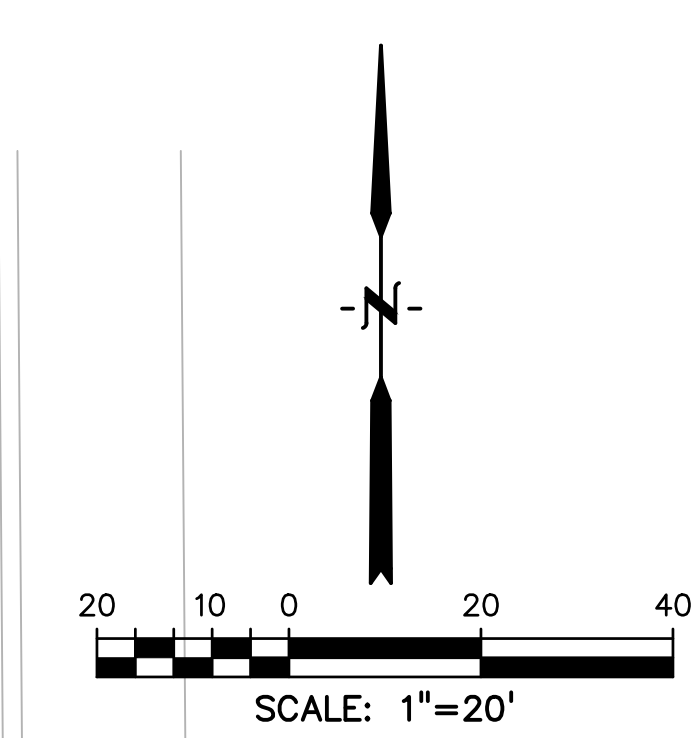
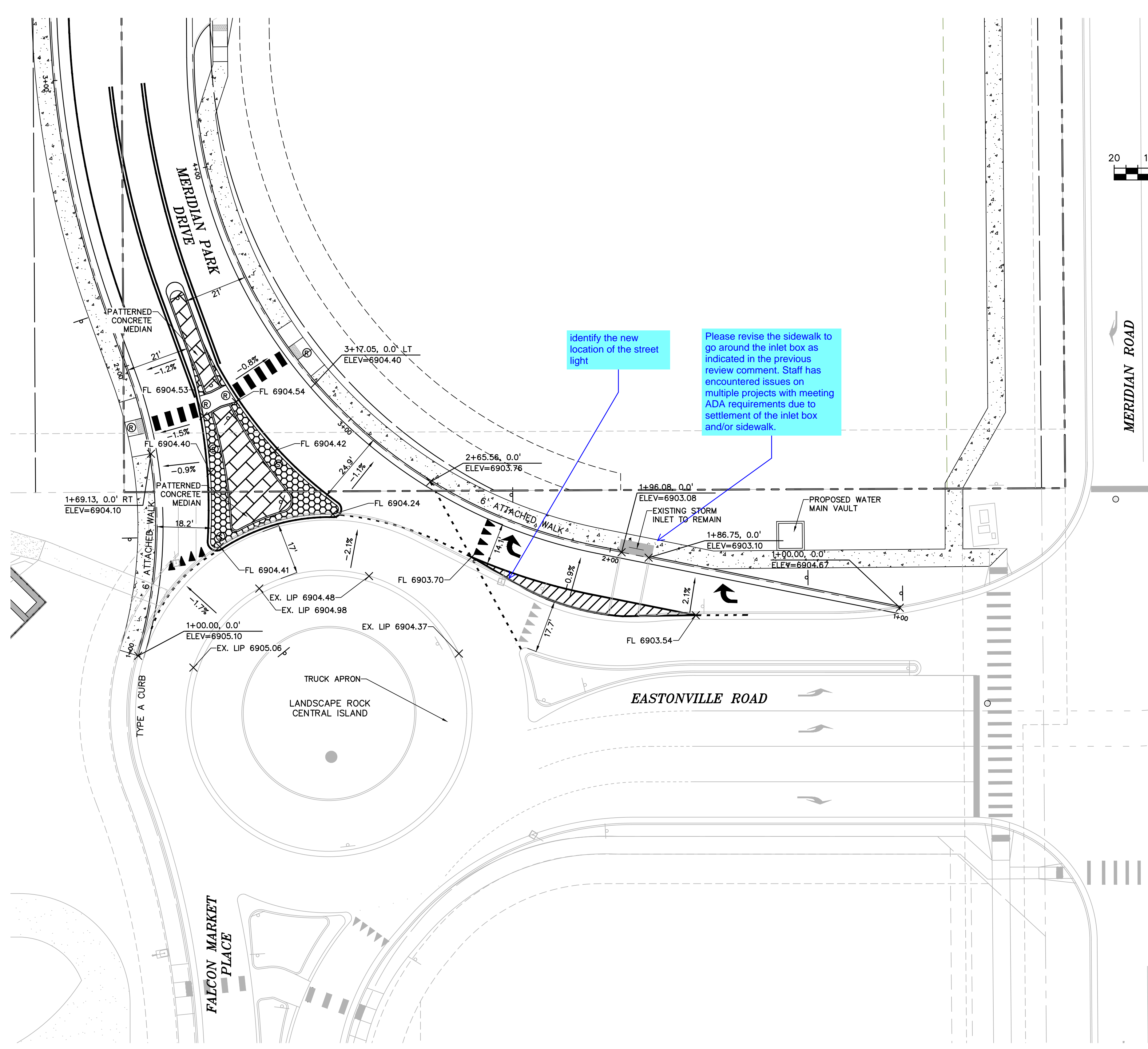


MERIDIAN PARK DRIVE
LEFT (WEST) CURB FLOWLINE
HORIZ. SCALE: 1"=40', VERT. SCALE: 1"=4'

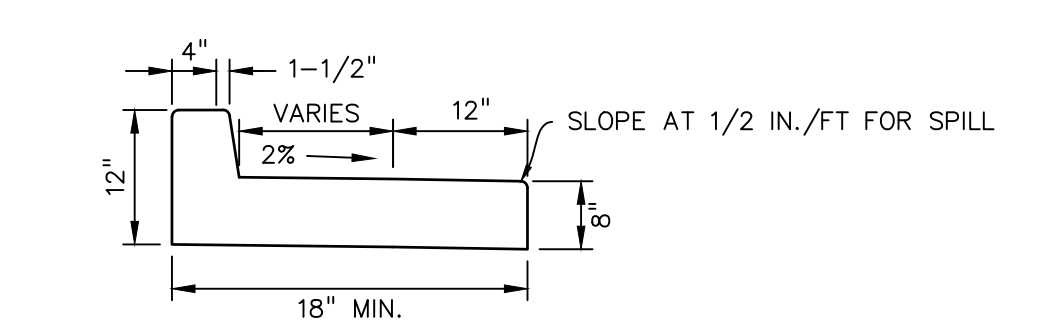


MERIDIAN PARK DRIVE
RIGHT (EAST) CURB FLOWLINE
HORIZ. SCALE: 1"=40', VERT. SCALE: 1"=4'

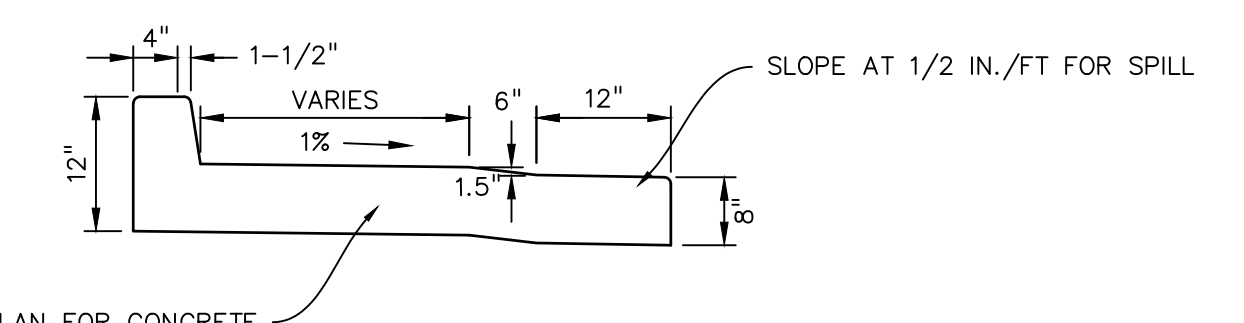
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PCD FILE: VR2321



- LEGEND**
- Ⓡ PEDESTRIAN RAMP
 - Ⓢ SPLITTER ISLAND CURB & GUTTER (CATCH)
 - Ⓣ SPLITTER ISLAND CURB & GUTTER (SPILL)
 - ▨ PATTERNED CONCRETE MEDIAN. FOR PATTERN SEE COUNTY DETAIL SD_2-21.
 - ▩ PATTERNED CONCRETE PLATFORM. SEE DETAIL THIS SHEET. FOR PATTERN SEE COUNTY DETAIL SD_2-21. NO PIGMENT.



SPLITTER ISLAND CURB/GUTTER (SPILL)
NTS

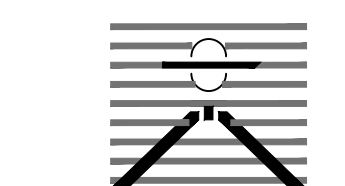


SEE PLAN FOR CONCRETE PATTERN LIMITS
RAISED CONCRETE PLATFORM
NTS

identify the new location of the street light

Please revise the sidewalk to go around the inlet box as indicated in the previous review comment. Staff has encountered issues on multiple projects with meeting ADA requirements due to settlement of the inlet box and/or sidewalk.

PREPARED BY:



DREXEL, BARRELL & CO.
Engineers • Surveyors
101 SAWATCH ST., STE #100
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CLIENT:

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CONSTRUCTION PLANS FOR:

OWL MARKETPLACE

EL PASO COUNTY, COLORADO

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| DESIGNED BY: | KGV |
| DRAWN BY: | CGH |
| CHECKED BY: | KGV |
| FILE NAME: | 21611-RAB |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: 1"=20'
VERTICAL: 1"=5'

ROUNDBOUT PLAN

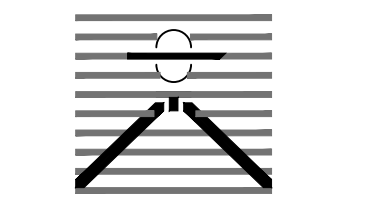
PROJECT NO. 21611-01CSCV
DRAWING NO.

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PCD FILE: VR2321

C7

SHEET: 7 OF 18

PREPARED BY:



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Engineers • Surveyors
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DESIGNED BY: KGV
 DRAWN BY: CGH
 CHECKED BY: TDM
 FILE NAME: 21611-MER

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

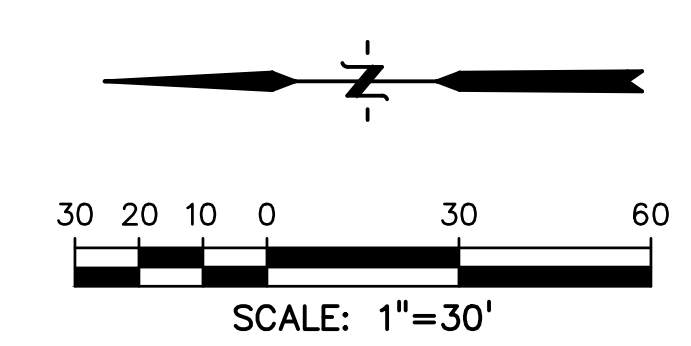
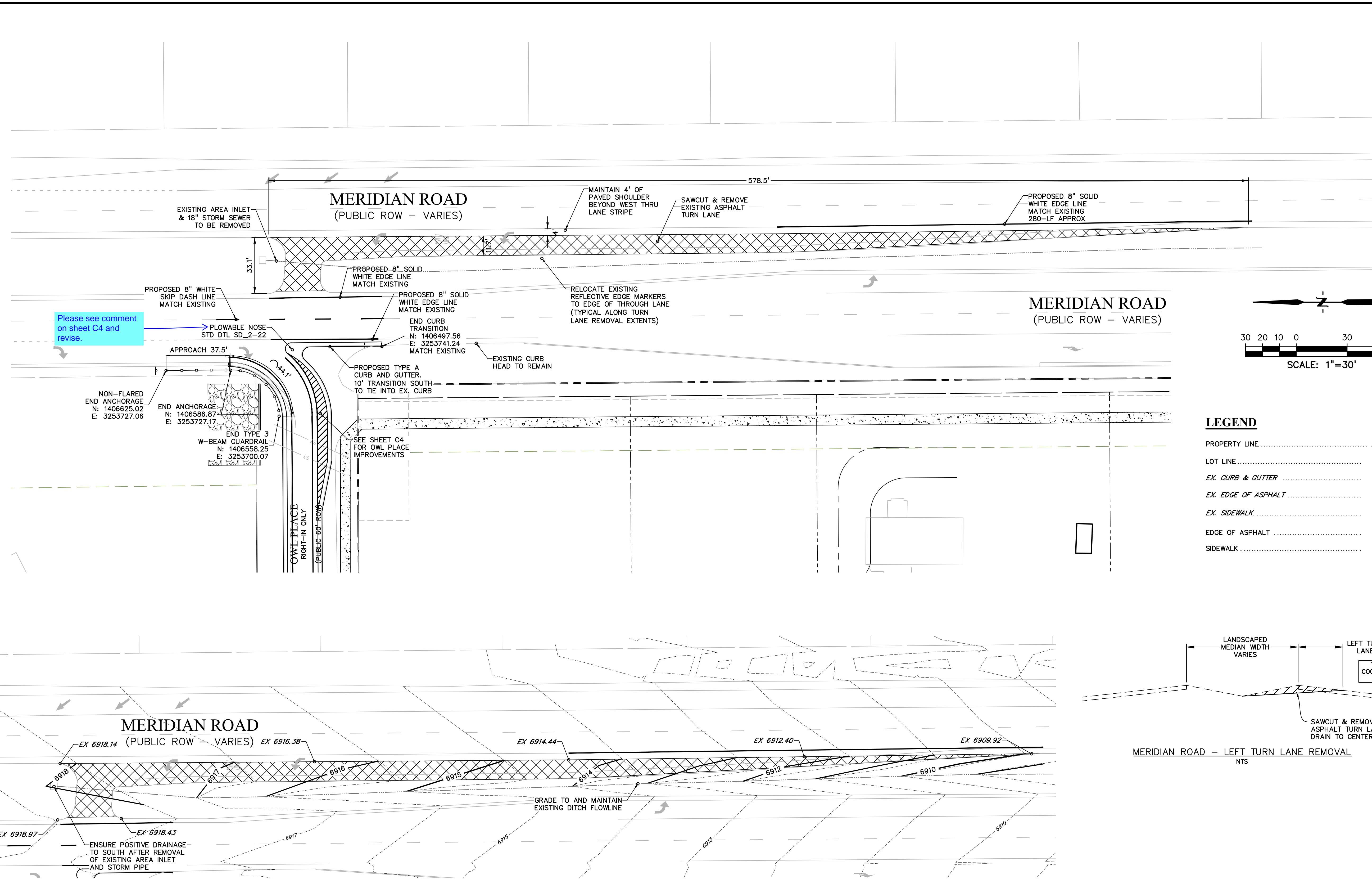
DRAWING SCALE:
HORIZONTAL: 1" = 30"
VERTICAL: N/A

**MERIDIAN RD &
OWL PLACE
IMPROVEMENTS**

PROJECT NO. 21611-01CSCV
DRAWING NO.

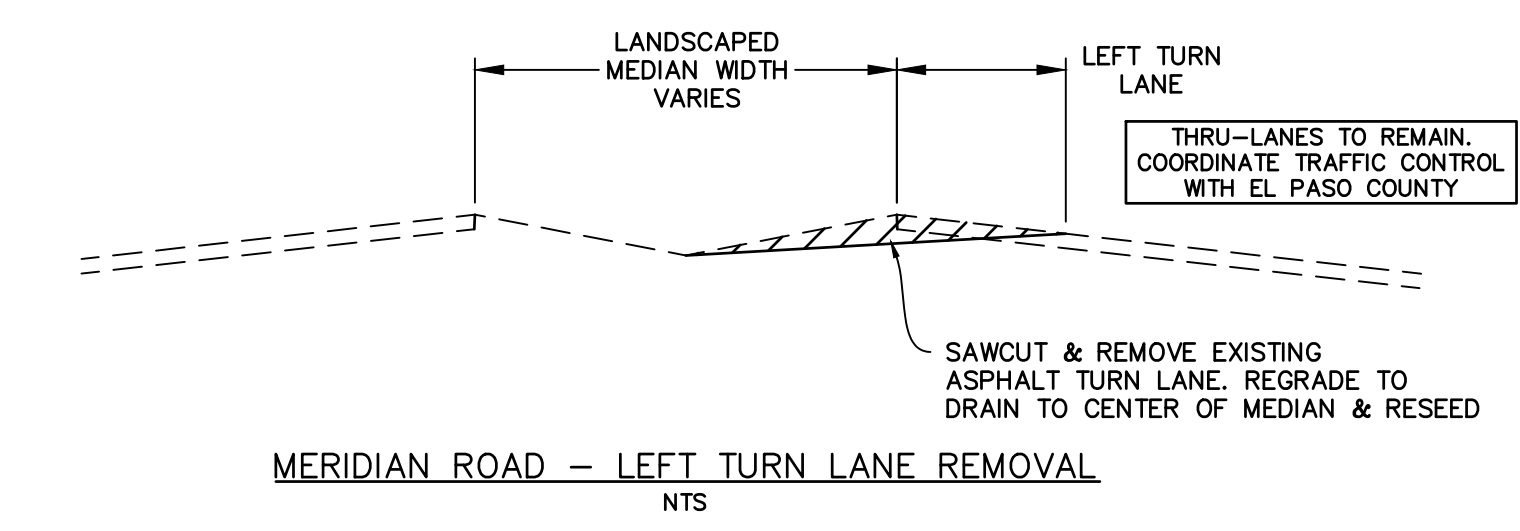
C8

SHEET: 8 OF 18

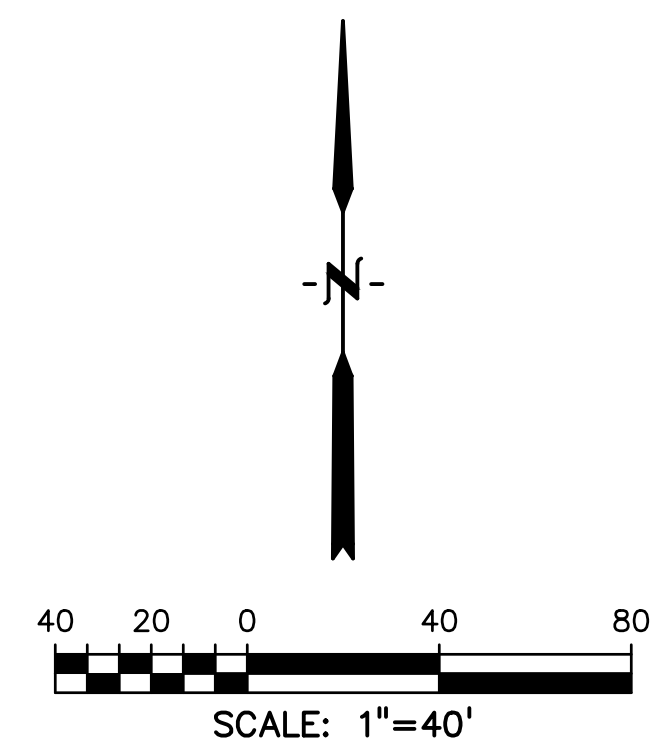
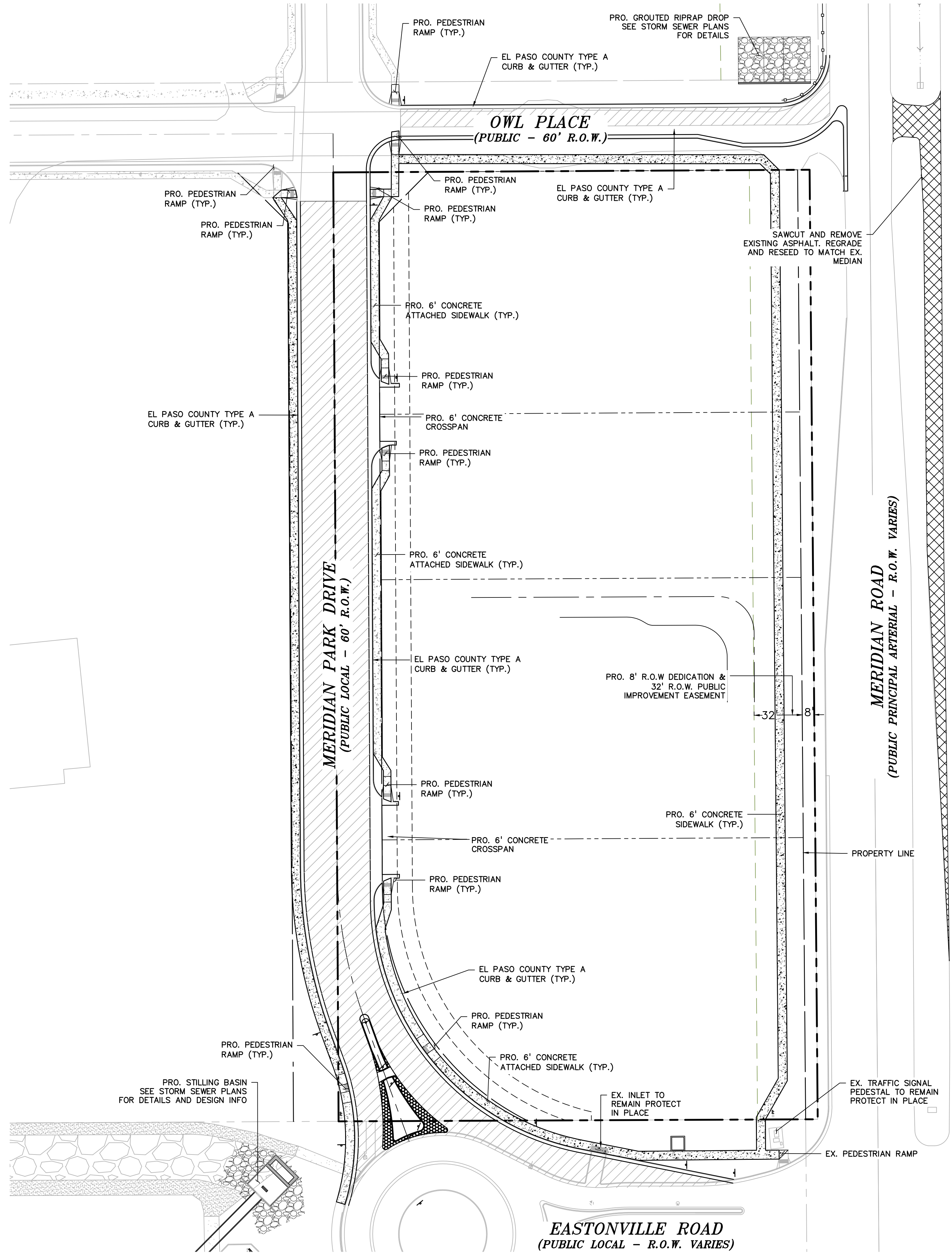


