

STATE HIGHWAY 105 / JACKSON CREEK PARKWAY - PHASE 2

COUNTY OF EL PASO, TOWN OF MONUMENT, STATE OF COLORADO

CONSTRUCTION PLANS

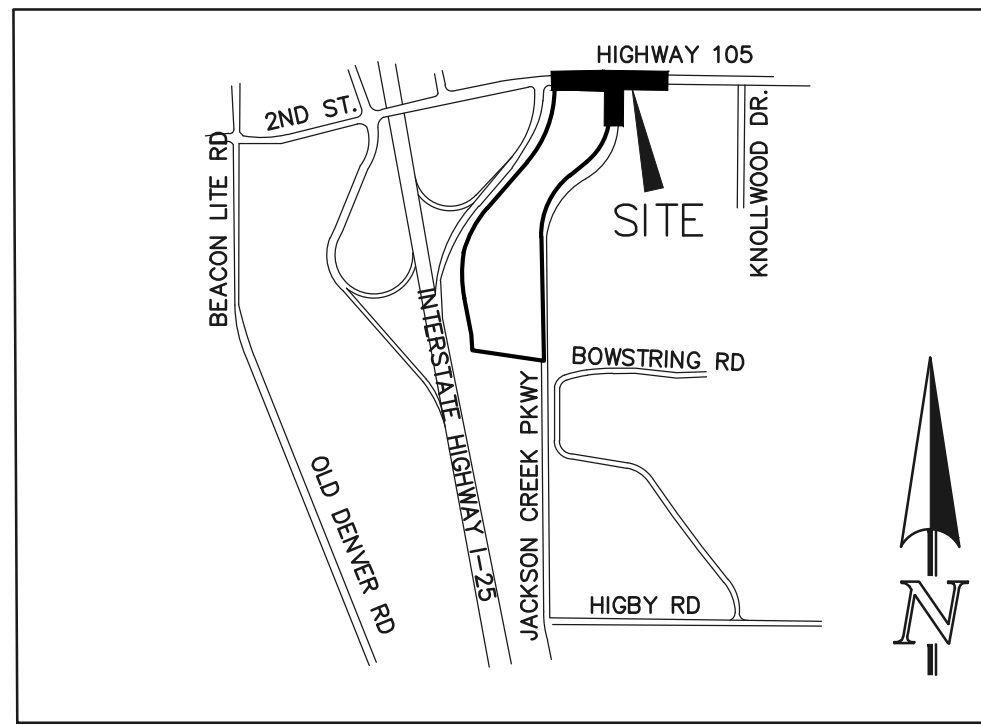
FEBRUARY 2023

Comments provided by DPW Development Services

Comments provided by DPW Capital Projects

EL PASO COUNTY GENERAL CONSTRUCTION NOTES:

- THE LOCATION OF EXISTING UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND MAY NOT INCLUDE ALL UTILITIES. THE EXCAVATION CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES.
- BEFORE COMMENCING ANY EXCAVATION, CALL 1-800-922-1987 FOR EXISTING UTILITY LOCATIONS.
- THE CONTRACTOR WILL TAKE THE NECESSARY PRECAUTIONS TO PROTECT EXISTING UTILITIES FROM DAMAGE DUE TO THIS OPERATION. ANY DAMAGE TO THE UTILITIES WILL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, AND ANY SERVICE DISRUPTION WILL BE SETTLED BY THE CONTRACTOR.
- ALL BACKFILL, SUB-BASE AND/OR BASE COURSE (CLASS 6) MATERIAL SHALL BE COMPACTED TO THE SOILS ENGINEER'S RECOMMENDATIONS, AND APPROVED BY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (PCD).
- ALL STATIONING IS CENTERLINE UNLESS OTHERWISE INDICATED. ALL ELEVATIONS ARE CENTERLINE UNLESS OTHERWISE INDICATED.
- THE CONTRACTOR SHALL REVEGETATE ALL DISTURBED AREAS AS SOON AS POSSIBLE AND EROSION CONTROL SHALL BE INSTALLED AND MAINTAINED IN A FUNCTIONAL MANNER AT ALL TIMES. DEVELOPER RESPONSIBLE FOR MAINTAINING DISTURBED AREAS UNTIL REVEGETATION IS COMPLETE.
- ALL DISTURBED PAVEMENT EDGES SHALL BE CUT TO NEAT LINES. REPAIR SHALL CONFORM TO THE EPC ECM APPENDIX K - 1.2C.
- ADDITIONAL EROSION CONTROL STRUCTURES MAY BE REQUIRED AT THE TIME OF CONSTRUCTION.
- BUILDING CONTRACTORS WILL BE RESPONSIBLE FOR CONSTRUCTING POSITIVE DRAINAGE AWAY FROM ALL STRUCTURES.
- ASPHALT THICKNESS AND BASE COURSE THICKNESS (COMPACTED) FOR ROADS SHALL BE PER DESIGN REPORT BY OWNER'S GEOTECHNICAL ENGINEER. OWNER'S GEOTECHNICAL ENGINEER TO BE ON SITE AT TIME OF ROAD CONSTRUCTION TO EVALUATE SOIL CONDITIONS AND DETERMINE IF ADDITIONAL MEASURES ARE NECESSARY TO ASSURE STABILITY OF THE NEW ROADS. PAVEMENT DESIGN SHALL BE APPROVED BY PLANNING AND COMMUNITY DEVELOPMENT PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL REVEGETATE ALL DISTURBED AREAS WITHIN 21 DAYS OF SUBSTANTIAL GRADING COMPLETION. EROSION CONTROL SHALL BE INSTALLED AND MAINTAINED IN A FUNCTIONAL MANNER AT ALL TIMES. DEVELOPER IS RESPONSIBLE FOR MAINTAINING DISTURBED AREAS UNTIL REVEGETATION IS COMPLETE.
- TYPE M RIP-RAP WITH 4" OF TYPE II GRANULAR BEDDING AND MIRAFI 180N OR EQUAL MAY BE SUBSTITUTED WHERE TYPE L RIP-RAP WITH MIRAFI FW 700 OR EQUAL IS SPECIFIED
- ALL MATERIALS AND INSTALLATION PROCEDURES SHALL BE IN COMPLIANCE WITH ANY AND ALL APPLICABLE EL PASO COUNTY STANDARDS.



VICINITY MAP
N.T.S.

SHEET INDEX

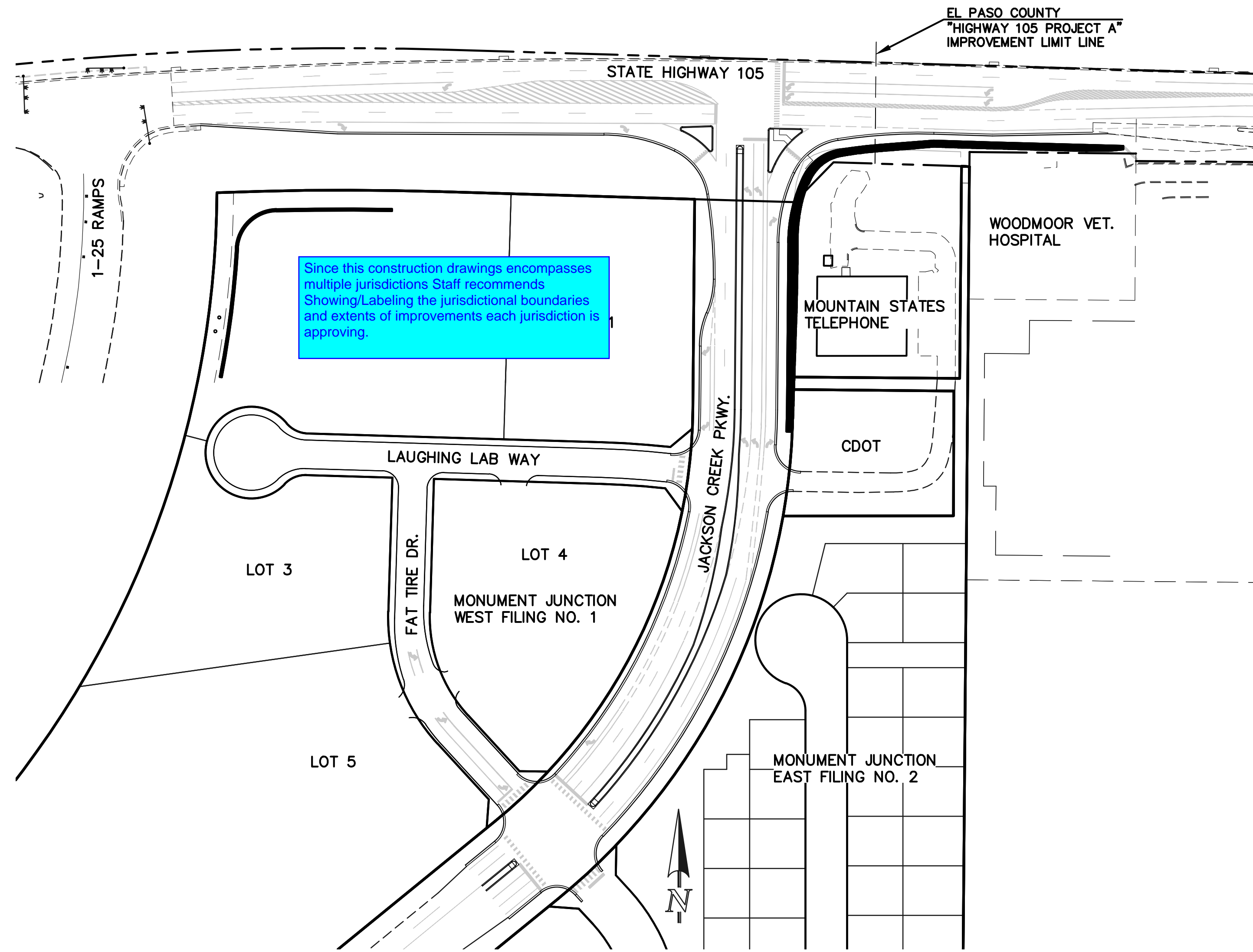
SHEET 1	TITLE SHEET
SHEET 2	STANDARD NOTES
SHEET 3	CDOT STANDARD PLAN LIST
SHEET 4	DEMO PLAN
SHEET 5	EROSION CONTROL PLAN
SHEET 6	RETAINING WALL PLAN
SHEET 7	RETAINING WALL DETAILS
SHEETS 8 THRU 9	ROADWAY SECTION PLANS
SHEETS 10 THRU 13	ROADWAY IMPROVEMENT PLAN & PROFILE
SHEET 14	ROADWAY STRIPING PLAN
SHEET 15	ROADWAY SIGNAGE PLAN
SHEETS 16 THRU 17	STORMWATER QUALITY POND PLANS
SHEETS 18 THRU 19	STORM SEWER PLAN & PROFILES
SHEETS 20	DETAIL SHEET
SHEETS 21 THRU 23	ROADWAY SIGNAL DESIGN PLAN (FHU)

STANDARD NOTES FOR EL PASO COUNTY CONSTRUCTION PLANS:

- ALL DRAINAGE AND ROADWAY CONSTRUCTION SHALL MEET THE STANDARDS AND SPECIFICATIONS OF THE CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2, AND THE EL PASO COUNTY ENGINEERING CRITERIA MANUAL.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION AND FIELD NOTIFICATION OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, BEFORE BEGINNING CONSTRUCTION. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CALL 811 TO CONTACT THE UTILITY NOTIFICATION CENTER OF COLORADO (UNCC).
- CONTRACTOR SHALL KEEP A COPY OF THESE APPROVED PLANS, THE GRADING AND EROSION CONTROL PLAN, THE STORMWATER MANAGEMENT PLAN (SWMP), THE SOILS AND GEOTECHNICAL REPORT, AND THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES, INCLUDING THE FOLLOWING:
 - EL PASO COUNTY ENGINEERING CRITERIA MANUAL (ECM)
 - CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2
 - COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
 - CDOT M & S STANDARDS
- NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS FROM REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING. ANY MODIFICATIONS NECESSARY TO MEET CRITERIA AFTER-THE-FACT WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- IT IS THE DESIGN ENGINEER'S RESPONSIBILITY TO ACCURATELY SHOW EXISTING CONDITIONS, BOTH ONSITE AND OFFSITE, ON THE CONSTRUCTION PLANS. ANY MODIFICATIONS NECESSARY DUE TO CONFLICTS, OMISSIONS, OR CHANGED CONDITIONS WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT - INSPECTIONS, PRIOR TO STARTING CONSTRUCTION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE REQUIREMENTS OF ALL JURISDICTIONAL AGENCIES AND TO OBTAIN ALL REQUIRED PERMITS, INCLUDING BUT NOT LIMITED TO EL PASO COUNTY EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQCP), REGIONAL BUILDING FLOODPLAIN DEVELOPMENT PERMIT, U.S. ARMY CORPS OF ENGINEERS-ISSUED 401 AND/OR 404 PERMITS, AND COUNTY AND STATE FUGITIVE DUST PERMITS.
- CONTRACTOR SHALL NOT DEVIATE FROM THE PLANS WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE DESIGN ENGINEER AND PCD. CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY UPON DISCOVERY OF ANY ERRORS OR INCONSISTENCIES.
- ALL STORM DRAIN PIPE SHALL BE CLASS III RCP OR CLASS IV WITH WATER TIGHT JOINTS WHERE CALLED OUT, UNLESS OTHERWISE NOTED AND APPROVED BY PCD.
- CONTRACTOR SHALL COORDINATE GEOTECHNICAL TESTING PER ECM STANDARDS. PAVEMENT DESIGN SHALL BE APPROVED BY EL PASO COUNTY PCD PRIOR TO PLACEMENT OF CURB AND GUTTER AND PAVEMENT.
- ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE AT APPROVED CONSTRUCTION ACCESS POINTS.
- SIGHT VISIBILITY TRIANGLES AS IDENTIFIED IN THE PLANS SHALL BE PROVIDED AT ALL INTERSECTIONS. OBSTRUCTIONS GREATER THAN 18 INCHES ABOVE FLOWLINE ARE NOT ALLOWED WITHIN SIGHT TRIANGLES.
- SIGNING AND STRIPING SHALL COMPLY WITH EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS AND MUTCD CRITERIA.
- CONTRACTOR SHALL OBTAIN ANY PERMITS REQUIRED BY EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS, INCLUDING WORK WITHIN THE RIGHT-OF-WAY AND SPECIAL TRANSPORT PERMITS.
- THE LIMITS OF CONSTRUCTION SHALL REMAIN WITHIN THE PROPERTY LINE UNLESS OTHERWISE NOTED. THE OWNER/DEVELOPER SHALL OBTAIN WRITTEN PERMISSION AND EASEMENTS, WHERE REQUIRED, FROM ADJOINING PROPERTY OWNER(S) PRIOR TO ANY OFF-SITE DISTURBANCE, GRADING, OR CONSTRUCTION.

EL PASO COUNTY SIGNING AND STRIPING NOTES:

- ALL SIGNS AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY A METHOD THAT DOES NOT MATERIALLY DAMAGE THE PAVEMENT. THE PAVEMENT MARKINGS SHALL BE REMOVED TO THE EXTENT THAT THEY WILL NOT BE VISIBLE UNDER DAY OR NIGHT CONDITIONS. AT NO TIME WILL IT BE ACCEPTABLE TO PAINT OVER EXISTING PAVEMENT MARKINGS.
- ANY DEVIATION FROM THE STRIPING AND SIGNING PLAN SHALL BE APPROVED BY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT.
- ALL SIGNS SHOWN ON THE SIGNING AND STRIPING PLAN SHALL BE NEW SIGNS. EXISTING SIGNS MAY REMAIN OR BE REUSED IF THEY MEET CURRENT EL PASO COUNTY AND MUTCD STANDARDS.
- STREET NAME AND REGULATORY STOP SIGNS SHALL BE ON THE SAME POST AT INTERSECTIONS.
- ALL REMOVED SIGNS SHALL BE DISPOSED OF IN A PROPER MANNER BY THE CONTRACTOR.
- ALL STREET NAME SIGNS SHALL HAVE "D" SERIES LETTERS, WITH LOCAL ROADWAY SIGNS BEING 4" UPPER-LOWER CASE LETTERING ON 8" BLANK AND NON-LOCAL ROADWAY SIGNS BEING 6" LETTERING, UPPER-LOWER CASE ON 12" BLANK, WITH A WHITE BORDER THAT IS NOT RECESSED. MULTI-LANE ROADWAYS WITH SPEED LIMITS OF 40 MPH OR HIGHER SHALL HAVE 8" UPPER-LOWER CASE LETTERING ON 18" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. THE WIDTH OF THE NON-RECESSED WHITE BORDERS SHALL MATCH PAGE 255 OF THE 2012 MUTCD "STANDARD HIGHWAY SIGNS."
- ALL TRAFFIC SIGNS SHALL HAVE A MINIMUM HIGH INTENSITY PRISMATIC GRADE SHEETING.
- ALL LOCAL RESIDENTIAL STREET SIGNS SHALL BE MOUNTED ON A 1.75" X 1.75" SQUARE TUBE SIGN POST AND STUB POST BASE. FOR OTHER APPLICATIONS, REFER TO THE CDOT STANDARD S-614-8 REGARDING USE OF THE P2 TUBULAR STEEL POST SUBBASE DESIGN.
- ALL SIGNS SHALL BE SINGLE SHEET ALUMINUM WITH 0.100" MINIMUM THICKNESS.
- ALL LIMIT LINES/STOP LINES, CROSSWALK LINES, PAVEMENT LEGENDS, AND ARROWS SHALL BE A MINIMUM 125 MIL THICKNESS PREFORMED THERMOPLASTIC PAVEMENT MARKINGS WITH TAPERED LEADING EDGES PER CDOT STANDARD S-627-1. WORD AND SYMBOL MARKINGS SHALL BE THE NARROW TYPE. STOP BARS SHALL BE 24" IN WIDTH. CROSSWALKS LINES SHALL BE 12" WIDE AND 8' LONG PER CDOT S-627-1.
- ALL LONGITUDINAL LINES SHALL BE A MINIMUM 15MIL THICKNESS EPOXY PAINT. ALL NON-LOCAL RESIDENTIAL ROADWAYS SHALL INCLUDE BOTH RIGHT AND LEFT EDGE LINE STRIPING AND ANY ADDITIONAL STRIPING AS REQUIRED BY CDOT S-627-1.
- THE CONTRACTOR SHALL NOTIFY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (719) 520-6819 PRIOR TO AND UPON COMPLETION OF SIGNING AND STRIPING.
- THE CONTRACTOR SHALL OBTAIN A WORK IN THE RIGHT OF WAY PERMIT FROM THE EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS (DPW) PRIOR TO ANY SIGNAGE OR STRIPING WORK WITHIN AN EXISTING EL PASO COUNTY ROADWAY.



Since this construction drawings encompasses multiple jurisdictions Staff recommends Showing/Labeling the jurisdictional boundaries and extents of improvements each jurisdiction is approving.

Change dimensions to 24" & 8' Reference reference to CDOT detail.

KEY MAP
1" = 100'

EDARP Project Number: CDR237

AGENCIES

DEVELOPER:	ELITE PROPERTIES OF AMERICA, INC. 2138 FLYING HORSE CLUB DR COLORADO SPRINGS, CO 80921 MR. STEVE SCHLOSSER, (719) 592-9333
CIVIL ENGINEER:	CLASSIC CONSULTING 619 N. CASCADE AVENUE, SUITE 200 COLORADO SPRINGS, COLORADO 80903 MR. MARC A. WHORTON, P.E. (719) 785-2802
COLORADO DEPARTMENT OF TRANSPORTATION, PERMIT REVIEW:	REGION 2 - ACCESS 5615 WILLS BOULEVARD PUEBLO, COLORADO 81008 MR. ART GONZALES (719) 546-5732
COLORADO DEPARTMENT OF TRANSPORTATION, NORTH PROGRAM	REGION 2 - NORTH PROGRAM 1480 QUAIL LAKE LOOP ROAD, SUITE A COLORADO SPRINGS, COLORADO 80906 MR. ANDY STECKLEIN, P.E. (719) 227-3264
COLORADO DEPARTMENT OF TRANSPORTATION, ENVIRONMENTAL / ROW	REGION 2 - ACCESS 5615 WILLS BOULEVARD PUEBLO, COLORADO 81008 MR. GABRIEL COSYLEON (719) 562-5528 (ENV. MANAGER) MR. WAYNE TRUJILLO (719) 227-5725 (ROW MANAGER)
COUNTY ENGINEERING:	EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT 2880 INTERNATIONAL CIRCLE, SUITE 110 COLORADO SPRINGS, COLORADO 80910 MR. GILBERT LAFORCE (719) 520-7945
GAS COMPANY:	BLACKHILLS ENERGY 37 WIDEFIELD BOULEVARD WIDEFIELD, COLORADO 80911 MR. GEORGE M. PETERSON, (719) 392-3491
ELECTRIC COMPANY:	MOUNTAIN VIEW ELECTRIC P.O. BOX 1600 LIMON, COLORADO 80828 MR. LES ULFERS, (719) 495-2283
TELEPHONE COMPANY:	U.S. WEST COMMUNICATIONS (LOCATORS) (800) 922-1987

APPROVALS:

DESIGN ENGINEER'S STATEMENT:
THESE DETAILED PLANS AND SPECIFICATIONS WERE PREPARED UNDER MY DIRECTION AND SUPERVISION. SAID PLANS AND SPECIFICATIONS HAVE BEEN PREPARED ACCORDING TO THE CRITERIA ESTABLISHED BY THE COUNTY FOR DETAILED ROADWAY, DRAINAGE, GRADING AND EROSION CONTROL PLANS AND SPECIFICATIONS, AND SAID PLANS AND SPECIFICATIONS ARE IN CONFORMITY WITH APPLICABLE MASTER DRAINAGE PLANS AND MASTER TRANSPORTATION PLANS. SAID PLANS AND SPECIFICATIONS MEET THE PURPOSES FOR WHICH THE PARTICULAR ROADWAY AND DRAINAGE FACILITIES ARE DESIGNED AND ARE CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. I ACCEPT RESPONSIBILITY FOR ANY LIABILITY DIRECTLY CAUSED BY THE NEGLIGENT ACTS, ERRORS, OR OMISSIONS ON MY PART IN PREPARATION OF THESE DETAILED PLANS AND SPECIFICATIONS.

MARC A WHORTON, COLORADO P.E. #37155 DATE _____
FOR AND ON THE BEHALF OF CLASSIC CONSULTING ENGINEERS & SURVEYORS

OWNER/DEVELOPER'S STATEMENT:
I, THE OWNER/DEVELOPER HAVE READ AND WILL COMPLY WITH THE REQUIREMENTS OF THE GRADING AND EROSION CONTROL PLAN AND ALL OF THE REQUIREMENTS SPECIFIED IN THESE DETAILED PLANS AND SPECIFICATIONS.

STEVE SCHLOSSER DATE _____

TOWN OF MONUMENT APPROVAL:
THESE PLANS HAVE BEEN REVIEWED BY TOWN OF MONUMENT STAFF AND FOUND TO BE IN GENERAL COMPLIANCE WITH TOWN STANDARDS. IT IS THE RESPONSIBILITY OF THE SITE ENGINEER AND GENERAL CONTRACTOR TO ENSURE CONSTRUCTION IS IN COMPLIANCE WITH THESE PLANS AND IN CONFORMANCE WITH THE TOWN OF MONUMENT ORDINANCES AND REGULATIONS. THE SPECIFICATIONS, STANDARDS, AND INTENT OF THE TOWN OF MONUMENT ORDINANCES, REGULATIONS AND THE CITY OF COLORADO SPRINGS DRAINAGE CRITERIA MANUALS VOLUMES 1 AND 2 SHALL PREVAIL IN ANY INSTANCES WHERE THESE PLANS DIFFER FROM THOSE REQUIREMENTS. THE DEVELOPMENT SERVICES DEPARTMENT SHALL BE NOTIFIED IF ANY CHANGES NEED TO BE MADE.

SIGNED _____ DATE _____
TOWN OF MONUMENT

EL PASO COUNTY:
COUNTY PLAN REVIEW IS PROVIDED ONLY FOR GENERAL CONFORMANCE WITH COUNTY DESIGN CRITERIA. THE COUNTY IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND/OR ELEVATIONS WHICH SHALL BE CONFIRMED AT THE JOB SITE. THE COUNTY THROUGH THE APPROVAL OF THIS DOCUMENT ASSUMES NO RESPONSIBILITY FOR COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT.

FILED IN ACCORDANCE WITH THE REQUIREMENTS OF THE EL PASO COUNTY LAND DEVELOPMENT CODE, DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2 AND ENGINEERING CRITERIA MANUAL AS AMENDED.
IN ACCORDANCE WITH WITH ECM SECTION 1.12, THESE CONSTRUCTION DOCUMENTS WILL BE VALID FOR CONSTRUCTION FOR A PERIOD OF 2 YEARS FROM THE DATE SIGNED BY THE EL PASO COUNTY ENGINEER. IF CONSTRUCTION HAS NOT STARTED WITHIN THOSE 2 YEARS, THE PLANS WILL NEED TO BE RESUBMITTED FOR APPROVAL, INCLUDING PAYMENT OF REVIEW FEES AT THE PLANNING AND COMMUNITY DEVELOPMENT DIRECTOR'S DISCRETION.

This review is only for the proposed construction in Highway 105, east of Jackson Creek Parkway.

JOSHUA PALMER, P.E. DATE _____
COUNTY ENGINEER / ECM ADMINISTRATOR

48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS 811 UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW. THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.	NO. REVISION	DATE	REVIEW:
			PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC A. WHORTON, COLORADO P.E. #37155	DATE
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STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS			
ROADWAY IMPROVEMENT PLANS			
TITLE SHEET			
DESIGNED BY	PRA	SCALE	DATE 02-23-23
DRAWN BY	PRA	(H) 1"=VARIES	SHEET 1 OF 23
CHECKED BY	(V) 1"= N/A	JOB NO.	1302.22

CDOT GENERAL NOTES

1. ALL CONSTRUCTION MATERIALS, TECHNIQUES, AND PROCEDURES WITHIN THE HIGHWAY LIMITS SHALL BE IN CONFORMANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION AS SET FORTH IN THE LATEST "M & S" STANDARDS MANUAL.
2. A FULLY EXECUTED COMPLETE COPY OF THE ACCESS PERMITS AND A VALID NOTICE TO PROCEED TO CONSTRUCTION MUST BE ON THE JOB SITE WITH THE CONTRACTOR AT ALL TIMES DURING CONSTRUCTION.
3. ANY ADDITIONAL PERMITS AND CLEARANCES REQUIRED BY OTHER FEDERAL, STATE, AND LOCAL GOVERNMENT AGENCIES IS THE RESPONSIBILITY OF THE OWNER/DEVELOPER.
4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE AND LOCATION OF ALL UNDERGROUND UTILITIES ALONG THE ROUTE OF HIGHWAY CONSTRUCTION. THE OMISSION FROM OR THE INCLUSION OF UTILITY LOCATIONS ON THE PLANS IS NOT TO BE CONSIDERED AS THE NONEXISTENCE OF OR A DEFINITE LOCATION OF EXISTING UNDERGROUND UTILITIES.
5. THE CONTRACTOR WILL TAKE THE NECESSARY PRECAUTIONS TO PROTECT EXISTING UTILITIES FROM DAMAGE DUE TO THIS OPERATION. ANY DAMAGE TO THE UTILITIES WILL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, AND ANY SERVICE DISRUPTION WILL BE SETTLED BY THE CONTRACTOR.
6. ALL STATIONING IS CENTERLINE UNLESS OTHERWISE INDICATED. ALL ELEVATIONS ARE CENTERLINE UNLESS OTHERWISE INDICATED.
7. FIVE WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, THE OWNER/CONTRACTOR MUST CONTACT MR. ART GONZALES (719) 546-5732, CDOT ACCESS/TRAFFIC MANAGER, TO COORDINATE CONSTRUCTION.
8. WORK SHALL BEGIN AFTER 8:30 AM AND ALL EQUIPMENT SHALL BE OUT OF THE RIGHT-OF-WAY BEFORE 3:30 PM. NO WORK IS ALLOWED WITHIN THE HIGHWAY RIGHT-OF-WAY ON WEEKENDS OR STATE/FEDERAL HOLIDAYS. NO CONSTRUCTION VEHICLES SHALL BE PARKED, OR CONSTRUCTION MATERIALS STOCKPILED IN THE HIGHWAY RIGHT-OF-WAY OVERNIGHT. NO PRIVATE VEHICLES MAY BE PARKED IN THE HIGHWAY RIGHT-OF-WAY AT ANY TIME DURING CONSTRUCTION.
9. TWO WAY TRAFFIC SHALL BE MAINTAINED THROUGHOUT THE WORK AREA AT ALL TIMES.
10. ALL SIGNS SHALL BE MANUFACTURED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.). THE SHEETING FOR THE SIGNS BE HIGHWAY INTENSITY SHEETING (ASTM TYPE IV RETRO REFLECTIVE SHEETING).
11. ALL DISTURBED PAVEMENT EDGES SHALL BE CUT TO NEAT LINES 1' FROM EXISTING EDGE OF PAVEMENT. NEW SURFACING FOR THE ACCESS AND THE HIGHWAY WIDENING SHALL BE A FULL DEPTH TOTAL OF 7 INCHES OF COMPACTED HOT MIX ASPHALT (HMA), GRAD-S(100), PG. 58-28 OR PG. 64-22 (WITH 1% LIME) PLACED IN 3 LIFTS, OVER 12 INCHES OF CEMENT TREATED SUBGRADE, OVER COMPACTED RANDOM FILL SUBGRADE WITH A MINIMUM R-VALUE OF 17.
12. PLACEMENT AND COMPACTION OF HOT MIX ASPHALT (HMA) SHALL COMPLY WITH SECTION 401 OF THE 2005 CDOT STANDARD SPECIFICATIONS AS WELL AS THE LATEST CDOT STANDARD SPECIAL PROVISIONS.
13. PLACEMENT AND COMPACTION OF SUB-GRADE, EMBANKMENTS, AND BACKFILLS SHALL COMPLY WITH SECTION 203 OF THE CDOT STANDARD SPECIFICATIONS AS WELL AS THE LATEST CDOT STANDARD SPECIAL PROVISIONS.
14. SOIL PREPARATION INCLUDING TOPSOIL, SEEDING, AND MULCHING IS REQUIRED WITHIN THE HIGHWAY RIGHT-OF-WAY ON ALL DISTURBED AREAS NOT SURFACED AND THOSE AREAS BEYOND THE HIGHWAY THAT MAY ERODE AND SEND DEBRIS INTO THE HIGHWAY RIGHT-OF-WAY.
15. ADDITIONAL EROSION CONTROL STRUCTURES MAY BE REQUIRED AT THE TIME OF CONSTRUCTION.
16. WIDENED AREAS OF THE ROADWAY SHALL BE A MINIMUM OF 20-YEAR DESIGN LIFE.
17. OVERLAY OF EXISTING PAVEMENT SHALL ALSO BE A MINIMUM OF 20-YEAR DESIGN LIFE TO AVOID DEFERRING MAINTENANCE ISSUES BETWEEN THE OVERLAY SECTION AND THE NEW PAVEMENT WIDENING.
18. WATER SHALL BE USED AS A DUST PALLIATIVE WHERE REQUIRED. COST OF WATER SHALL BE INCLUDED IN THE WORK.
19. THIS DESIGN IS IN FULL COMPLIANCE WITH SECTION 4 OF THE STATE HIGHWAY ACCESS CODE, 2 CCR 601-1 EXCEPT FOR THE FOLLOWING APPROVED VARIANCES: NONE
20. THIS DESIGN IS IN FULL COMPLIANCE WITH TITLE II ADA ACCESSIBILITY REQUIREMENTS EXCEPT FOR THE FOLLOWING APPROVED VARIANCES: NONE

Include the following signal notes. For higher resolution see the approved signal plans under CDR2010 for Trails at Aspen signal design.

TOWN OF MONUMENT GENERAL NOTES:

1. ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION OF SITE IMPROVEMENTS SHALL MEET OR EXCEED THE SITE WORK STANDARDS AND SPECIFICATIONS AND THE STANDARDS AND SPECIFICATIONS SET FORTH IN THE TOWN OF MONUMENT MUNICIPAL CODE CRITERIA, AND APPLICABLE STANDARDS, AND APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS, WHERE THERE IS CONFLICT BETWEEN THESE PLANS AND THE SPECIFICATIONS, OR ANY APPLICABLE STANDARDS, THE HIGHER QUALITY STANDARD SHALL APPLY. ALL WORK WITHIN PUBLIC R.O.W. OR EASEMENTS SHALL BE INSPECTED AND APPROVED BY THE TOWN OF MONUMENT INSPECTOR. THE TOWN WILL ALSO INSPECT ALL WORK ON PRIVATE PROPERTY.
2. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES, AS SHOWN ON THESE PLANS, IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY PERTINENT LOCATIONS AND ELEVATIONS, ESPECIALLY AT THE CONNECTION POINTS AND AT POTENTIAL UTILITY CONFLICTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE TOWN OF MONUMENT AND ALL APPLICABLE AGENCIES. THE CONTRACTOR SHALL NOTIFY THE TOWN INSPECTOR AT LEAST 48 HOURS PRIOR TO THE START OF ANY EARTH DISTURBING ACTIVITY, OR CONSTRUCTION ON ANY AND ALL PUBLIC IMPROVEMENTS.
4. THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH THE TOWN AND ALL UTILITY COMPANIES INVOLVED WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION AND TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION AND WITH THE MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY UTILITY SERVICE.
5. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE APPROVED PLANS, ONE (1) COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND A COPY OF ALL PERMITS NEEDED FOR THE JOB, ON-SITE AT ALL TIMES.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING, BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.
7. IF, DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER OF RECORD AND THE TOWN INSPECTOR IMMEDIATELY.
8. ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARD, UNLESS SPECIFICALLY STATED OTHERWISE.
9. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN, IN ACCORDANCE WITH M.U.T.C.D. TO THE TOWN OF MONUMENT FOR APPROVAL, PRIOR TO ANY CONSTRUCTION ACTIVITIES WITHIN, OR AFFECTING THE RIGHT-OF-WAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY AND ALL TRAFFIC CONTROL DEVICES AS MAY BE REQUIRED BY THE CONSTRUCTION ACTIVITIES.
10. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF THE INTENDED IMPROVEMENTS SHOWN ON THESE DRAWINGS OR DESIGNATED TO BE PROVIDED, INSTALLED, OR CONSTRUCTED, UNLESS SPECIFICALLY NOTED OTHERWISE.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING AS-BUILT INFORMATION ON A SET OF RECORD DRAWINGS KEPT AT THE CONSTRUCTION SITE, WHICH SHALL BE AVAILABLE TO THE TOWN OF MONUMENT DEVELOPMENT SERVICES DEPARTMENT INSPECTOR AT ALL TIMES. A REPRODUCIBLE SET OF AS-BUILT DRAWINGS SHALL BE FURNISHED TO THE TOWN OF MONUMENT AT THE COMPLETION OF THE PROJECT, PRIOR TO FINAL APPROVAL BY THE TOWN AND AS A CONDITION FOR OBTAINING A CERTIFICATE OF OCCUPANCY.
13. DIMENSIONS FOR LAYOUT AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWING. IF PERTINENT DIMENSIONS ARE NOT SHOWN, CONTACT THE ENGINEER-OF-RECORD FOR CLARIFICATION, AND ANNOTATE THE DIMENSION ON THE AS-BUILT RECORD DRAWINGS.
14. ALL STRUCTURAL EROSION CONTROL MEASURES SHALL BE INSTALLED, AT THE LIMITS OF CONSTRUCTION, PRIOR TO ANY OTHER GROUND DISTURBING ACTIVITY. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED IN GOOD REPAIR BY THE CONTRACTOR, UNTIL SUCH TIME AS THE ENTIRE DISTURBED AREAS ARE STABILIZED WITH HARD SURFACE OR LANDSCAPING.
15. THE CONTRACTOR SHALL SEQUENCE THE INSTALLATION OF UTILITIES IN SUCH A MANNER AS TO MINIMIZE POTENTIAL UTILITY CONFLICTS. IN GENERAL, STORM SEWER AND SANITARY SEWER SHOULD BE CONSTRUCTED PRIOR TO INSTALLATION OF WATER LINES AND DRY UTILITIES.
16. NO SITE-RELATED IMPROVEMENTS MAY COMMENCE UNTIL A PRE-CONSTRUCTION MEETING IS HELD WITH THE TOWN OF MONUMENT AND ALL APPLICABLE PERMITS ARE OBTAINED.
17. THE DEVELOPER MUST IDENTIFY TO THE TOWN OF MONUMENT, PRIOR TO THE START OF ANY WORK, A QUALIFIED PLAN PERSON RESPONSIBLE FOR REVIEWING AND MONITORING ALL OPERATIONS IN ORDER TO PREVENT OR MINIMIZE THE IMPACT OF VIBRATION, NOISE, DUST, DRAINAGE, AND EROSION DAMAGE, AND OTHER FORMS OF POLLUTION ON NEARBY PROPERTY AND THE PUBLIC AS A WHOLE. THE DEVELOPER MUST WRITE TO THE OWNERS/OCCUPANTS OF PROPERTIES WITHIN AT LEAST 100 YARDS OF THE LIMITS OF THE WORKSITE, INFORMING THEM OF THE NATURE AND TIMING OF THE PROJECT AND PROVIDING CONTACT DETAILS FOR COMPLAINTS. THE TOWN, PRIOR TO THE COMMENCEMENT OF THE PROJECT, MUST APPROVE A COPY OF THE LETTER, MAILING LIST, AND DELIVERY DATES.
18. *****TOWN OF MONUMENT DISCLAIMER*****
THE APPROVAL OF THESE SITE CONSTRUCTION PLANS PRIOR TO SITE PLAN APPROVAL DOES NOT IN ANY WAY OBLIGATE THE TOWN TO APPROVE SUBSEQUENT SUBMITTALS (E.G., SITE PLANS AND PLATS), AND THE TOWN HAS NO LIABILITY IN ANY FORM DUE TO ITS ACTIONS IN THE APPROVAL OF THESE SITE CONSTRUCTION PLANS.

TOWN OF MONUMENT CONSTRUCTION NOTES:

1. THE TOWN OF MONUMENT DESIGN CRITERIA & CONSTRUCTION SPECIFICATIONS MANUAL IS CONSIDERED PART OF THIS CONSTRUCTION DRAWING SET. THIS DESIGN AND PLAN SET IS INCOMPLETE WITHOUT THIS SPECIFICATIONS MANUAL. THE CONTRACTOR SHALL OBTAIN A COPY OF THIS MANUAL AND BE FAMILIAR WITH IT FOR ALL CONSTRUCTION ACTIVITIES. A COPY CAN BE OBTAINED FROM THE TOWN BY CALLING (719)-499-3375.
2. ALL MATERIALS AND WORKMANSHIP SHALL BE IN COMPLIANCE WITH THE TOWN CRITERIA AND CONSTRUCTION SPECIFICATION MANUAL AND TOWN OF MONUMENT MUNICIPAL CODE AND SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF MONUMENT INSPECTOR.
3. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING UTILITIES PRIOR TO ACTUAL CONSTRUCTION. ALL EXISTING UTILITIES SHOWN ARE BASED ON INFORMATION OF RECORD. THE CONTRACTOR IS RESPONSIBLE TO TAKE PRECAUTIONARY MEASURES TO PROTECT THE EXISTING UTILITIES SHOWN HEREON AND ANY OTHER EXISTING UTILITIES NOT OF RECORD OR NOT SHOWN ON THESE PLANS AND AGREES TO ACCEPT FULL RESPONSIBILITY FOR FAILURE TO LOCATE AND PRESERVE ANY AND ALL EXISTING UTILITIES.
4. THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING IMPROVEMENTS TO BE MET BY THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS SHALL BE CONFIRMED BY FIELD MEASUREMENTS PRIOR TO CONSTRUCTION. ANY SIGNIFICANT DISCREPANCIES FOUND BETWEEN THIS PLAN SET AND ACTUAL FIELD CONDITIONS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER OF RECORD FOR APPROPRIATE ACTION.
5. THE CONTRACTOR IS ADVISED THAT ALL EXISTING CONDITIONS OUTSIDE THE AREA OF WORK SHALL BE PROTECTED, IF DAMAGE OCCURS DURING CONSTRUCTION, IT WILL BE REPLACED IN THE ORIGINAL EXISTING CONDITION AT THE CONTRACTOR'S EXPENSE.
6. CONCRETE USED FOR CONSTRUCTION OF CURB AND GUTTER, SIDEWALK, AND CROSSPANS SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 P.S.I. AFTER 28 DAYS.
7. ALL DISTURBED PAVEMENT EDGES WILL BE CUT TO NEAT LINES. THE THICKNESS OF ANY REPLACED ASPHALT AND BASE COURSE SHALL BE EQUAL OR EXCEED THE EXISTING THICKNESS.
8. WHEN ABUTTING NEW PAVEMENT TO EXISTING, OR TO REMOVE ANY BROKEN OR CRACKED PAVEMENT, SAWCUT EXISTING PAVEMENT TO A STRAIGHT EDGE AND AT A RIGHT ANGLE, OR AS APPROVED BY THE TOWN INSPECTOR.
9. FINAL GRADING OF CURBS AND PAVING SHALL PROVIDE POSITIVE DRAINAGE. STANDING WATER POCKETS OR PONDING WILL NOT BE ACCEPTABLE.
10. WHERE REMOVAL OF EXISTING CURB, GUTTER, SIDEWALK, OR PAVEMENT IS REQUIRED, THE CONTRACTOR SHALL SAWCUT AND/OR REMOVE TO THE NEAREST JOINT, CURB, GUTTER, AND SIDEWALK SHOWN AS EXISTING ON THESE PLANS IS NOT TO BE REMOVED UNLESS OTHERWISE NOTED. IF ANY OF THE EXISTING CURB, GUTTER, OR SIDEWALK IS DAMAGED BY THE CONTRACTOR, THEN IT SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
11. CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL AND ROUTING DURING CONSTRUCTION, IF REQUIRED. TWO-WAY TRAFFIC SHALL BE MAINTAINED THROUGH THE WORK AREA AT ALL TIMES.
12. ALL DISTURBED AREAS THAT ARE TO REMAIN UNCOVERED FOR A PERIOD GREATER THAN 2 MONTHS SHALL BE RESEEDED AND WATERED UNTIL STABLE VEGETATION IS ESTABLISHED.
13. AT LEAST ONE SIGNED AND STAMPED SET OF THESE CONSTRUCTION DRAWINGS SHALL BE KEPT ON-SITE AT ALL TIMES.

TRAFFIC SIGNAL NOTES:

1. THE CONTRACTOR SHALL PROVIDE, FOR REVIEW BY THE ENGINEER, A COMPLETE TRAFFIC SIGNAL MATERIAL SUBMITTAL PACKAGE THAT CONTAINS ALL OF THE SPECIFICATIONS OF PUBLIC WORKING TRAFFIC SIGNAL STAFF FOR SCHEDULING THE ON-SITE FIELD IMPLEMENTATION OF ALL TRAFFIC SIGNAL TIMING AND OPERATIONAL PROGRAMMING, VEHICLE DETECTION ZONE PLACEMENT, AND DETECTION EQUIPMENT INSTALLATION. THIS WORK SHALL BE SCHEDULED NEAR THE END OF THE PROJECT, PRIOR TO PROJECT ACCEPTANCE, AND ONLY AFTER ALL FINAL PERMITTING MARKINGS AND TRAFFIC SIGNAL WORK HAS BEEN COMPLETED.
2. FUNCTIONAL AND OPERATIONAL RESPONSIBILITY FOR ALL NEWLY INSTALLED AND EXISTING TRAFFIC SIGNAL EQUIPMENT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR UNTIL FINAL ACCEPTANCE OF THE PROJECT. THE CONTRACTOR SHALL CONSIDER THIS WORK INCIDENTAL TO THE OVERALL WORK BEING PERFORMED AND SHALL BE INCLUDED AS PART OF THE PROJECT.
3. SEE COLORADO DEPARTMENT OF TRANSPORTATION SIGNAL DETAILS FOR CONSTRUCTION/INSTALLATION DETAILS NOT SHOWN ON THESE PLANS.
4. ALL SIGNAL EQUIPMENT REMOVED BY THE CONTRACTOR SHALL BE SALVAGED AND BECOME THE PROPERTY OF EL PASO COUNTY. THE SALVAGED EQUIPMENT SHALL BE DELIVERED AS DIRECTED BY THE ENGINEER. DELIVERY OF THE SIGNAL EQUIPMENT WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE WORKING FOR SIGNAL OF TRAFFIC SIGNAL EQUIPMENT.
5. OVERHEAD STREET NAME SIGN DESIGN AND LAYOUT INFORMATION SHALL BE PER THE STREET NAME SIGN DETAIL CONTAINED IN THE PROJECT PLAN.
6. TRAFFIC SIGNALS MOUNTED ON SIGNAL POLES, MAST ARMS, AND PEDESTALS SHALL BE MOUNTED USING BANDING, ALUMINUM CHANNELS, AND BACKING ZEES PER APPLICABLE CDOT STANDARD PLANS, OR SIMILAR RIGID SIGN BRACING MOUNTING ASSEMBLY.
7. ONCE THE PROFESSIONAL ENGINEERING CONSULTANT HAS COMPLETED ALL TRAFFIC SIGNAL CONTROLLER TIMING DEVELOPMENT AND CONTROLLER PROGRAMMING, THE CONTRACTOR WILL COORDINATE THE DELIVERY DATE OF THE PROGRAMMED TRAFFIC SIGNAL CONTROLLER FOR REVIEW OF EPC DEPARTMENT OF PUBLIC WORKS, HIGHWAY DIVISION SIGN SHOP AND ALLOW FOR A MINIMUM TWO WEEK REVIEW PERIOD. AFTER WHICH TIME THE CONTRACTOR MAY MAKE ARRANGEMENTS FOR PICKING UP THE SIGNAL CONTROLLER.
8. CONTROLLER CABINET SHALL BE FURNISHED WITH A "BEST" DOOR LOCK KIT LOCK AND CODE IS "BEST" BLUR LEFT AND RIGHT.
9. CONDUIT TO BE REPLACED IN THE EVENT THAT EXISTING CONDUIT IS DAMAGED AND AS DIRECTED BY THE ENGINEER.
10. ELECTRICAL SERVICE DISCONNECT BOXES SHALL BE LOCKABLE AND WEATHER PROOF WITH NEMA TYPE CIRCUIT BREAKER ENCLOSURES SHALL BE PROVIDED AT THE CONNECTION POINT OF EACH POWER SOURCE OR POINT OF SERVICE AS DIRECTED BY THE ENGINEER.
11. THE CONTRACTOR SHALL FURNISH AND INSTALL ALL WIRING REQUIRED TO COMPLETE THE INSTALLATION AND ESTABLISH THE FUNCTIONALITY OF ALL TRAFFIC SIGNAL EQUIPMENT.
12. ALL INCIDENTAL ITEMS NOT SHOWN IN THE SUMMARY OF APPROXIMATE QUANTITIES OR TABULATION OF SIGNAL EQUIPMENT SHALL BE CONSIDERED TO BE INCLUDED AS PART OF THE TRAFFIC SIGNAL INSTALLATION AND WILL NOT BE MEASURED AND PAID FOR SEPARATELY. ALL QUANTITIES ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL WORK NECESSARY TO COMPLETE THE CONSTRUCTION SHOWN ON THESE PLANS.
13. THE SIGNAL SHALL NOT BE TURNED ON OR STARTED UNTIL DIRECTED BY THE ENGINEER. PRIOR TO SIGNAL ACTIVATION, THE ENGINEER SHALL CONFIRM THAT THE APPROPRIATE PAVEMENT MARKINGS AND SIGNING ARE IN PLACE AND THAT ALL WORK NECESSARY FOR PROPER SIGNAL OPERATION HAS BEEN COMPLETED.
14. THE SIGNAL CONTROLLER SHALL BE A MAGNAC 2070 FILE ATC CONTROLLER AND THE CONFLICT MONITOR SHALL BE MODEL 3015 ECLIP BY ETHERNET FORT 800. THE CONTROLLER CABINET SHALL BE A CDOT SPECIFICATION MODEL 300 WITH BATTERY BACKUP AND AUX BACK AND SHALL CONTAIN ANTI-STATIC BLUE GRAY FINISH COATING. THE CABINET SHALL BE MOUNTED ON A CAST-IN-PLACE CONCRETE FOUNDATION PER APPLICABLE CDOT'S STANDARDS STANDARD PLAN AND THE CABINET SHALL BE POSITIONED SUCH THAT WITH THE FRONT DOOR OPEN, BOTH THE CONTROLLER DISPLAY AND THE SIGNAL INSTALLATION IS VISIBLE.
15. LUMINAIRES SHALL CONSIST OF AN ASSEMBLY THAT UTILIZES LEDs AS THE LIGHT SOURCE. IN ADDITION, A COMPLETE LUMINAIRE SHALL CONSIST OF A HOUSING, LED ARRAY, AND ELECTRONIC DRIVER POWER SUPPLY. ALL LUMINAIRES SHALL BE WIRED 120 VOLTS TO 40 WITH HEAT SINK HEADS. THE LED FEATURE MUST HAVE A COOLING TEMPERATURE OF 4100K (+/- 500K) MUST BE DESIGNED TO OPERATE AT A TEMPERATURE RANGE OF -40°F TO 100°F (-40°C TO 40°C) AND PROVIDE A MINIMUM OF 70,000 HOURS OF OPERATION. LUMINAIRES SHALL BE EIGHTY (80) LED STREET LIGHT OR APPROVED EQUIV. THE CONTRACTOR SHALL PROVIDE A RECOMMENDATION FOR TYPE OF THE STREET LIGHT BASED ON THE CONSTRUCTION AND MANUFACTURER SPECIFICATIONS. THE CONTRACTOR SHALL BE APPROVED BY THE ENGINEER. THE LIGHTS MUST BE CAST ALUMINUM, PROVIDED WITH FURRING, SURGE SUPPRESSION AND MUST BE UL LISTED FOR WET LOCATIONS. THE FIXTURE MUST HAVE AN INTERNAL WEATHER-TIGHT LED DRIVE. NO ACTIVE COOLING FEATURES (FANS, ETC.) WILL BE ALLOWED. THE FINISHED SIGNAL SHALL MATCH THE FINISHED SIGNAL. THE LUMINAIRES WILL BE INSTALLED ON 15 FOOT EXTERIOR ARM SHAFTS AT NOMINAL HEIGHT OF 40 FEET AND SHALL BE WELDED TO THE SIGNAL POLE PER CDOT TYPICAL TRAFFIC SIGNAL INSTALLATION DETAILS STANDARD PLAN 6014-40. LUMINAIRE ARM SHAFT SHALL BE IN ACCORDANCE WITH THE PROJECT PLANS.
16. THE INTERSECTION DETECTION SYSTEM (MICROWAVE RADAR) CONTRACT ITEM INCLUDES DEVICE INSTALLATION (I.E., DETECTOR UNIT, HARDWARE, WIRING, PROCESSOR MODULE, ETC.) AND VERIFICATION OF SUCCESSFUL IN-FIELD DETECTION ZONE OPERATION BASED ON SEVERAL VEHICLE ACTUATIONS IN ALL DETECTION ZONES.

TRAFFIC SIGNAL NOTES (CONT):

17. THE CONTRACTOR SHALL COORDINATE THE SCHEDULES OF THE CONTRACTED PROFESSIONAL ENGINEERING CONSULTANT AND THE EPC DEPARTMENT OF PUBLIC WORKING TRAFFIC SIGNAL STAFF FOR SCHEDULING THE ON-SITE FIELD IMPLEMENTATION OF ALL TRAFFIC SIGNAL TIMING AND OPERATIONAL PROGRAMMING, VEHICLE DETECTION ZONE PLACEMENT, AND DETECTION EQUIPMENT INSTALLATION. THIS WORK SHALL BE SCHEDULED NEAR THE END OF THE PROJECT, PRIOR TO PROJECT ACCEPTANCE, AND ONLY AFTER ALL FINAL PERMITTING MARKINGS AND TRAFFIC SIGNAL WORK HAS BEEN COMPLETED.
18. THE MICROWAVE RADAR DETECTION SYSTEM SHALL UTILIZE MS SEDCO INTERSECTOR TO-CX1-SBE WITH INTERFACE BOARD.
19. PEDESTRIAN SIGNAL HEAD INSTALLATION SHALL INCLUDE ALUMINUM AND POWDER COATED GLOSS BLACK SIGNAL HEAD WITH APPROVED DISPLAY. ALUMINUM OR PLYWOOD DETECTION ZONE PLACEMENT, AND DETECTION EQUIPMENT SHALL BE SCHEDULED NEAR THE END OF THE PROJECT, PRIOR TO PROJECT ACCEPTANCE, AND ONLY AFTER ALL FINAL PERMITTING MARKINGS AND TRAFFIC SIGNAL WORK HAS BEEN COMPLETED. CUSTOM MESSAGE SIGNS SHALL NOT INTERFERE WITH TRAFFIC SIGNAL INFORMATION. NON-ARM FORMATS SPECIFIED IN 4E OF THE MUTCD AND SHALL ONLY PROVIDE ADDITIONAL INFORMATION AS DIRECTED BY THE ENGINEER.
20. ALL TRAFFIC SIGNAL POLES, MAST ARMS, PEDESTALS, AND LUMINAIRE ARMS SHALL HAVE A GLOSS BLACK COAT FINISH OVER HOT DIP GALVANIZED BASE COAT. INSTALLED IN ACCORDANCE WITH THE PAINT MANUFACTURER'S INSTRUCTIONS.
21. ALL SIGNAL POLE AND CONTROLLER LOCATIONS SHOWN ARE APPROXIMATE ONLY. MAST ARMS SHALL BE OF SUFFICIENT LENGTH AND DESIGN TO ALLOW PROPER PLACEMENT OF SIGNAL HEADS AND OVERHEAD SIGNING PER THE PLANS. ACTUAL LOCATIONS SHALL BE STAKED IN THE FIELD AND FIELD VERIFIED BY THE ENGINEER PRIOR TO DRILLING, EXCAVATION, AND ORDERING THE SIGNAL EQUIPMENT AND MAST ARMS. THE LOCATION OF EACH SIGNAL POLE FOUNDATION SHALL BE POTHOLED PRIOR TO DRILLING TO CONFIRM WHETHER OR NOT ANY UTILITY CONFLICTS EXIST.
22. LATERAL OFFSETS FROM THE NEAR EDGE OF TRAFFIC SIGNAL POLES, PEDESTALS, AND CABINETS TO THE FACE OF CURB OR EDGE OF PAVED SHOULDER SHOULD BE AT LEAST SIX FEET. HOWEVER, A MINIMUM LATERAL OFFSET OF AT LEAST FOUR FEET MAY BE PROVIDED FOR CURB OFFSETS. IF NO PAVED SHOULDER EXISTS, A MINIMUM LATERAL OFFSET OF AT LEAST EIGHT FEET SHOULD BE PROVIDED FROM THE EDGE OF PAVEMENT FOR AN AUXILIARY LANE AND A MINIMUM LATERAL OFFSET OF AT LEAST THIRTY FEET SHOULD BE PROVIDED FROM THE EDGE OF PAVEMENT FOR A THROUGH LANE.
23. SHOULD THE CONTRACTOR ENCOUNTER WATER IN THE CASSION, ANY DEWATERING METHODS AND NECESSARY PERMITS SHALL BE INCLUDED IN THE COST OF THE CASSION AND WILL BE CONSIDERED INCIDENTAL TO THE WORK.
24. ALL TRAFFIC SIGNAL COMPONENT FULL BOXES SHALL BE PRE CAST HIGH DENSITY POLYMER CONCRETE (HPC) MATERIAL WITH THE FOLLOWING SIZES: 36 INCH X 48 INCH X 18 INCH FOR THE FULL BOX; TWO (2) 24 INCH AND ONE (1) 3 INCH; BETWEEN THE SIGNAL POLE FOUNDATION AND ADJACENT SIGNAL POLE FULL BOX; TWO (2) 24 INCH AND ONE (1) 3 INCH; BETWEEN THE SIGNAL POLE FULL BOXES; TWO (2) 24 INCH AND THREE (3) 3 INCH; BETWEEN THE CONTROLLER CABINET FOUNDATION AND ADJACENT FULL BOX; THREE (3) 24 INCH AND FOUR (4) 3 INCH.
25. BETWEEN THE SECONDARY SERVICE PEDESTAL, METER FOUNDATION AND THE CONTROLLER CABINET FOUNDATION ONE (1) 2 INCH FOR THE ELECTRICAL SERVICE FEED.
26. ALL CONDUIT AND FITTINGS SHALL BE SCHEDULED 80 PVC AND ALL CONDUIT SHALL HAVE A PULL ROPE LEFT IN THEM WHEN CONSTRUCTION IS COMPLETED. ALL CONDUIT ENTERING THE CABINET FOUNDATION AND PULL BOXES SHALL HAVE BELL END STYLE COUPLERS ON ALL CONDUIT ENDS.
27. ALL CONDUIT THAT IS DIRECTIONALLY BORED SHALL BE A MINIMUM OF THREE FEET BELOW THE EXISTING PAVEMENT. THIS WORK SHALL AVOID DISTURBING OR DAMAGING EXISTING FACILITIES AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECT RECONSTRUCTION, ALTERATION, REPAIR OR MAINTENANCE OF HIGHWAY PROPERTY, AS NECESSARY, TO REPAIR DAMAGE CAUSED BY THE ACCOMMODATION OF THE UTILITY, AND TO RESTORE THE HIGHWAY TO PRE-EXISTING OR BETTER CONDITIONS.
28. ALL SIGNAL CABLE SHALL BE CONTINUOUS FROM CONNECTIONS MADE IN THE HANDHOLE COMPARTMENT OF THE SIGNAL POLE TO THE TERMINAL COMPARTMENT OF THE CONTROLLER CABINET AND SHALL CONTAIN NO SPLICES. EACH SIGNAL HEAD SHALL CONTAIN SEPARATE AND CONTINUOUS SIGNAL CABLE FROM THE SIGNAL HEAD TO THE ABOVE GROUND HANDHOLE AT THE BASE OF THE SIGNAL POLE AND SHALL CONTAIN NO SPLICES.
29. A SEPARATE AND CONTINUOUS 21 CONDUCTION CABLE SHALL RUN FROM THE CONTROLLER CABINET TO THE HANDHOLE AT EACH SIGNAL POLE AND SHALL CONTAIN NO SPLICES.