LEGEND

| | |
|---------------------|-------|
| PROPERTY LINE | ----- |
| LOT LINE | |
| EX. CURB & GUTTER | ===== |
| EX. EDGE OF ASPHALT | ----- |
| EX. SIDEWALK | ----- |
| EDGE OF ASPHALT | ----- |
| SIDEWALK | ----- |



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PCD FILE: VR2321



LEGEND

| | |
|---------------------------|-------|
| PROPERTY LINE | --- |
| LOT LINE | ---- |
| EX. CURB & GUTTER | ===== |
| EX. EDGE OF ASPHALT | ===== |
| EX. SIDEWALK | ===== |
| EDGE OF ASPHALT | ===== |
| SIDEWALK | ===== |
| PAVEMENT | ===== |

NORMAL SECTION 6" HMA ON 8" ABC OR 7.0" PORTLAND CEMENT CONCRETE OR AS DIRECTED BY GEOTECHNICAL REPORT APPROVED BY EL PASO COUNTY. VALUES LISTED ARE FOR BIDDING PURPOSES ONLY.

PREPARED BY:

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 Engineers • Surveyors
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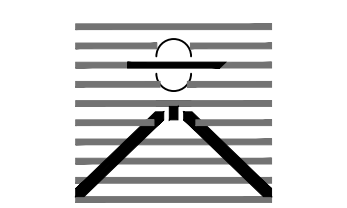
| ISSUE | DATE |
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| INITIAL ISSUE | 08/04/2023 |
| RESUBMITTAL | 04/12/2024 |
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| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.
 DRAWING SCALE:
 HORIZONTAL: 1"=40'
 VERTICAL: N/A

SITE PAVING & CONCRETE PLAN
 PROJECT NO. 21611-01CSCV
 DRAWING NO.

811 Know what's below.
 Call before you dig.
 CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.
 PCD FILE: VR2321

PREPARED BY:



DREXEL, BARRELL & CO.
 Engineers • Surveyors
 101 SAWATCH ST., STE #100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
 (719)260-0887
 COLORADO SPRINGS • LAFAYETTE

CLIENT:

BH RE INVESTMENTS, LLC
 450 N MCCLINTOCK DRIVE
 CHANDLER, AZ 85226
 (480) 313-2724

CONSTRUCTION PLANS FOR:

OWL MARKETPLACE

EL PASO COUNTY, COLORADO

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| FILE NAME: | |

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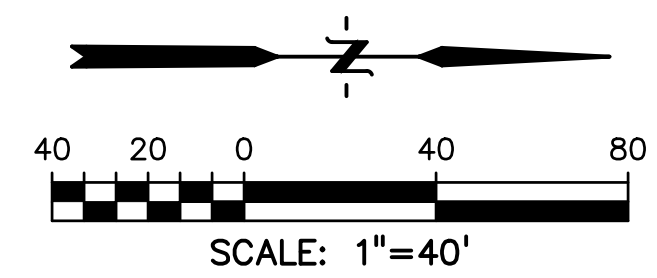
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 HORIZONTAL: 1"=40'
 VERTICAL: N/A

SIGNAGE & STRIPING PLAN

PROJECT NO. 21611-01CSCV
 DRAWING NO.

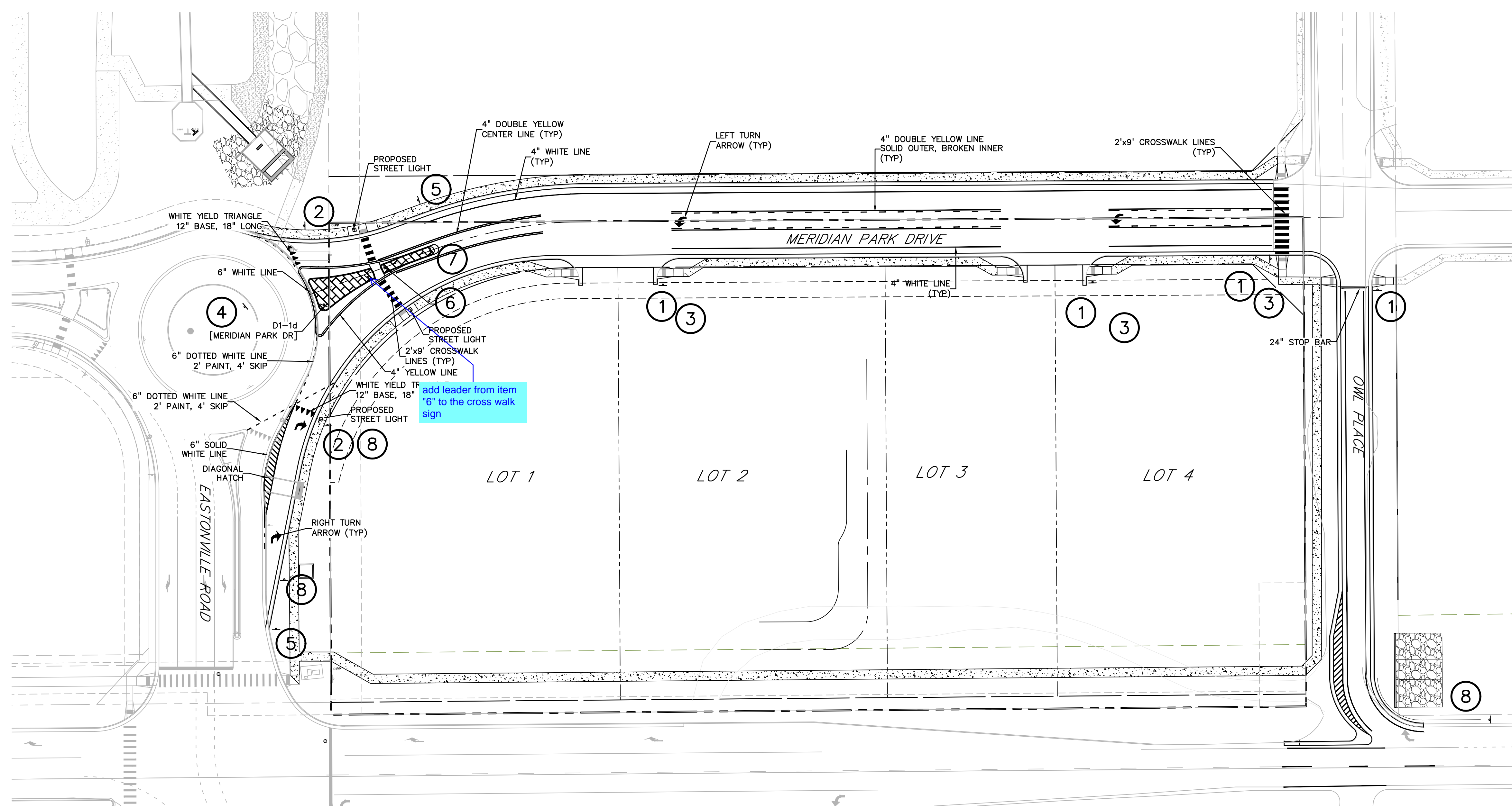
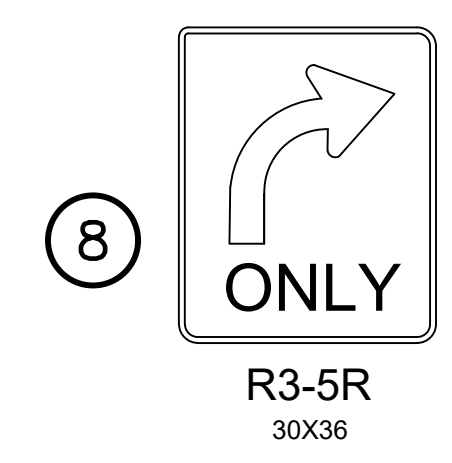
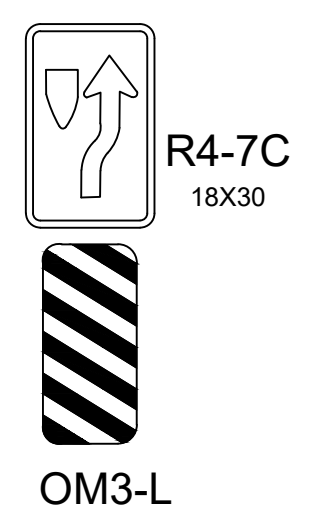
C10

SHEET: 10 OF 18



SIGN LEGEND

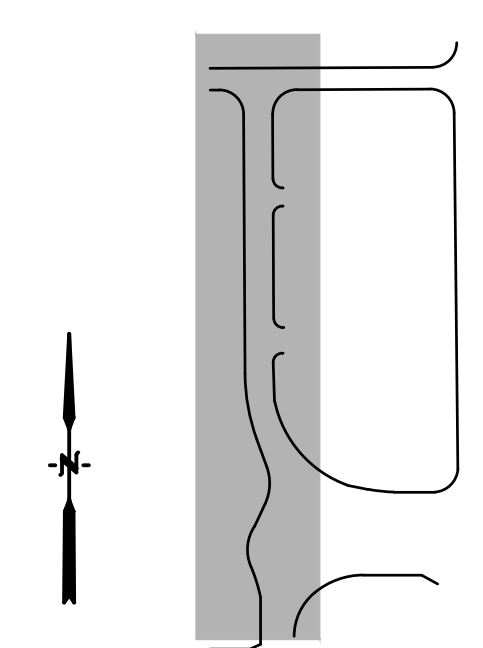
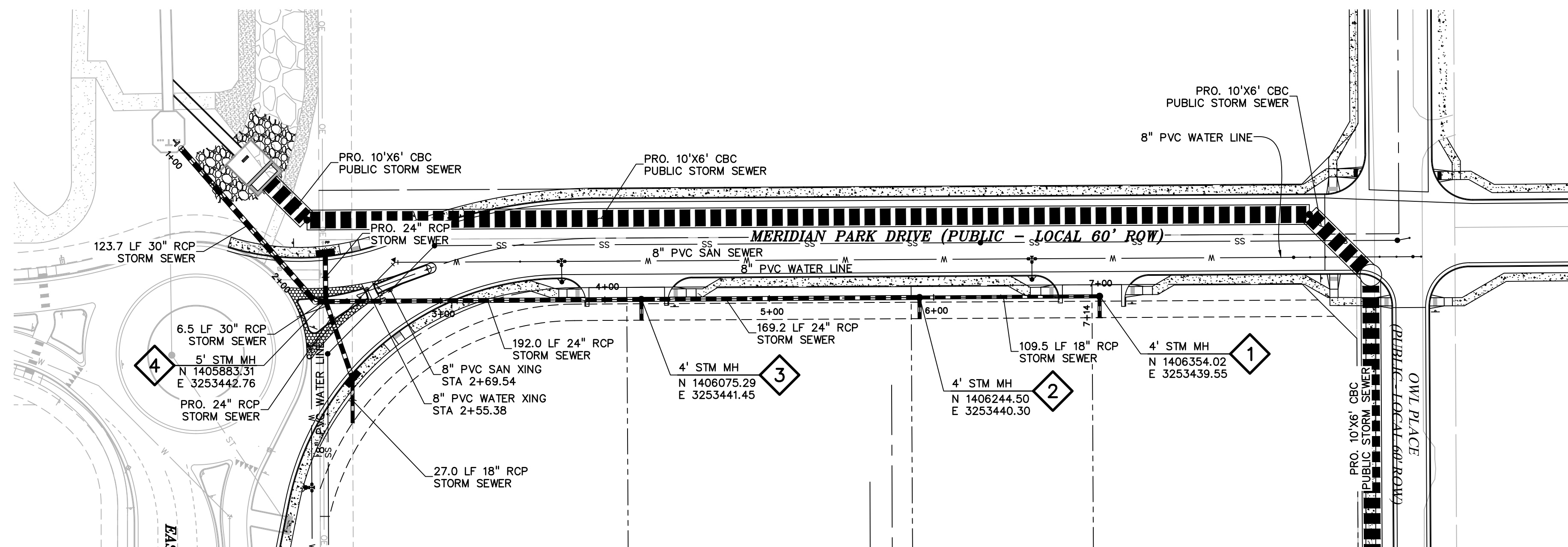
- ① R1-1
30"x30"
- ② R1-2
36"x36"
- ③ W11A-2
30"x30"
- ④ R6-4
30"x24"
- ⑤ W2-6
30"x30"
- ⑥ R1-6
36"x12"
- ⑦ R4-7C
18X30
- ⑧ R3-5R
30X36



LEGEND

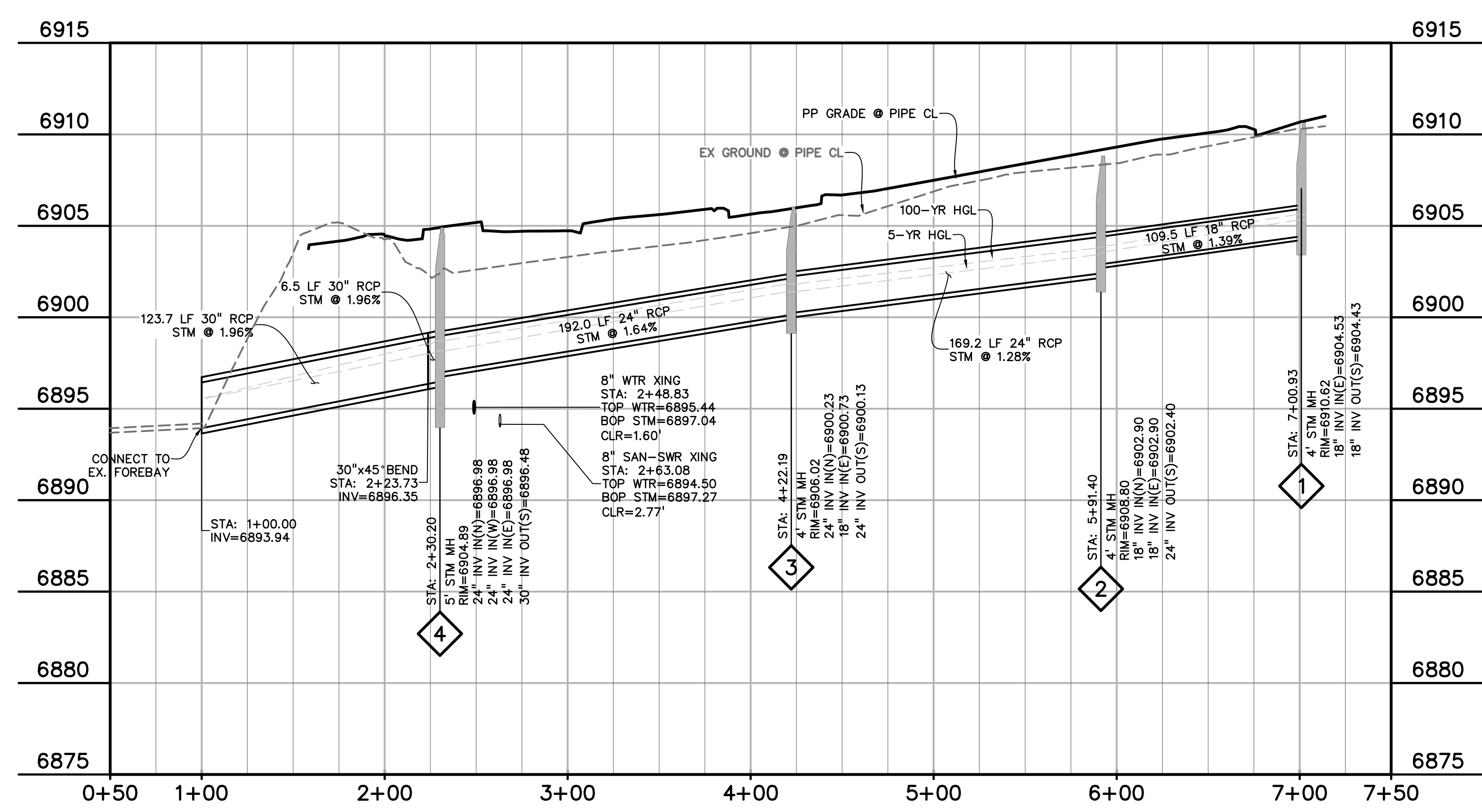
- PROPERTY LINE
- LOT LINE
- EASEMENT
- CURB & GUTTER
- SIDEWALK
- EX. OVERHEAD ELECTRIC..... OE
- EX. SANITARY SEWER..... SS
- EX. PETROLEUM..... UP
- EX. GAS..... G
- EX. RAW WATER..... RW
- EX. WATER..... W
- EX. STORM SEWER..... ST

811 Know what's below.
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 PCD FILE: VR2321

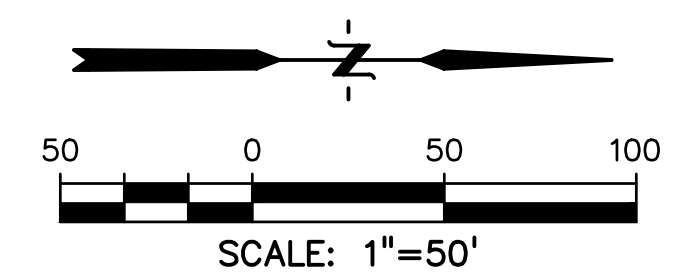


LEGEND

| | |
|-----------------------|-----|
| PROPERTY LINE | --- |
| LOT LINE | --- |
| EASEMENT | --- |
| CURB & GUTTER | --- |
| SIDEWALK | --- |
| PRO. WATER LINE | --- |
| PRO. FIRE HYDRANT | --- |
| PRO. RAW WATER | --- |
| PRO. SANITARY SEWER | --- |
| PRO. STORM SEWER | --- |
| EX. OVERHEAD ELECTRIC | OE |
| EX. SANITARY SEWER | SS |
| EX. PETROLEUM | UP |
| EX. GAS | G |
| EX. RAW WATER | RW |
| EX. WATER | W |
| EX. STORM SEWER | ST |



NOTE:
ALL STORM SEWER PIPE AND STRUCTURES ARE TO BE PRIVATELY OWNED AND MAINTAINED WITH THE EXCEPTION OF THE 10'x6' BOX CULVERT AND ASSOCIATED STRUCTURES OR AS OTHERWISE NOTED.