TRAFFIC SIGNAL NOTES (CONT):

31. ALL SIGNS MOUNTED ON SIGNAL POLES, MAST ARMS, AND PEDESTALS SHALL BE MOUNTED USING BANDING ALUMINUM CHANNELS, AND BACKING ZEES PER CDOT TYPICAL POLE MOUNT SIGN INSTALLATIONS STANDARD PLAN S-614-20, OR SIMILAR RIGID SIGN BRACING MOUNTING ASSEMBLY, AS DIRECTED BY THE ENGINEER. MAST ARM SIGNS THAT REQUIRE Z-BRACKETS SHALL BE MOUNTED ON ASTRO-STYLE BRACKETS AND RISERS. THE COST OF ALL HARDWARE FITTINGS, TOOLS, AND EQUIPMENT NECESSARY FOR A COMPLETE INSTALLATION OF MAST ARM SIGNS WILL NOT BE MEASURED AND PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE WORK.
32. ALL CONDUCTORS AND CABINET WIRING SHALL BE COLOR CODED AND PERMANENTLY TAGGED PER ENGINEER DIRECTION AND IN ACCORDANCE WITH THE SIGNAL PHASE NUMBERING AND DETECTION ZONE PHASE NUMBERING INFORMATION CONTAINED IN THE PROJECT PLANS.
33. ALL VEHICLE SIGNAL HEADS SHALL HAVE APPROVED 12 INCH LED INDICATORS AND SHALL BE ALUMINUM WITH POWDER COATED GLOSS BLACK FINISH AND SHALL CONTAIN 12 INCH ALUMINUM TUNNEL VISORS WITH THE OUTSIDE POWDER COATED GLOSS BLACK. ALL VEHICLE SIGNAL HEADS SHALL HAVE ALUMINUM LOUVERED BACK PLATES WITH POWDER COATED GLOSS BLACK FINISH AND YELLOW RETRO REFLECTIVE BORDER. MAST ARM SIGNAL HEADS SHALL USE ASTRO-TYPE MOUNTING ASSEMBLIES AND SHALL BE INSTALLED APPROXIMATELY LEVEL WITH ONE ANOTHER AT A 17 TO 19 FOOT VERTICAL CLEARANCE ABOVE THE HIGH POINT OF THE PAVEMENT GRADE.
34. ALL VEHICLE AND PEDESTRIAN SIGNAL HEADS THAT HAVE NOT BEEN PLACED IN SERVICE SHALL BE COVERED WITH PREFABRICATED WEATHER RESISTANT NYLON FORM FITTING SIGNAL FACE COVER MATERIAL. THE SIGNAL FACE SHALL REMAIN COMPLETELY COVERED UNTIL THE SIGNAL HEAD IS PLACED IN SERVICE AND IS FULLY FUNCTIONAL AND OPERATIONAL.
35. ALL DETECTION EQUIPMENT, DETECTION ZONES, AND SIGNAL TIMING OPERATION SHALL BE CONFIRMED IN THE FIELD BY THE PROFESSIONAL ENGINEERING CONSULTANT TO BE ACHIEVING SATISFACTORY TRAFFIC SIGNAL OPERATION.
36. COMMUNICATION SYSTEM SHALL BE A DYMEC ETHERNET SWITCH KY-317EMX AND CELLULAR MODEM MICROHARD BULLET LTE, WITH CTEL SURGE SUPPRESSION ON ALL COMPONENTS.
38. THE CONTRACTED PROFESSIONAL ENGINEERING TRAFFIC SIGNAL TIMING, CONTROLLER PROGRAMMING AND CONSULTANT SERVICES DELIVERABLES SHALL BE CONSISTENT WITH NATIONAL PUBLICATIONS, INCLUDING BUT NOT LIMITED TO: MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) LATEST EDITION / REVISION, FHWA-HRP-08-024 "TRAFFIC SIGNAL TIMING MANUAL" (JUNE 2008), FHWA-HRT-04-091 "SIGNALIZED INTERSECTIONS: INFORMATION GUIDE" (AUGUST 2004), FHWA-HRP-06-008 "TRAFFIC CONTROL SYSTEMS HANDBOOK" (OCTOBER 2003), FHWA-DT-98-011-00183 "SIGNAL TIMING PROCESS FINAL REPORT" (DECEMBER 2003), NCHRP REPORT 731 "GUIDELINES FOR TIMING YELLOW AND ALL-RED INTERVALS AT SIGNALIZED INTERSECTIONS" (2012), NCHRP REPORT 812 "SIGNAL TIMING MANUAL SECOND EDITION" (2015)

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UNDERGROUND UTILITIES ARE SHOWN ON THESE PLANS. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

REVIEW:	
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC	
MARC A. WHORTON, COLORADO P.E. #37155	DATE

619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903
(719)785-0790
(719)785-0799(Fax)

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS			
ROADWAY IMPROVEMENT PLANS			
STANDARD NOTES			
DESIGNED BY	PRA	SCALE	DATE 02-23-23
DRAWN BY	PRA	(H) 1"=VARIES	SHEET 2 OF 23
CHECKED BY	(V) 1"= N/A	JOB NO.	1.302.22

CLASSIC CONSULTING

PLAN NUMBER	M STANDARD TITLE	PAGE NUMBER
<input type="checkbox"/> M-100-1	STANDARD SYMBOLS (3 SHEETS).....	1-3
<input type="checkbox"/> M-100-2	ACRONYMS AND ABBREVIATIONS (4 SHEETS).....	4-7
<input type="checkbox"/> M-203-1	APPROACH ROADS	8
<input type="checkbox"/> M-203-2	DITCH TYPES.....	9
<input type="checkbox"/> M-203-11	SUPERELEVATION CROWNED AND DIVIDED HIGHWAYS.....	10-12 (3 SHEETS)
<input checked="" type="checkbox"/> M-203-12	SUPERELEVATION STREETS (2 SHEETS).....	13-14
<input checked="" type="checkbox"/> M-206-1	EXCAVATION AND BACKFILL FOR STRUCTURES	15-16 (2 SHEETS)
<input type="checkbox"/> M-206-2	EXCAVATION AND BACKFILL FOR BRIDGES (2 SHEETS).....	17-18
<input checked="" type="checkbox"/> M-208-1	TEMPORARY EROSION CONTROL (11 SHEETS)	19-29
<input type="checkbox"/> M-210-1	MAILBOX SUPPORTS (2 SHEETS)	30-31
<input type="checkbox"/> M-214-1	NURSERY STOCK DETAILS	32
<input checked="" type="checkbox"/> M-216-1	SOIL RETENTION COVERING (2 SHEETS)	33-34
<input type="checkbox"/> M-412-1	CONCRETE PAVEMENT JOINTS (5 SHEETS)	35-39
<input type="checkbox"/> M-510-1	STRUCTURAL PLATE PIPE H-20 LOADING.....	40
<input type="checkbox"/> M-601-1	SINGLE CONCRETE BOX CULVERT (CAST-IN-PLACE).....	41-42 (2 SHEETS)
<input type="checkbox"/> M-601-2	DOUBLE CONCRETE BOX CULVERT (CAST-IN-PLACE).....	43-44 (2 SHEETS)
<input type="checkbox"/> M-601-3	TRIPLE CONCRETE BOX CULVERT (CAST-IN-PLACE).....	45-46 (2 SHEETS)
<input checked="" type="checkbox"/> M-601-10	HEADWALL FOR PIPES	47
<input type="checkbox"/> M-601-11	TYPE "S" SADDLE HEADWALLS FOR PIPE.....	48
<input type="checkbox"/> M-601-12	HEADWALLS AND PIPE OUTLET PAVING	49
<input type="checkbox"/> M-601-20	WINGWALLS FOR PIPE OR BOX CULVERTS (2 SHEETS).....	50-51
<input type="checkbox"/> M-603-1	METAL PIPE (4 SHEETS).....	52-55
<input checked="" type="checkbox"/> M-603-2	REINFORCED CONCRETE PIPE	56
<input type="checkbox"/> M-603-3	PRECAST CONCRETE BOX CULVERT.....	57
<input type="checkbox"/> M-603-4	CORRUGATED POLYETHYLENE PIPE (AASHTO M294)	58
	AND POLYPROPYLENE PIPE (AASHTO M330)	
<input type="checkbox"/> M-603-5	POLYVINYL CHLORIDE (PVC) PIPE (AASHTO M304).....	59
<input type="checkbox"/> M-603-6	STEEL REINFORCED POLYETHYLENE	60
	AND POLYPROPYLENE PIPE (AASHTO M330)	
<input type="checkbox"/> M-603-10	CONCRETE AND METAL END SECTIONS	61
<input type="checkbox"/> M-603-12	TRAVERSABLE END SECTIONS AND SAFETY GRATES.....	62-64 (3 SHEETS)
<input checked="" type="checkbox"/> M-604-10	INLET, TYPE C.....	65
<input type="checkbox"/> M-604-11	INLET, TYPE D.....	66
<input checked="" type="checkbox"/> M-604-12	CURB INLET TYPE R (2 SHEETS).....	67-68
<input type="checkbox"/> M-604-13	CONCRETE INLET TYPE 13.....	69
<input checked="" type="checkbox"/> M-604-20	MANHOLES (3 SHEETS).....	70-72
<input type="checkbox"/> M-604-25	VANE GRATE INLET (5 SHEETS).....	73-77
<input checked="" type="checkbox"/> M-605-1	SUBSURFACE DRAINS	78

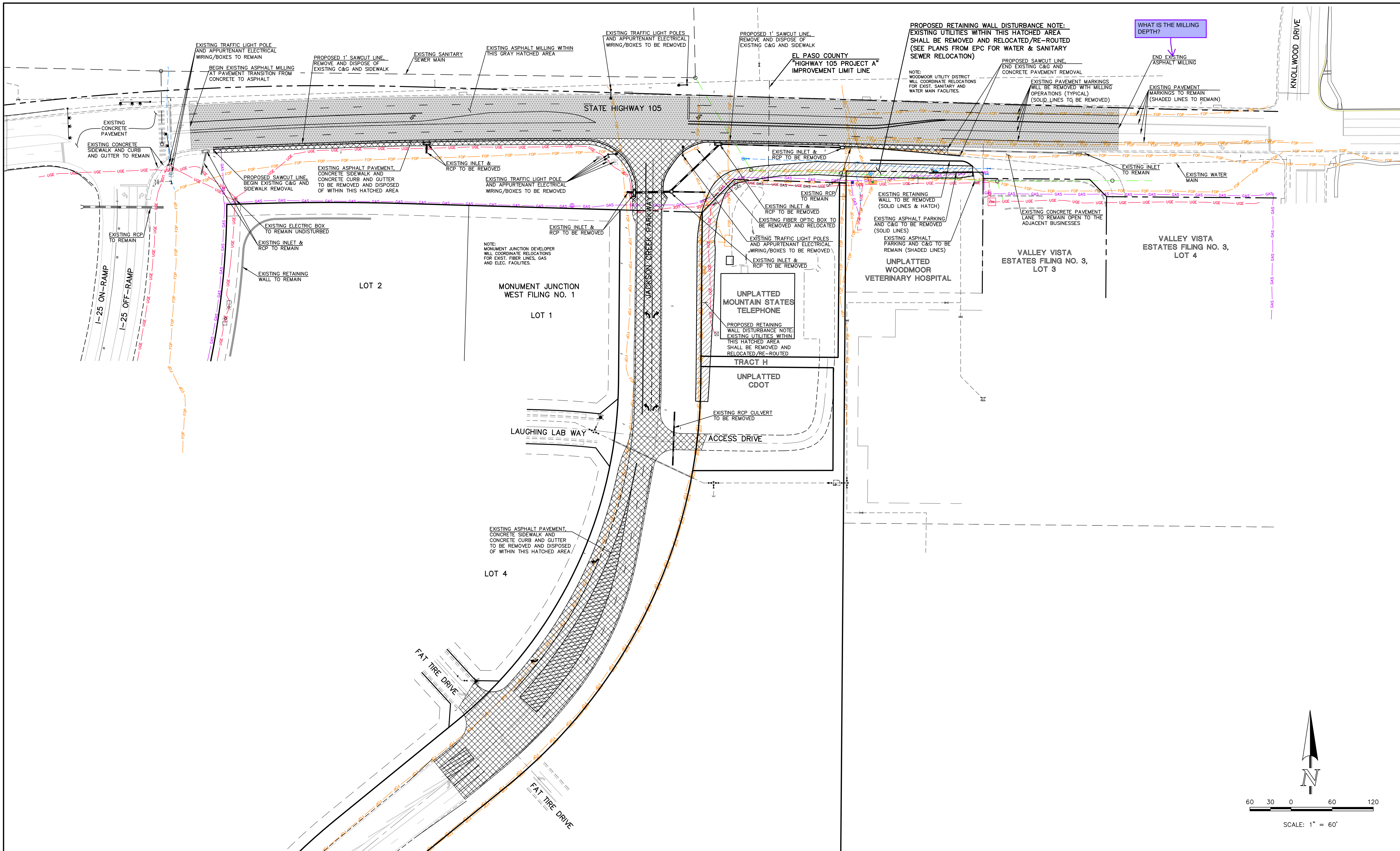
PLAN NUMBER	M STANDARD TITLE	PAGE NUMBER
<input type="checkbox"/> M-606-1	MIDWEST GUARDRAIL SYSTEM TYPE 3 W-BEAM.....	79-97 (31 INCHES (19 SHEETS))
<input type="checkbox"/> M-606-13	GUARDRAIL TYPE 7 F-SHAPE BARRIER (4 SHEETS).....	98-101
<input type="checkbox"/> M-606-14	PRECAST TYPE 7 CONCRETE BARRIER (3 SHEETS).....	102-104
<input type="checkbox"/> M-606-15	GUARDRAIL TYPE 9 SINGLE SLOPE BARRIER	105-115 (11 SHEETS)
<input type="checkbox"/> M-607-1	WIRE FENCES AND GATES (3 SHEETS).....	116-118
<input type="checkbox"/> M-607-2	CHAIN LINK FENCE (3 SHEETS).....	119-121
<input type="checkbox"/> M-607-3	BARRIER FENCE.....	122
<input type="checkbox"/> M-607-4	DEER FENCE, GATES, AND GAME RAMPS (5 SHEETS).....	123-127
<input type="checkbox"/> M-607-10	PICKET SNOW FENCE	128
<input type="checkbox"/> M-607-15	ROAD CLOSURE GATE (9 SHEETS).....	129-137
<input checked="" type="checkbox"/> M-608-1	CURB RAMPS (10 SHEETS).....	138-147
<input checked="" type="checkbox"/> M-609-1	CURBS, GUTTERS, AND SIDEWALKS (4 SHEETS).....	148-151
<input type="checkbox"/> M-611-1	CATTLE GUARD (2 SHEETS).....	152-153
<input type="checkbox"/> M-611-2	DEER GUARD (2 SHEETS).....	154-155
<input type="checkbox"/> M-614-1	RUMBLE STRIPS (3 SHEETS).....	156-158
<input type="checkbox"/> M-614-2	SAND BARREL ARRAYS (2 SHEETS).....	159-160
<input type="checkbox"/> M-615-1	EMBANKMENT PROTECTOR TYPE 3	161
<input type="checkbox"/> M-615-2	EMBANKMENT PROTECTOR TYPE 5	162
<input type="checkbox"/> M-616-1	INVERTED SIPHON.....	163
<input type="checkbox"/> M-620-1	FIELD LABORATORY CLASS 1.....	164
<input type="checkbox"/> M-620-2	FIELD LABORATORY CLASS 2 (2 SHEETS).....	165-166
<input type="checkbox"/> M-620-11	FIELD OFFICE CLASS 1.....	167
<input type="checkbox"/> M-620-12	FIELD OFFICE CLASS 2.....	168
<input type="checkbox"/> M-629-1	SURVEY MONUMENTS (2 SHEETS).....	169-170

PLAN NUMBER	S STANDARD TITLE	PAGE NUMBER
<input type="checkbox"/> S-612-1	DELINEATOR INSTALLATIONS (8 SHEETS)	171-178
<input type="checkbox"/> S-613-1	ROADWAY LIGHTING (8 SHEETS).....	179-186
<input checked="" type="checkbox"/> S-614-1	TYPICAL GROUND SIGN PLACEMENT (2 SHEETS).....	187-188
<input checked="" type="checkbox"/> S-614-2	CLASS I SIGNS	189
<input checked="" type="checkbox"/> S-614-3	CLASS II SIGNS	190
<input type="checkbox"/> S-614-4	CLASS III SIGNS (3 SHEETS).....	191-193
<input type="checkbox"/> S-614-5	BREAK-AWAY SIGN SUPPORT DETAILS	194-195 (2 SHEETS)
<input type="checkbox"/> S-614-6	CONCRETE FOOTINGS AND SIGN ISLANDS.....	196-197 (2 SHEETS)
<input checked="" type="checkbox"/> S-614-8	TUBULAR STEEL SIGN SUPPORT DETAILS (7 SHEETS).....	198-204
<input checked="" type="checkbox"/> S-614-9	PEDESTRIAN PUSH BUTTON POST ASSEMBLY (2 SHEETS).....	205-206
<input type="checkbox"/> S-614-10	MARKER ASSEMBLY INSTALLATIONS	207
<input type="checkbox"/> S-614-11	MILEPOST SIGN DETAIL FOR HIGH SNOW AREAS	208
<input type="checkbox"/> S-614-12	STRUCTURE NUMBER INSTALLATION (2 SHEETS)	209-210
<input type="checkbox"/> S-614-14	FLASHING BEACON AND SIGN INSTALLATIONS (4 SHEETS).....	211-214
<input checked="" type="checkbox"/> S-614-20	TYPICAL POLE MOUNT SIGN INSTALLATIONS.....	215
<input type="checkbox"/> S-614-21	CONCRETE BARRIER SIGN POST INSTALLATIONS.....	216-217 (2 SHEETS)
<input checked="" type="checkbox"/> S-614-22	TYPICAL MULTI-SIGN INSTALLATIONS.....	218
<input type="checkbox"/> S-614-40	TYPICAL TRAFFIC SIGNAL 30'-75' DOUBLE MAST ARMS.....	219-223 (65'-75' SINGLE MAST ARMS (5 SHEETS))
<input checked="" type="checkbox"/> S-614-40A	ALTERNATIVE TRAFFIC SIGNAL	224-227 (25'-55' SINGLE MAST ARMS (4 SHEETS))
<input type="checkbox"/> S-614-41	TEMPORARY SPAN WIRE SIGNALS (13 SHEETS).....	228-240
<input checked="" type="checkbox"/> S-614-42	CABINET FOUNDATION DETAIL (4 SHEETS).....	241-244
<input type="checkbox"/> S-614-43	TRAFFIC LOOP AND MISCELLANEOUS SIGNAL DETAILS.....	245-252 (8 SHEETS)
<input type="checkbox"/> S-614-44	PEDESTAL POLE SIGNALS (2 SHEETS).....	253-254
<input type="checkbox"/> S-614-50	STATIC SIGN MONOTUBE STRUCTURES (12 SHEETS).....	255-266
<input type="checkbox"/> S-614-60	DYNAMIC SIGN MONOTUBE STRUCTURES (14 SHEETS).....	267-280
<input checked="" type="checkbox"/> S-627-1	PAVEMENT MARKINGS (9 SHEETS).....	281-289
<input checked="" type="checkbox"/> S-630-1	TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION.....	290-313 (24 SHEETS)
<input checked="" type="checkbox"/> S-630-2	BARRICADES, DRUMS, CONCRETE BARRIERS (TEMP).....	314 AND VERTICAL PANELS
<input type="checkbox"/> S-630-3	FLASHING BEACON (PORTABLE) DETAILS	315
<input type="checkbox"/> S-630-4	STEEL SIGN SUPPORT (TEMPORARY) INSTALLATION.....	316-317 (2 SHEETS)
<input type="checkbox"/> S-630-5	PORTABLE RUMBLE STRIPS (TEMPORARY) (2 SHEETS)	318-319
<input type="checkbox"/> S-630-6	EMERGENCY PULL-OFF AREA (TEMPORARY)	320
<input type="checkbox"/> S-630-7	ROLLING ROADBLOCKS FOR TRAFFIC CONTROL	321-323 (3 SHEETS)

COLORADO
DEPARTMENT OF TRANSPORTATION
TABLE OF CONTENTS
M&S STANDARDS
JULY 31, 2019

<p style="text-align: center;">48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS</p> <p style="text-align: center;">811</p> <p style="text-align: center;">UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW</p> <p style="font-size: 8px;">THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO. REVISION</th> <th>DATE</th> </tr> </thead> <tbody> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </tbody> </table>	NO. REVISION	DATE															<p>REVIEW:</p> <p>PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC</p> <p>MARC A. WHORTON, COLORADO P.E. #37155 DATE</p>		<p>STATE HIGHWAY 105 / JACKSON CREEK PRWY. - PHASE 2 CONSTRUCTION PLANS</p> <p>CDOT STANDARD PLAN SHEET</p> <table border="1" style="width: 100%; border-collapse: collapse; font-size: 8px;"> <tr> <td>DESIGNED BY</td> <td>PRA</td> <td>SCALE</td> <td>DATE</td> <td>02-23-23</td> </tr> <tr> <td>DRAWN BY</td> <td>PRA</td> <td>(H) 1"= N/A</td> <td>SHEET</td> <td>3 OF 23</td> </tr> <tr> <td>CHECKED BY</td> <td></td> <td>(V) 1"= N/A</td> <td>JOB NO.</td> <td>1.302.22</td> </tr> </table>	DESIGNED BY	PRA	SCALE	DATE	02-23-23	DRAWN BY	PRA	(H) 1"= N/A	SHEET	3 OF 23	CHECKED BY		(V) 1"= N/A	JOB NO.	1.302.22
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REVIEW:	
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MARC. A. WHORTON, COLORADO P.E. #37155	DATE

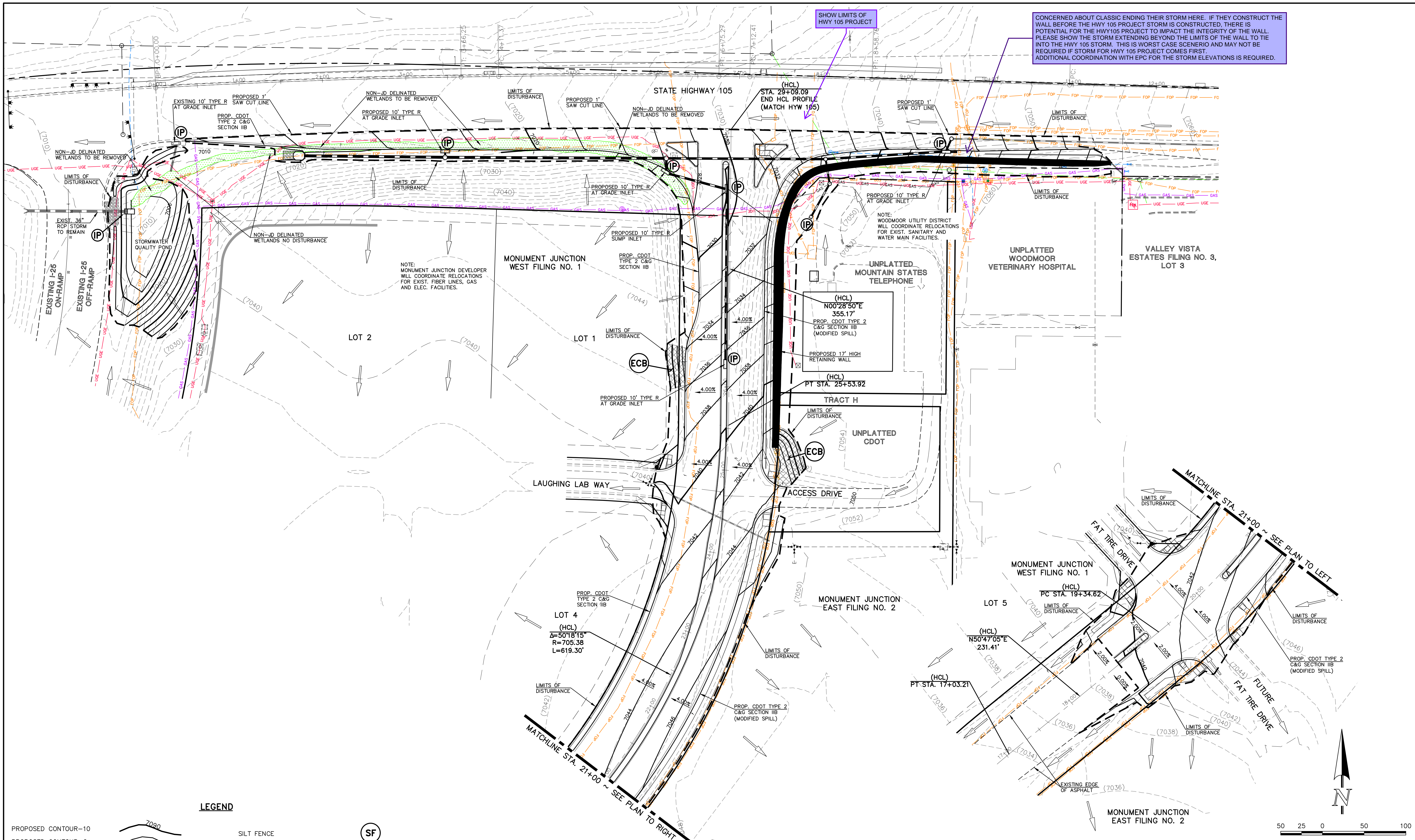
619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903

(719) 785-0790
(719) 785-0799 (fax)

CLASSIC
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STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS			
ROADWAY IMPROVEMENT PLANS			
DEMOLITION PLAN			
DESIGNED BY	PRA	SCALE	DATE 02-23-23
DRAWN BY	PRA	(H) 1" = 60'	SHEET 4 OF 23
CHECKED BY	(V) 1" = N/A	JOB NO.	1302.22

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LEGEND

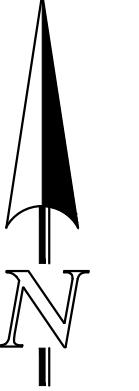
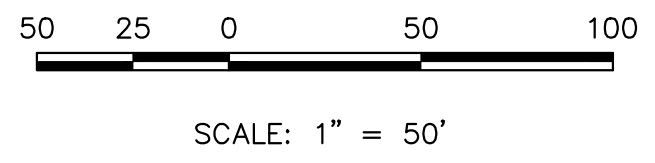
- PROPOSED CONTOUR-10
- PROPOSED CONTOUR-2
- EXISTING CONTOUR-10
- EXISTING CONTOUR-2
- DIRECTION OF FLOW
- EXIST. DIRECTION OF FLOW
- EROSION CONTROL BLANKET (NORTH AMERICAN GREEN - SC150 OR EQUIVALENT) TO BE INSTALLED ON ALL 3:1 SLOPES OR GREATER
- SILT FENCE
- ROCK CHECK DAM
- INLET PROTECTION
- LIMIT OF DISTURBANCE
- SILT FENCE

48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS		NO. REVISION	DATE
811 UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW			
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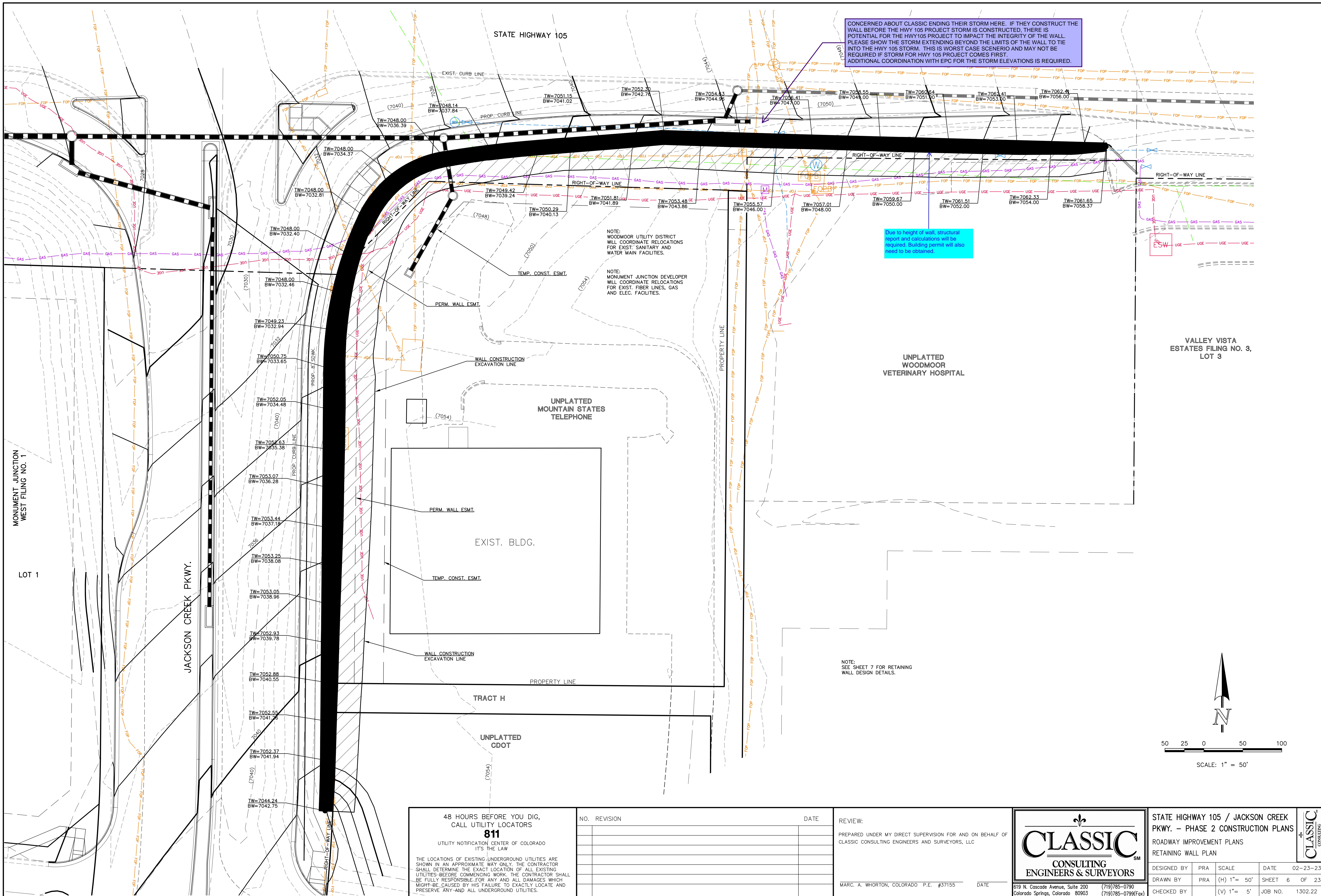
REVIEW:	DATE
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC	
MARC. A. WHORTON, COLORADO P.E. #37155	DATE

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STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS			
ROADWAY IMPROVEMENTS PLANS			
GRADING & EROSION CONTROL PLAN			
DESIGNED BY	PRA	SCALE	DATE 02-23-23
DRAWN BY	PRA	(H) 1" = 50'	SHEET 5 OF 23
CHECKED BY	(V) 1" = 5'	JOB NO.	1302.22



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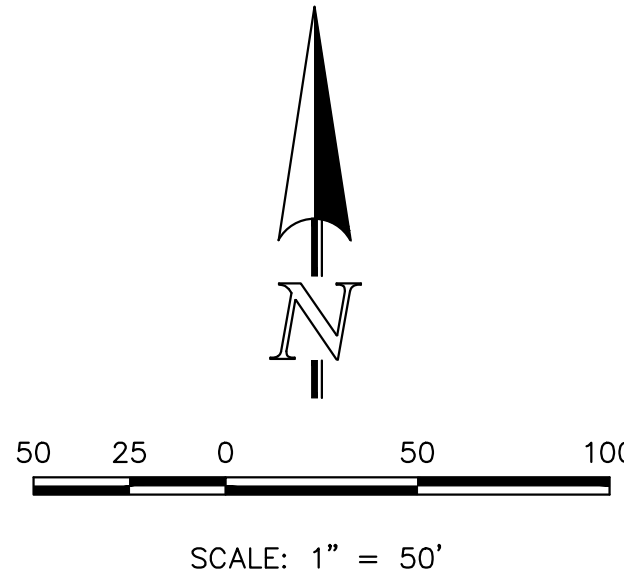
CONCERNED ABOUT CLASSIC ENDING THEIR STORM HERE. IF THEY CONSTRUCT THE WALL BEFORE THE HWY 105 PROJECT STORM IS CONSTRUCTED, THERE IS POTENTIAL FOR THE HWY 105 PROJECT TO IMPACT THE INTEGRITY OF THE WALL. PLEASE SHOW THE STORM EXTENDING BEYOND THE LIMITS OF THE WALL TO TIE INTO THE HWY 105 STORM. THIS IS WORST CASE SCENARIO AND MAY NOT BE REQUIRED IF STORM FOR HWY 105 PROJECT COMES FIRST. ADDITIONAL COORDINATION WITH EPC FOR THE STORM ELEVATIONS IS REQUIRED.

Due to height of wall, structural report and calculations will be required. Building permit will also need to be obtained.

NOTE: WOODMOOR UTILITY DISTRICT WILL COORDINATE RELOCATIONS FOR EXIST. SANITARY AND WATER MAIN FACILITIES.

NOTE: MONUMENT JUNCTION DEVELOPER WILL COORDINATE RELOCATIONS FOR EXIST. FIBER LINES, GAS AND ELEC. FACILITIES.

NOTE: SEE SHEET 7 FOR RETAINING WALL DESIGN DETAILS.