ABBREVIATION

| | |
|-----------|---------------------------|
| EX | EXISTING |
| MH | MANHOLE |
| PP | PROPOSED |
| PT | POINT |
| SS OR SAN | SANITARY SEWER |
| ST | STORM SEWER |
| WT | WATER |
| XING | CROSSING |
| EOP | END OF PIPE |
| CDS | CUL-DE-SAC |
| BT | BEGIN TRANSITION |
| ET | END TRANSITION |
| EOR | EDGE OF ROAD |
| SW | SIDEWALK |
| CL | CENTERLINE |
| C&G | CURB AND GUTTER |
| FED | PEDESTRIAN |
| PC | POINT OF CURVATURE |
| PT | POINT OF TANGENCY |
| ROW | RIGHT OF WAY |
| XWALK | CROSSWALK |
| XPAN | CROSS-PAN |
| PRC | POINT OF REVERSE CURVE |
| MPD | MERIDIAN PARK DRIVE |
| ETV | NORTH FL EASTONVILLE ROAD |
| OWL | OWL PLACE |
| FL | FLOWLINE |
| CBC | CONCRETE BOX CULVERT |

24" EASTERN STORM PIPE - MERIDIAN PARK DRIVE
STA 1+00.00 - 7+01.00
HORIZ. SCALE: 1"=50'
VERT. SCALE: 1"=5'

PREPARED BY:

DREXEL, BARRELL & CO.
Engineers • Surveyors
101 SAWATCH ST., STE #100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719)260-0887
COLORADO SPRINGS • LAFAYETTE

CLIENT:

BH RE INVESTMENTS, LLC
450 N MCCLINTOCK DRIVE
CHANDLER, AZ 85226
(480) 313-2724

CONSTRUCTION PLANS FOR:

OWL MARKETPLACE
EL PASO COUNTY, COLORADO

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| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-STM1 |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: 1"=50'
VERTICAL: 1"=5'

STORM SEWER PLAN & PROFILE EAST MPD

PROJECT NO. 21611-01CSCV
DRAWING NO.

C11

SHEET: 11 OF 18

811 Know what's below.
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PCD FILE: VR2321

PREPARED BY:



CLIENT:

BH RE INVESTMENTS, LLC
450 N MCCLINTOCK DRIVE
CHANDLER, AZ 85226
(480) 313-2724

CONSTRUCTION PLANS FOR:

**OWL
MARKETPLACE**
EL PASO COUNTY, COLORADO

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| FILE NAME: | 21611-STM1 |

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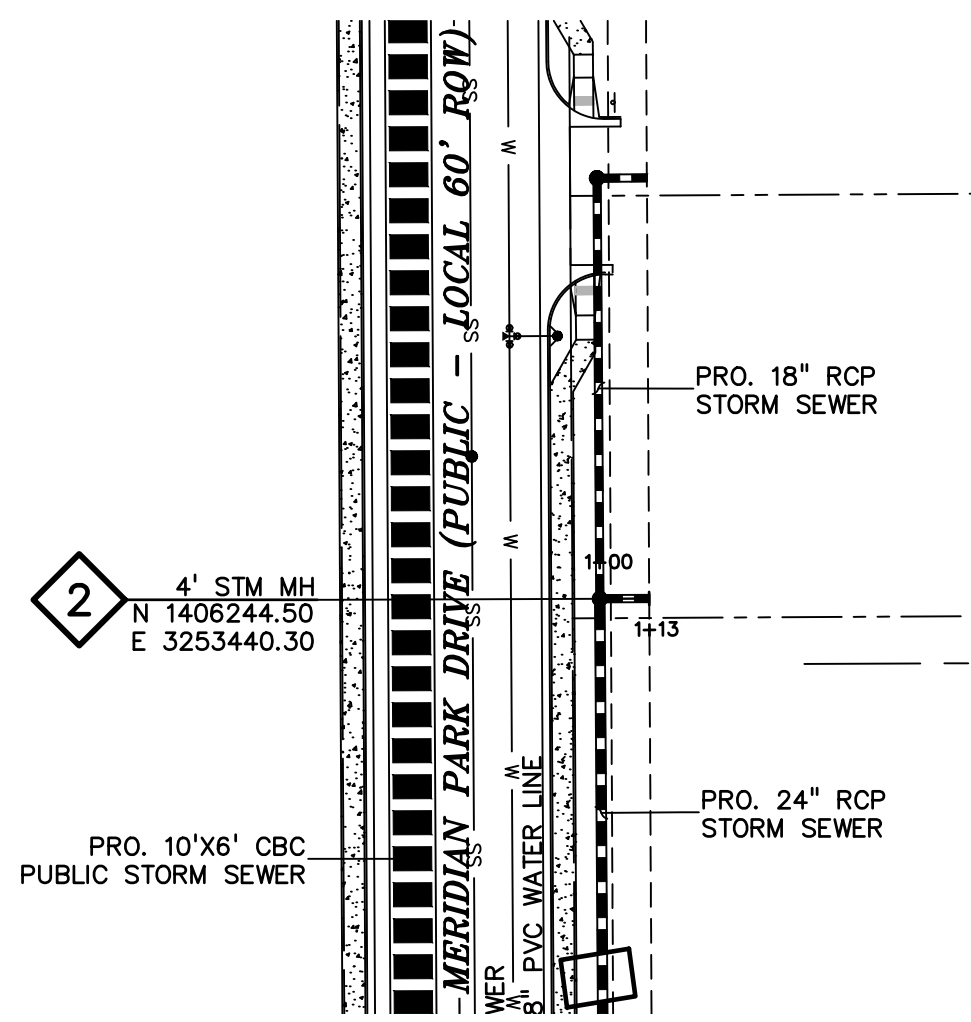
DRAWING SCALE:
HORIZONTAL: 1"=50'
VERTICAL: 1"=5'

**STORM SEWER
PLAN & PROFILE
EAST MPD**

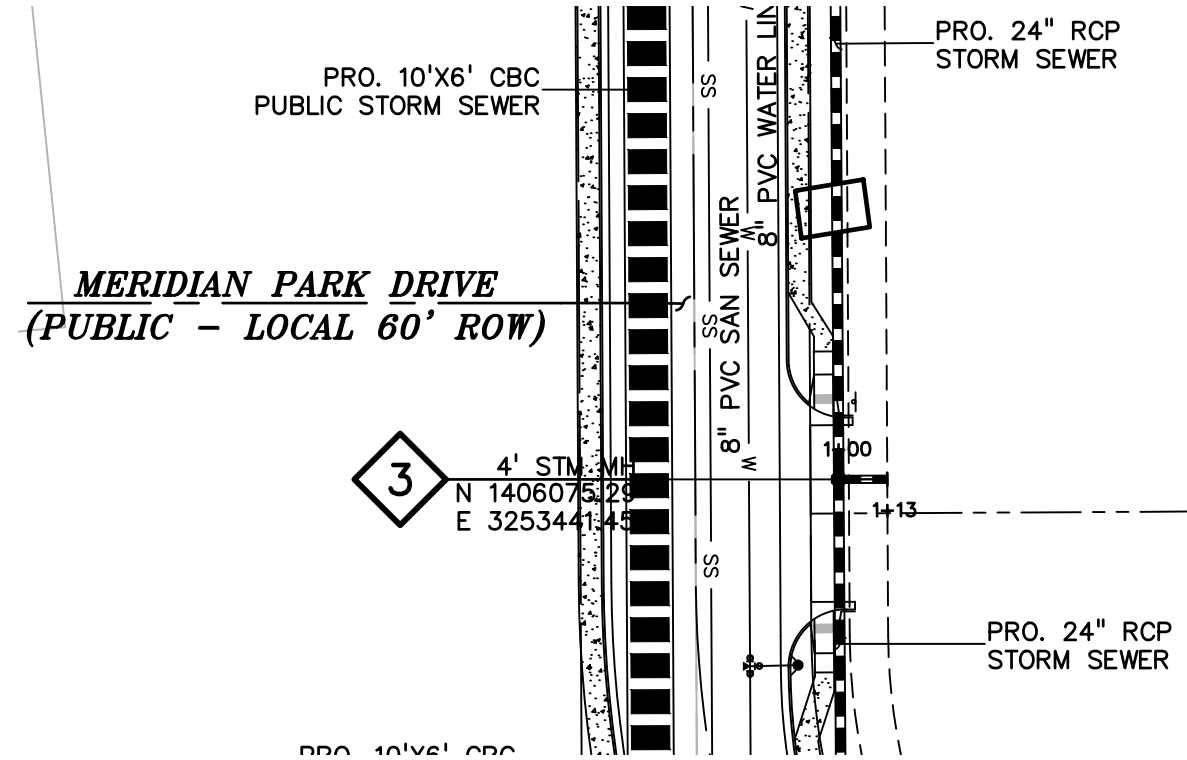
PROJECT NO. 21611-01CSCV
DRAWING NO.

C12

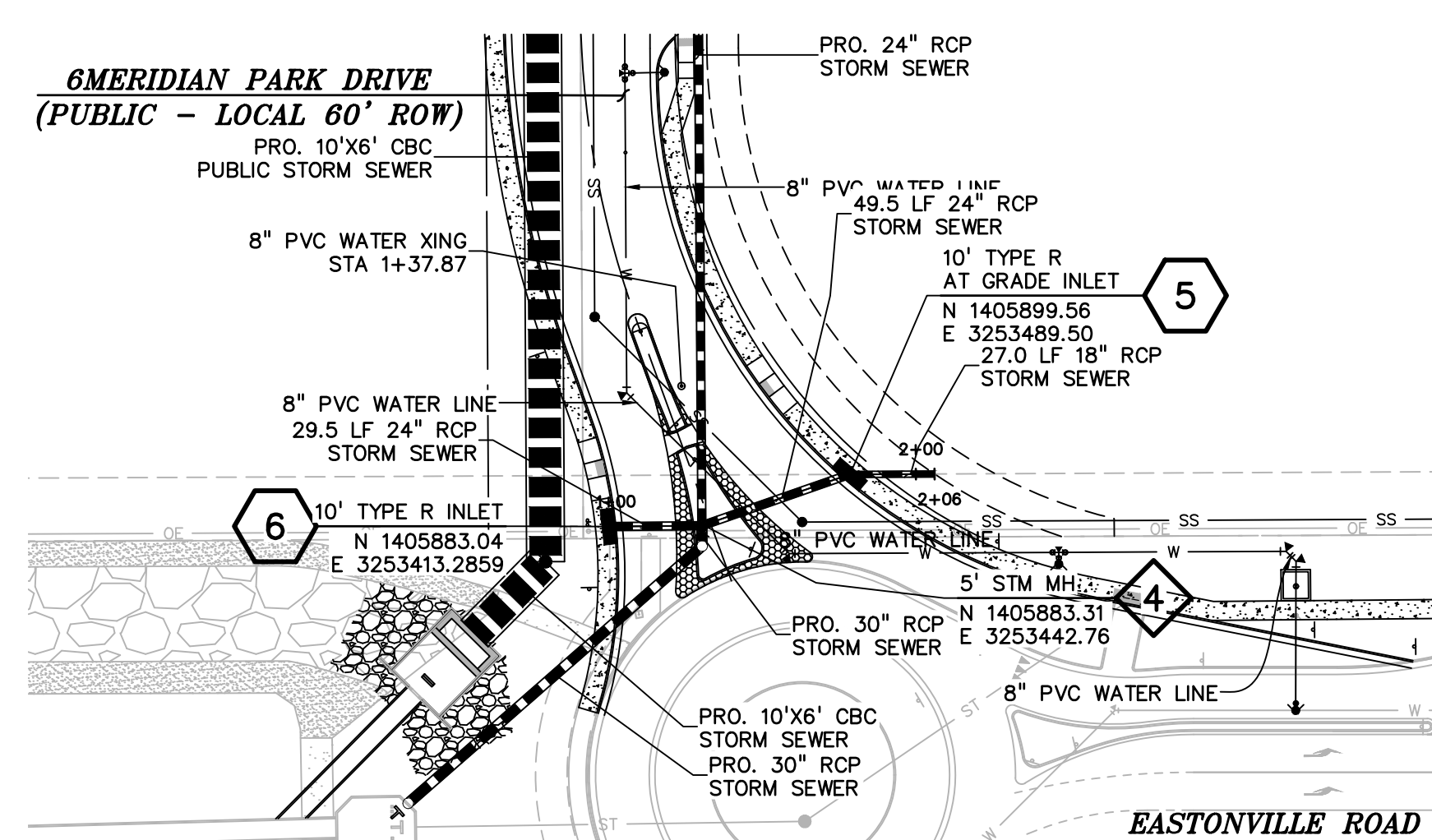
SHEET: 12 OF 18



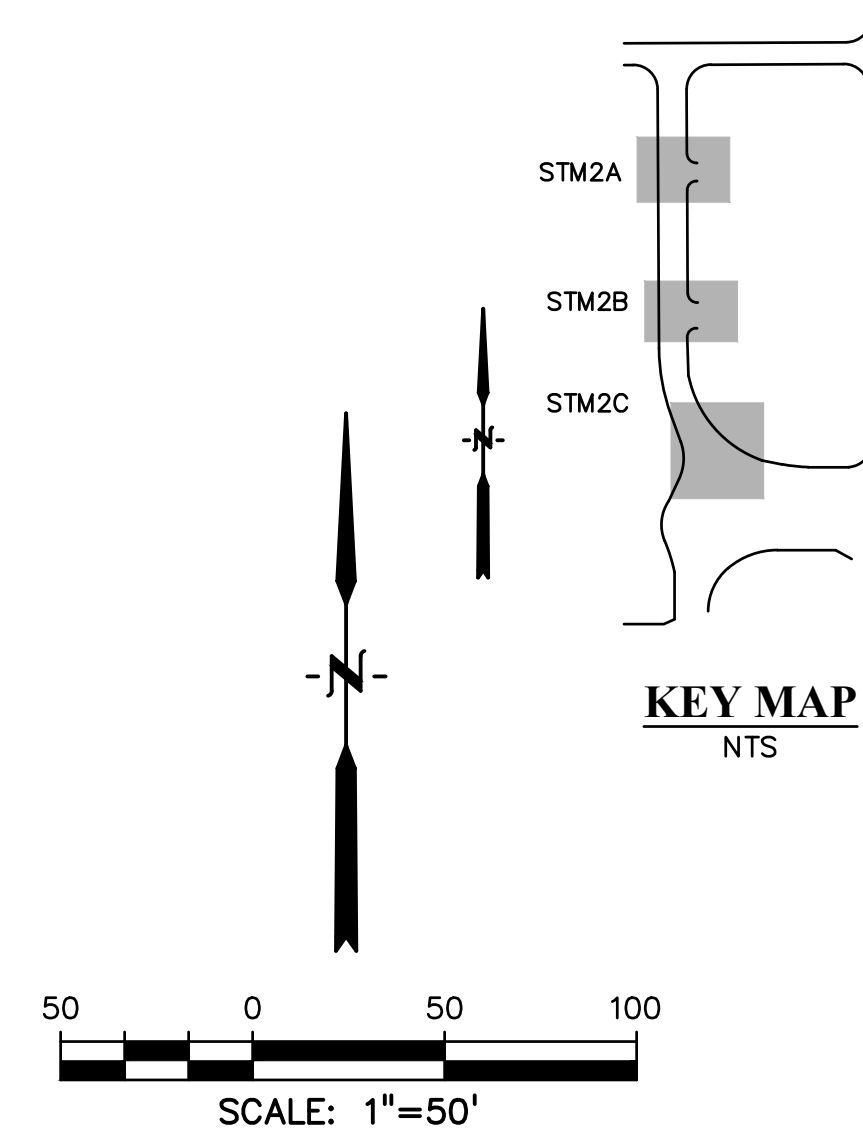
18" EASTERN STORM PIPE - MPD (STM2A)
STA 1+00.00 - 1+21.00
SCALE: 1"=50'



18" EASTERN STORM PIPE - MPD (STM2B)
STA 1+00.00 - 1+21.00
SCALE: 1"=50'



24" EASTERN STORM PIPE - MPD (STM2C)
STA 1+00.00 - 1+72.00
SCALE: 1"=50'



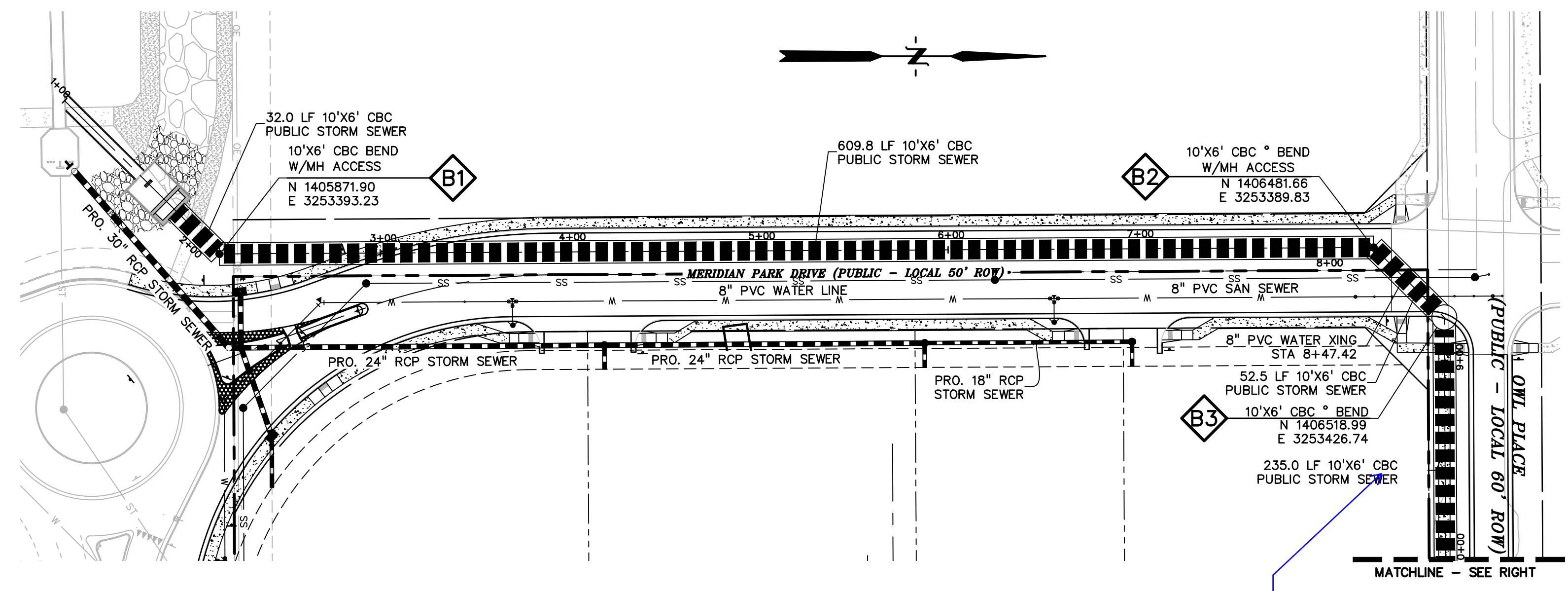
LEGEND

| | |
|-----------------------|------|
| PROPERTY LINE | --- |
| LOT LINE | ---- |
| EASEMENT | ---- |
| CURB & GUTTER | ==== |
| SIDEWALK | ---- |
| PRO. WATER LINE | —W— |
| PRO. FIRE HYDRANT | —FH— |
| PRO. RAW WATER | —RW— |
| PRO. SANITARY SEWER | —SS— |
| PRO. STORM SEWER | —ST— |
| EX. OVERHEAD ELECTRIC | —OE— |
| EX. SANITARY SEWER | —SS— |
| EX. PETROLEUM | —UP— |
| EX. GAS | —G— |
| EX. RAW WATER | —RW— |
| EX. WATER | —W— |
| EX. STORM SEWER | —ST— |