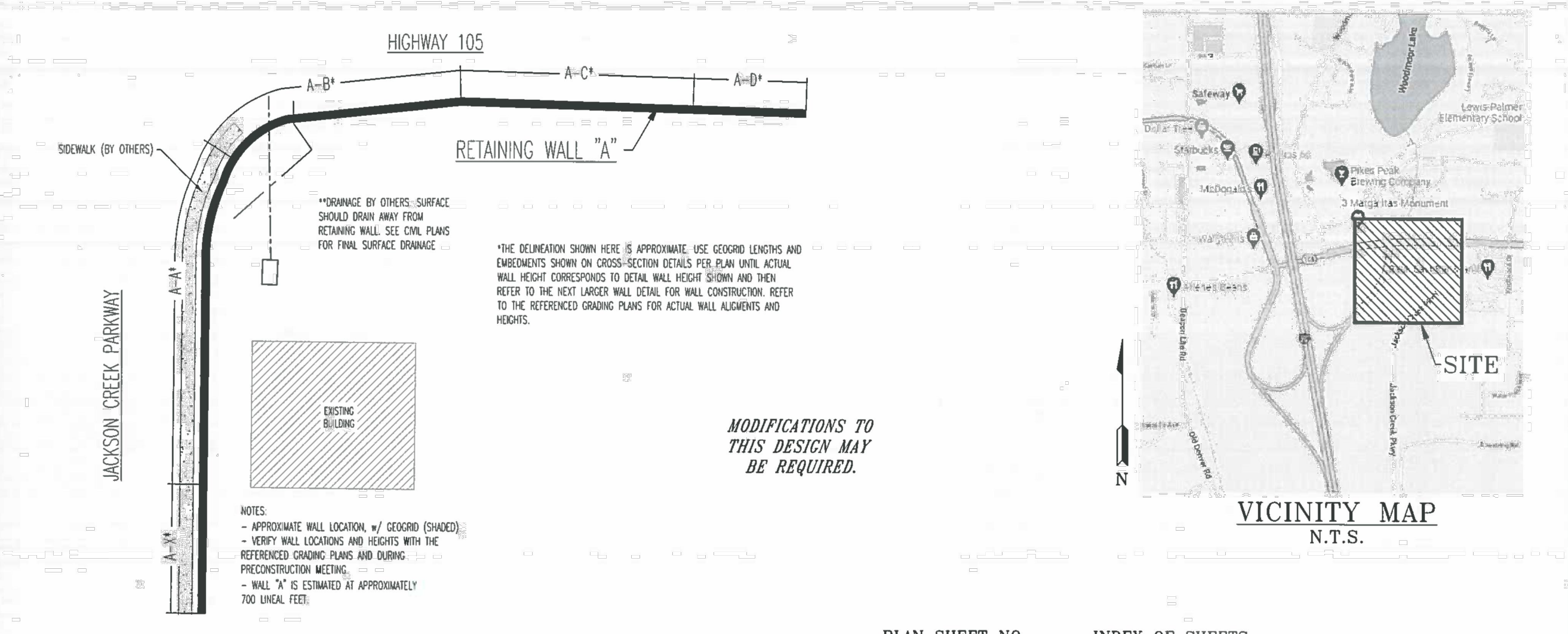
<p>48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS 811 UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW</p> <p>THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</p>	NO. REVISION	DATE	REVIEW:

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STATE HIGHWAY 105 / JACKSON CREEK PKWY. -- PHASE 2 CONSTRUCTION PLANS			
ROADWAY IMPROVEMENT PLANS			
RETAINING WALL PLAN			
DESIGNED BY	PRA	SCALE	DATE 02-23-23
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CHECKED BY	(V) 1" = 5'	JOB NO.	1302.22

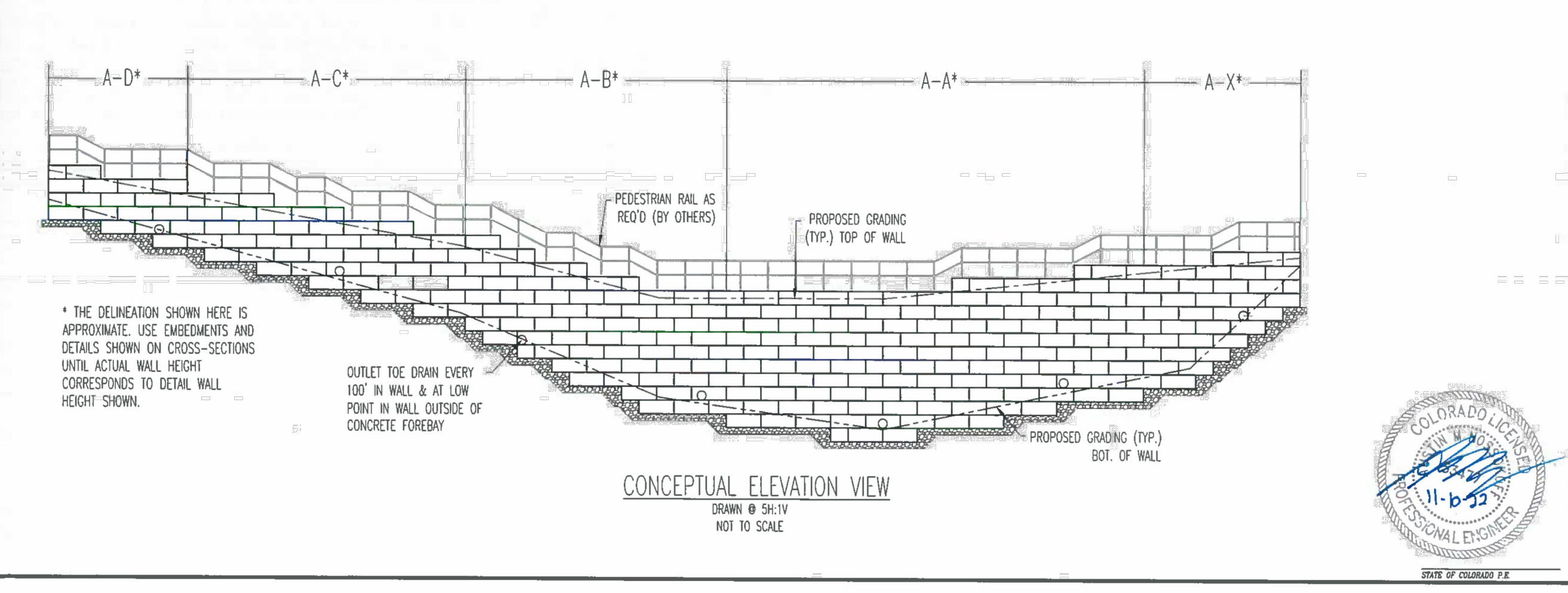
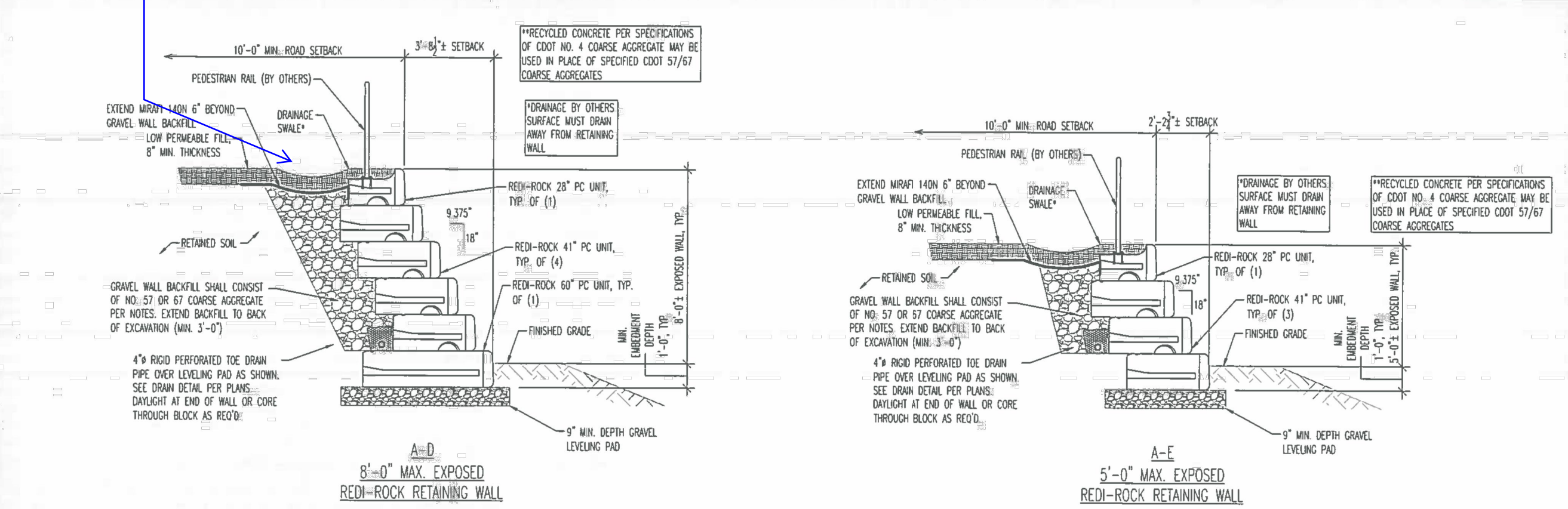
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REDI-ROCK RETAINING WALL JACKSON CREEK PARKWAY & STATE HIGHWAY 105



NOTE: THIS DESIGN WAS BASED ON CONSTRUCTION PLANS BY CLASSIC CONSULTING ENGINEERS & SURVEYORS, AND THE SUBSURFACE SOILS INVESTIGATION BY ENTECH ENGINEERING, INC., DATED OCTOBER 20, 2022. ENTECH JOB NO. 221489. CONTRACTOR TO VERIFY RETAINING WALL DESIGN TO FINAL PLANS. IF THE RETAINED SOILS DIFFER FROM OUR ASSUMED SOIL TYPES, THEN THE ENGINEER MUST BE NOTIFIED AND MODIFICATIONS TO THIS DESIGN MAY BE REQUIRED.

Include calculation in drainage report to show that swales is sized to adequately handle flow.



ENTECH ENGINEERING, INC.
CONSULTING ENGINEERS & SURVEYORS
1110 S. W. 10th St., Suite 200
Colorado Springs, CO 80903
(719) 533-9999

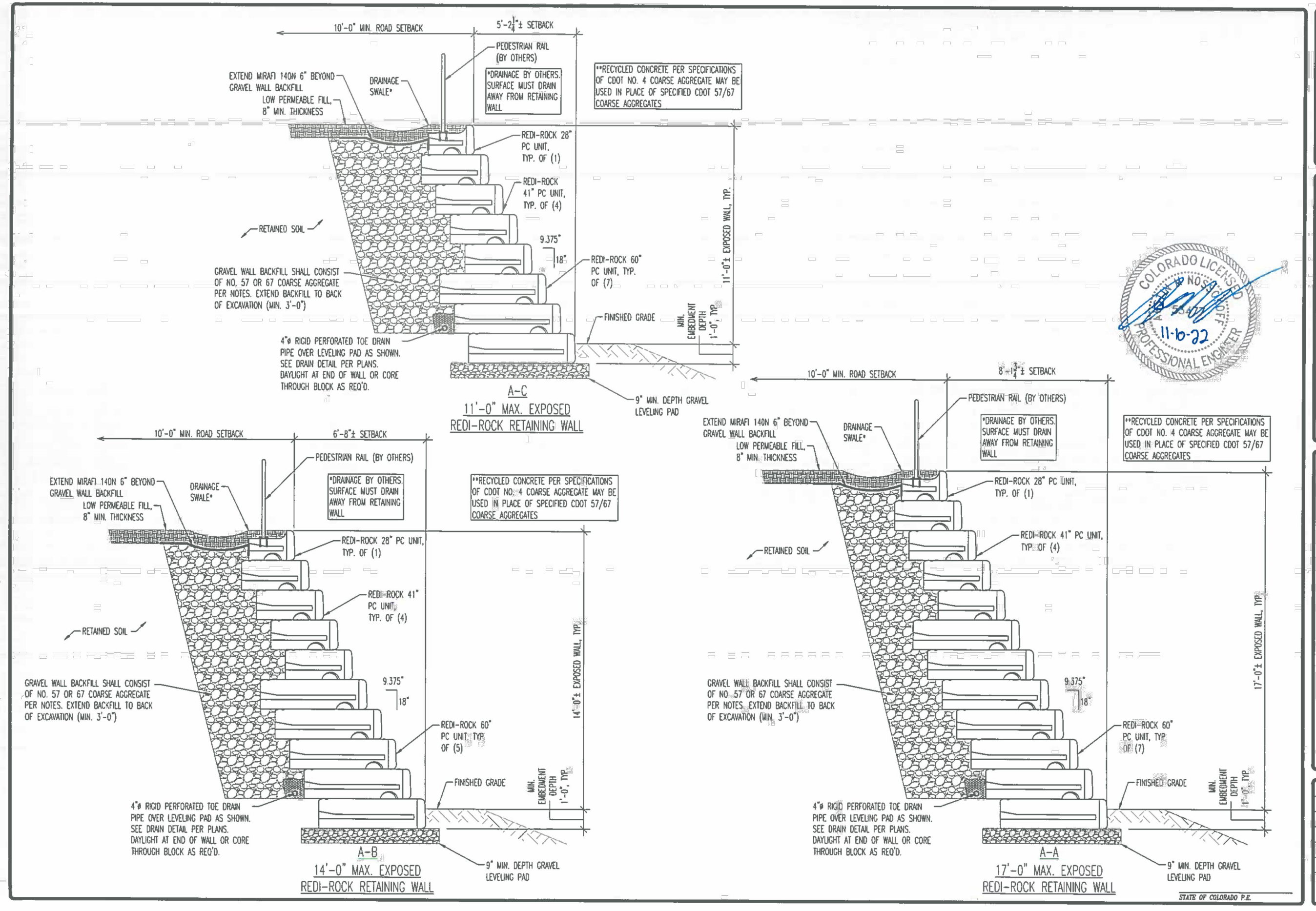
**REDI-ROCK RETAINING WALL
PLAN & COVER SHEET
JACKSON CREEK PKWY & STATE HWY 105
FOR: CLASSIC COMMUNITIES**

DESIGN BY: JAW
CHECKED BY: JAW
DATE: 11/16/2022
SCALE: N.T.S.
JOB NO: 221489
SHEET NO: 1
OF 4 SHEETS

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CONSULTING ENGINEERS & SURVEYORS
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**REDI-ROCK RETAINING WALL
GENERAL NOTES
JACKSON CREEK PARKWAY & HWY 105
FOR: CLASSIC COMMUNITIES**

DESIGN BY: JAW
CHECKED BY: JAW
DATE: 11/16/2022
SCALE: N.T.S.
JOB NO: 221489
SHEET NO: 2
OF 4 SHEETS



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**REDI-ROCK RETAINING WALL
GENERAL NOTES
JACKSON CREEK PARKWAY & HWY 105
FOR: CLASSIC COMMUNITIES**

DESIGN BY: JAW
CHECKED BY: JAW
DATE: 11/16/2022
SCALE: N.T.S.
JOB NO: 221489
SHEET NO: 3
OF 4 SHEETS

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**REDI-ROCK RETAINING WALL
WALL "A" SECTIONS
JACKSON CREEK PARKWAY & HWY 105
FOR: CLASSIC COMMUNITIES**

DESIGN BY: JAW
CHECKED BY: JAW
DATE: 11/16/2022
SCALE: N.T.S.
JOB NO: 221489
SHEET NO: 4
OF 4 SHEETS

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NO. REVISION	DATE	REVIEW:

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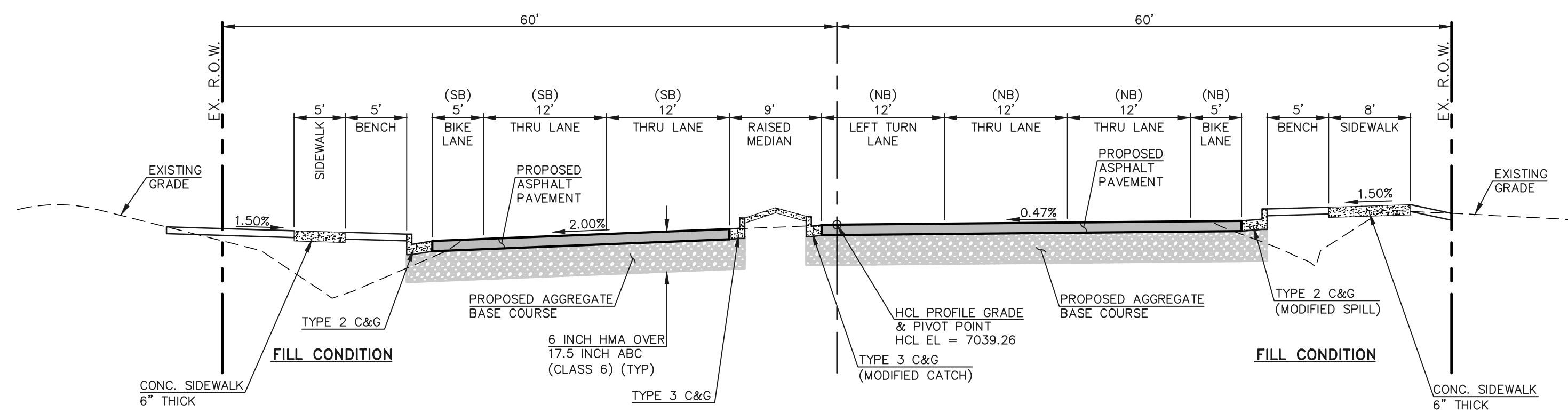
MARC. A. WHORTON, COLORADO P.E. #37155 DATE: _____

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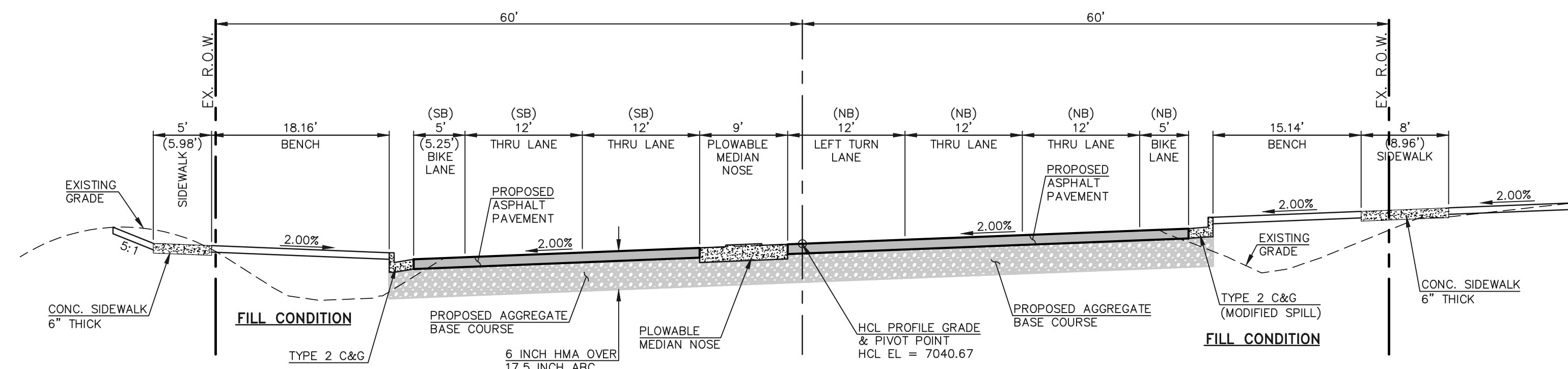
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Colorado Springs, Colorado 80903
(719) 785-0790
(719) 785-0799 (Fax)

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS ROADWAY IMPROVEMENT PLANS RETAINING WALL DETAILS			
DESIGNED BY:	PRA	SCALE:	DATE: 02-23-23
DRAWN BY:	PRA	(H) 1" = 50'	SHEET 7 OF 23
CHECKED BY:	(V) 1" = 5'	JOB NO.:	1302.22

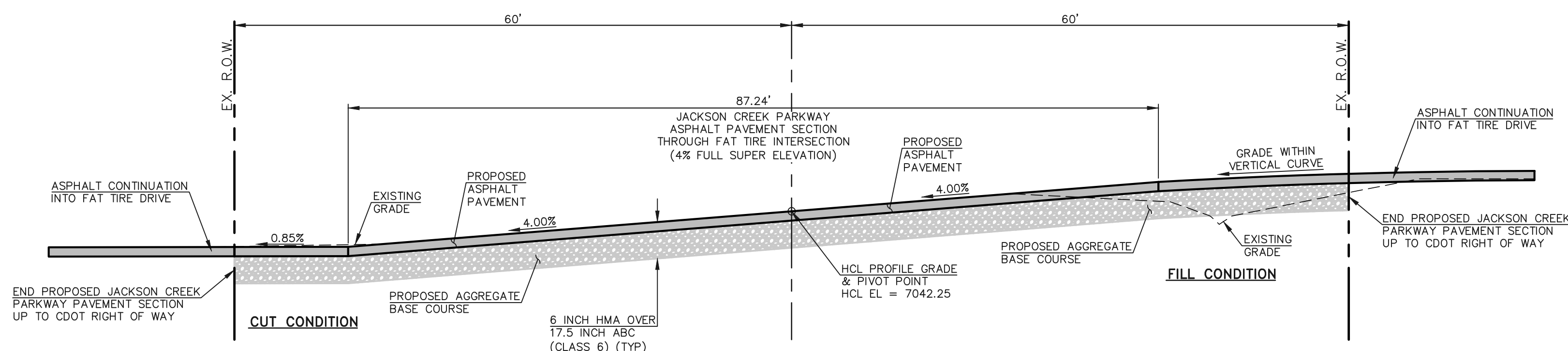
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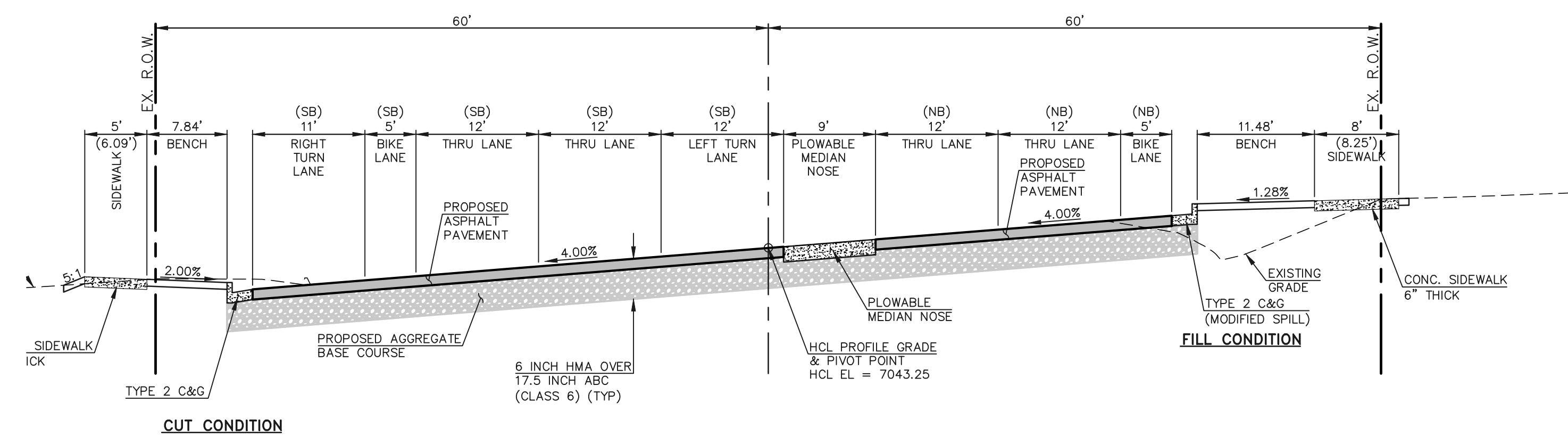
JACKSON CREEK PARKWAY SECTION
STA. 18+73.48
SCALE: 1"=10' (H), 1"=5' (V)



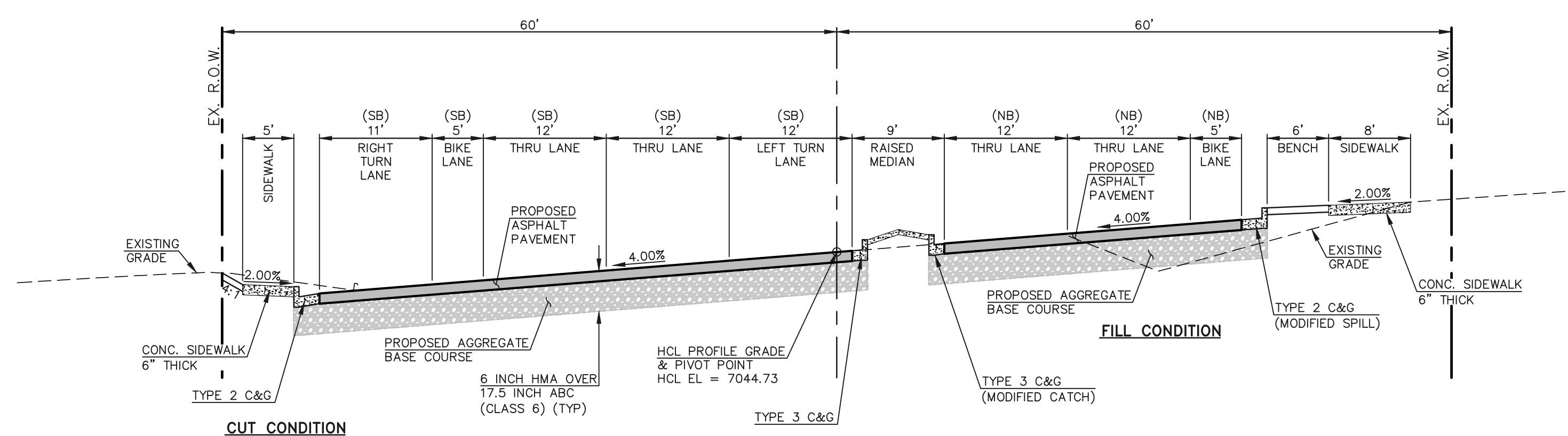
JACKSON CREEK PARKWAY SECTION
STA. 19+22.22
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 19+84.20
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 20+34.01
SCALE: 1"=10' (H), 1"=5' (V)



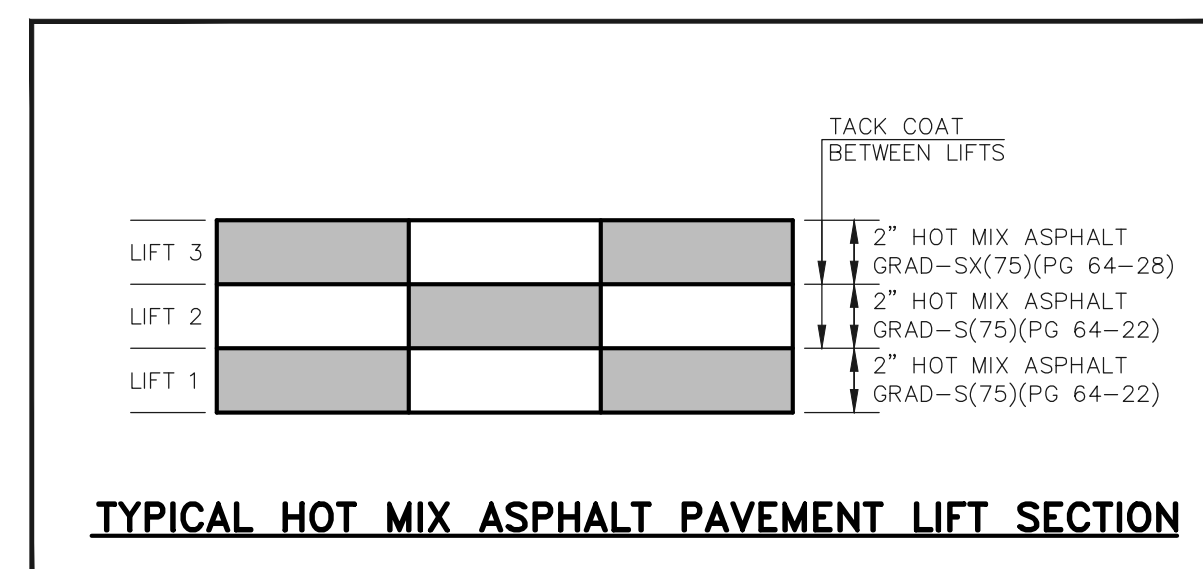
JACKSON CREEK PARKWAY SECTION
STA. 22+63.43
SCALE: 1"=10' (H), 1"=5' (V)

TYPICAL SECTION NOTES

- BREAK POINTS IN SLOPES SHALL BE ROUNDED BY THE CONTRACTOR FOR A PLEASING APPEARANCE DURING CONSTRUCTION.
- SEE JACKSON CREEK PARKWAY IMPROVEMENT PLANS (SHEETS 11 THRU 13) FOR ADDITIONAL INFORMATION ON THE ROADWAY SECTION DIMENSIONS.
- A TWO-INCH OVERLAY OF THE ROADWAY IN AREAS OF RECONSTRUCTION AND/OR RE-STRIPING IS REQUIRED. THE WIDTH OF THE OVERLAY SHALL ENCOMPASS ALL LANES CONTIGUOUS TO AREAS OF RECONSTRUCTION AND/OR RE-STRIPING.
- SEE PROPOSED JACKSON CREEK PARKWAY WIDENING, HIGHWAY 105 TO HIGBY ROAD - GEOTECHNICAL EVALUATION REPORT (DRAFT) BY VIVID ENGINEERING GROUP "VIVID PROJECT NO.: D21-2-456" DATED 2-18-2022 FOR INFORMATION ON SUBGRADE PREPARATION.
- THE CONTRACTOR SHALL VERIFY ALL SUBGRADE CONDITIONS IN CUT AREAS PRIOR TO PREPARATION AND PAVING.
- FOR FILL CONDITIONS, THE CONTRACTOR SHALL VERIFY SUBGRADE CONDITIONS THROUGH COMPACTION AND DENSITY TESTING PRIOR TO PAVING.
- SMOOTHNESS CATEGORY SHALL BE CATEGORY 2.