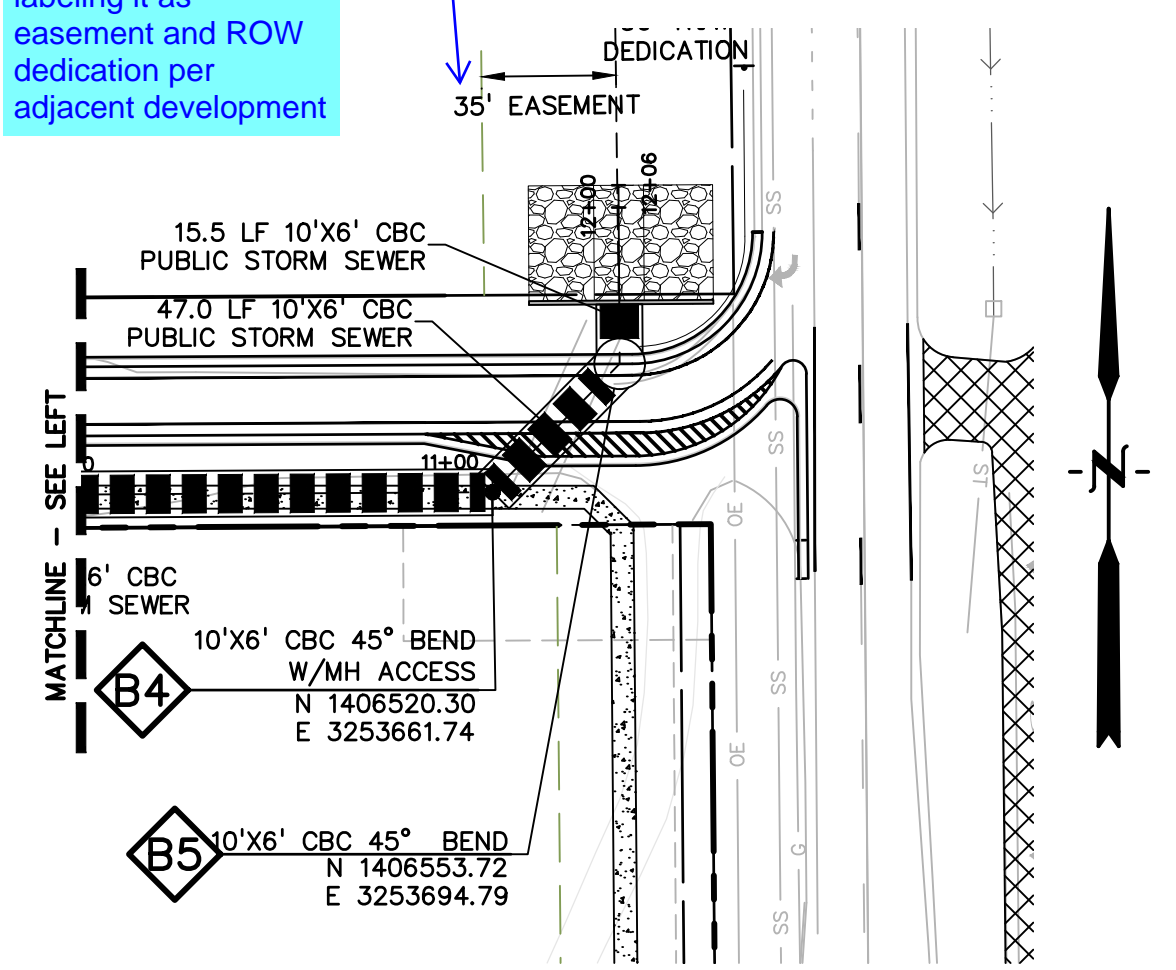
ABBREVIATION

| | |
|-----------|---------------------------|
| EX | EXISTING |
| MH | MANHOLE |
| PP | PROPOSED |
| PT | POINT |
| SS OR SAN | SANITARY SEWER |
| ST | STORM SEWER |
| WT | WATER |
| XING | CROSSING |
| EOP | END OF PIPE |
| CDS | CUL-DE-SAC |
| BT | BEGIN TRANSITION |
| ET | END TRANSITION |
| EOR | EDGE OF ROAD |
| SW | SIDEWALK |
| CL | CENTERLINE |
| C&G | CURB AND GUTTER |
| FED | PEDESTRIAN |
| PC | POINT OF CURVATURE |
| PT | POINT OF TANGENCY |
| ROW | RIGHT OF WAY |
| XWALK | CROSSWALK |
| XPAN | CROSS-PAN |
| PRC | POINT OF REVERSE CURVE |
| MPD | MERIDIAN PARK DRIVE |
| ETV | NORTH FL EASTONVILLE ROAD |
| OWL | OWL PLACE |
| FL | FLOWLINE |
| CBG | CONCRETE BOX CULVERT |

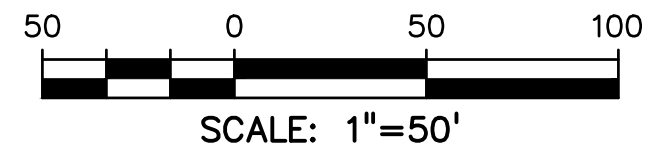
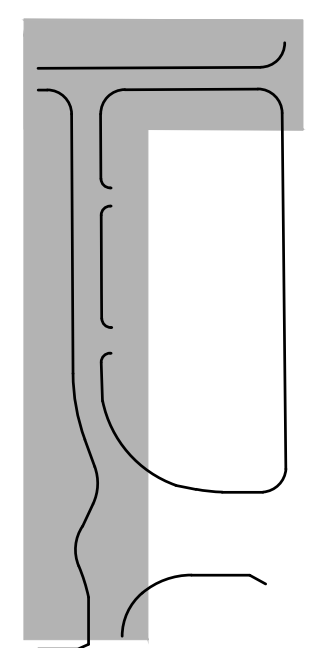
NOTE:
ALL STORM SEWER PIPE AND STRUCTURES ARE TO BE PRIVATELY OWNED AND MAINTAINED WITH THE EXCEPTION OF THE 10"X6" BOX CULVERT AND ASSOCIATED STRUCTURES OR AS OTHERWISE NOTED.



Verify as the previous submittal from the adjacent property owner indicated 30'. Recommend just labeling it as easement and ROW dedication per adjacent development



Provide detail for the proposed box culvert. will this be cast in place or precast?

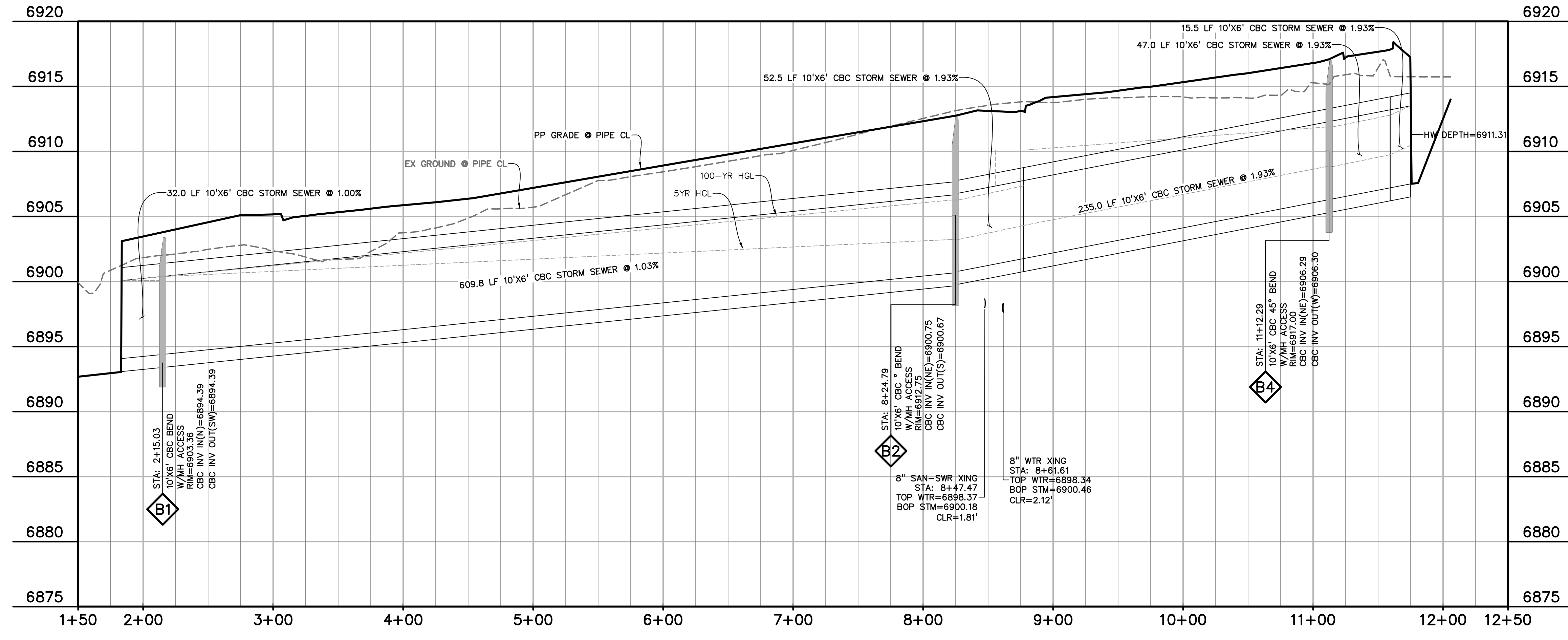


LEGEND

| | |
|-----------------------|----------|
| PROPERTY LINE | --- |
| LOT LINE | ---- |
| EASEMENT | |
| CURB & GUTTER | ===== |
| SIDEWALK | ----- |
| PRO. WATER LINE | ----- W |
| PRO. FIRE HYDRANT | ----- H |
| PRO. RAW WATER | ----- R |
| PRO. SANITARY SEWER | ----- SS |
| PRO. STORM SEWER | ----- ST |
| EX. OVERHEAD ELECTRIC | ----- OE |
| EX. SANITARY SEWER | ----- SS |
| EX. PETROLEUM | ----- UP |
| EX. GAS | ----- G |
| EX. RAW WATER | ----- RW |
| EX. WATER | ----- W |
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ABBREVIATION

| | |
|-----------|---------------------------|
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| MH | MANHOLE |
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| PT | POINT |
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| WT | WATER |
| XING | CROSSING |
| EOP | END OF PIPE |
| CDS | CUL-DE-SAC |
| BT | BEGIN TRANSITION |
| ET | END TRANSITION |
| EOR | EDGE OF ROAD |
| SW | SIDEWALK |
| CL | CENTERLINE |
| C&G | CURB AND GUTTER |
| PED | PEDESTRIAN |
| PC | POINT OF CURVATURE |
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| PRC | POINT OF REVERSE CURVE |
| MPD | MERIDIAN PARK DRIVE |
| ETV | NORTH FL EASTONVILLE ROAD |
| OWL | OWL PLACE |
| FL | FLOWLINE |
| CBC | CONCRETE BOX CULVERT |



10'x6' CBC WESTERN STORM SEWER - MERIDIAN PARK DRIVE
 STA 1+00.00 - 12+06.00
 SCALE: 1"=50'

PREPARED BY:

 DREXEL, BARRELL & CO.
 Engineers • Surveyors
 101 SAWATCH ST., STE #100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
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CONSTRUCTION PLANS FOR:
OWL MARKETPLACE
 EL PASO COUNTY, COLORADO

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 CHECKED BY: TDM
 FILE NAME: 21611-STM2

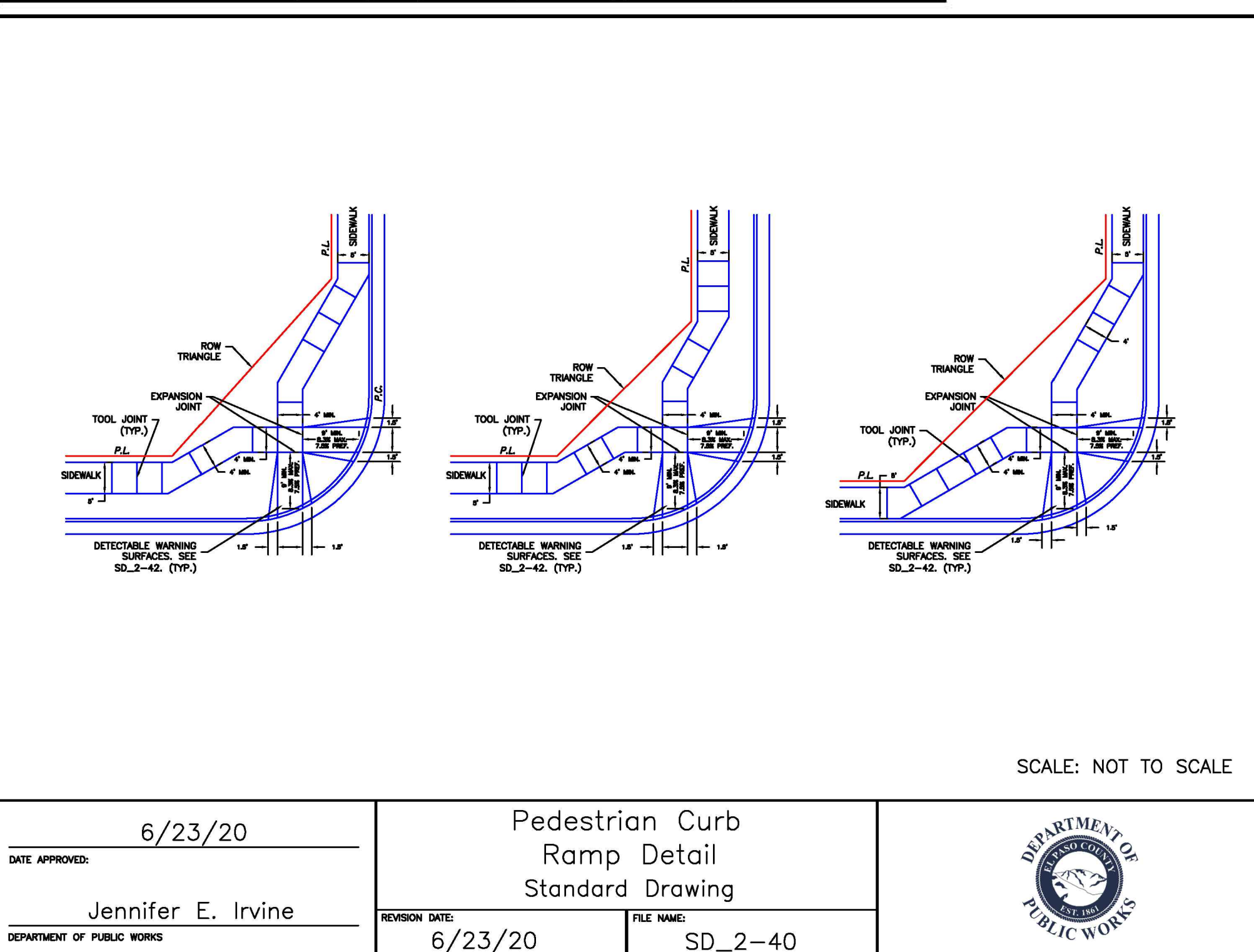
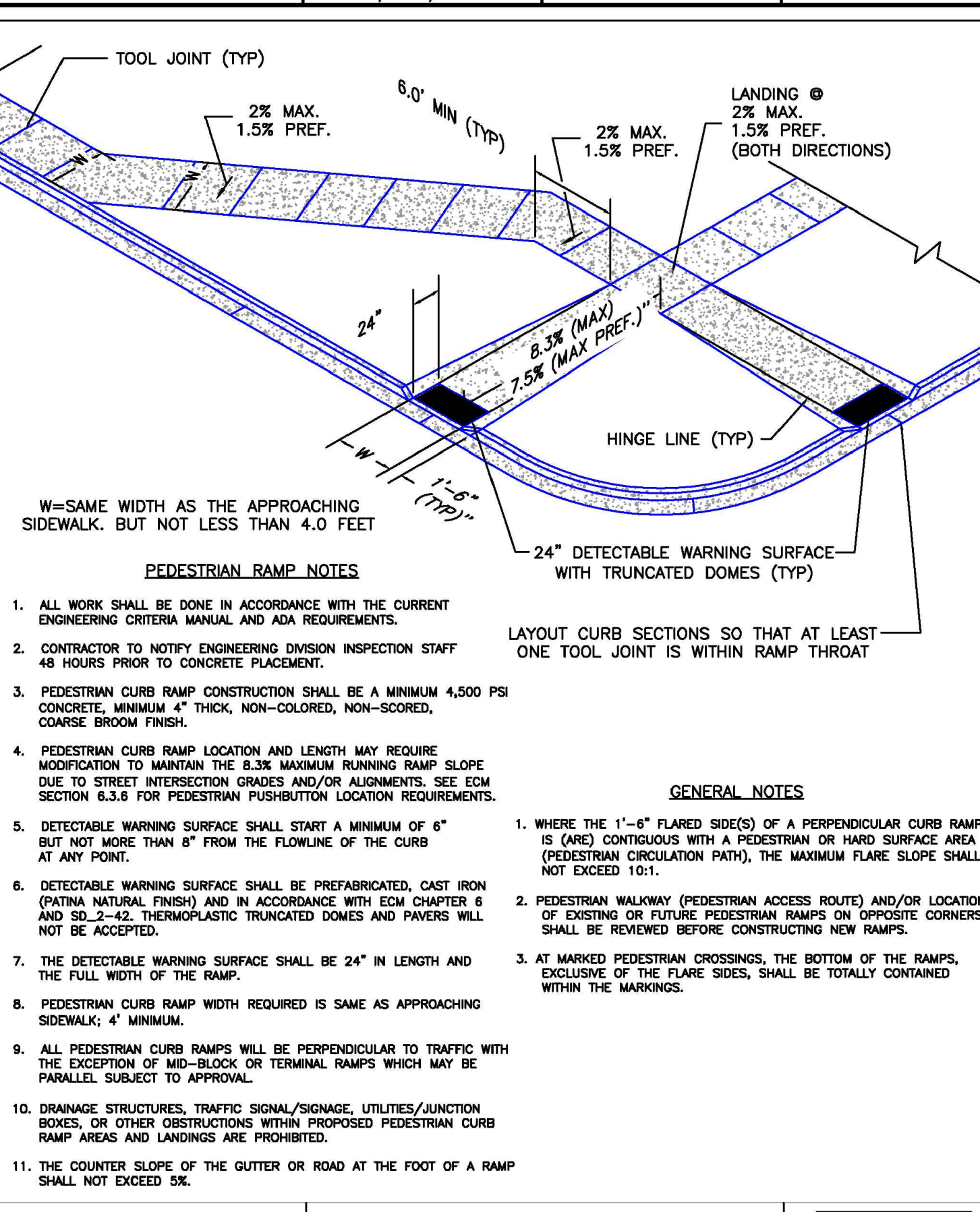
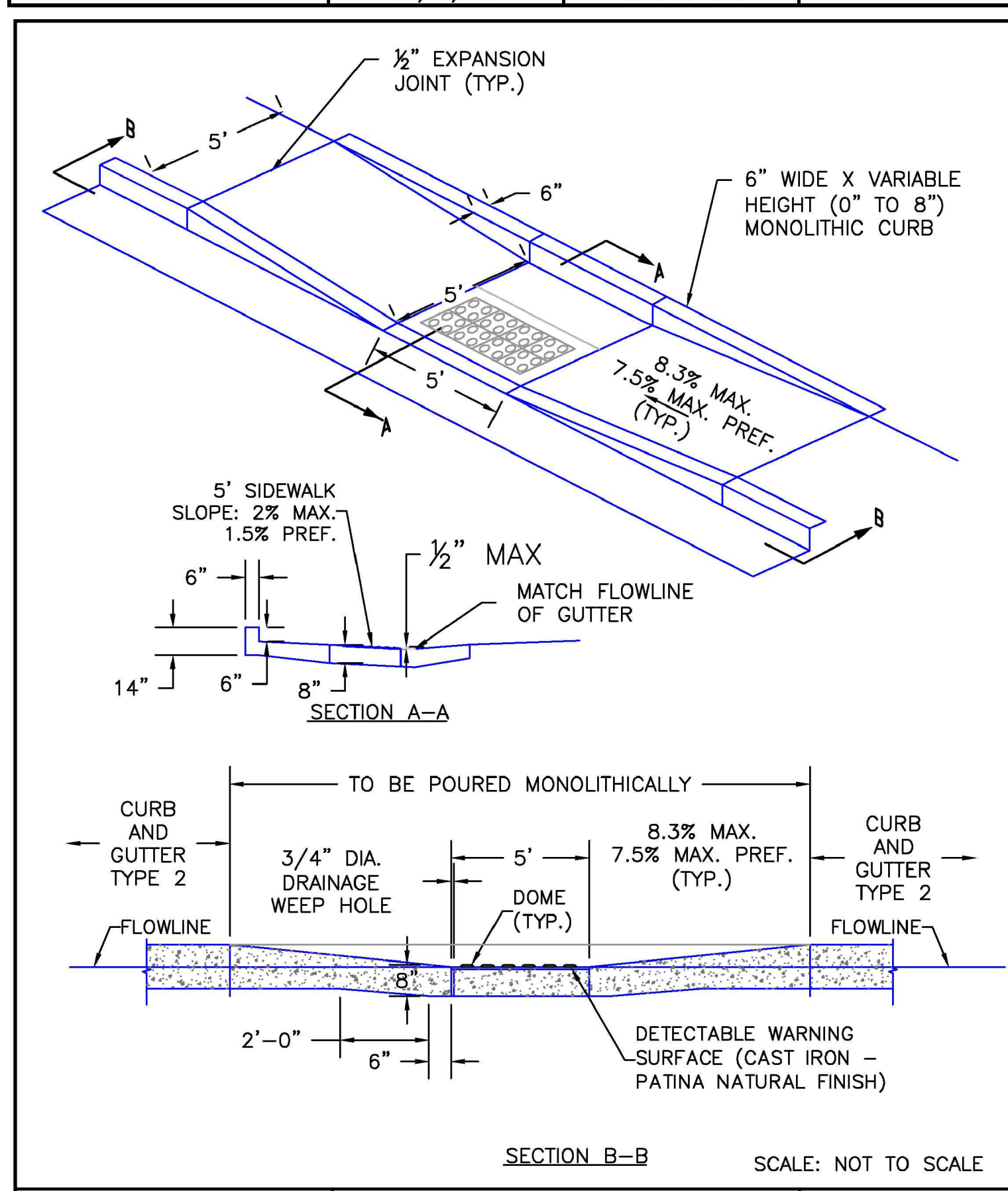
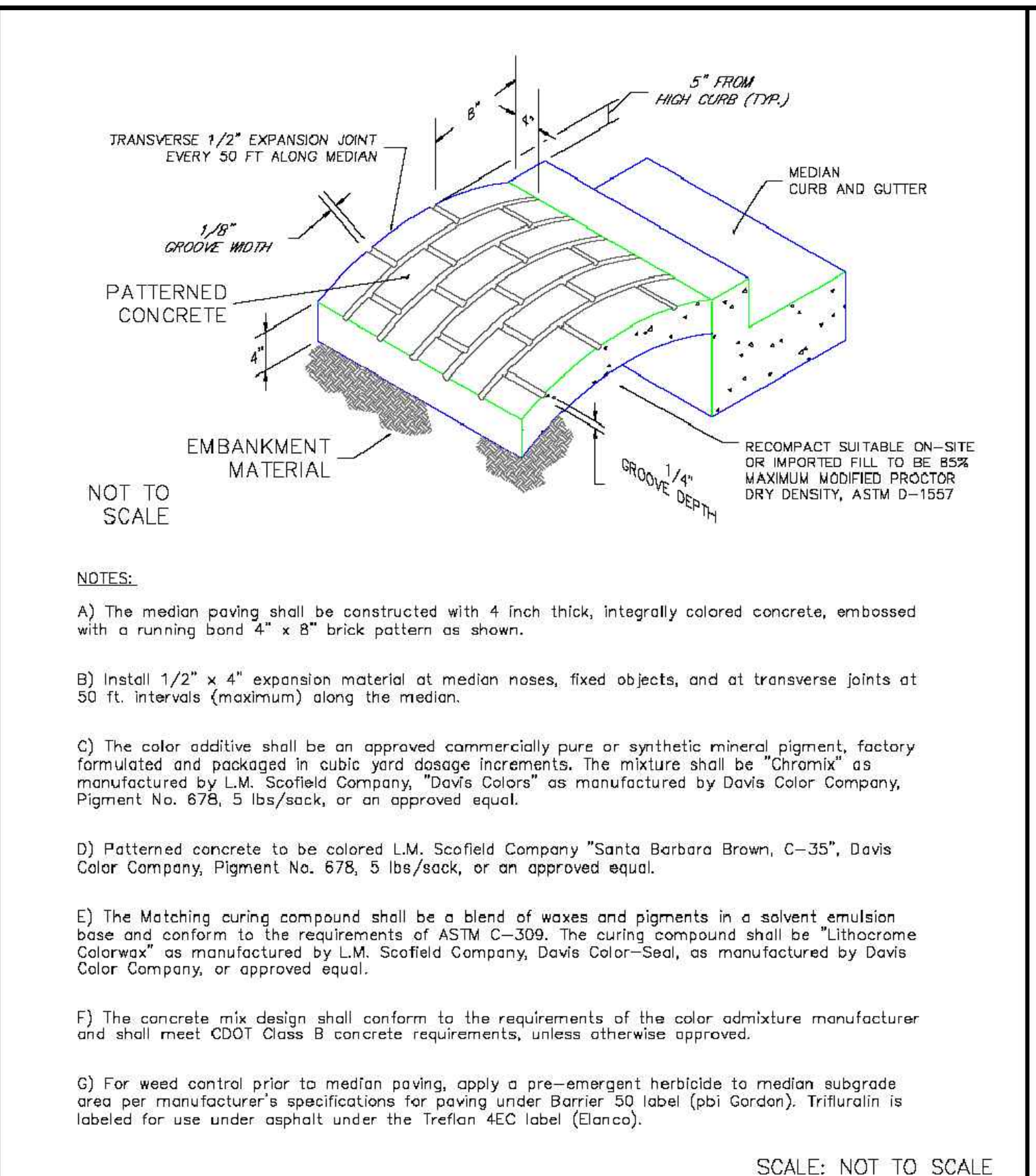
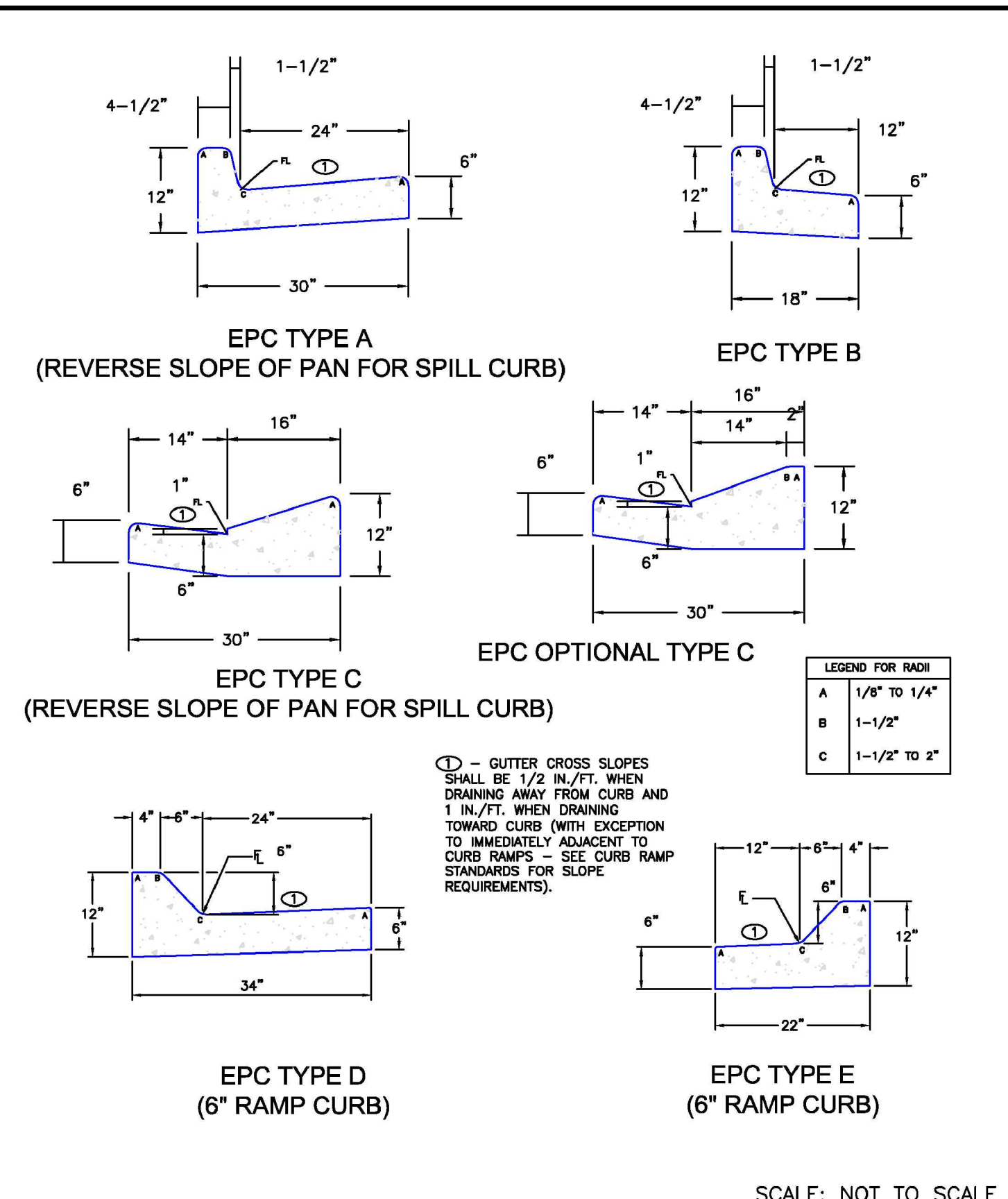
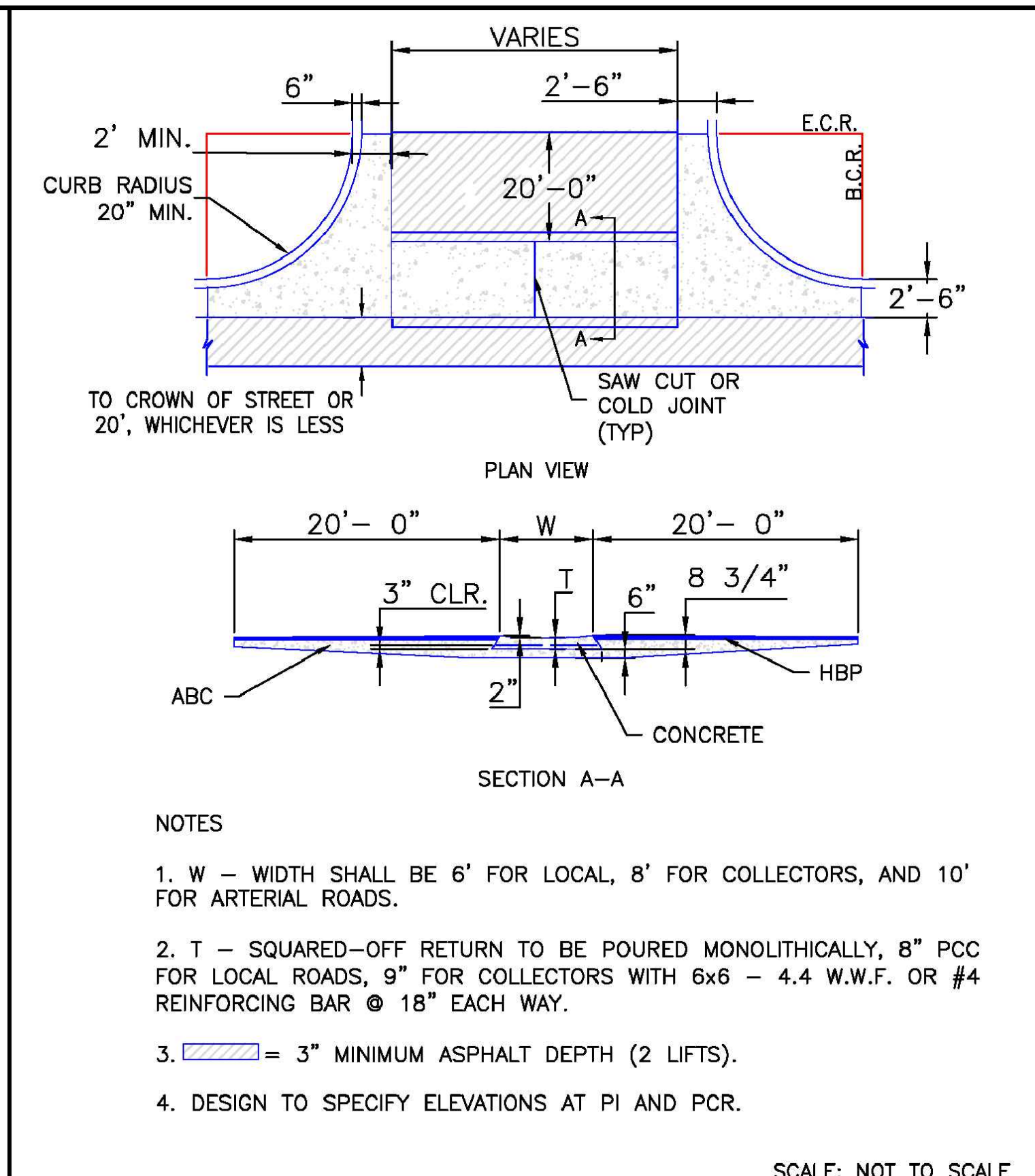
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.
 DRAWING SCALE:
 HORIZONTAL: 1"=50'
 VERTICAL: 1"=5'