HOT MIX ASPHALT PAVEMENT STRUCTURE TABLE

TOTAL HOT MIX ASPHALT DEPTH	6.0"
# OF LIFTS	3
HOT MIX ASPHALT BINDER COURSE	LIFT 1 2.0"
GRAD-S(75) (PG 64-22)	LIFT 2 2.0"
HOT MIX ASPHALT SURFACE COURSE	LIFT 3 2.0"
GRAD-SX(75) (PG 64-28)	

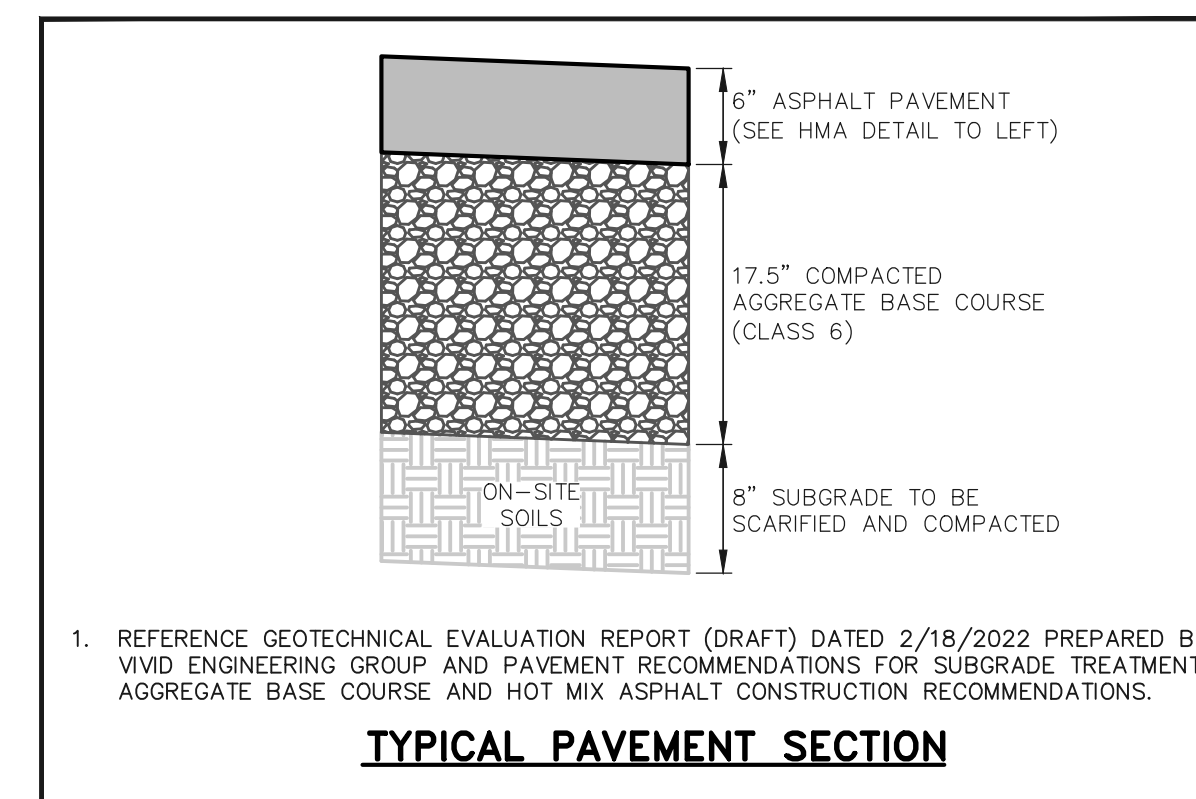


SUBGRADE NOTES:

SUBGRADE FOR CUT CONDITIONS:
 IF SUBGRADE IS R >= 17, THEN RECONDITION 12 INCHES OF SUBGRADE BELOW THE 12 INCHES OF CEMENT TREATED SUBGRADE. [RECONDITIONING IS SPECIFIED IN SECTION 306 OF THE CDOT STANDARD SPECIFICATIONS].

IF SUBGRADE IS R < 17, THEN OVER EXCAVATE 24 INCHES AND REPLACE WITH R >= 17 MATERIAL AND PERFORM CEMENT TREATED SUBGRADE ON TOP 12 INCHES.

SUBGRADE FOR FILL CONDITIONS:
 ALL FILL SHALL BE R >= 17 MATERIAL WITH 12 INCHES OF CEMENT TREATED SUBGRADE ON TOP OF FILL MATERIAL.



- REFERENCE GEOTECHNICAL EVALUATION REPORT (DRAFT) DATED 2/18/2022 PREPARED BY VIVID ENGINEERING GROUP AND PAVEMENT RECOMMENDATIONS FOR SUBGRADE TREATMENT, AGGREGATE BASE COURSE AND HOT MIX ASPHALT CONSTRUCTION RECOMMENDATIONS.

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NO.	REVISION	DATE	REVIEW:

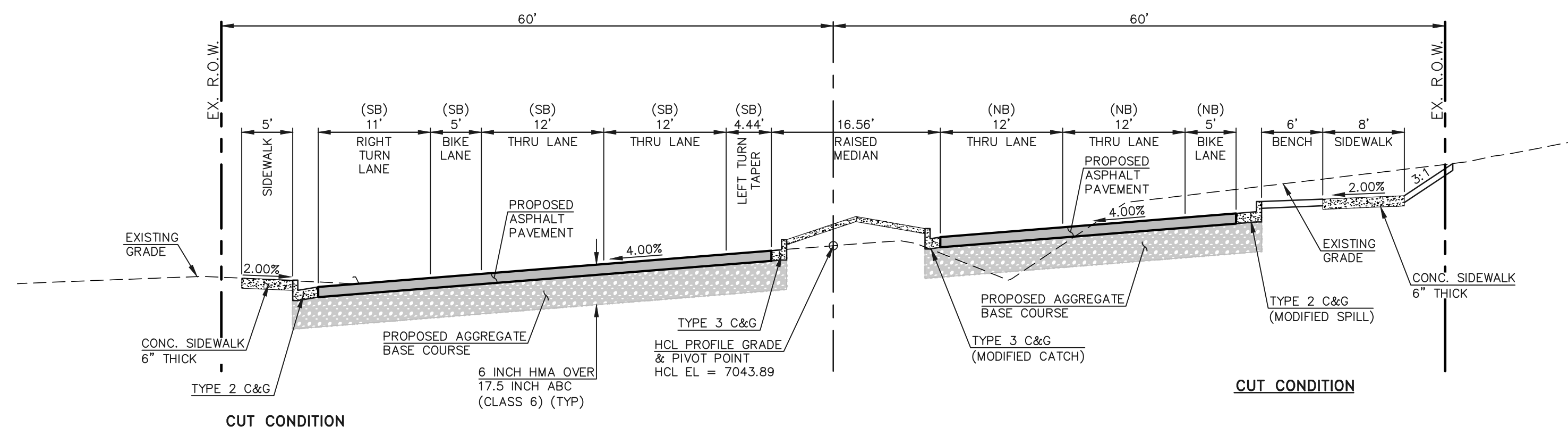
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC A. WHORTON, COLORADO P.E. #37155 DATE

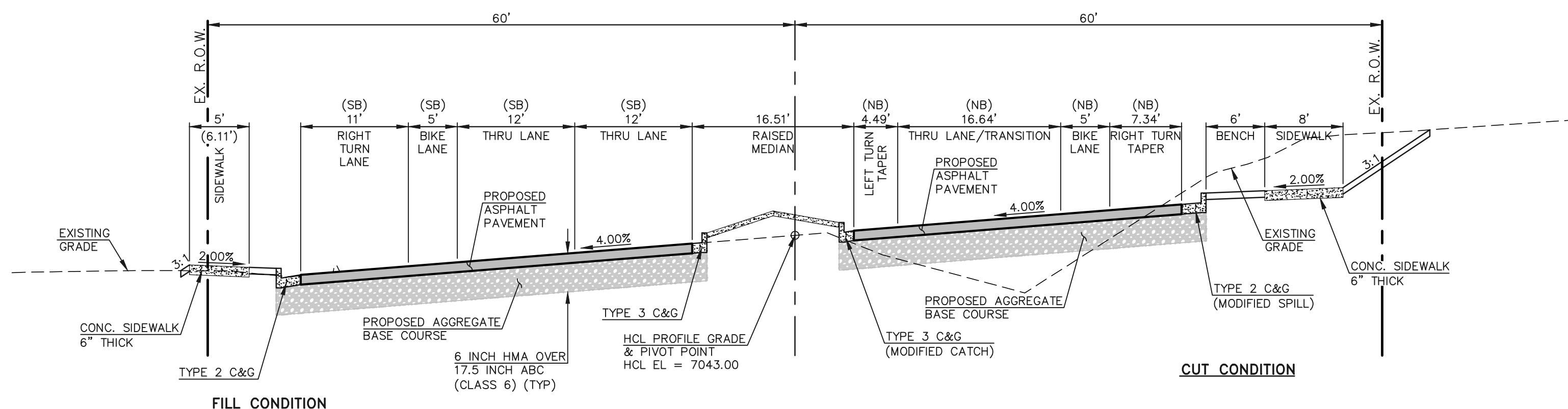
619 N. Cascade Avenue, Suite 200
 Colorado Springs, Colorado 80903
 (719) 785-0790
 (719) 785-0799 (Fax)

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
 ROADWAY IMPROVEMENT PLANS
 JACKSON CREEK PKWY. ROADWAY SECTIONS

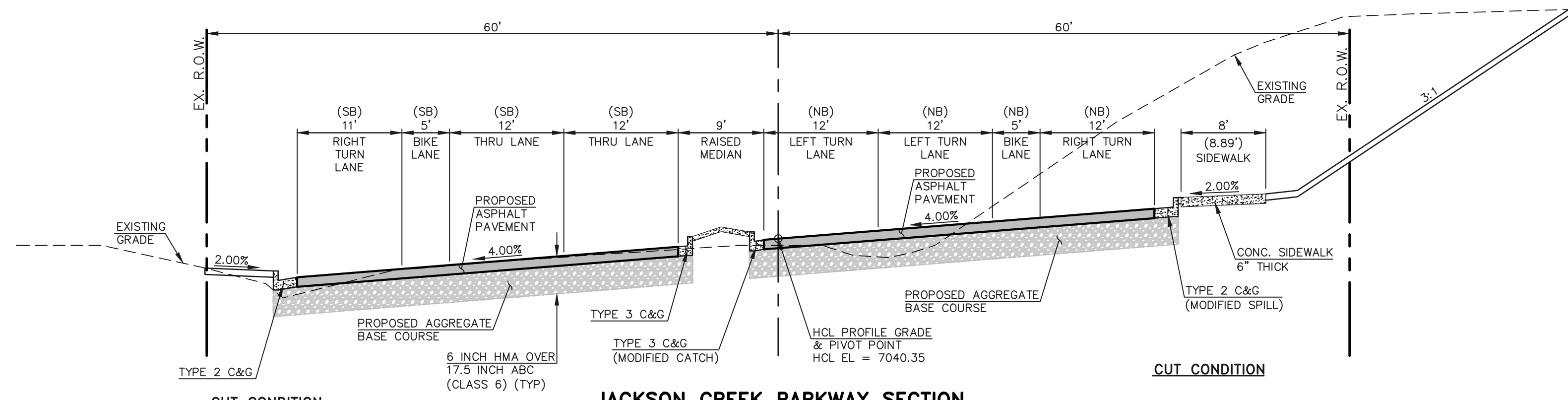
DESIGNED BY	PRA	SCALE	DATE	02-23-23
DRAWN BY	PRA	(H) 1"= 50'	SHEET	8 OF 23
CHECKED BY		(V) 1"= N/A	JOB NO.	1302.22



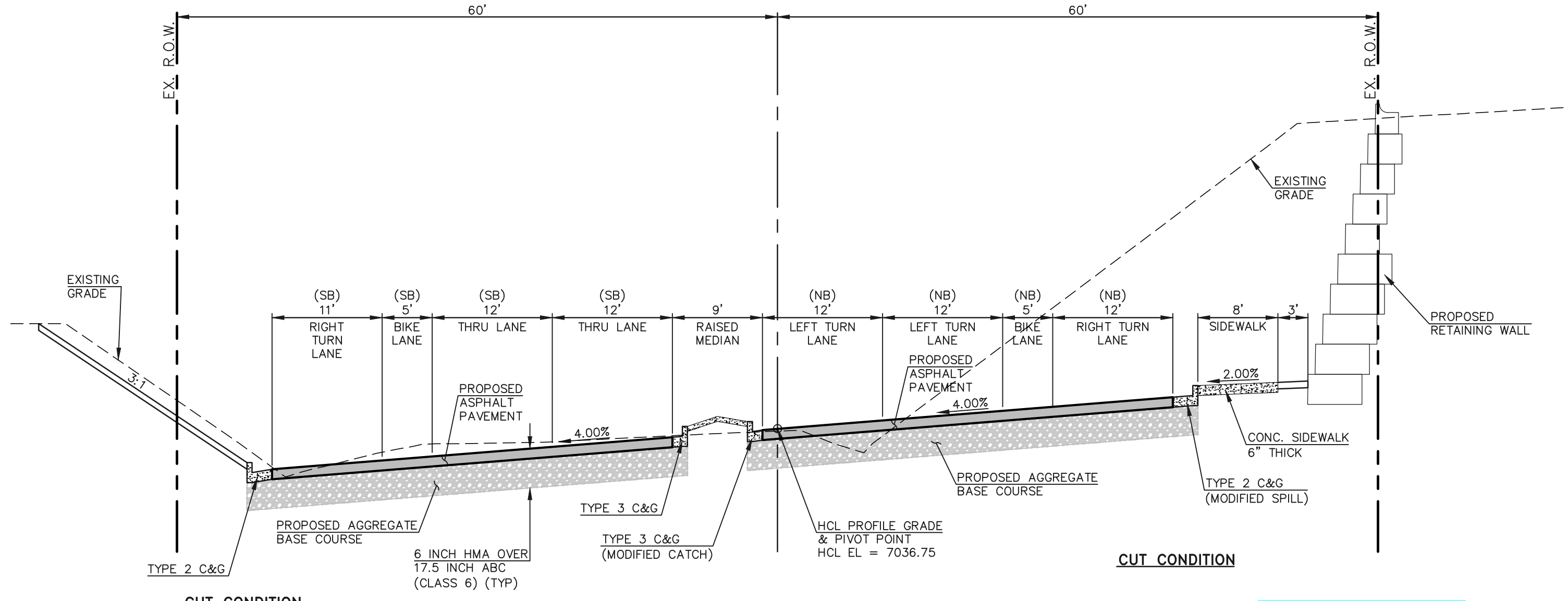
JACKSON CREEK PARKWAY SECTION
STA. 23+55.45
SCALE: 1"=10' (H), 1"=5' (V)



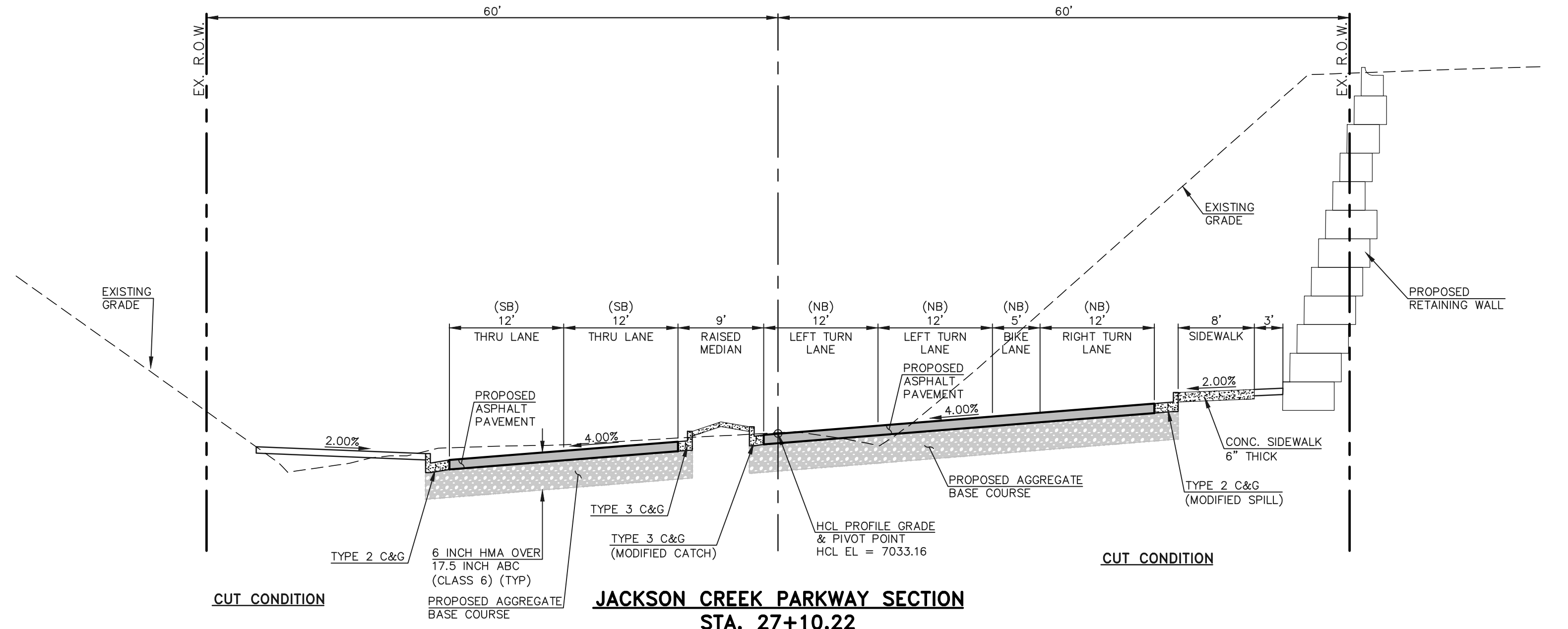
JACKSON CREEK PARKWAY SECTION
STA. 24+09.60
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 25+26.86
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 26+29.82
SCALE: 1"=10' (H), 1"=5' (V)



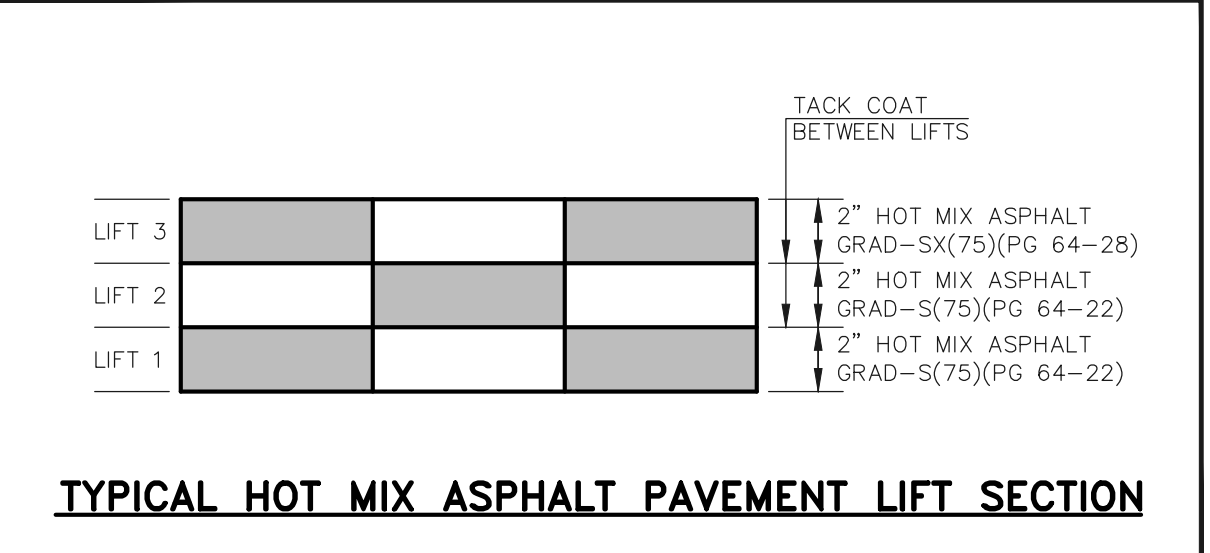
JACKSON CREEK PARKWAY SECTION
STA. 27+10.22
SCALE: 1"=10' (H), 1"=5' (V)

TYPICAL SECTION NOTES

- BREAK POINTS IN SLOPES SHALL BE ROUNDED BY THE CONTRACTOR FOR A PLEASING APPEARANCE DURING CONSTRUCTION.
- SEE JACKSON CREEK PARKWAY IMPROVEMENT PLANS (SHEETS 11 THRU 13) FOR ADDITIONAL INFORMATION ON THE ROADWAY SECTION DIMENSIONS.
- A TWO-INCH OVERLAY OF THE ROADWAY IN AREAS OF RECONSTRUCTION AND/OR RE-STRIPING IS REQUIRED. THE WIDTH OF THE OVERLAY SHALL ENCOMPASS ALL LANES CONTIGUOUS TO AREAS OF RECONSTRUCTION AND/OR RE-STRIPING.
- SEE PROPOSED JACKSON CREEK PARKWAY WIDENING, HIGHWAY 105 TO HIGBY ROAD - GEOTECHNICAL EVALUATION REPORT (DRAFT) BY VIVID ENGINEERING GROUP "VIVID PROJECT NO.: D21-2-456" DATED 2-18-2022 FOR INFORMATION ON SUBGRADE PREPARATION.
- THE CONTRACTOR SHALL VERIFY ALL SUBGRADE CONDITIONS IN CUT AREAS PRIOR TO PREPARATION AND PAVING.
- FOR FILL CONDITIONS, THE CONTRACTOR SHALL VERIFY SUBGRADE CONDITIONS THROUGH COMPACTION AND DENSITY TESTING PRIOR TO PAVING.
- SMOOTHNESS CATEGORY SHALL BE CATEGORY 2.

HOT MIX ASPHALT PAVEMENT STRUCTURE TABLE

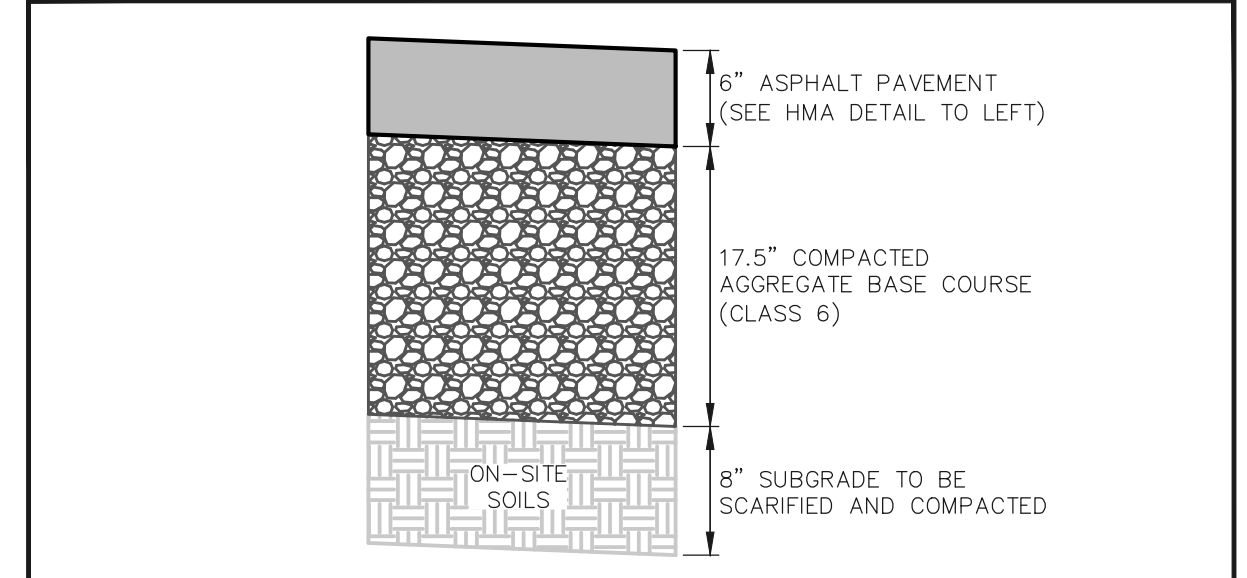
TOTAL HOT MIX ASPHALT DEPTH	6.0"
# OF LIFTS	3
HOT MIX ASPHALT BINDER COURSE GRAD-S(75) (PG 64-22)	LIFT 1 2.0"
HOT MIX ASPHALT SURFACE COURSE GRAD-SX(75) (PG 64-28)	LIFT 2 2.0"
	LIFT 3 2.0"



SUBGRADE NOTES:

SUBGRADE FOR CUT CONDITIONS:
 IF SUBGRADE IS R >= 17, THEN RECONDITION 12 INCHES OF SUBGRADE BELOW THE 12 INCHES OF CEMENT TREATED SUBGRADE. [RECONDITIONING IS SPECIFIED IN SECTION 306 OF THE CDOT STANDARD SPECIFICATIONS].
 IF SUBGRADE IS R < 17, THEN OVER EXCAVATE 24 INCHES AND REPLACE WITH R >= 17 MATERIAL AND PERFORM CEMENT TREATED SUBGRADE ON TOP 12 INCHES.

SUBGRADE FOR FILL CONDITIONS:
 ALL FILL SHALL BE R >= 17 MATERIAL WITH 12 INCHES OF CEMENT TREATED SUBGRADE ON TOP OF FILL MATERIAL.



1. REFERENCE GEOTECHNICAL EVALUATION REPORT (DRAFT) DATED 2/18/2022 PREPARED BY VIVID ENGINEERING GROUP AND PAVEMENT RECOMMENDATIONS FOR SUBGRADE TREATMENT, AGGREGATE BASE COURSE AND HOT MIX ASPHALT CONSTRUCTION RECOMMENDATIONS.

PROVIDE SECTION FOR HWY 105 (EPC). SHOW MILL & OVERLAY DEPTHS ON SECTION. ALSO SHOW ASPHALT AND BASE DEPTHS - SHOULD MATCH HWY 105 DESIGN DEPTHS FOR WIDENED AREA

Roadway Cross sections along Highway 105 would be helpful

Include section for SH 105

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 ROADWAY IMPROVEMENT PLANS
 JACKSON CREEK PKWY. ROADWAY SECTIONS

DESIGNED BY PRA SCALE DATE 02-23-23
 DRAWN BY PRA (H) 1"= 50' SHEET 9 OF 23
 CHECKED BY (V) 1"= N/A JOB NO. 1302.22



MONUMENT JUNCTION WEST FILING NO. 1

LOT 5

MONUMENT JUNCTION WEST FILING NO. 1

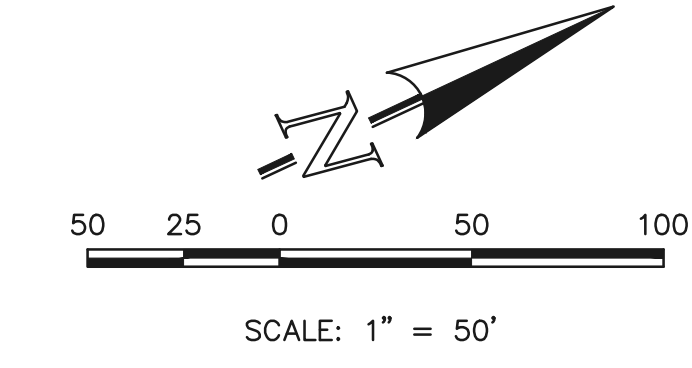
LOT 1

LOT 4

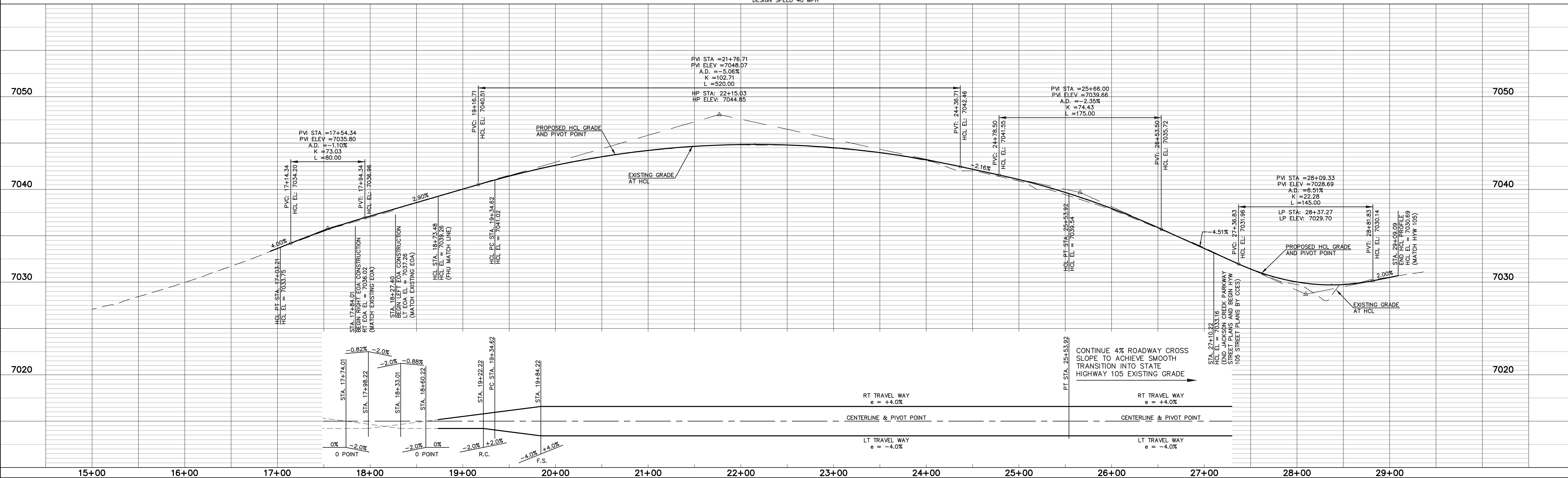
MONUMENT JUNCTION EAST FILING NO. 2

MONUMENT JUNCTION EAST FILING NO. 2

JACKSON CREEK PARKWAY
(120' ROW - MAJOR COLLECTOR)
DESIGN SPEED 40 MPH



CURB & GUTTER FLOWLINE CURVE TABLE	CURB & GUTTER FLOWLINE LINE TABLE
CURVE LENGTH RADIUS DELTA	LINE LENGTH BEARING
C7 45.40 30.00 86°42'55"	L6 49.63 N50°47'05"E
C8 45.27 30.00 86°27'28"	L7 11.02 S42°30'00"E
C9 443.55 746.88 34°01'34"	L8 156.30 N00°28'50"E
C10 42.53 30.00 81°14'08"	L9 45.43 N50°47'05"E
C11 48.47 30.00 92°33'49"	L10 75.90 N00°28'50"E
C12 42.01 746.88 031°3'21"	
C13 48.84 30.00 93°17'15"	
C14 49.63 30.00 94°47'20"	
C15 357.54 652.88 31°22'39"	
C16 49.75 30.00 95°01'08"	
C17 49.06 30.00 93°41'56"	
C18 25.04 652.88 021°15'22"	
C19 41.25 105.00 22°30'38"	
C20 41.25 105.00 22°30'38"	