STORM SEWER PLAN & PROFILE WEST MPD

PROJECT NO. 21611-01CSCV
 DRAWING NO.

C13

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 PCD FILE: VR2321



PREPARED BY:

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 Engineers & Surveyors
 101 SAWATCH ST., STE #100
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 COLORADO SPRINGS • LAFAYETTE

CLIENT:

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CONSTRUCTION PLANS FOR:

**OWL
MARKETPLACE**

EL PASO COUNTY, COLORADO

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| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-DTL |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: N/A
 VERTICAL: N/A

STANDARD DETAILS

PROJECT NO. 21611-01CSCV
 DRAWING NO.

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 PCD FILE: VR2321

SHEET: 14 OF 18

GENERAL NOTES

- CONCRETE SHALL BE CLASS 3.
- HEADWALL SHALL BE PERPENDICULAR TO THE PIPE UNLESS OTHERWISE SHOWN ON THE PLANS. TOLERANCES DIMENSIONS AND QUANTITIES MUST BE ADJUSTED FOR BENDING REINFORCEMENT.
- FOR VERTICAL DETAILS, SEE STANDARD PLAN W-400-20.
- VEHICLE DESIGN BY PIPE HAS BEEN DEDUCTED FROM STEEL AND CONCRETE QUANTITIES.
- EXPOSED CONCRETE CORNERS SHALL BE CHAMFERED 3/4" IN.
- ALL REINFORCING BARS SHALL HAVE A 2 IN. MINIMUM CLEARANCE.
- WHEN TWO OR MORE PIPES ARE LAD SIDE BY SIDE, THEY SHALL BE PLACED ON THE EXISTING GRADE OR 3 FT. AWAY INCLUDING WALL THICKNESS, UNLESS NOTED.
- ADD 0.8" x 4" OR 1.3" x 4" APRON IS REQUIRED.

TYPICAL BAR LAYOUT FOR CONCRETE HEADWALLS

HEADWALL FOR RIGID ROUND PIPE

HEADWALL FOR FLEXIBLE PIPE ARCH

HEADWALL FOR FLEXIBLE ROUND PIPE

HEADWALL FOR STRUCTURAL PLATE ARCH

SKYW FACTOR TABLE

| THICKNESS | MATERIAL | 18" | 24" | 30" | 36" | 42" | 48" |
|-----------|----------|-----|-----|-----|-----|------|------|
| 6" | CONCRETE | 0.4 | 0.8 | 1.2 | 2.4 | 3.6 | 4.7 |
| 18" | REBAR | 2.0 | 2.5 | 3.4 | 7.6 | 10.7 | 13.9 |

Computer File Information: Creation Date: 07/04/12, Initials: EG, Last Modification Date: 07/04/12, Issue: LTA, Full Path: www.colorado.gov/business/designsupport, Drawing File Name: 60100101.dwg, CDO Ver: MicroStation V8, Scale: Not to Scale, Units: English.

Sheet Revisions: Date: , Comments: .

Colorado Department of Transportation: 4201 East Arkansas Avenue, Denver, Colorado 80222, Phone: (303) 757-6083, Fax: (303) 757-6020, Project Development Branch DD/LTA.

HEADWALL FOR PIPES STANDARD PLAN NO. M-601-10 Sheet No. 1 of 1

GENERAL NOTES

- FOR SIZE AND LOCATION OF PIPES, SEE THE PLANS.
- ALL CONCRETE SHALL BE CLASS 3.
- POSTINGS IN ROCK SHALL BE EXCAVED OUT TO ROCK AND NOT FORMED IN ACCORDANCE WITH SUBSECTION 90.04(A).
- EXPOSED CONCRETE CORNERS SHALL BE CHAMFERED 3/4" IN.
- HEADWALL SHALL HAVE REINFORCING STEEL INSTALLED IN A PATTERN SIMILAR TO STANDARD PLAN W-400-20.
- THE COST OF REINFORCING STEEL SHALL BE INCLUDED IN THE WORK UNDER THE STEEL QUANTITIES ARE LISTED IN THE PLANS AND ARE PAID FOR SEPARATELY.

SINGLE PIPE **DOUBLE PIPE**

CONCRETE HEADWALL INSTALLATIONS

PIPE OUTLET PAVING

CONCRETE QUANTITIES FOR ONE CONCRETE HEADWALL (CUBIC YARDS)

PIPE OUTLET PAVING (CUBIC YARDS)

TYPE OF PIPE HEADWALL DIMENSIONS

TYPE OF PIPE HEADWALL DIMENSIONS

Computer File Information: Creation Date: 07/04/12, Initials: EG, Last Modification Date: 07/04/12, Issue: LTA, Full Path: www.colorado.gov/business/designsupport, Drawing File Name: 60100101.dwg, CDO Ver: MicroStation V8, Scale: Not to Scale, Units: English.

Sheet Revisions: Date: , Comments: .

Colorado Department of Transportation: 4201 East Arkansas Avenue, Denver, Colorado 80222, Phone: (303) 757-6083, Fax: (303) 757-6020, Project Development Branch DD/LTA.

HEADWALLS AND PIPE OUTLET PAVING STANDARD PLAN NO. M-601-12 Sheet No. 1 of 1

PIPE ID **BW**

| | |
|-----------------|----------|
| 48" AND SMALLER | 6"-4" |
| 54" | 6"-10" |
| 60" AND LARGER | OD + 16" |

NOTES

- TYPE I MANHOLE SHALL BE USED WHEN APPROPRIATE AND TYPICALLY FOR PIPE SIZES LARGER THAN 30 INCHES I.D..
- VIEW AND DETAILS SHOWN ARE TYPICAL FOR STRAIGHT THROUGH DESIGN ONLY. DESIGN ENGINEER SHALL DETERMINE MANHOLE BASE CONFIGURATION AND DIMENSIONS FOR PARTICULAR PIPE SIZES AND ALIGNMENT.
- EITHER LADDER OR STEPS SHALL BE INSTALLED WHEN MANHOLE DEPTH EXCEEDS 30". LOWEST STEP SHALL BE A MAXIMUM OF 16" ABOVE THE FLOOR.
- FLOOR OF THE MANHOLE SHALL BE TROWELLED TO A SMOOTH, HARD SURFACE AND SHALL SLOPE TOWARDS THE OUTLET (8:1 MAX., 1/2" PER FT. MIN). FLOOR SHALL BE SHAPED AND CHANNELLED; SEE SD_3-2 FOR TYPICAL CHANNEL DETAILS.

SCALE: NOT TO SCALE

Computer File Information: Creation Date: 07/04/12, Initials: EG, Last Modification Date: 07/04/12, Issue: LTA, Full Path: www.colorado.gov/business/designsupport, Drawing File Name: 60100101.dwg, CDO Ver: MicroStation V8, Scale: Not to Scale, Units: English.

Sheet Revisions: Date: , Comments: .

Colorado Department of Transportation: 4201 East Arkansas Avenue, Denver, Colorado 80222, Phone: (303) 757-6083, Fax: (303) 757-6020, Project Development Branch DD/LTA.

Storm Sewer Manhole Detail Type I Standard Drawing

DATE APPROVED: 7/9/09, DEPARTMENT OF TRANSPORTATION, André Brackin

REVISION DATE: 7/9/09, FILE NAME: SD_3-1

NOTE

TYPE III MANHOLES SHALL BE USED WHEN APPROPRIATE AND TYPICALLY WHEN THE FOLLOWING CONDITIONS ARE MET:

- PIPE IS 48" OR LARGER INSIDE DIAMETER.
- NOT CHANGE IN PIPE SIZE.
- NO CHANGE IN PIPE MATERIAL.
- NO CHANGE IN HORIZONTAL ALIGNMENT.
- SLOPE IS FLAT AND CONTINUOUS.

2. TYPE III MANHOLES SHALL BE FABRICATED BY THE MANUFACTURER/SUPPLIER AND DELIVERED TO THE SITE AS A SINGLE UNIT. FIELD FABRICATION SHALL NOT BE PERMITTED.

3. EITHER LADDER OR STEPS SHALL BE INSTALLED. LOWEST STEP SHALL BE A MAXIMUM OF 30" ABOVE THE INVERT OF THE PIPE.

SCALE: NOT TO SCALE

Computer File Information: Creation Date: 07/04/12, Initials: EG, Last Modification Date: 07/04/12, Issue: LTA, Full Path: www.colorado.gov/business/designsupport, Drawing File Name: 60100101.dwg, CDO Ver: MicroStation V8, Scale: Not to Scale, Units: English.

Sheet Revisions: Date: , Comments: .

Colorado Department of Transportation: 4201 East Arkansas Avenue, Denver, Colorado 80222, Phone: (303) 757-6083, Fax: (303) 757-6020, Project Development Branch DD/LTA.

Storm Sewer Manhole Detail Type III Standard Drawing

DATE APPROVED: 8/11/11, DEPARTMENT OF TRANSPORTATION, André P. Brackin

REVISION DATE: 11/10/04, FILE NAME: SD_3-3

PREPARED BY:



CLIENT:

BH RE INVESTMENTS, LLC
450 N MCCLINTOCK DRIVE
CHANDLER, AZ 85226
(480) 313-2724

CONSTRUCTION PLANS FOR:
OWL MARKETPLACE
EL PASO COUNTY, COLORADO

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| INITIAL ISSUE | 08/04/2023 |
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| | |
| DESIGNED BY: | KGV |
| DRAWN BY: | KGV |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-DTL |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: N/A
VERTICAL: N/A

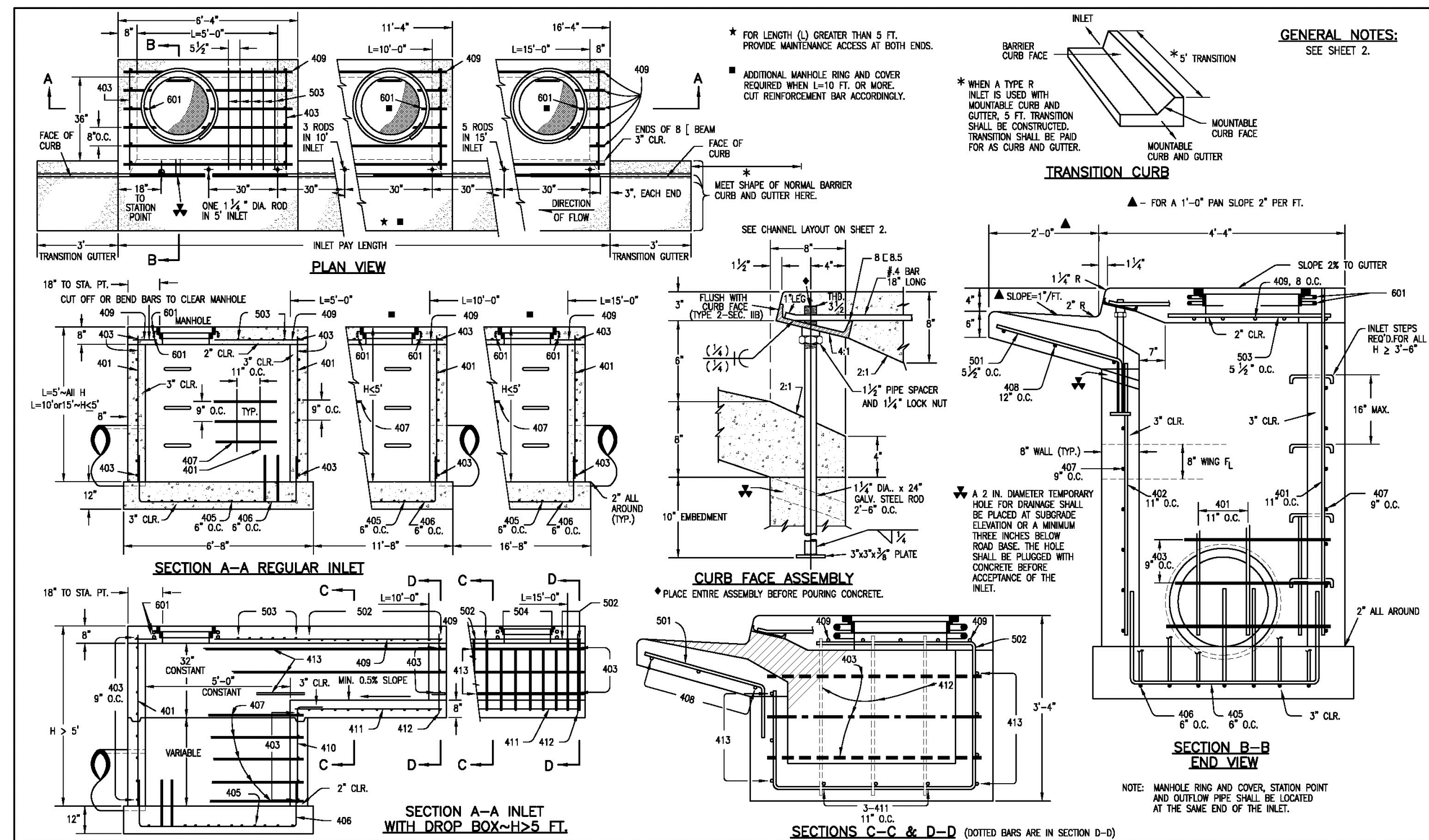
STANDARD DETAILS

PROJECT NO. 21611-01CSCV
DRAWING NO.

C15

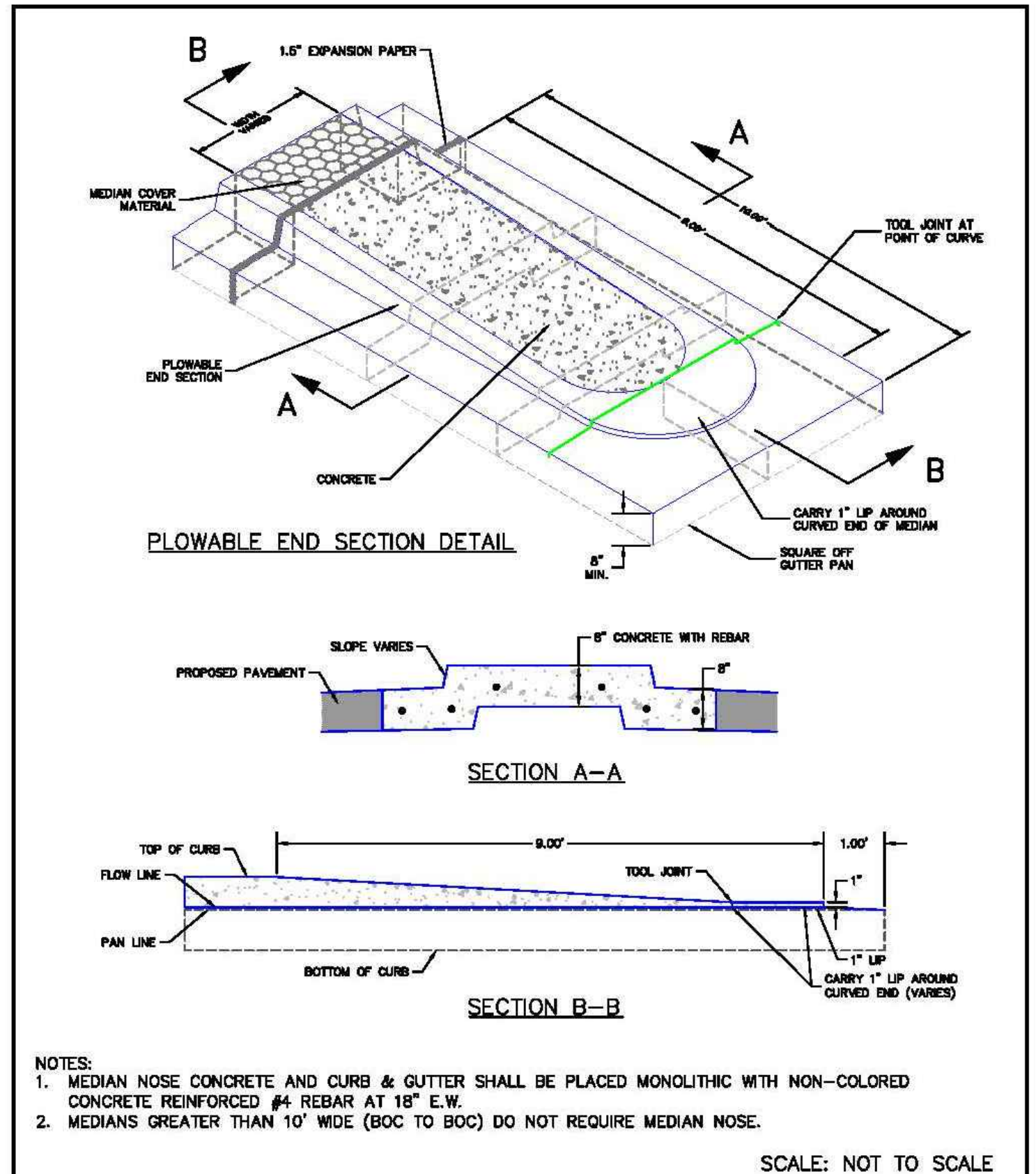
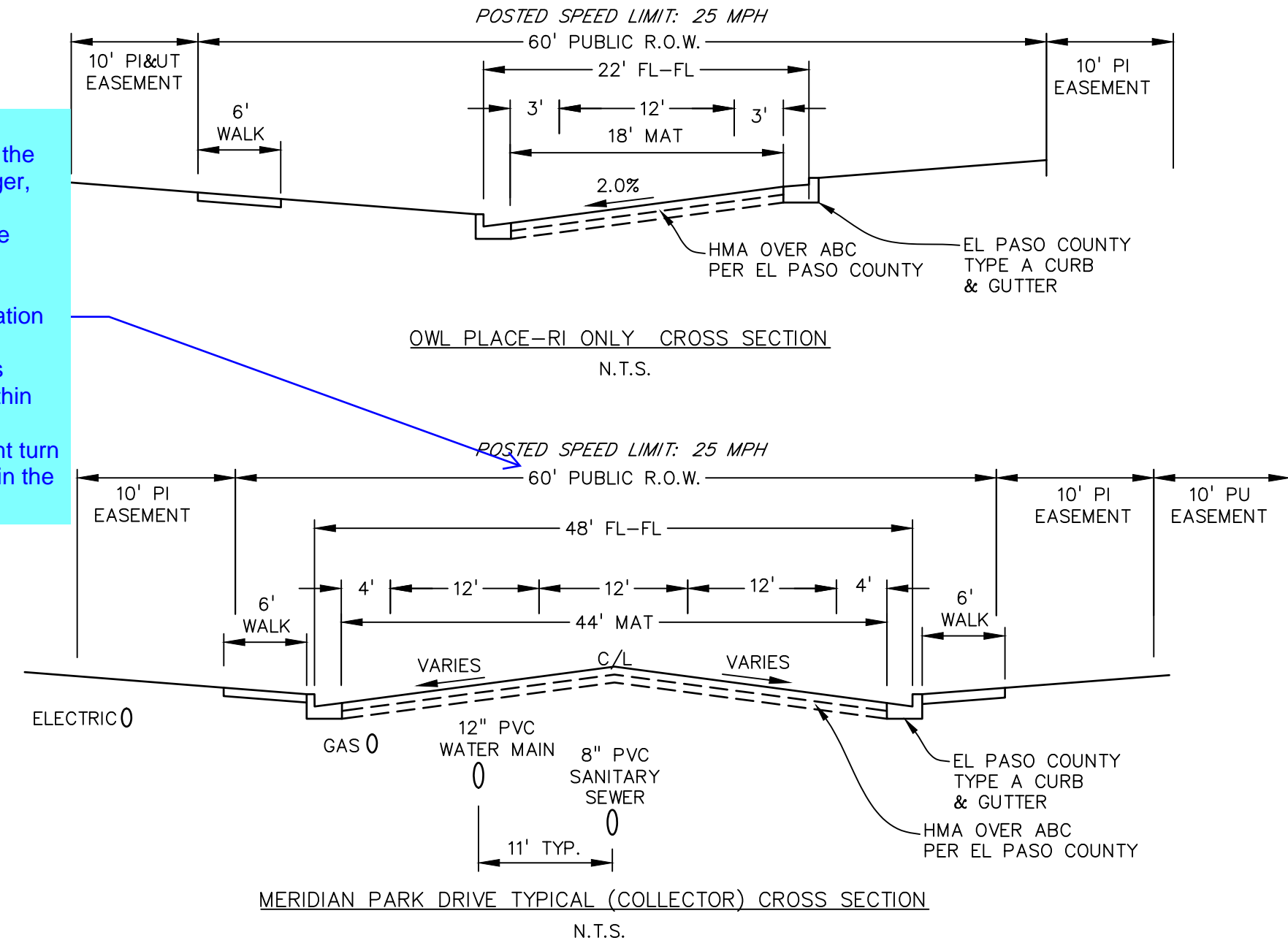
SHEET: 15 OF 18

811 Know what's below.
Call before you dig.
CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.
PCD FILE: VR2321

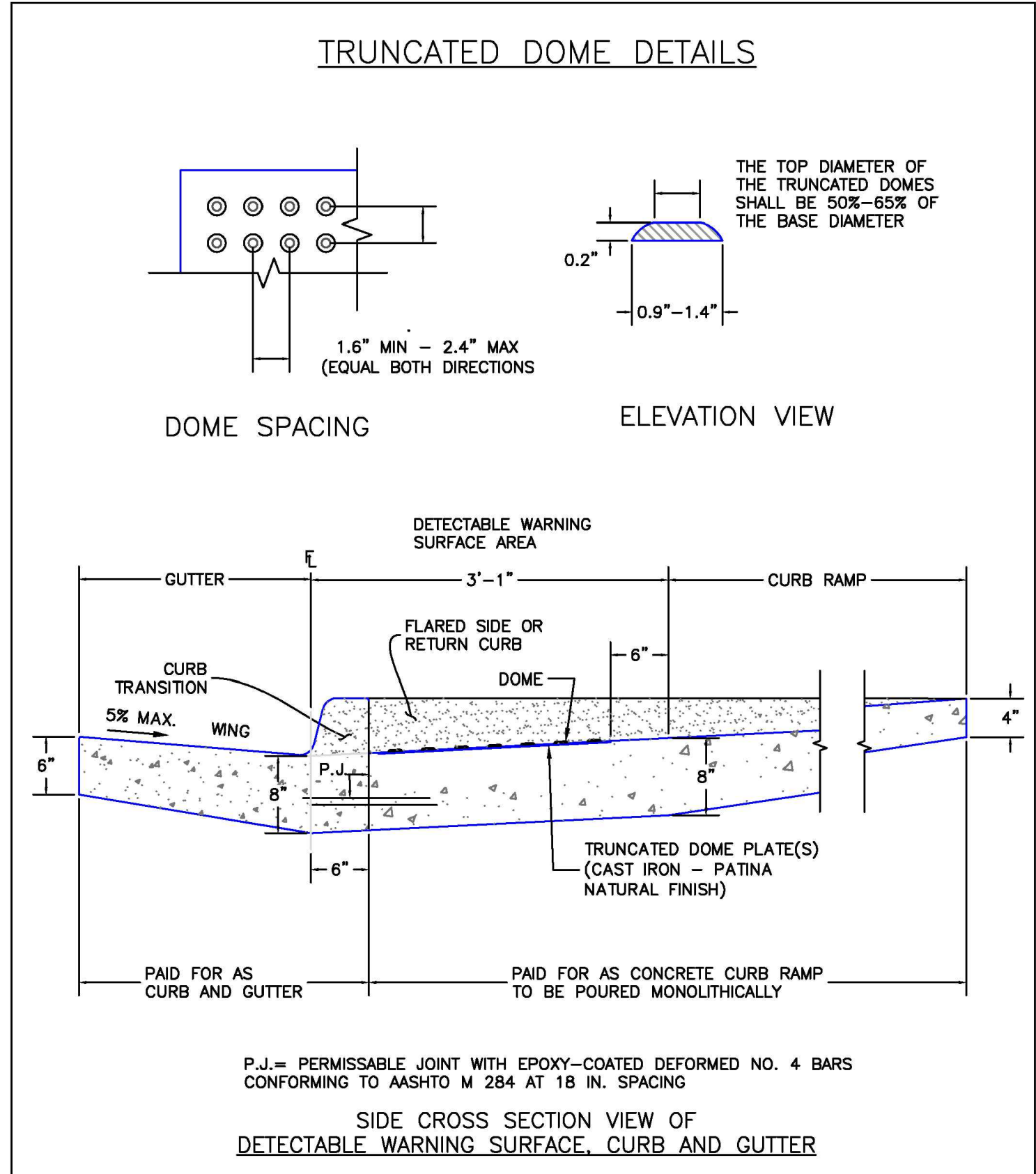


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|--|----------|---|------|----------|--|--|---|--|
| Date | Comments | | | | | | | |
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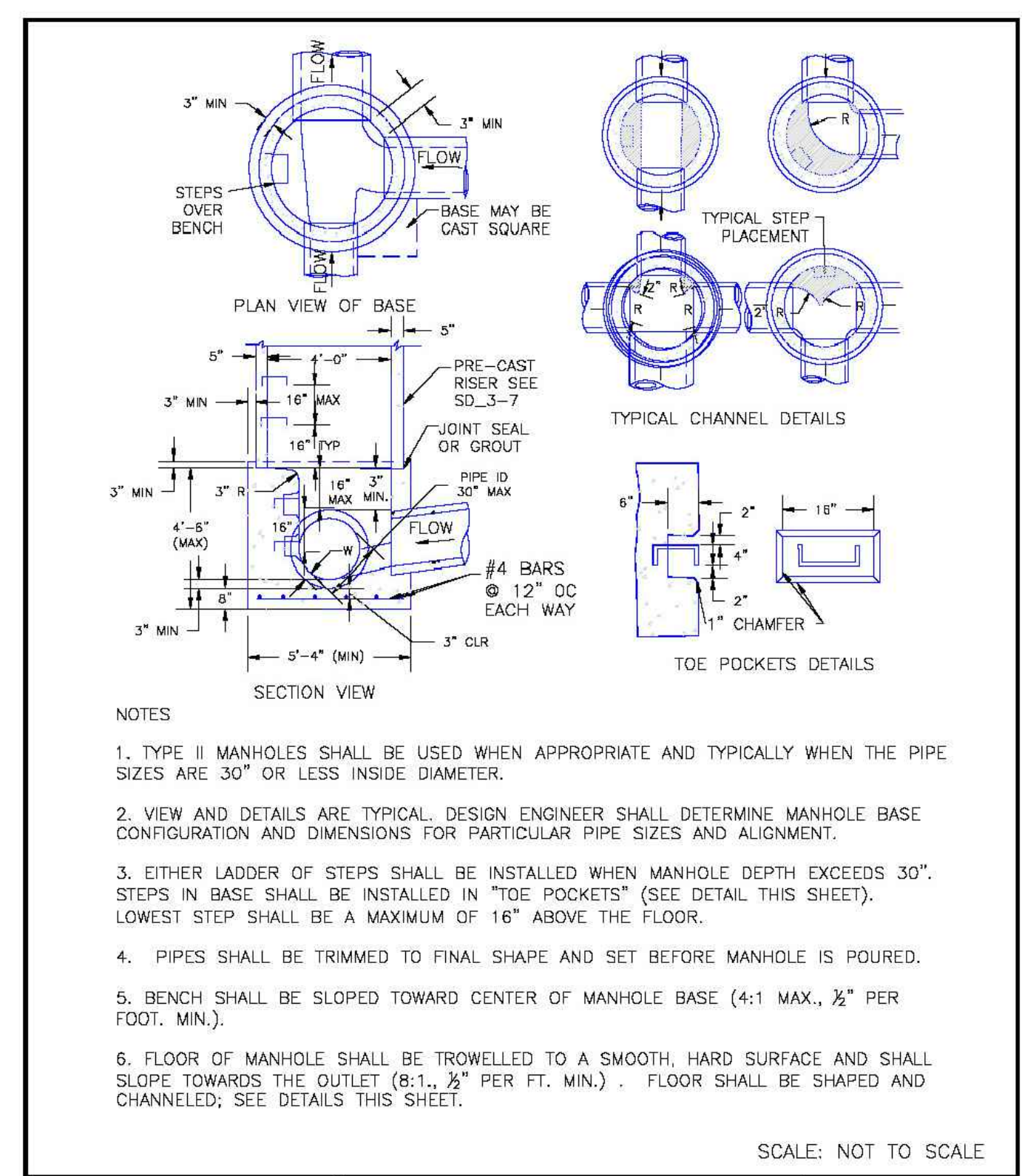
revise to 62" per conversations with the engineering manager, sidewalk shall be contained within the ROW.
 Also submit a deviation request for the non-standard cross section. Include within the deviation the omission of the right turn lanes as indicated in the TIS.



| | | |
|--|---|--|
| DATE APPROVED: 1/12/16 ANDRÉ P. BRACKIN DEPARTMENT OF TRANSPORTATION | Plowable Median Nose Detail Standard Drawing REVISION DATE: 1/12/16 FILE NAME: SD_2-22 | |
|--|---|--|



| | | |
|--|--|--|
| DATE APPROVED: 6/23/20 JENNIFER E. IRVINE DEPARTMENT OF PUBLIC WORKS | Detectable Warning Surface Details Standard Drawing REVISION DATE: 6/23/20 FILE NAME: SD_2-42 | |
|--|--|--|



| | | |
|---|--|--|
| DATE APPROVED: 11/10/04 ANDRÉ P. BRACKIN DEPARTMENT OF TRANSPORTATION | Storm Sewer Manhole Detail Type II Standard Drawing REVISION DATE: 11/10/04 FILE NAME: SD_3-2 | |
|---|--|--|

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 PCD FILE: VR2321

PREPARED BY:

 DREXEL, BARRELL & CO.
 Engineers • Surveyors
 101 SAWATCH ST., STE #100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
 (719)260-0887
 COLORADO SPRINGS • LAFAYETTE

CLIENT:
 BH RE INVESTMENTS, LLC
 450 N MCCLINTOCK DRIVE
 CHANDLER, AZ 85226
 (480) 313-2724

CONSTRUCTION PLANS FOR:
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 EL PASO COUNTY, COLORADO

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| RESUBMITTAL | 04/12/2024 |