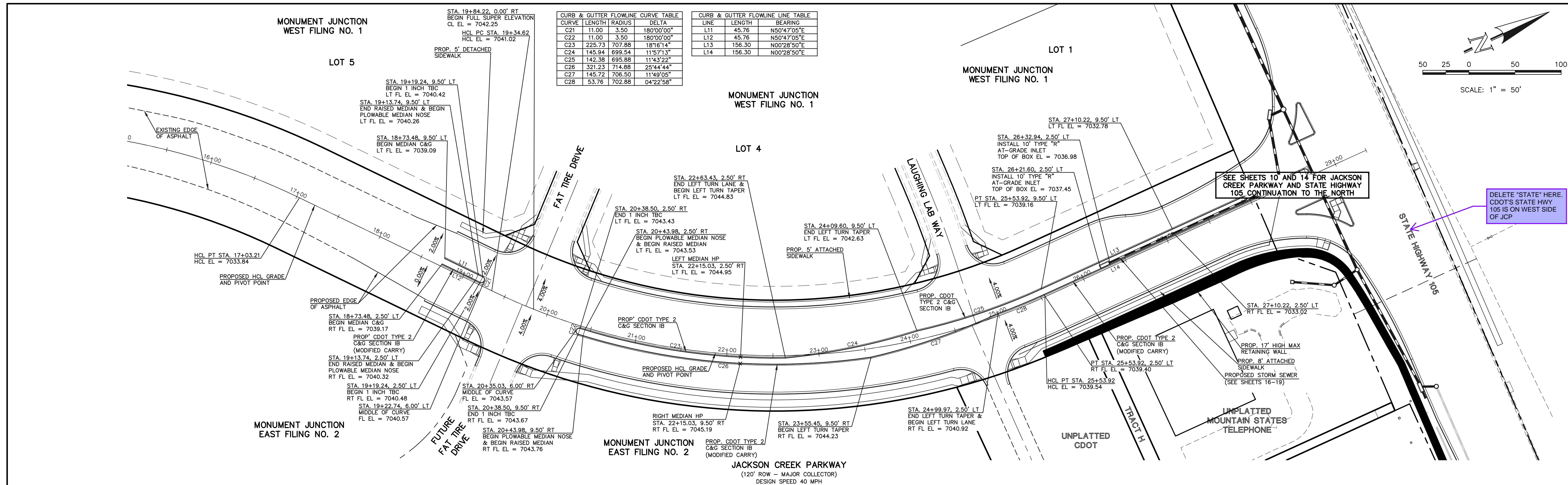


SEE SHEETS 10 AND 14 FOR JACKSON CREEK PARKWAY AND STATE HIGHWAY 105 CONTINUATION TO THE NORTH

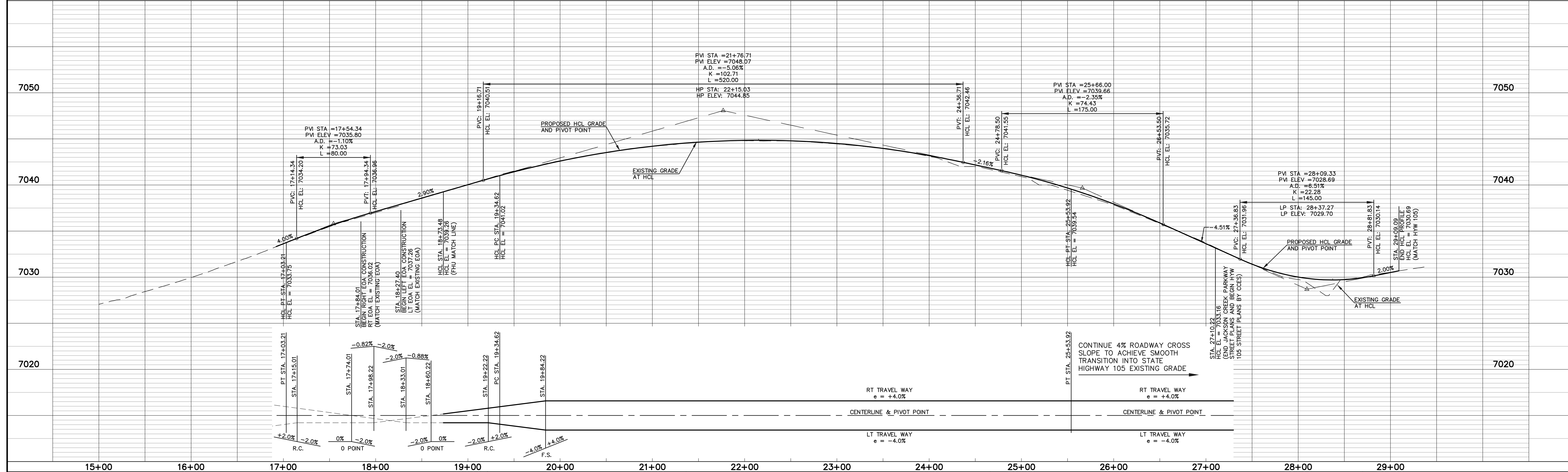
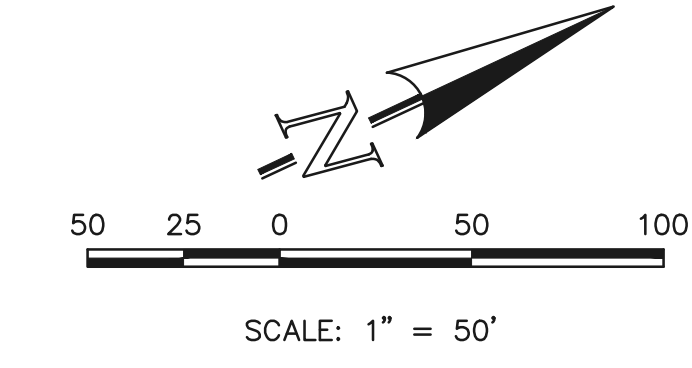
DELETE 'STATE' HERE. CDOT'S STATE HWY 105 IS ON WEST SIDE OF JCP.

V:\130222\PRJ\AMG\SS\CONSTRUCT\CON\STATE-11-8-02.dwg, 2/23/2023 4:02:27 PM, 1-1

<p>48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS</p> <p style="text-align: center;">811</p> <p style="text-align: center;">UTILITY NOTIFICATION CENTER OF COLORADO ITS THE LAW</p> <p style="font-size: small;">THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>REVISION</th> <th>DATE</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	REVISION	DATE							<p>REVIEW:</p> <p style="font-size: x-small;">PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC</p> <p>MARC. A. WHORTON, COLORADO P.E. #37155 DATE</p>	<p>CLASSIC CONSULTING ENGINEERS & SURVEYORS</p> <p style="font-size: x-small;">619 N. Cascade Avenue, Suite 200 Colorado Springs, Colorado 80903 (719)785-0790 (719)785-0799(fax)</p>	<p>STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS ROADWAY IMPROVEMENT PLANS JACKSON CREEK PKWY. PLAN & PROFILE</p> <table border="1" style="width: 100%; border-collapse: collapse; font-size: x-small;"> <tr> <td>DESIGNED BY</td> <td>PRA</td> <td>SCALE</td> <td>DATE</td> </tr> <tr> <td>DRAWN BY</td> <td>PRA</td> <td>(H) 1" = 50'</td> <td>SHEET 11 OF 23</td> </tr> <tr> <td>CHECKED BY</td> <td>(V) 1" = 5'</td> <td>JOB NO.</td> <td>1302.21</td> </tr> </table>	DESIGNED BY	PRA	SCALE	DATE	DRAWN BY	PRA	(H) 1" = 50'	SHEET 11 OF 23	CHECKED BY	(V) 1" = 5'	JOB NO.	1302.21
NO.	REVISION	DATE																							
DESIGNED BY	PRA	SCALE	DATE																						
DRAWN BY	PRA	(H) 1" = 50'	SHEET 11 OF 23																						
CHECKED BY	(V) 1" = 5'	JOB NO.	1302.21																						



CURB & GUTTER FLOWLINE DELTA TABLE				CURB & GUTTER FLOWLINE LINE TABLE			
CURVE	LENGTH	RADIUS	DELTA	LINE	LENGTH	BEARING	
C21	11.00	3.50	180°00'00"	L11	45.76	N50°47'05"E	
C22	11.00	3.50	180°00'00"	L12	45.76	N50°47'05"E	
C23	225.73	707.88	181°16'14"	L13	156.30	N00°28'50"E	
C24	145.94	699.54	115°57'13"	L14	156.30	N00°28'50"E	
C25	142.38	695.88	114°3'22"				
C26	321.23	714.88	25°44'44"				
C27	145.72	706.50	11°49'05"				
C28	53.76	702.88	04°22'58"				



STATION	PT STA.	HCL EL.	RT TRAVEL WAY	LT TRAVEL WAY
17+00	17+03.21	7033.84	+2.0%	-2.0%
17+15.01	17+15.01	7033.84	0%	0%
17+24.01	17+24.01	7033.84	-0.82%	-2.0%
17+36.22	17+36.22	7033.84	-2.0%	0%
18+33.01	18+33.01	7033.84	-2.0%	0%
18+60.22	18+60.22	7033.84	0%	-0.88%
19+22.22	19+22.22	7033.84	-2.0%	0%
19+34.62	19+34.62	7033.84	0%	-2.0%
19+84.22	19+84.22	7033.84	-2.0%	+2.0%
20+00			-4.0%	+4.0%

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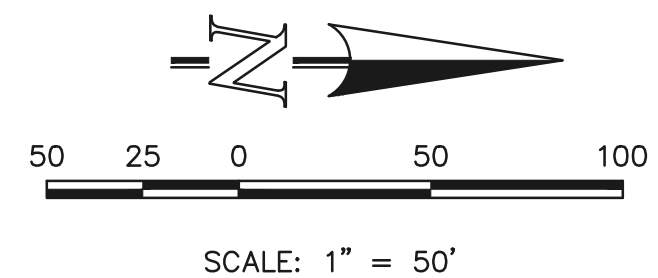
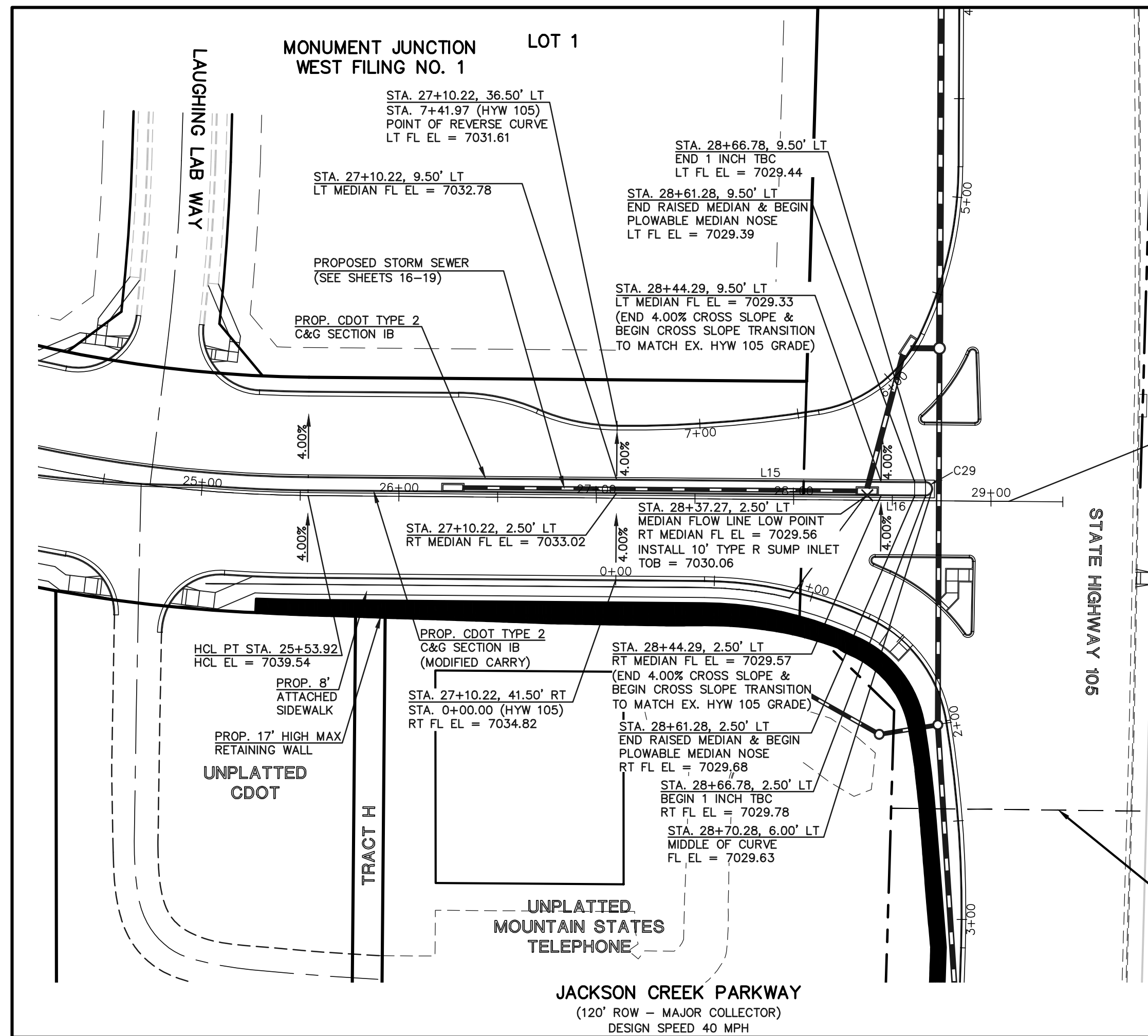
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MARC. A. WHORTON, COLORADO P.E. #37155 DATE

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS			
ROADWAY IMPROVEMENT PLANS			
JACKSON CREEK PKWY. RAISED MEDIAN PLAN			
DESIGNED BY	PRA	SCALE	DATE 02-23-23
DRAWN BY	PRA	(H) 1" = 50'	SHEET 12 OF 23
CHECKED BY	(V) 1" = 5'	JOB NO.	1302.22

619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903
(719) 785-0790
(719) 785-0799 (fax)

V:\130222\DRAWINGS\CONSTRUCTION\DRAWINGSHEET-12-9-03.dwg, 2/23/2023 4:00:57 PM, 1:1



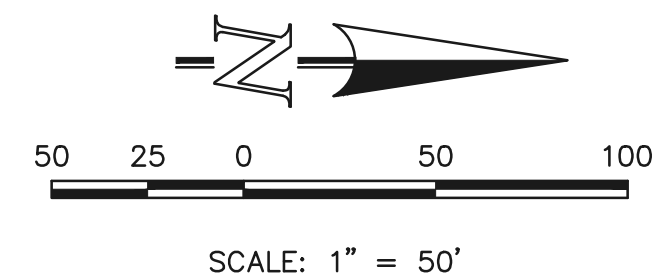
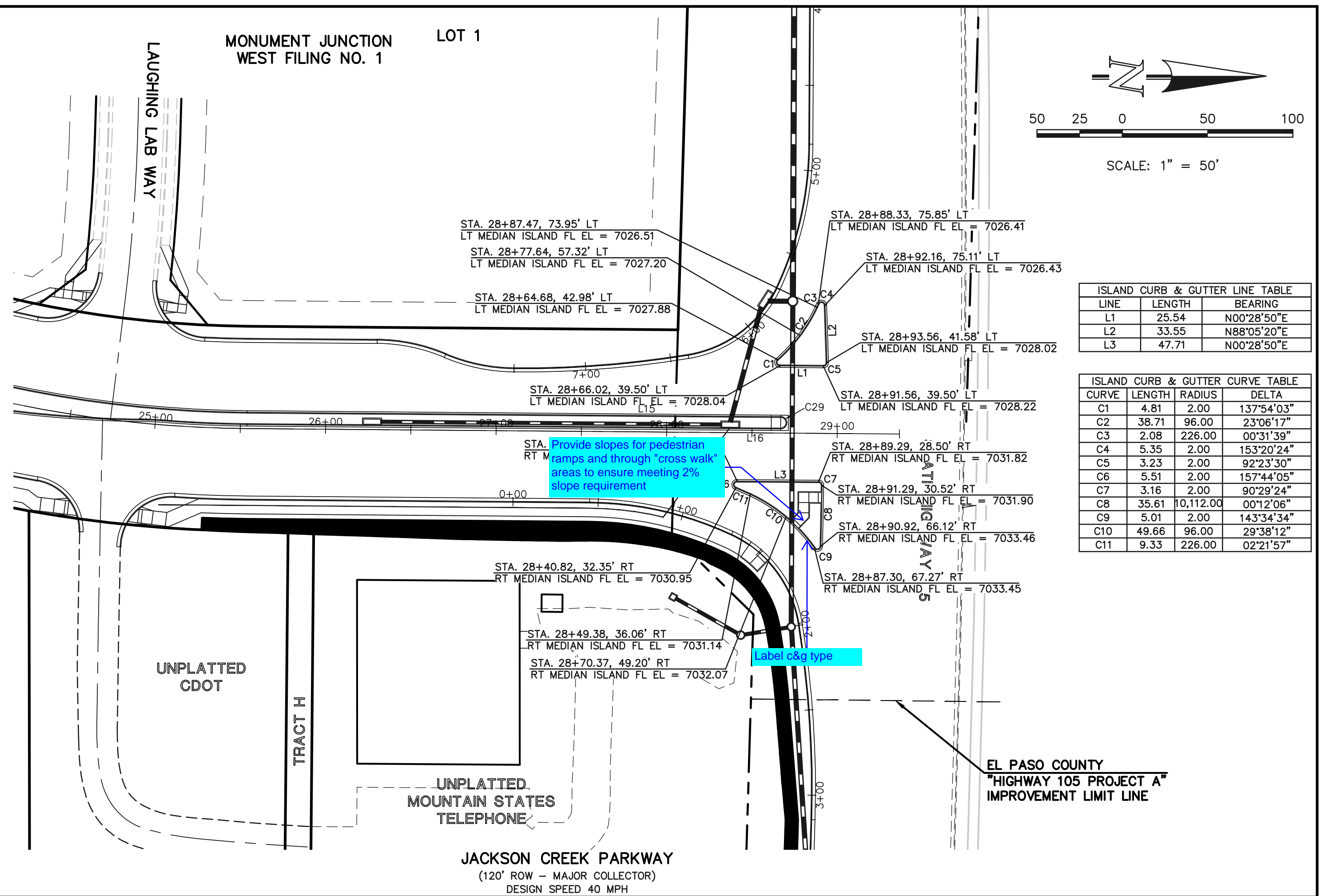
STA. 29+09.09
END HCL PROFILE
HCL EL = 7030.69
(MATCH HYW 105)

LINE	LENGTH	BEARING
L15	156.56	N00°28'50"E
L16	156.56	N00°28'50"E

CURVE	LENGTH	RADIUS	DELTA
C29	11.00	3.50	180°00'00"

EL PASO COUNTY
"HIGHWAY 105 PROJECT A"
IMPROVEMENT LIMIT LINE

JACKSON CREEK PARKWAY
(120' ROW - MAJOR COLLECTOR)
DESIGN SPEED 40 MPH



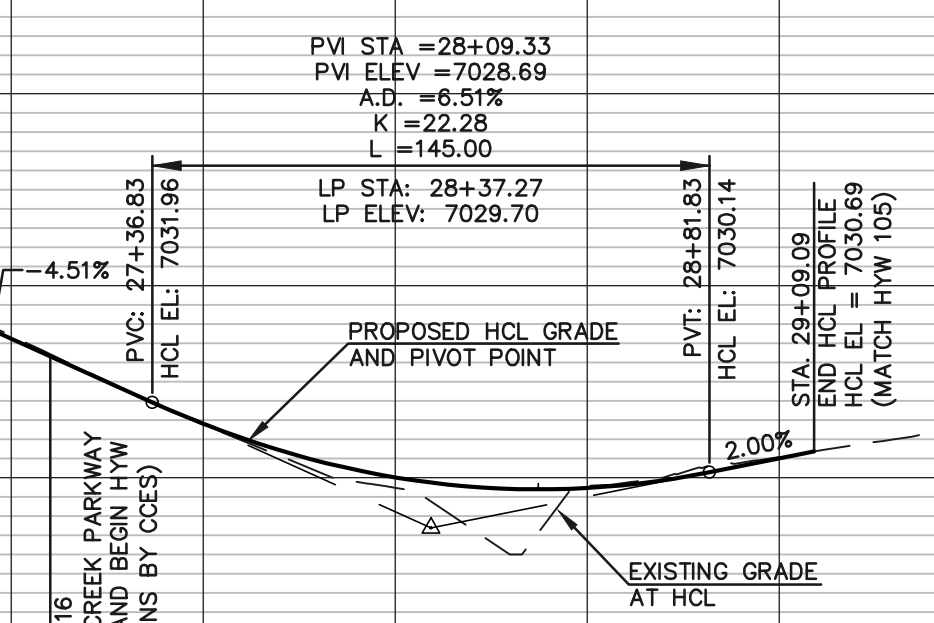
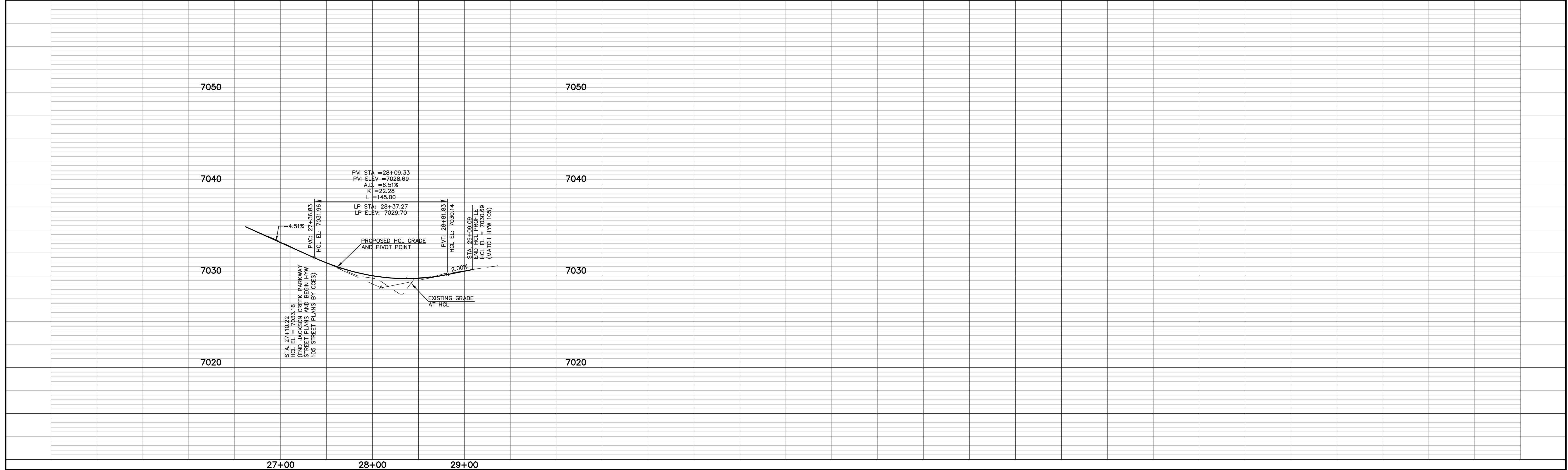
Provide slopes for pedestrian
ramps and through "cross walk"
areas to ensure meeting 2%
slope requirement

LINE	LENGTH	BEARING
L1	25.54	N00°28'50"E
L2	33.55	N88°05'20"E
L3	47.71	N00°28'50"E

CURVE	LENGTH	RADIUS	DELTA
C1	4.81	2.00	137°54'03"
C2	38.71	96.00	23°06'17"
C3	2.08	226.00	00°31'39"
C4	5.35	2.00	153°20'24"
C5	3.23	2.00	92°23'30"
C6	5.51	2.00	157°44'05"
C7	3.16	2.00	90°29'24"
C8	35.61	0,112.00	00°12'06"
C9	5.01	2.00	143°34'34"
C10	49.66	96.00	28°38'12"
C11	9.33	226.00	02°21'57"

EL PASO COUNTY
"HIGHWAY 105 PROJECT A"
IMPROVEMENT LIMIT LINE

JACKSON CREEK PARKWAY
(120' ROW - MAJOR COLLECTOR)
DESIGN SPEED 40 MPH



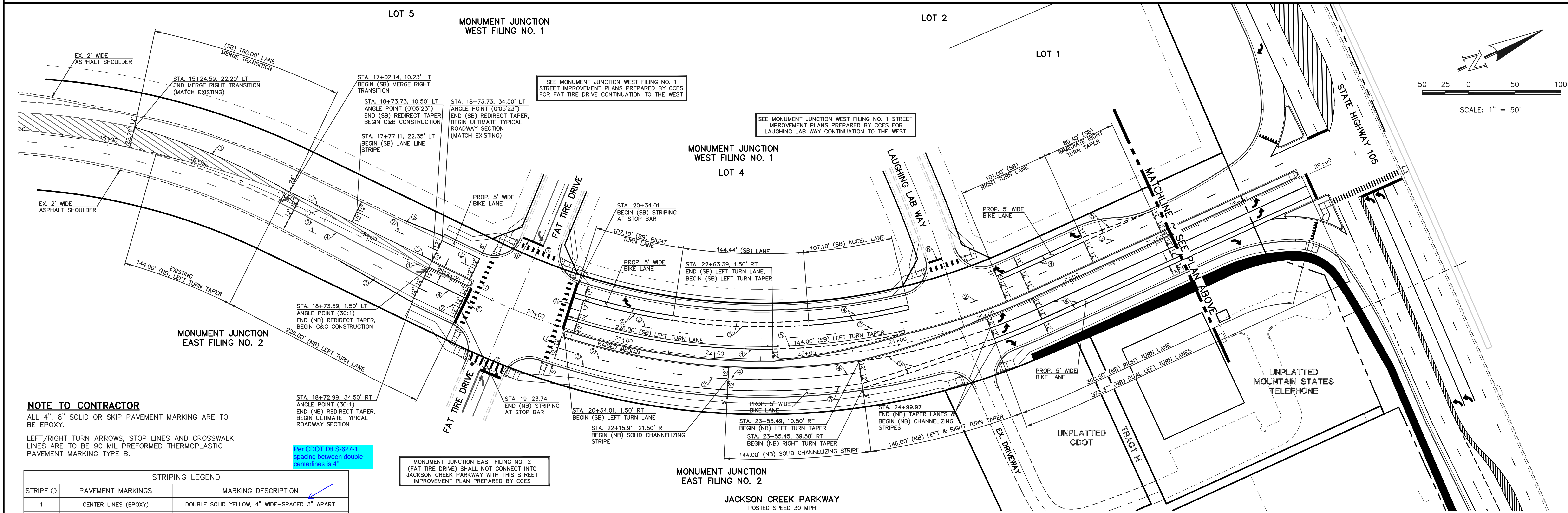
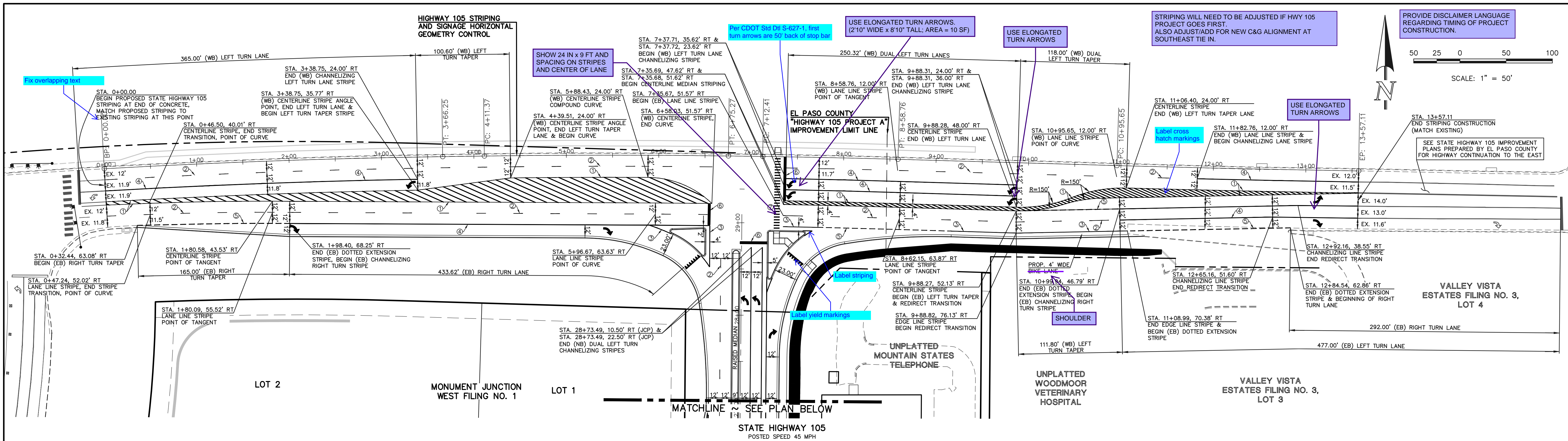
7050		7050
7040		7040
7030		7030
7020		7020

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INTERSECTION RAISED MEDIAN PLAN			
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DRAWN BY	PRA	(H) 1" = 50'	SHEET 13 OF 23
CHECKED BY	(V) 1" = 5'	JOB NO.	1302.22

N:\130222\DRAWINGS\CONSTRUCT\DRAW\SPRINT-13-3-04.dwg, 2/23/2023 3:56:52 PM, 1-1



NOTE TO CONTRACTOR
 ALL 4", 8" SOLID OR SKIP PAVEMENT MARKING ARE TO BE EPOXY.
 LEFT/RIGHT TURN ARROWS, STOP LINES AND CROSSWALK LINES ARE TO BE 90 MIL PREFORMED THERMOPLASTIC PAVEMENT MARKING TYPE B.

STRIPING LEGEND		
STRIPE	PAVEMENT MARKINGS	MARKING DESCRIPTION
1	CENTER LINES (EPOXY)	DOUBLE SOLID YELLOW, 4" WIDE-SPACED 3" APART
2	LANE LINES (EPOXY)	BROKEN WHITE, 4" WIDE-10' SEGMENTS WITH 30' GAPS
3	EDGE LINES (EPOXY)	SOLID WHITE, 4" WIDE
4	CHANNELIZING LINES (EPOXY)	SOLID WHITE, 8" WIDE
5	DOTTED EXTENSION OF TURN/TAPER LANES (EPOXY)	DASHED WHITE, 4" WIDE
6	STOP LINES (THERMO PLASTIC)	SOLID WHITE, 24" WIDE
7	CROSSWALK LINES (THERMO PLASTIC)	SOLID WHITE, 12" WIDE

*NOTE: ALL STRIPING INSTALLATION SHALL BE PER COLORADO DEPARTMENT OF TRANSPORTATION "M&S STANDARDS" STANDARD PLAN NO. S-627-1

MONUMENT JUNCTION EAST FILING NO. 2 (FAT TIRE DRIVE) SHALL NOT CONNECT INTO JACKSON CREEK PARKWAY WITH THIS STREET IMPROVEMENT PLAN PREPARED BY CCEs

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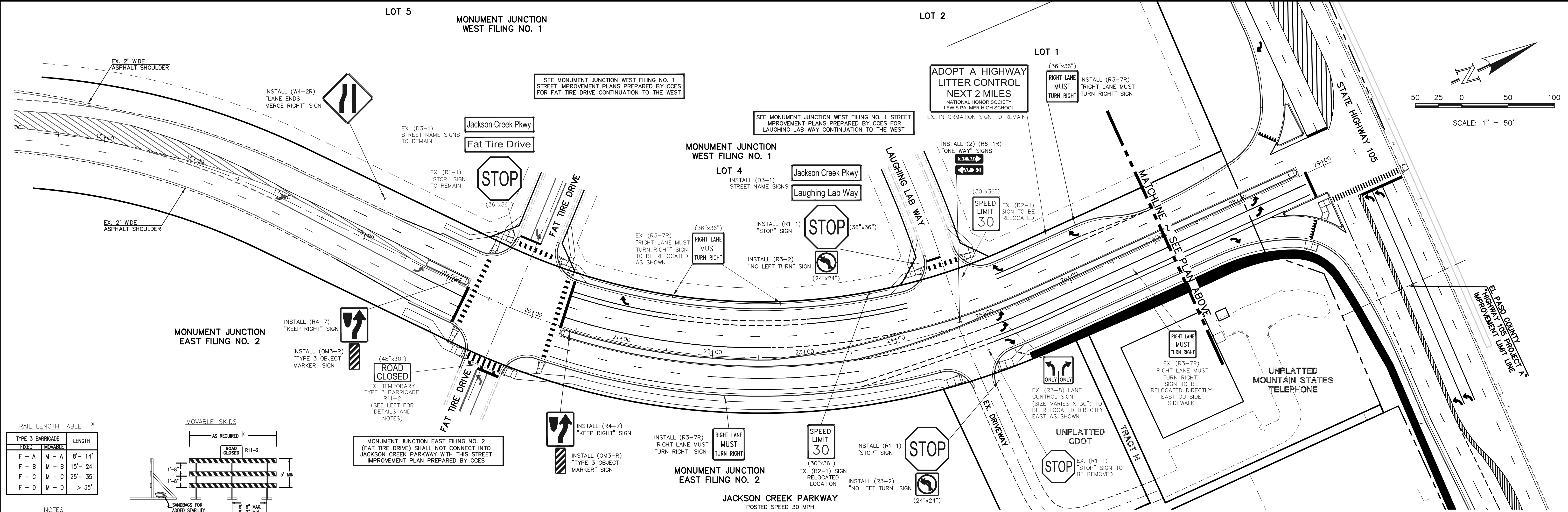
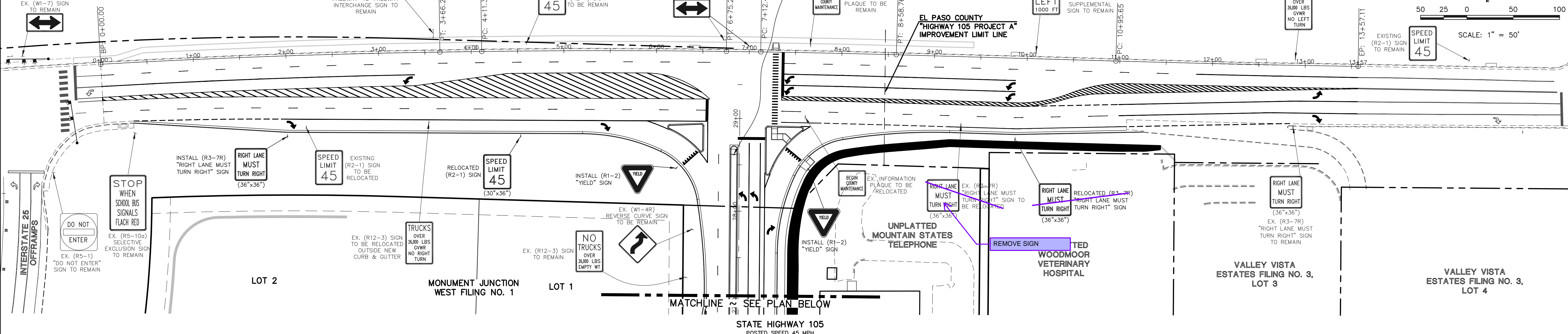
CLASSIC
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STRIPING PLAN			
DESIGNED BY	PRA	SCALE	DATE 02-23-23
DRAWN BY	PRA	(H) 1" = 50'	SHEET 14 OF 23
CHECKED BY	(V) 1" = N/A	JOB NO.	1302.22

V:\130222\ROADWAY\CONSTRUCTION\130222-14-STRIPING-01.dwg, 2/23/2023 4:13:17 PM, 1:1

NOTE TO CONTRACTOR
 SIGNS AND POSTS SHALL BE PER CDOT STANDARDS
 S-614-8, S-614-2, AND S-614-3, LATEST REVISION.
 ALL SIGNAGE INSTALLATION IS TO BE IN COMPLIANCE WITH
 THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

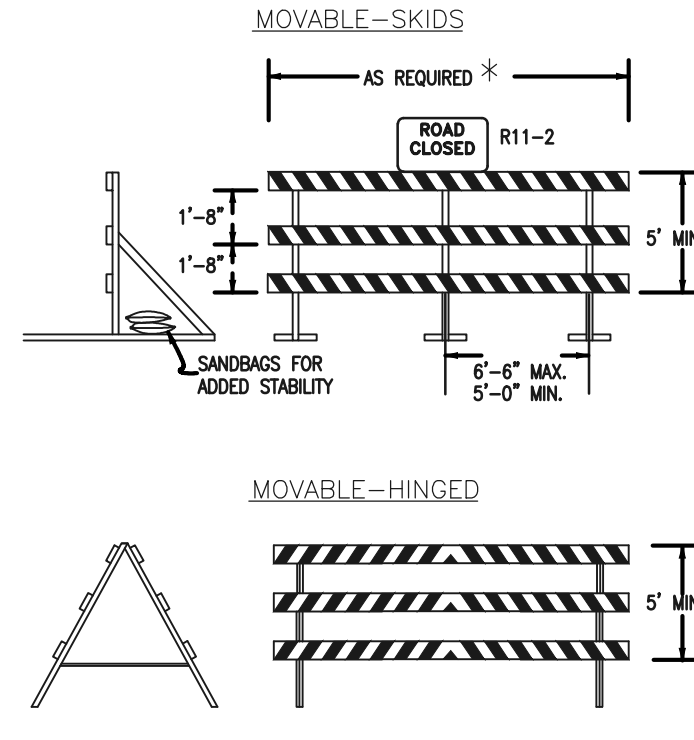
NOTE:
 ALL INTERNAL STREET NAME SIGNS SHALL BE
 4" FONT LETTER SIZE.
 6" FONT LETTER SIZE ON ALL STREETS POSTED
 30MPH OR GREATER



RAIL LENGTH TABLE *

TYPE 3 BARRICADE	LENGTH
F - A	8' - 14'
F - B	15' - 24'
F - C	25' - 35'
F - D	> 35'

- NOTES**
- TYPE 3 BARRICADES HAVE 3 REFLECTORIZED RAIL FACES IF FACING TRAFFIC IN ONE DIRECTION AND 6 IF FACING TRAFFIC IN TWO DIRECTIONS.
 - THE PORTION OF THE POST ABOVE THE GROUND LINE SHALL BE PAINTED IN ACCORDANCE WITH THE APPROPRIATE GENERAL NOTE.
 - DETACHABLE EXTENSION WING RAILS FOR BYPASSING OF CONSTRUCTION EQUIPMENT ARE PERMITTED, WHEN NECESSARY, ON FIXED OR MOVABLE TYPE 3 BARRICADES. THE LENGTH SHALL BE ADEQUATE TO CLOSE THE SHOULDER AS REQUIRED.



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NO.	REVISION	DATE

REVIEW:

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC. A. WHORTON, COLORADO P.E. #37155

CLASSIC
 CONSULTING ENGINEERS & SURVEYORS

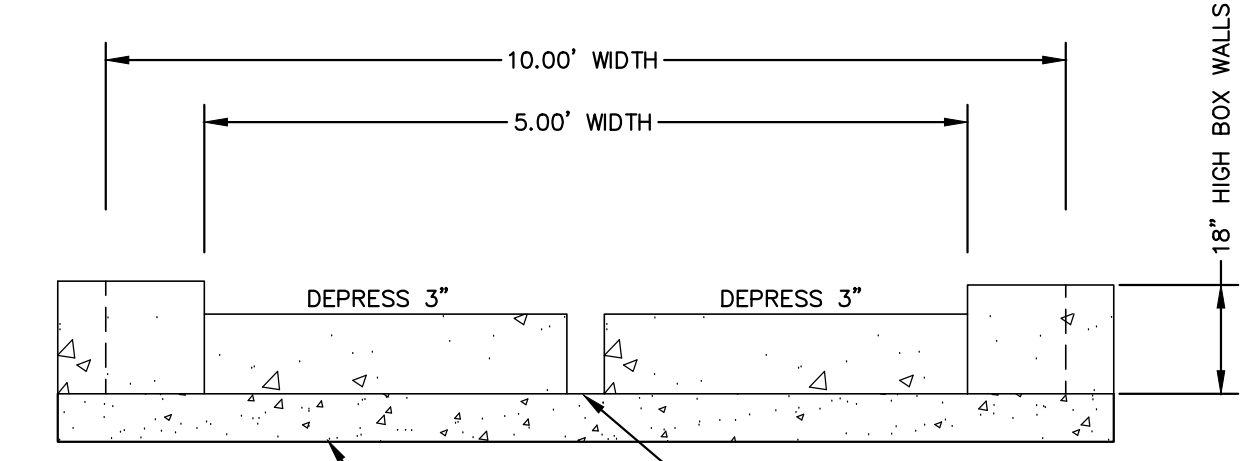
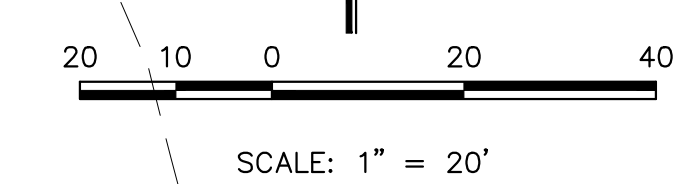
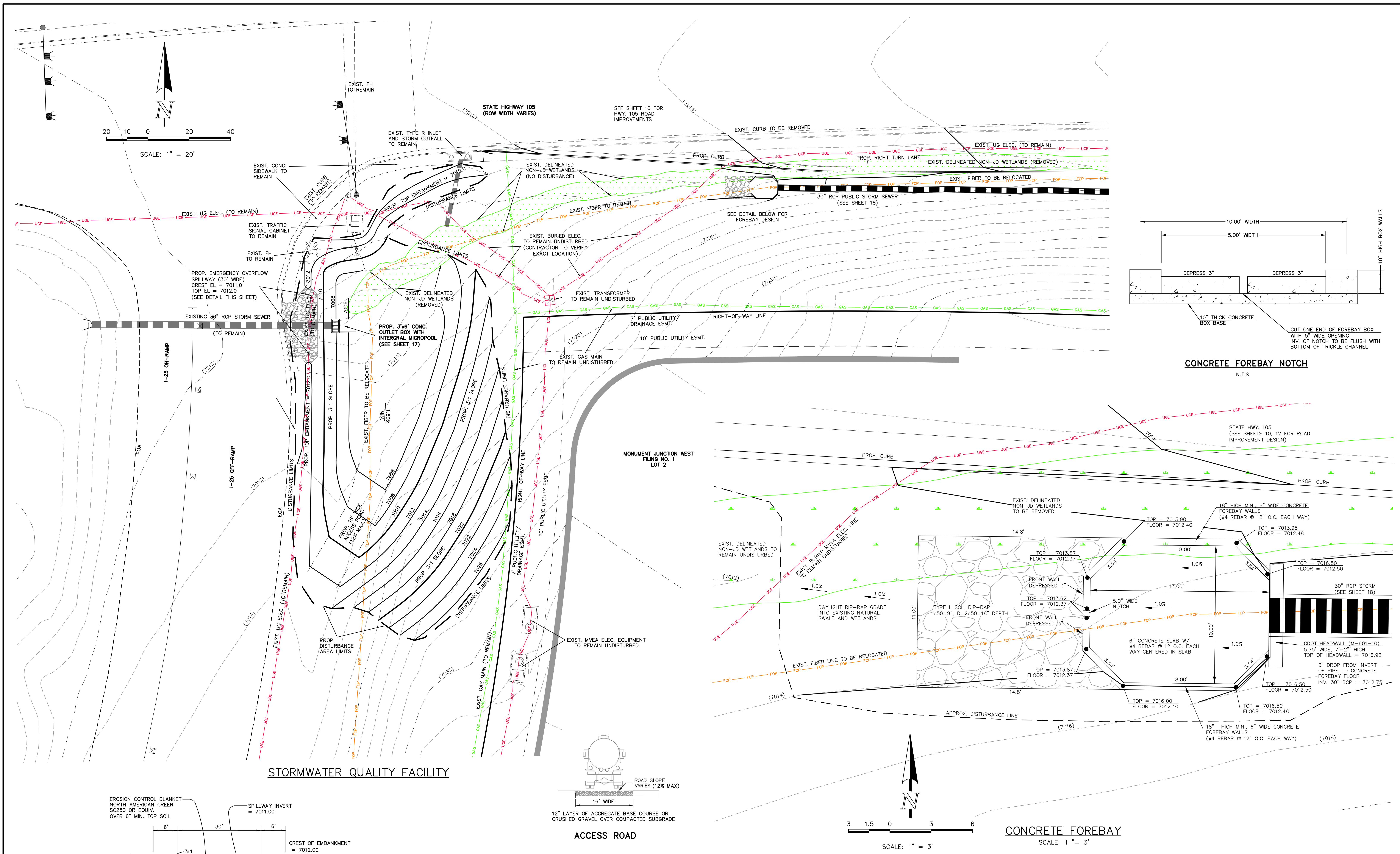
619 N. Cascade Avenue, Suite 200
 Colorado Springs, Colorado 80903

(719) 785-0790
 (719) 785-0799 (fax)

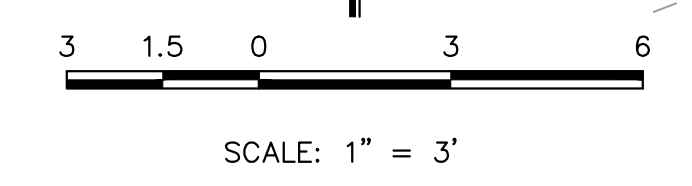
STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
 ROADWAY IMPROVEMENT PLANS
 SIGNAGE PLAN

DESIGNED BY	PRA	SCALE	DATE	02-23-23
DRAWN BY	PRA	(H) 1" = 50'	SHEET	15 OF 23
CHECKED BY	(V) 1" = N/A	JOB NO.	1302.22	

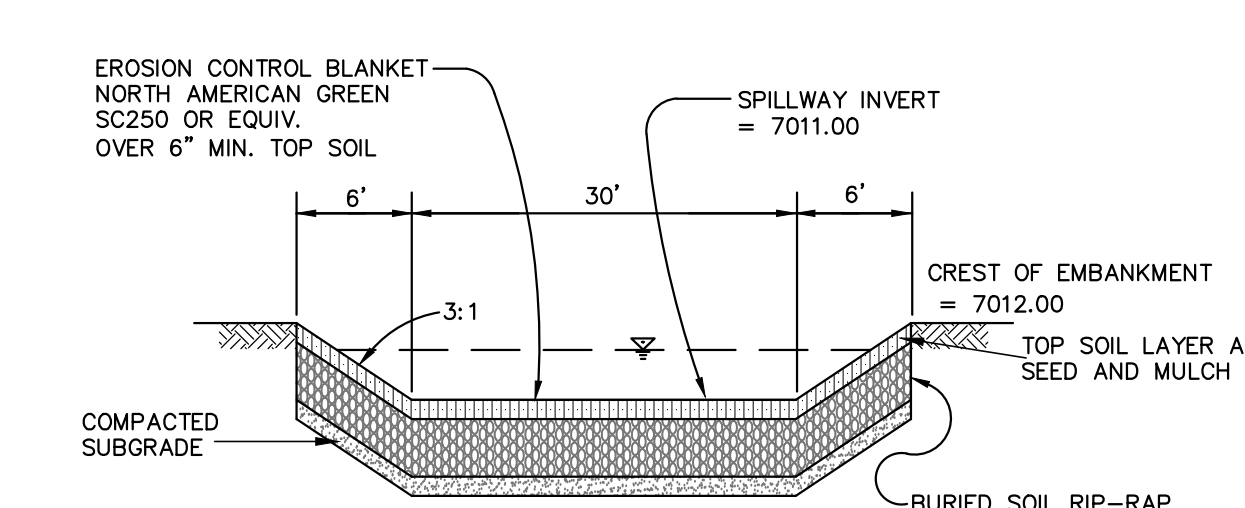
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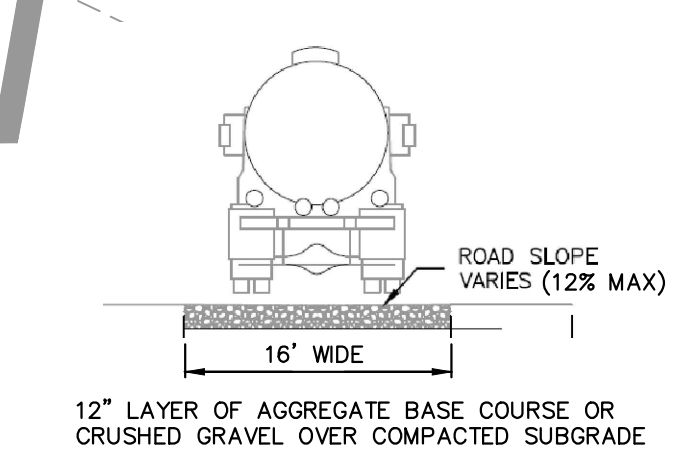
CONCRETE FOREBAY NOTCH
N.T.S.



CONCRETE FOREBAY
SCALE: 1" = 3'



EMERGENCY SPILLWAY CROSS SECTION
SCALE: N.T.S.



ACCESS ROAD

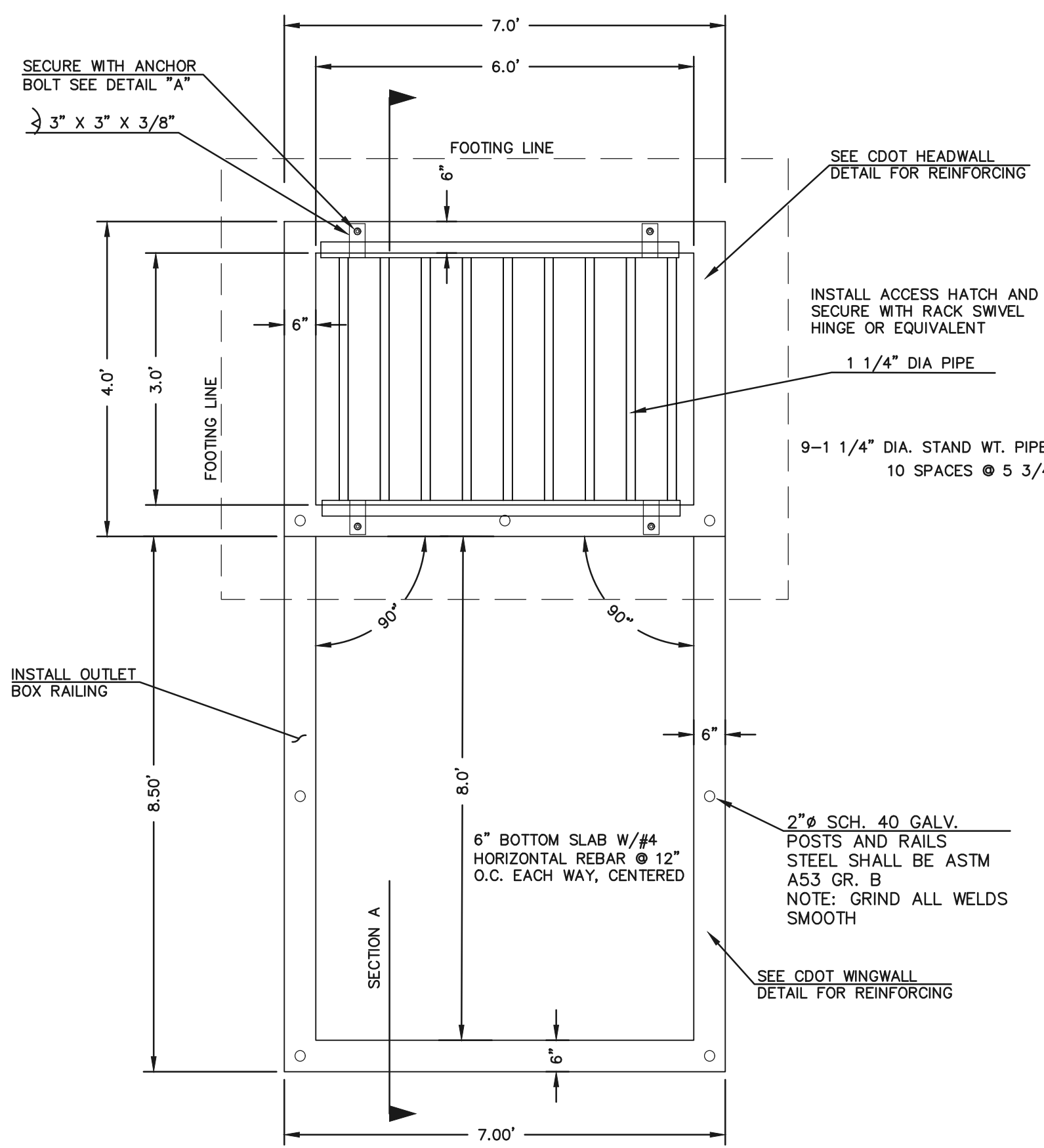
<p>48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS</p> <p>811</p> <p>UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW</p> <p>THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</p>		<p>NO. REVISION</p> <p>DATE</p>	<p>REVIEW:</p> <p>PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC</p> <p>MARC A. WHORTON, COLORADO P.E. #37155</p> <p>DATE</p>
---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------	--	---------------------------------	------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
STORMWATER QUALITY FACILITY
POND PLAN AND FOREBAY DETAIL

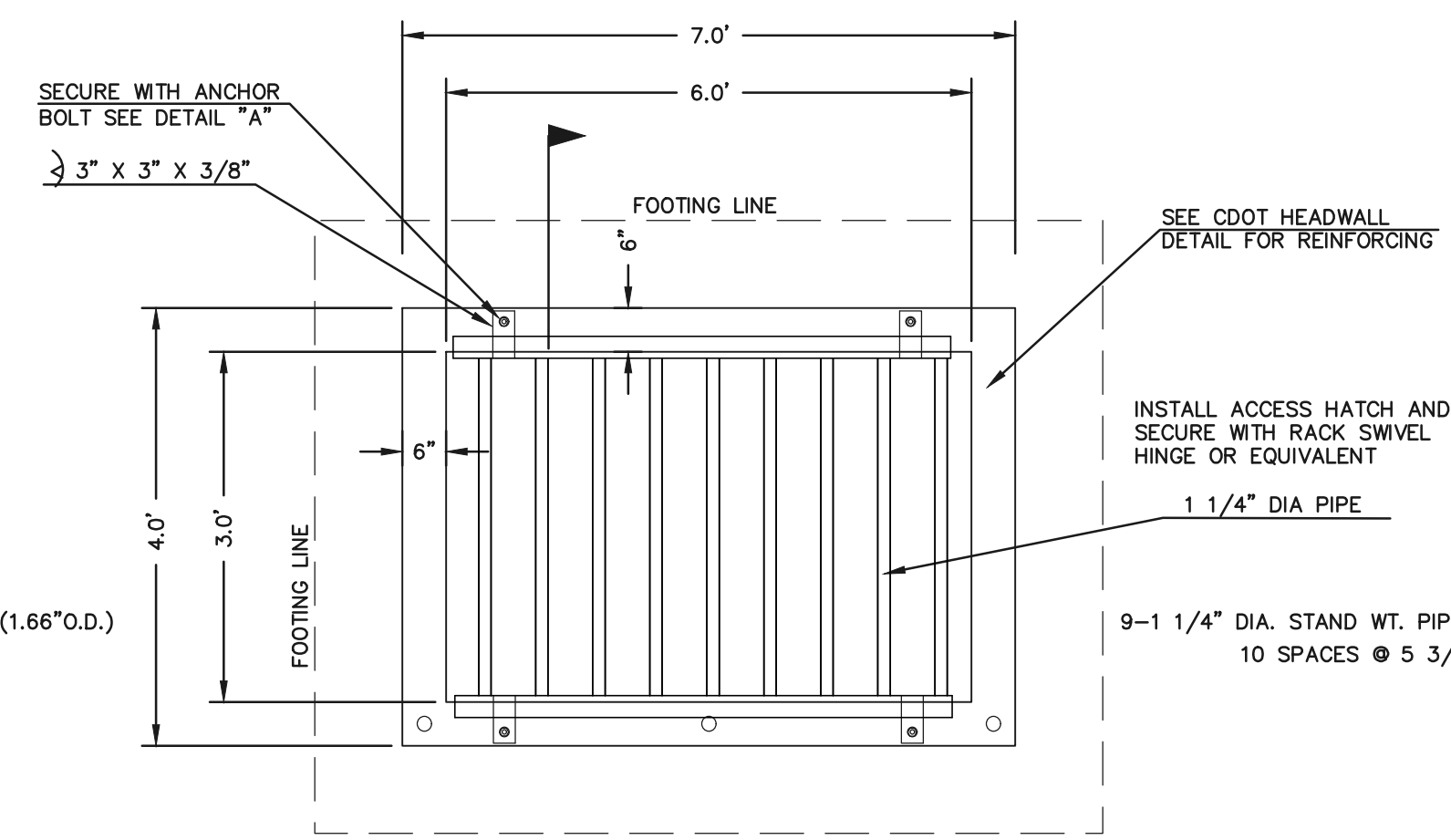
DESIGNED BY MAW SCALE DATE 02-23-23
DRAWN BY MAW (H) 1" = 20' SHEET 16 OF 23
CHECKED BY (V) 1" = N/A JOB NO. 1302.22

CLASSIC CONSULTING ENGINEERS & SURVEYORS
619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903
(719) 785-0790
(719) 785-0799 (fax)