DESIGNED BY: KGV
 DRAWN BY: KGV
 CHECKED BY: TDM
 FILE NAME: 21611-DTL

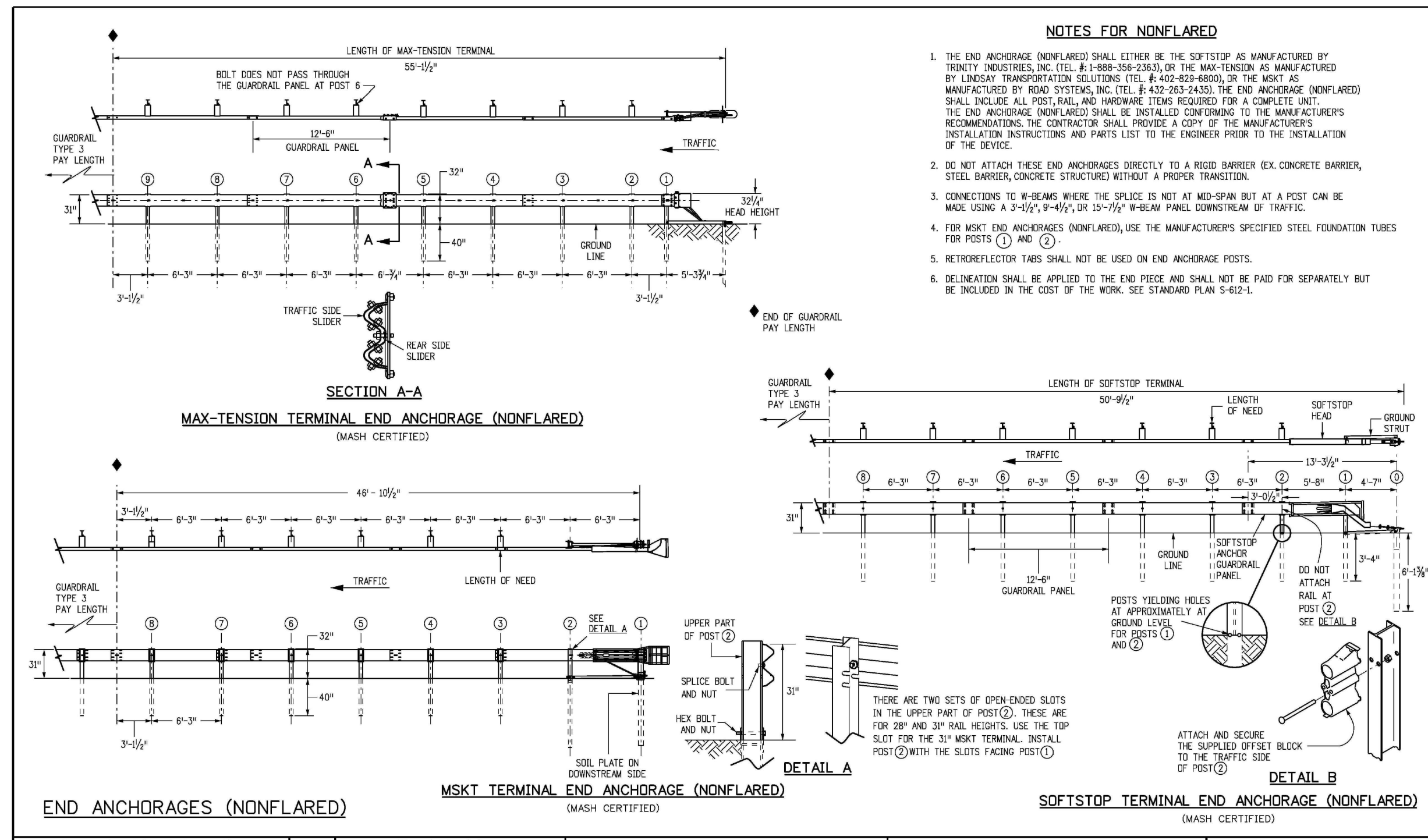
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: N/A
 VERTICAL: N/A

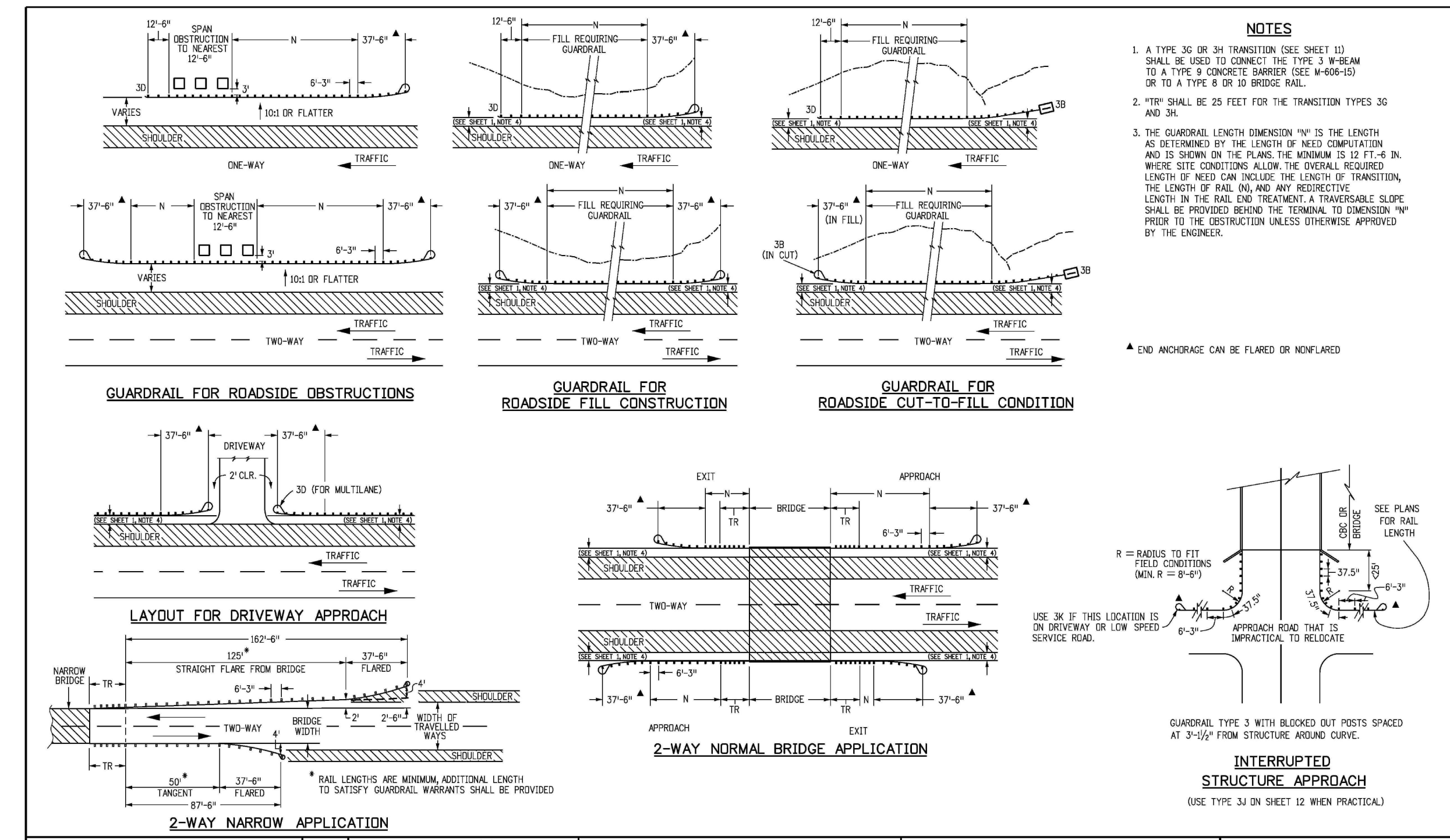
STANDARD DETAILS

PROJECT NO. 21611-01CSCV
 DRAWING NO.
C16

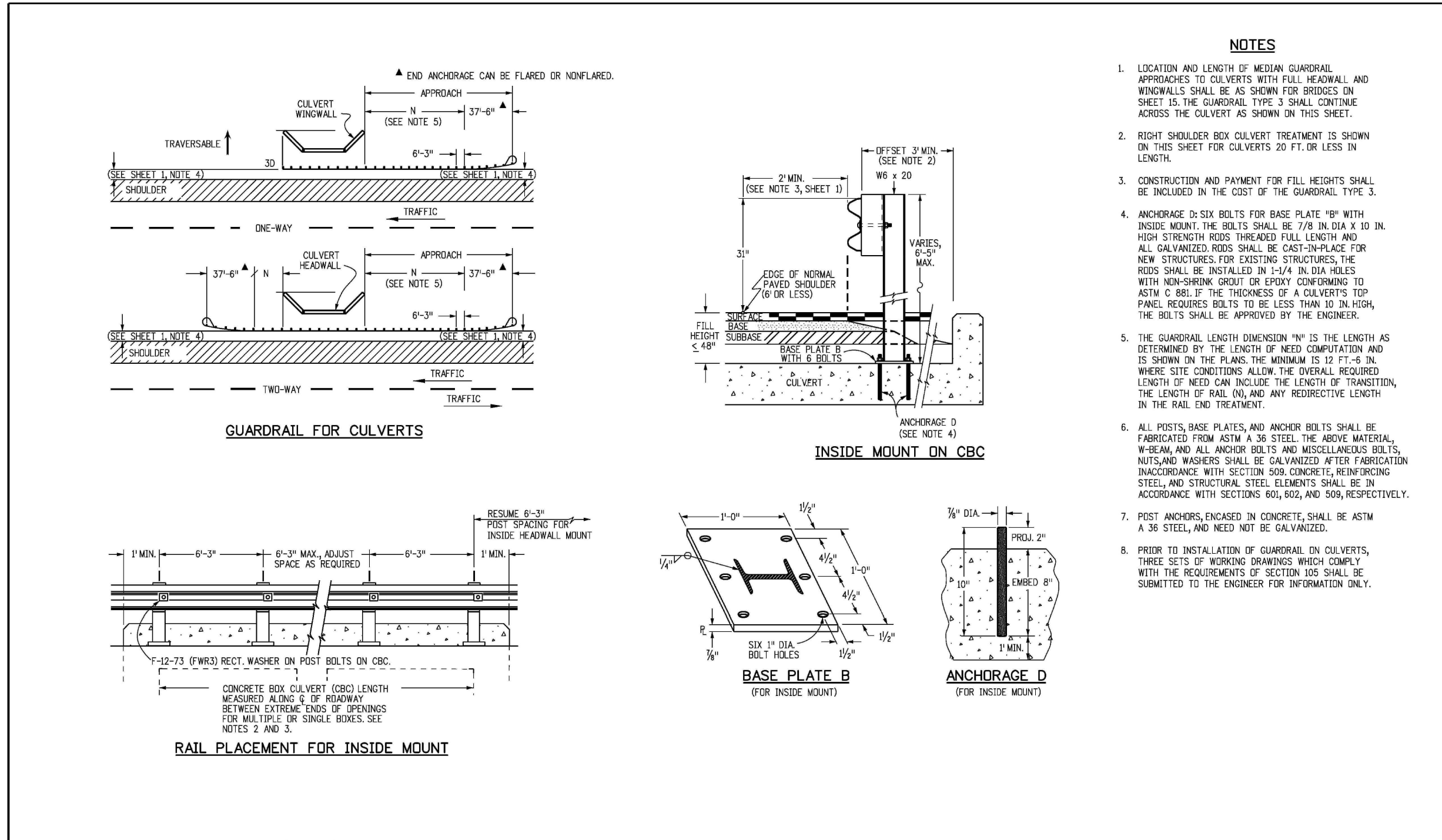
SHEET: 16 OF 18



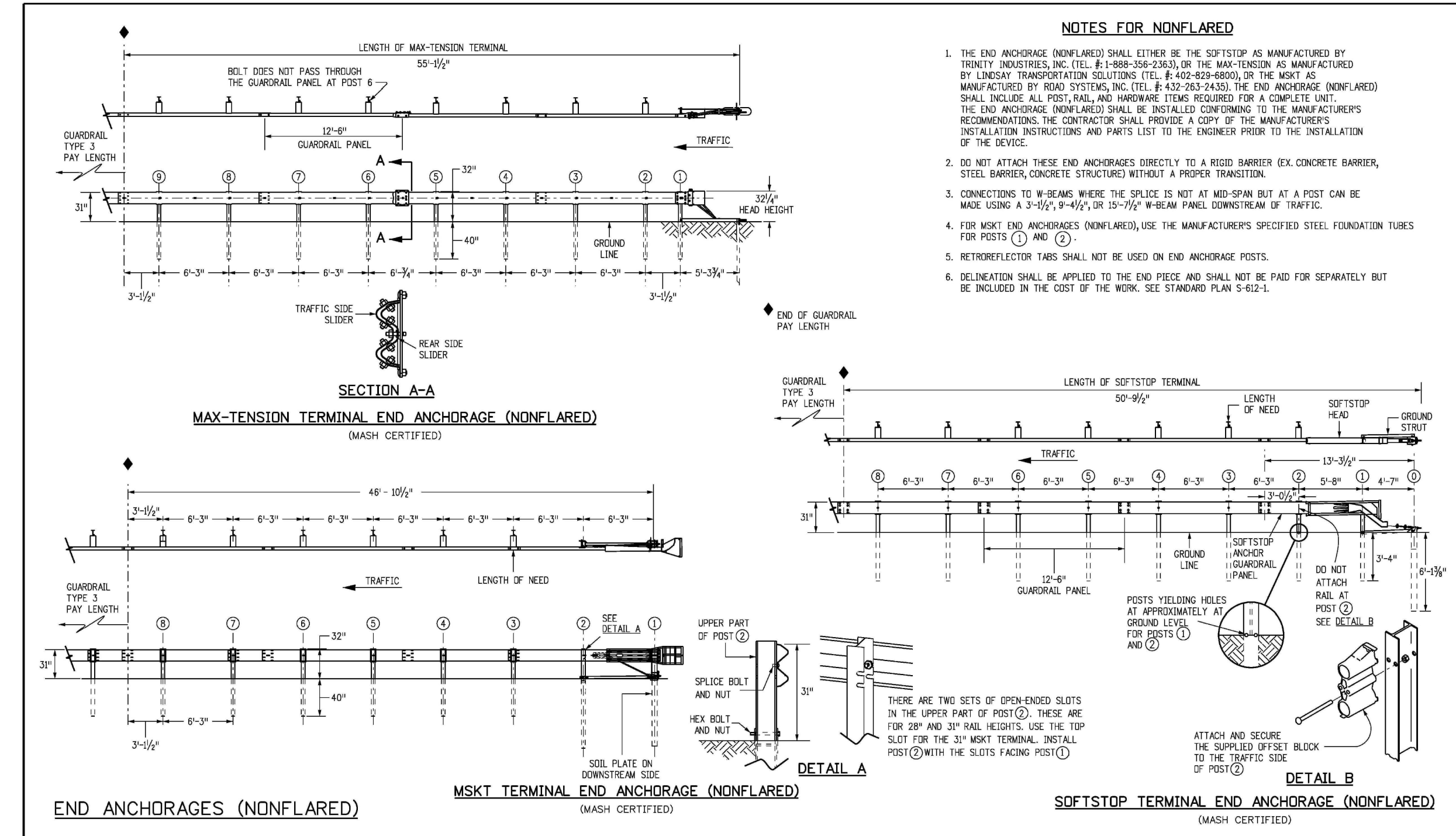
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| Computer File Information Creation Date: 07/31/19 Designer: JBK Last Modification Date: 03/05/20 Detailer: LTA CAD Ver: MicroStation V8 Scale: Not to Scale Units: English | Sheet Revisions Date: _____ Comments: _____ | Colorado Department of Transportation 2829 West Howard Place COOT HD, 3rd Floor Denver, CO 80204 Phone: 303-757-9021 FAX: 303-757-9888 Project Development Branch JBK | MIDWEST GUARDRAIL SYSTEM (MGS) TYPE 3 W-BEAM 31 INCHES Issued by the Project Development Branch July 31, 2019 | STANDARD PLAN NO. M-606-1 Standard Sheet No. 8 of 19 Project Sheet Number: _____ |
|--|--|---|---|---|



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| Computer File Information Creation Date: 07/31/19 Designer: JBK Last Modification Date: 03/05/20 Detailer: LTA CAD Ver: MicroStation V8 Scale: Not to Scale Units: English | Sheet Revisions Date: _____ Comments: _____ | Colorado Department of Transportation 2829 West Howard Place COOT HD, 3rd Floor Denver, CO 80204 Phone: 303-757-9021 FAX: 303-757-9888 Project Development Branch JBK | MIDWEST GUARDRAIL SYSTEM (MGS) TYPE 3 W-BEAM 31 INCHES Issued by the Project Development Branch July 31, 2019 | STANDARD PLAN NO. M-606-1 Standard Sheet No. 17 of 19 Project Sheet Number: _____ |
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PREPARED BY:



CLIENT:

BH RE INVESTMENTS, LLC
450 N MCCLINTOCK DRIVE
CHANDLER, AZ 85226
(480) 313-2724

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| CHECKED BY: | TDM |
| FILE NAME: | 21611-DTL |

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DRAWING SCALE:
HORIZONTAL: N/A
VERTICAL: N/A

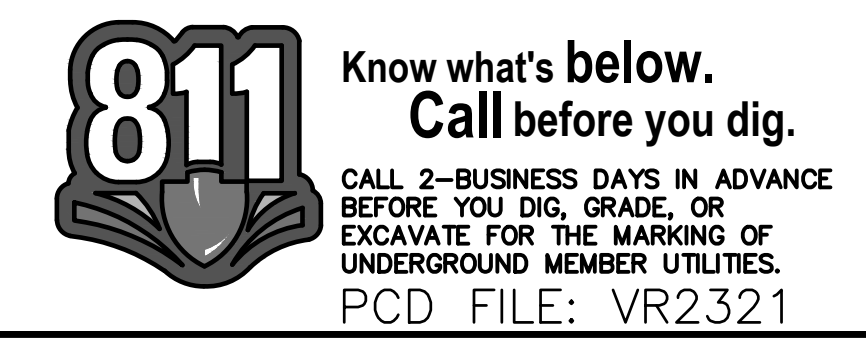
STANDARD DETAILS

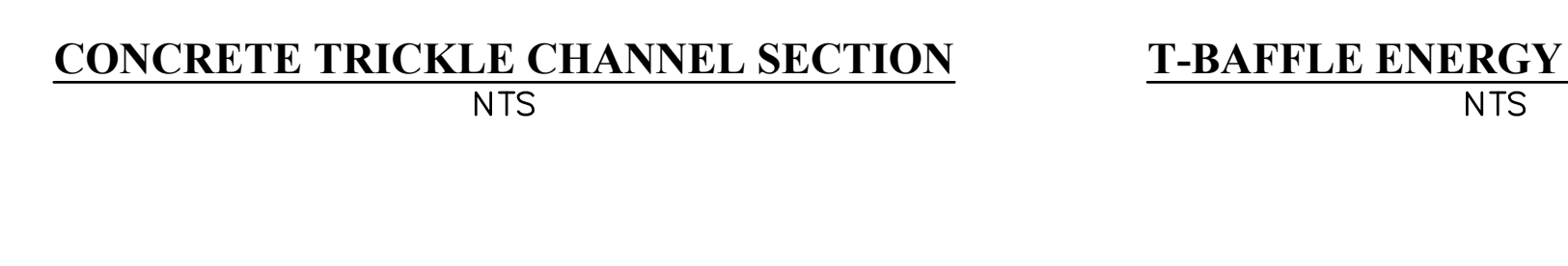
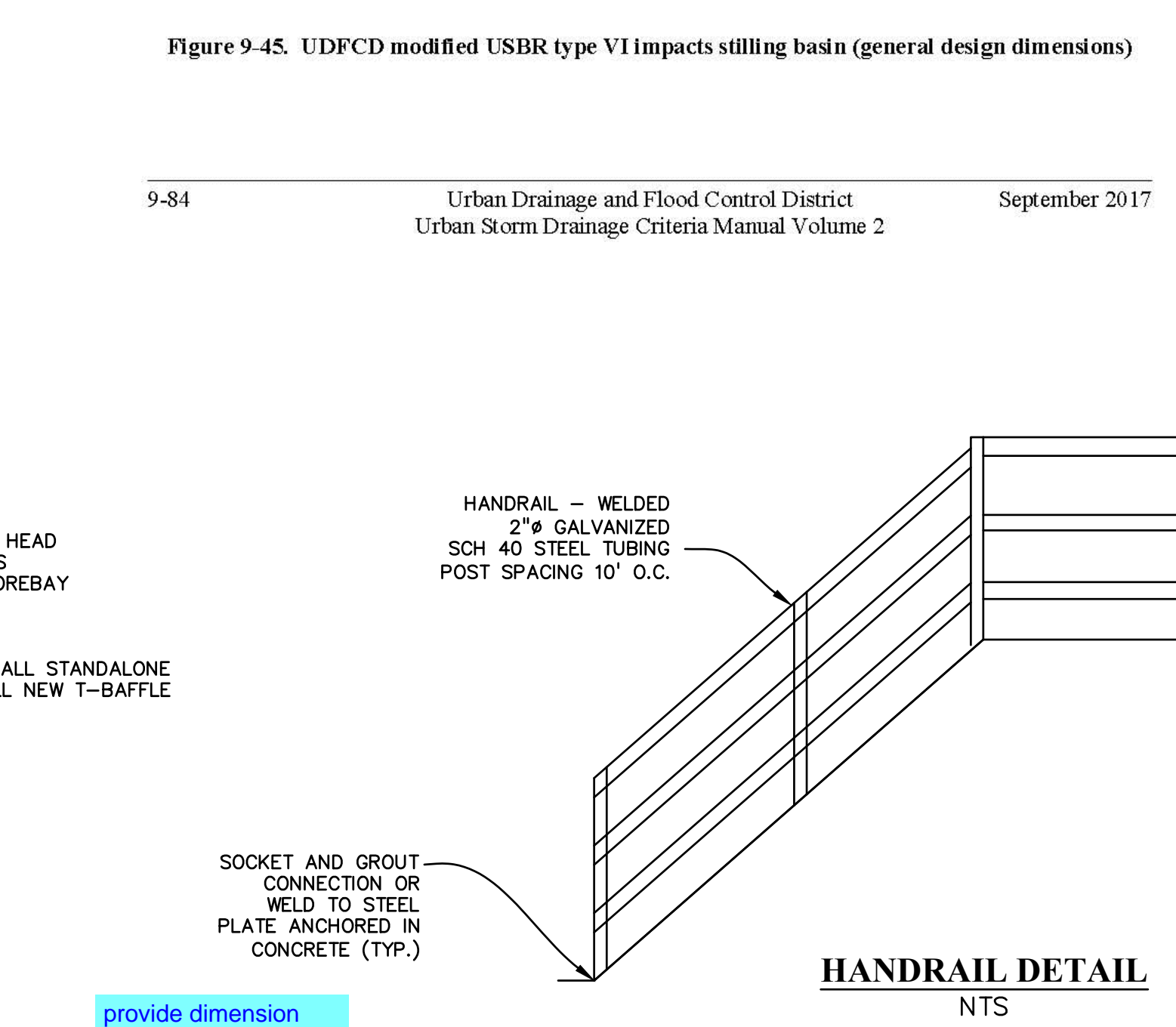
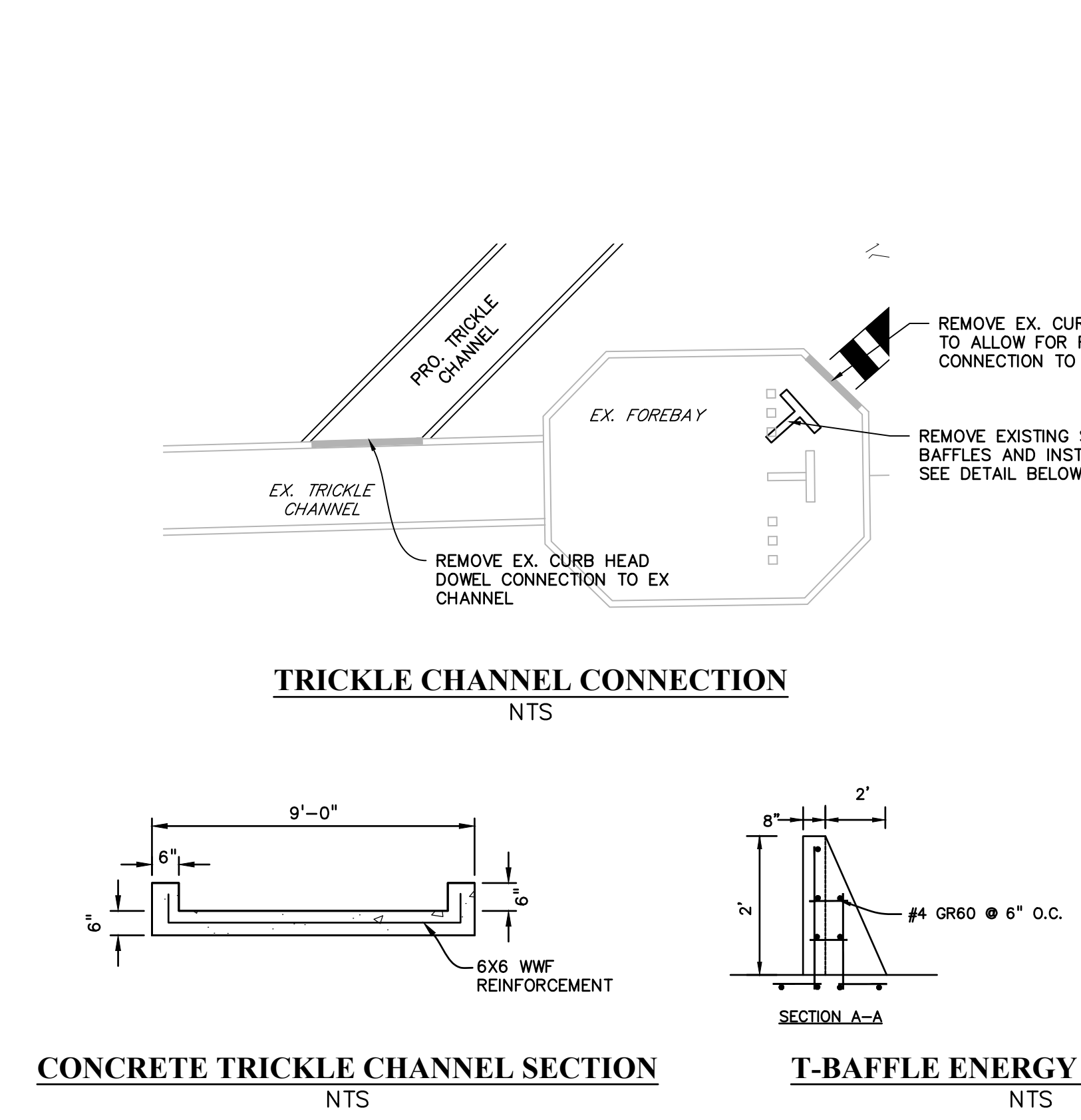
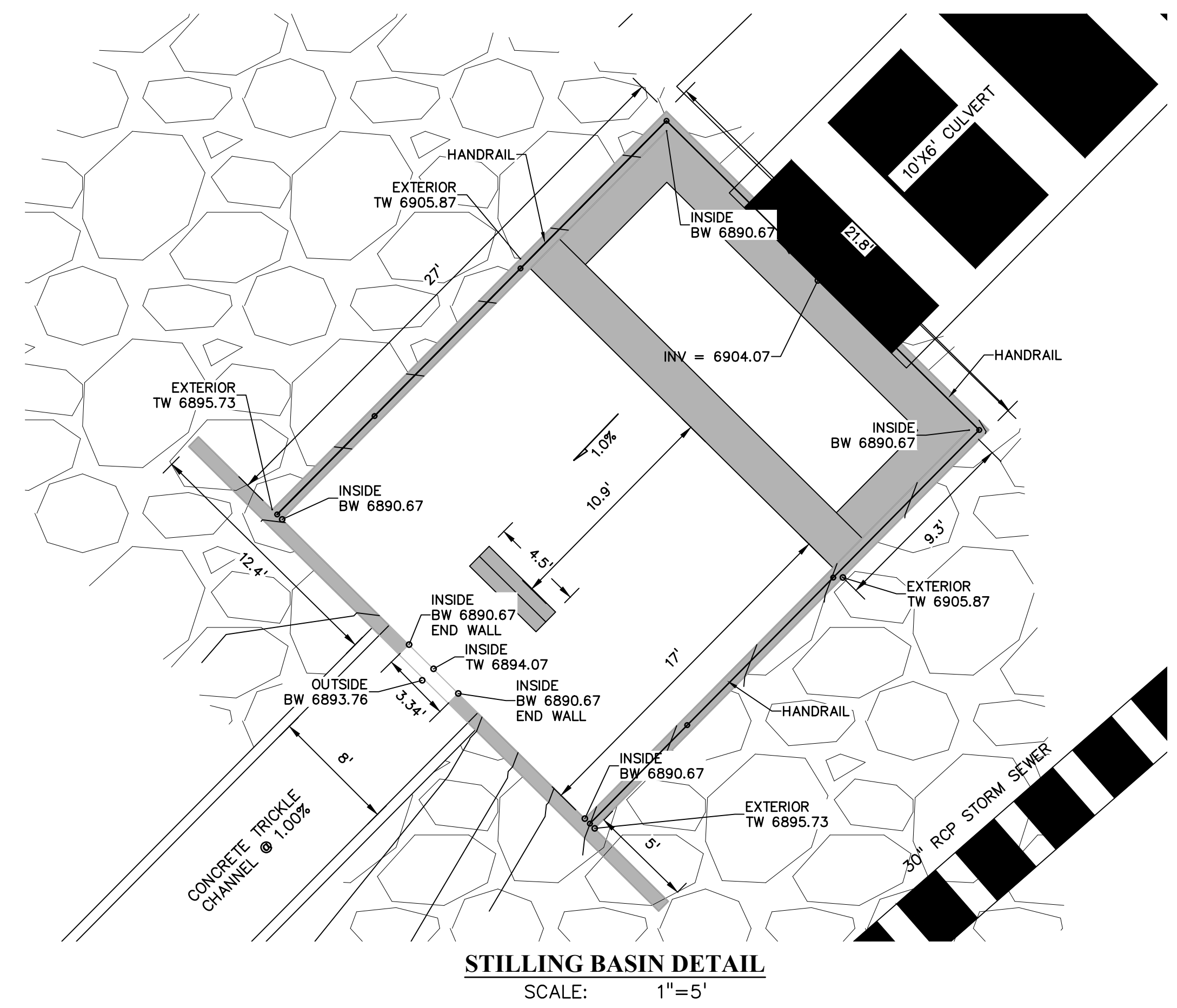
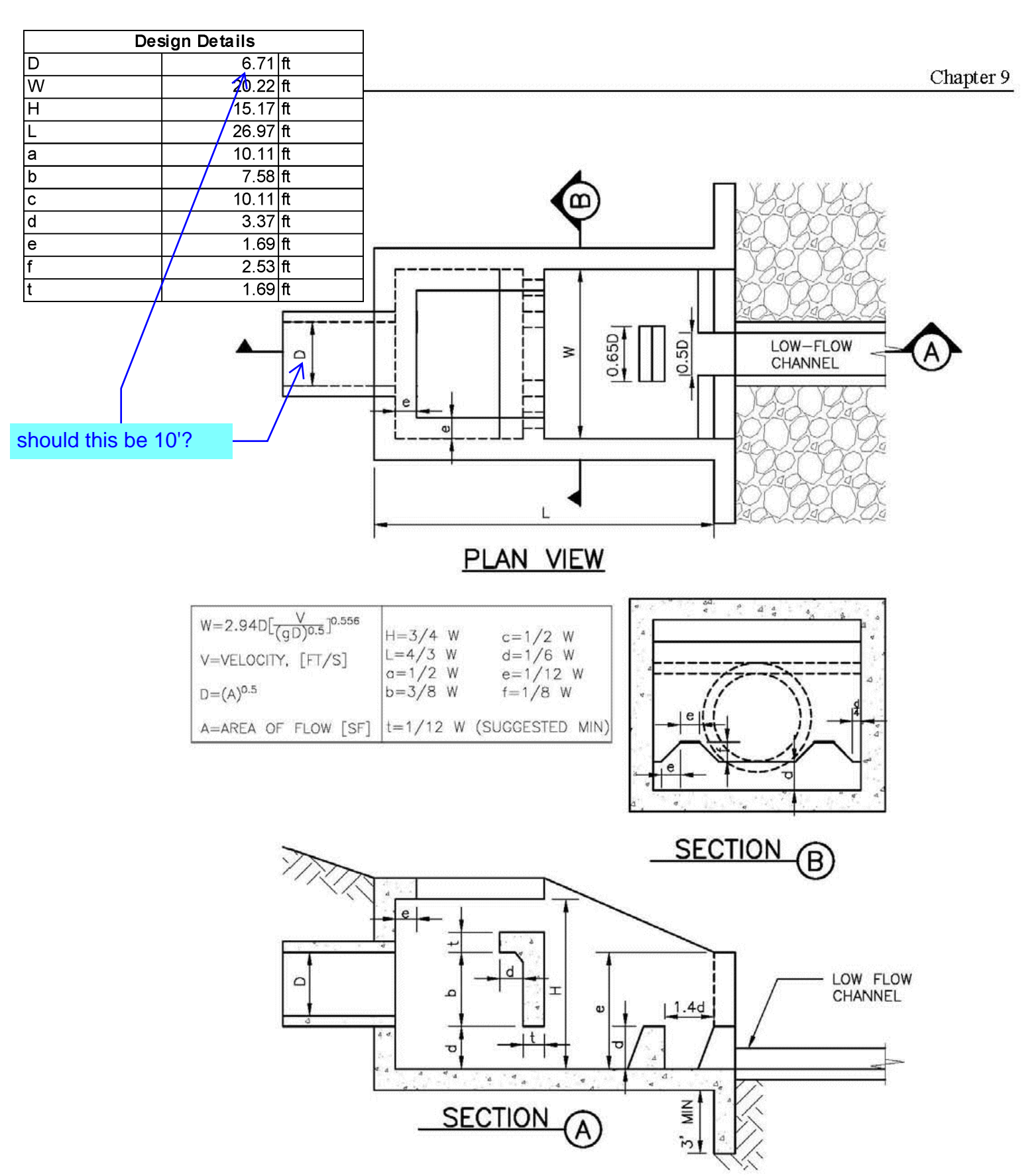
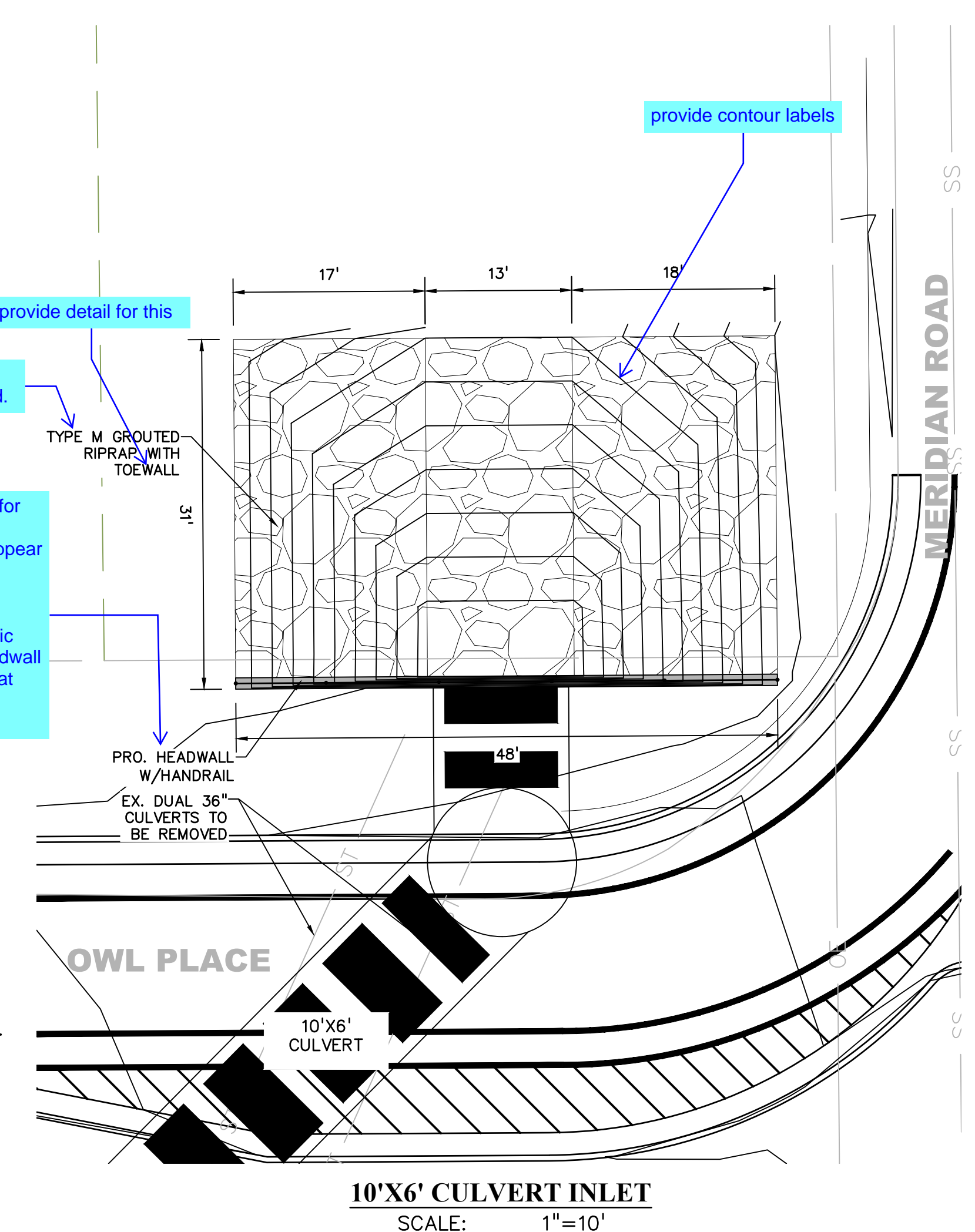
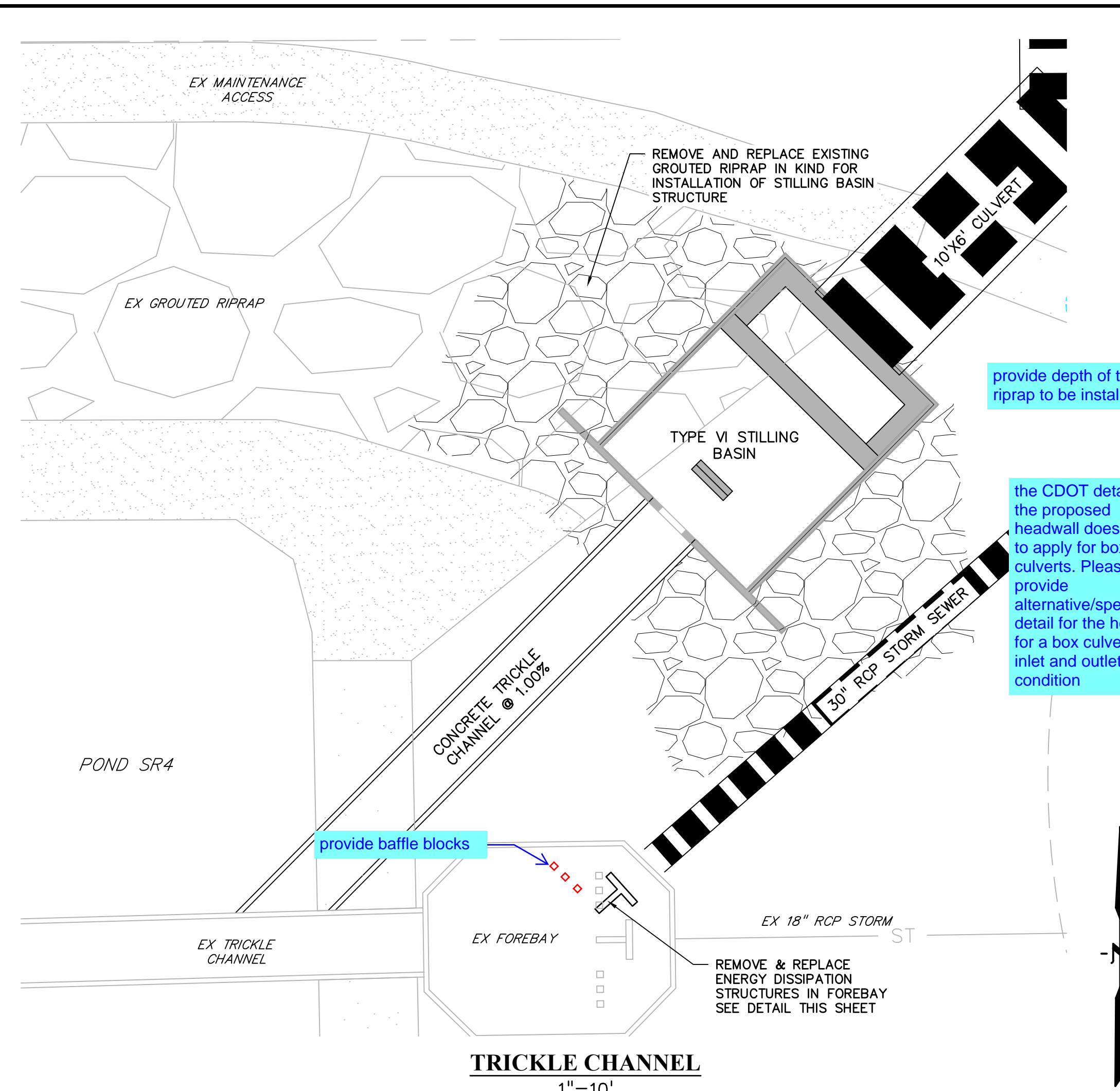
PROJECT NO. 21611-01CSCV

DRAWING NO.

C17

SHEET: 17 OF 18





9-84 Urban Drainage and Flood Control District September 2017
 Urban Storm Drainage Criteria Manual Volume 2

Figure 9-45. UD-FCO modified USBR type VI impactor (general design dimensions)

PREPARED BY:

 DREXEL, BARRELL & CO.
 Engineers-Surveyors
 101 SAWATCH ST., STE #100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
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| DESIGNED BY: | TDM |
| DRAWN BY: | KGW |
| CHECKED BY: | TDM |
| FILE NAME: | 21611-STD12 |

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.
 DRAWING SCALE:
 HORIZONTAL: 1" = 10'
 VERTICAL: N/A

STORM CULVERT DETAILS
 PROJECT NO. 21611-01CSCV
 DRAWING NO.

C18
 SHEET: 18 OF 18

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 PCD FILE: VR2321