N:\130222\DRAWINGS\CONSTRUCT\DRAW\SPRHEET-16-F000-01.dwg, 2/23/2023 4:26:16 PM, 1:1

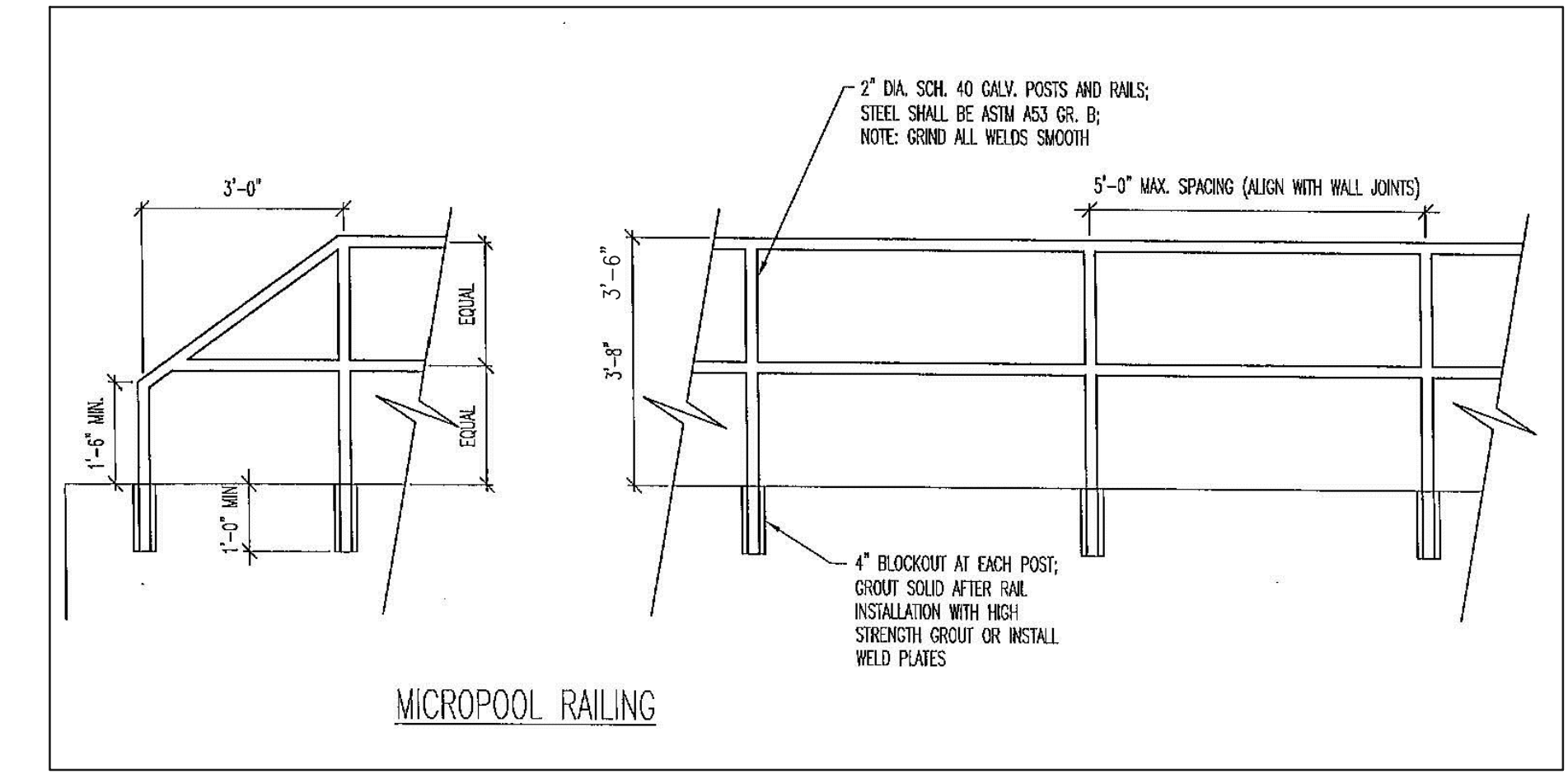
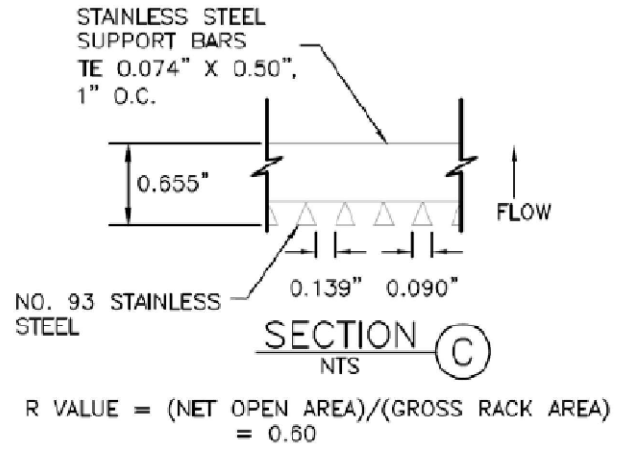
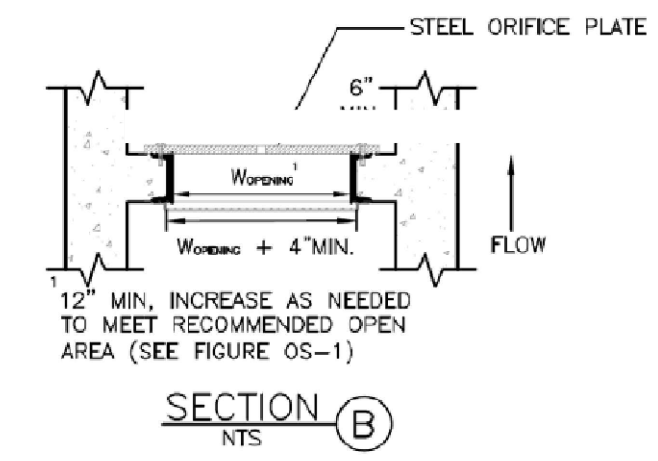
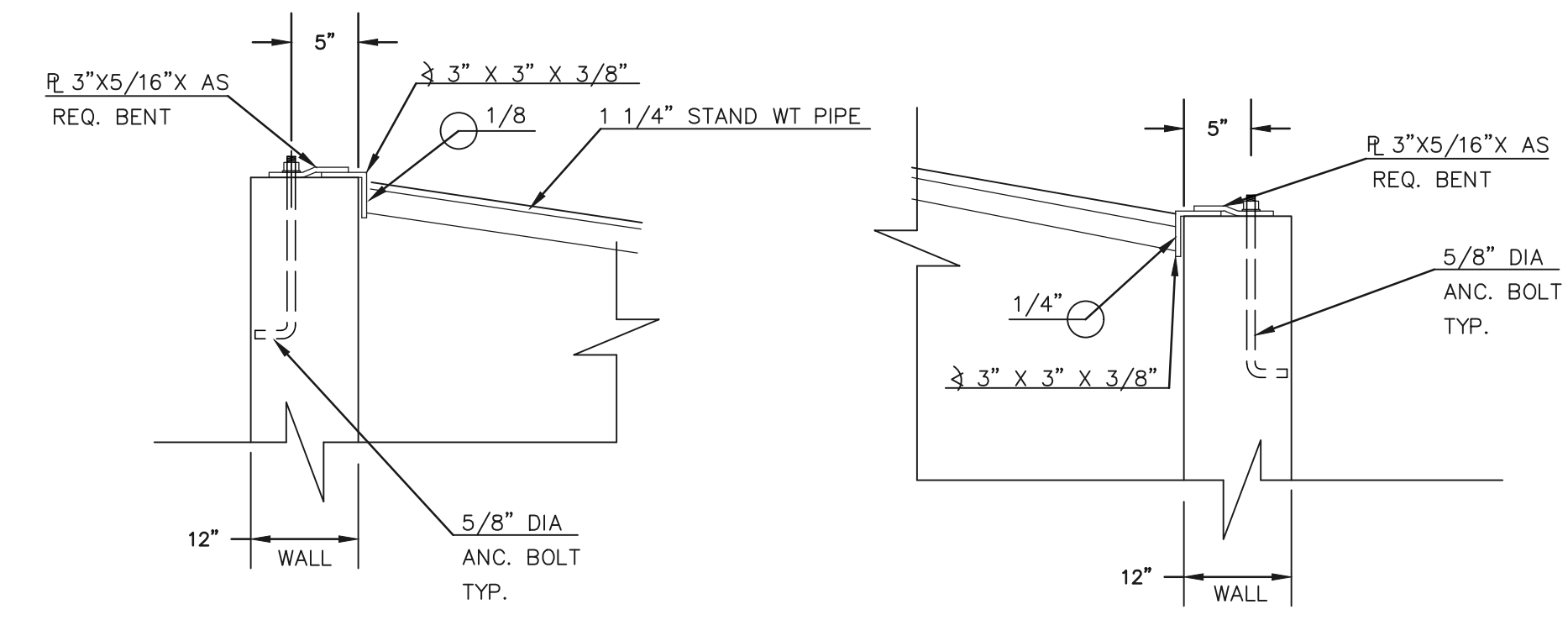


CONCRETE MICROPOOL
SCALE 1" = 2'

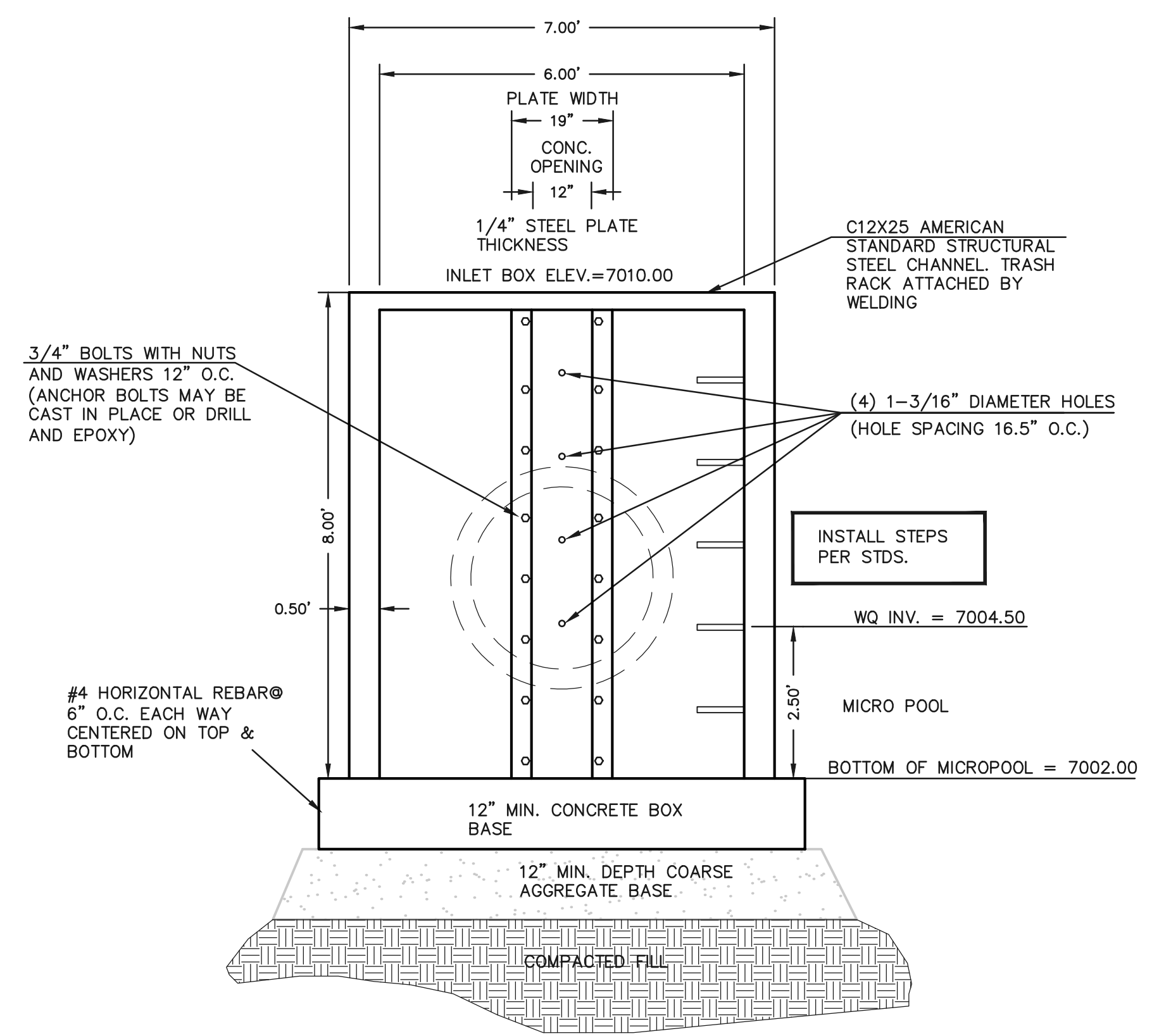


3'X6' OUTLET BOX OVERFLOW TRASH RACK
SCALE 1" = 2'

- NOTES:
1. WELD PLATES EMBEDS MAY BE SUBSTITUTED. DESIGN CRITERIA SHALL BE IN ACCORDANCE WITH AASHTO STANDARDS.
 2. HANDRAIL DESIGN SHALL BE COMPATIBLE WITH THE DESIGN OF THE WINGWALLS AND HEADWALLS. RAILING POSTS SHALL BE SET TO NORMAL TO GRADE. RAILS SHALL RUN PARALLEL TO THE SLOPES OF TOPS OF THE WALLS.
 3. ALL RAILS SHALL HAVE EXPANSION JOINTS SPACED AT 40'-0" MAX. JOINT ENDS SHALL BE FREE OF ANY SHARP EDGES OR CORNERS.

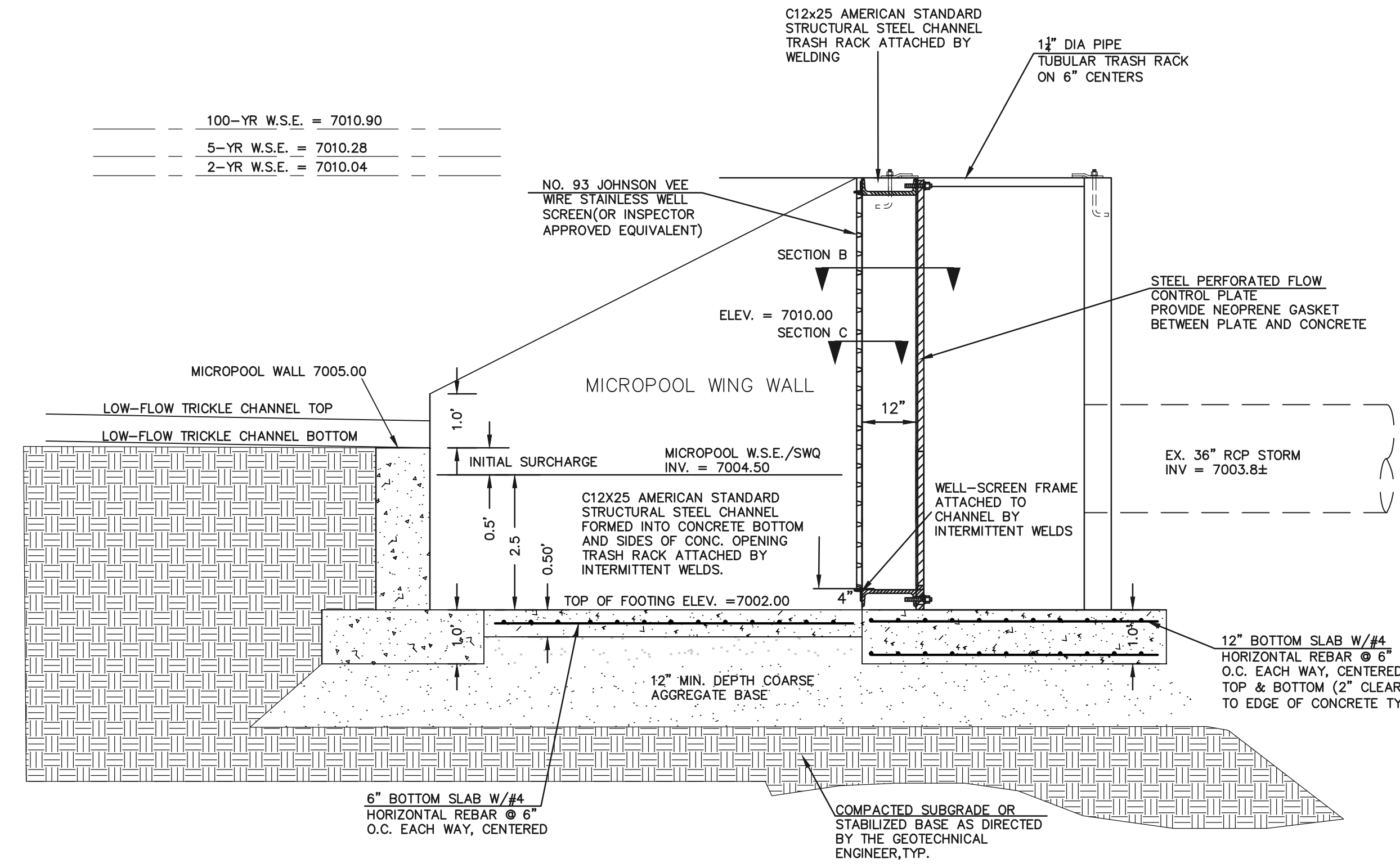


OUTLET BOX RAILING
N.T.S.



3'X6' OUTLET BOX ORIFICE PLATE
SCALE 1" = 2'

- (ALL MATERIALS PER CDOT SPECIFICATIONS)
- ORIFICE PLATE NOTES:
1. INSTALL HOLES AS SHOWN ON DETAIL TO LEFT.
 2. PROVIDE GASKET MATERIAL BETWEEN THE ORIFICE PLATE AND CONCRETE
 3. ALL STAINLESS STEEL USED TO BE GALVANIZED.
- EURV AND WQCV TRASH RACKS:
4. WELL-SCREEN TRASH RACKS SHALL BE STAINLESS STEEL AND SHALL BE ATTACHED BY INTERMITTENT WELDS ALONG THE EDGE OF THE MOUNTING FRAME.
 5. BAR GRATE TRASH RACKS SHALL BE ALUMINUM AND SHALL BE BOLTED USING STAINLESS STEEL HARDWARE.
 6. STRUCTURAL DESIGN OF TRASH RACKS SHALL BE BASED ON FULL HYDROSTATIC HEAD WITH ZERO HEAD DOWNSTREAM OF RACK
- OVERFLOW TRASH RACKS:
1. ALL TRASH RACKS SHALL BE MOUNTED USING STAINLESS STEEL HARDWARE AND PROVIDED WITH HINGED AND LOCKABLE OR BOLTABLE ACCESS PANELS.
 2. TRASH RACKS SHALL BE STAINLESS STEEL, ALUMINUM, OR STEEL. TRASH RACKS SHALL BE HOT DIP GALVANIZED AND MAY BE HOT POWDER COATED AFTER GALVANIZING.
 3. TRASH RACKS SHALL BE DESIGNED SUCH THAT THE DIAGONAL DIMENSION OF EACH OPENING IS SMALLER THAN THE DIAMETER OF THE OUTLET PIPE.
 4. STRUCTURAL DESIGN OF THE TRASH RACKS SHALL BE BASED ON FULL HYDROSTATIC HEAD WITH ZERO HEAD DOWNSTREAM OF THE RACK.



3'X6' OUTLET BOX MICRO POOL SECTION A
SCALE 1" = 2'

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NO.	REVISION	DATE	REVIEW:

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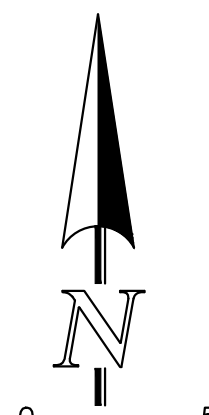
MARC A. WHORTON, COLORADO P.E. #37155 DATE

619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903
(719) 785-0790
(719) 785-0799 (Fax)

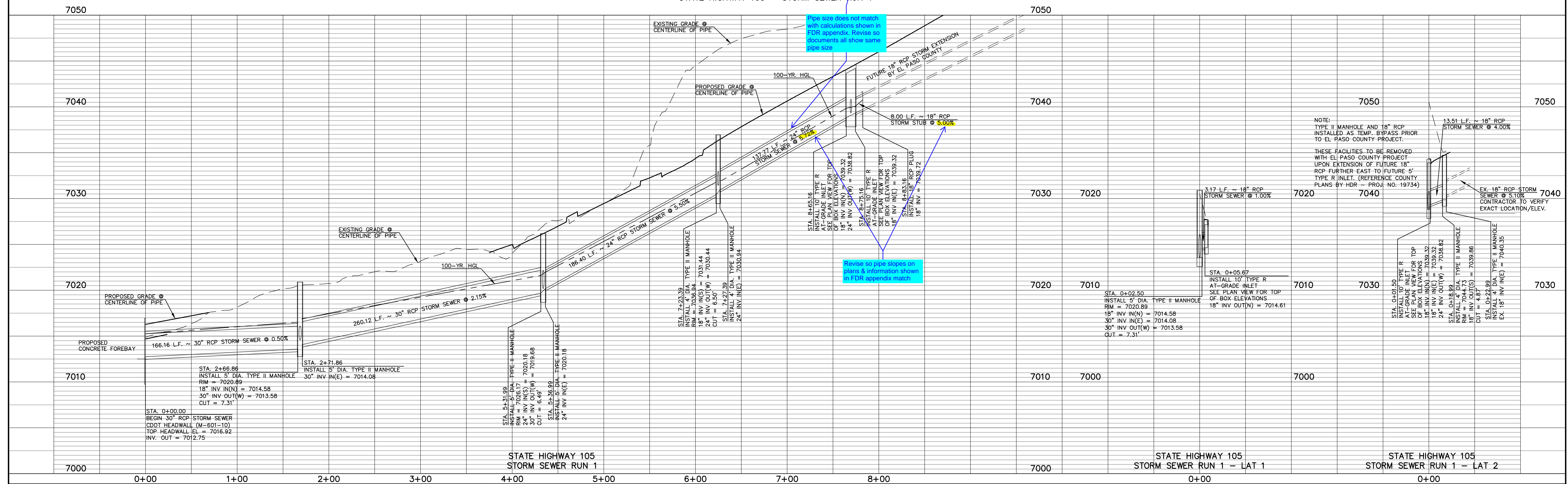
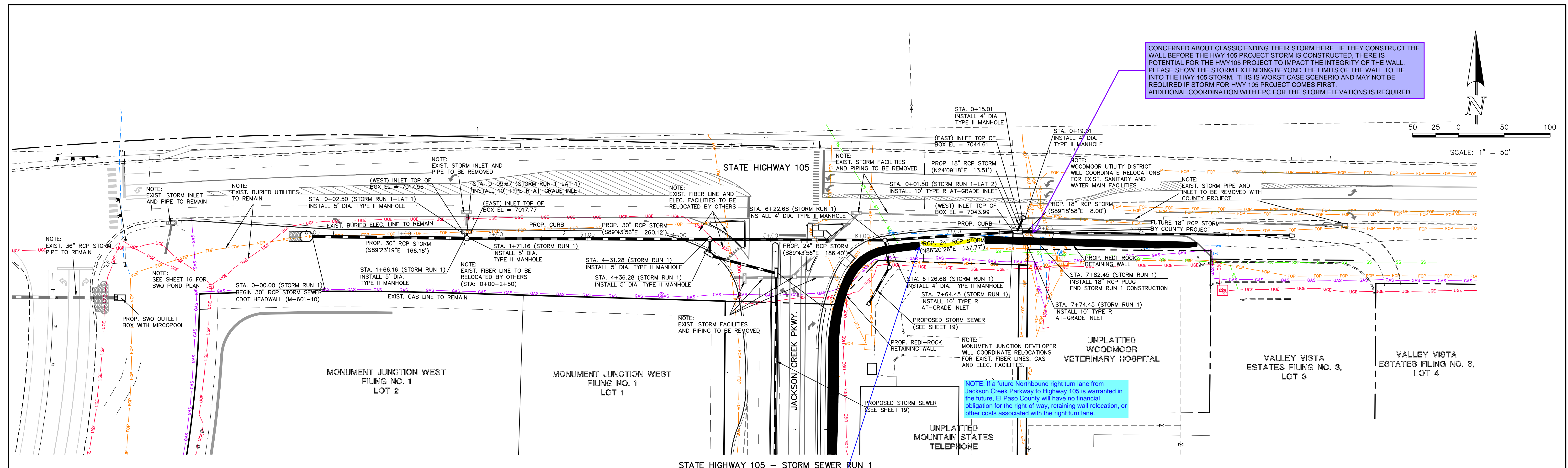
STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS			
STORMWATER QUALITY FACILITY			
OUTLET BOX DETAILS			
DESIGNED BY	MAW	SCALE	DATE 02-23-23
DRAWN BY	PRA	(H) 1" = N/A	SHEET 17 OF 23
CHECKED BY	(V) 1" = N/A	JOB NO.	1302.22

V:\130222\DRAWINGS\CDOT\STRUCT\CON\SH-17-2010-02.dwg, 2/23/2023 4:28:17 PM, 1:1

CONCERNED ABOUT CLASSIC ENDING THEIR STORM HERE. IF THEY CONSTRUCT THE WALL BEFORE THE HWY 105 PROJECT STORM IS CONSTRUCTED, THERE IS POTENTIAL FOR THE HWY105 PROJECT TO IMPACT THE INTEGRITY OF THE WALL. PLEASE SHOW THE STORM EXTENDING BEYOND THE LIMITS OF THE WALL TO TIE INTO THE HWY 105 STORM. THIS IS WORST CASE SCENARIO AND MAY NOT BE REQUIRED IF STORM FOR HWY 105 PROJECT COMES FIRST. ADDITIONAL COORDINATION WITH EPC FOR THE STORM ELEVATIONS IS REQUIRED.



SCALE: 1" = 50'



Pipe size does not match with calculations shown in FDR appendix. Revise so documents all show same pipe size

Revise so pipe slopes on plans & information shown in FDR appendix match

7050
7040
7030
7020
7010
7000

0+00 1+00 2+00 3+00 4+00 5+00 6+00 7+00 8+00

STATE HIGHWAY 105 STORM SEWER RUN 1

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MARC. A. WHORTON, COLORADO P.E. #37155 DATE

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS

STORM SEWER PLAN & PROFILE

DESIGNED BY PRA SCALE DATE 02-23-23

DRAWN BY PRA (H) 1" = 50' SHEET 18 OF 23

CHECKED BY (V) 1" = 5' JOB NO. 1302.22

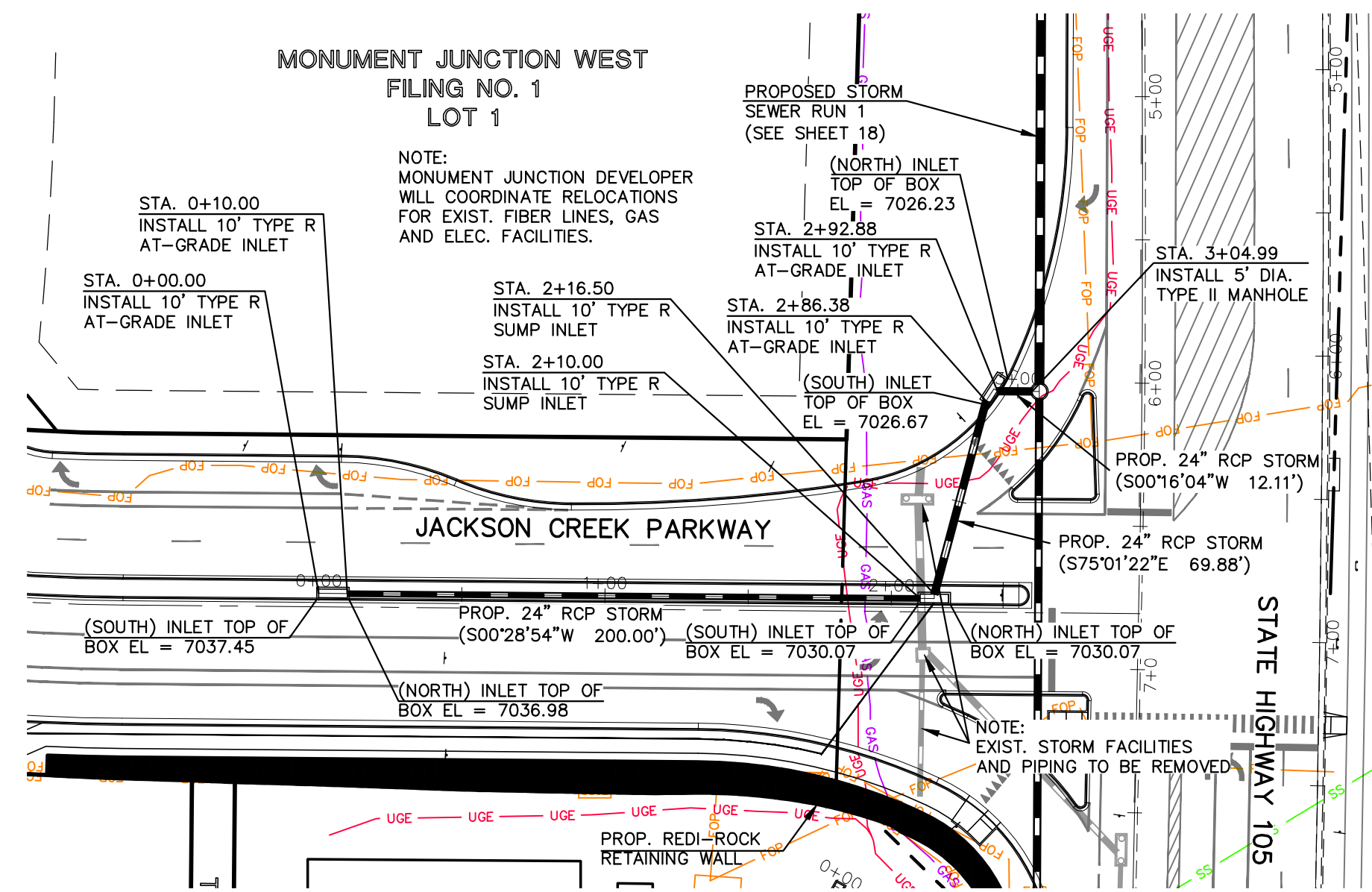
CLASSIC
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Colorado Springs, Colorado 80903 (719)785-0790 (719)785-0799(Fax)

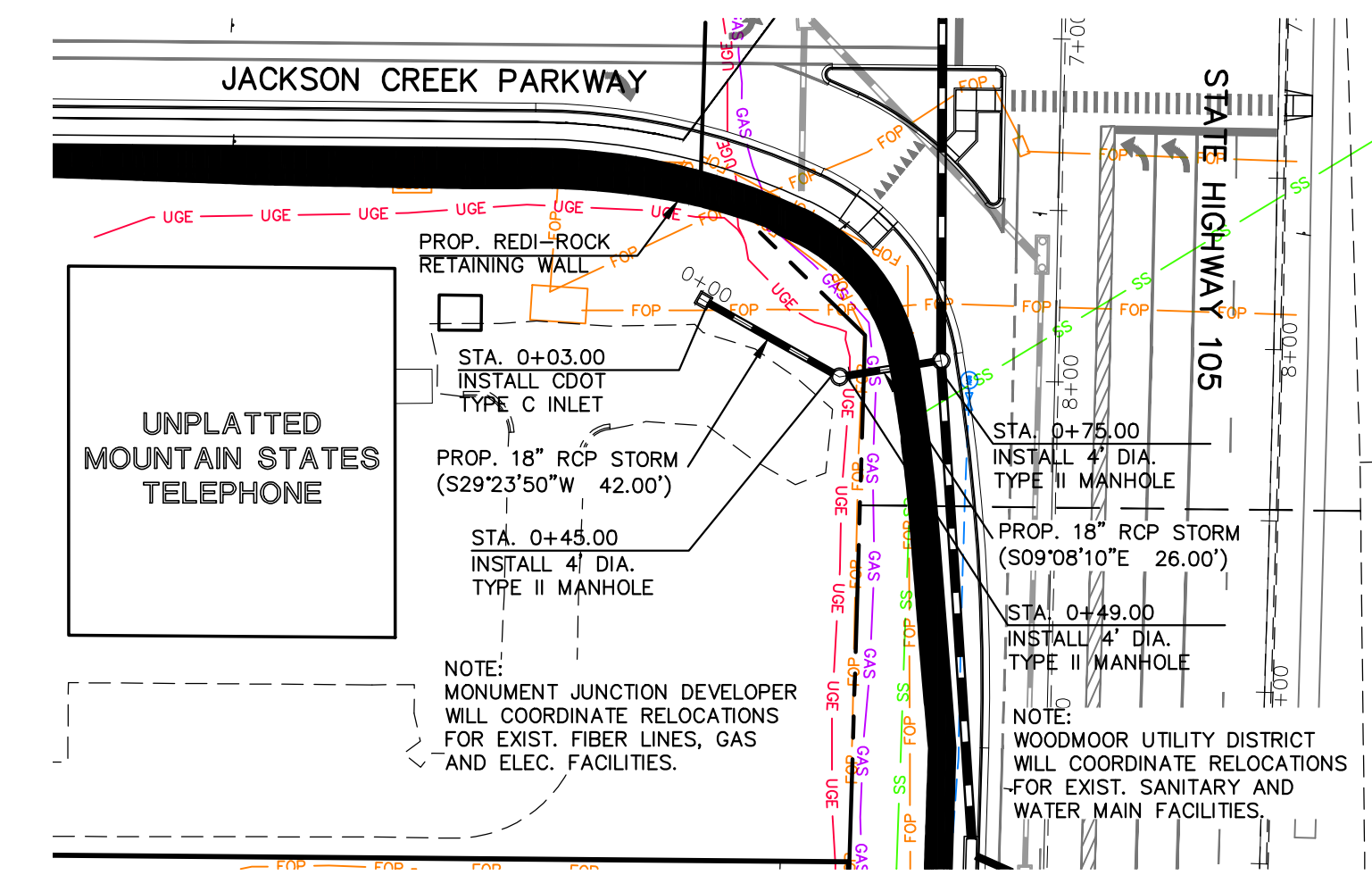
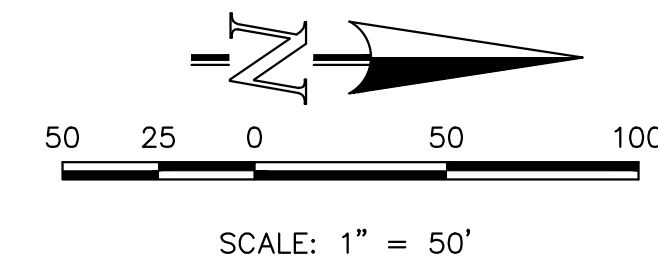
STATE HIGHWAY 105 STORM SEWER RUN 1 - LAT 1

STATE HIGHWAY 105 STORM SEWER RUN 1 - LAT 2

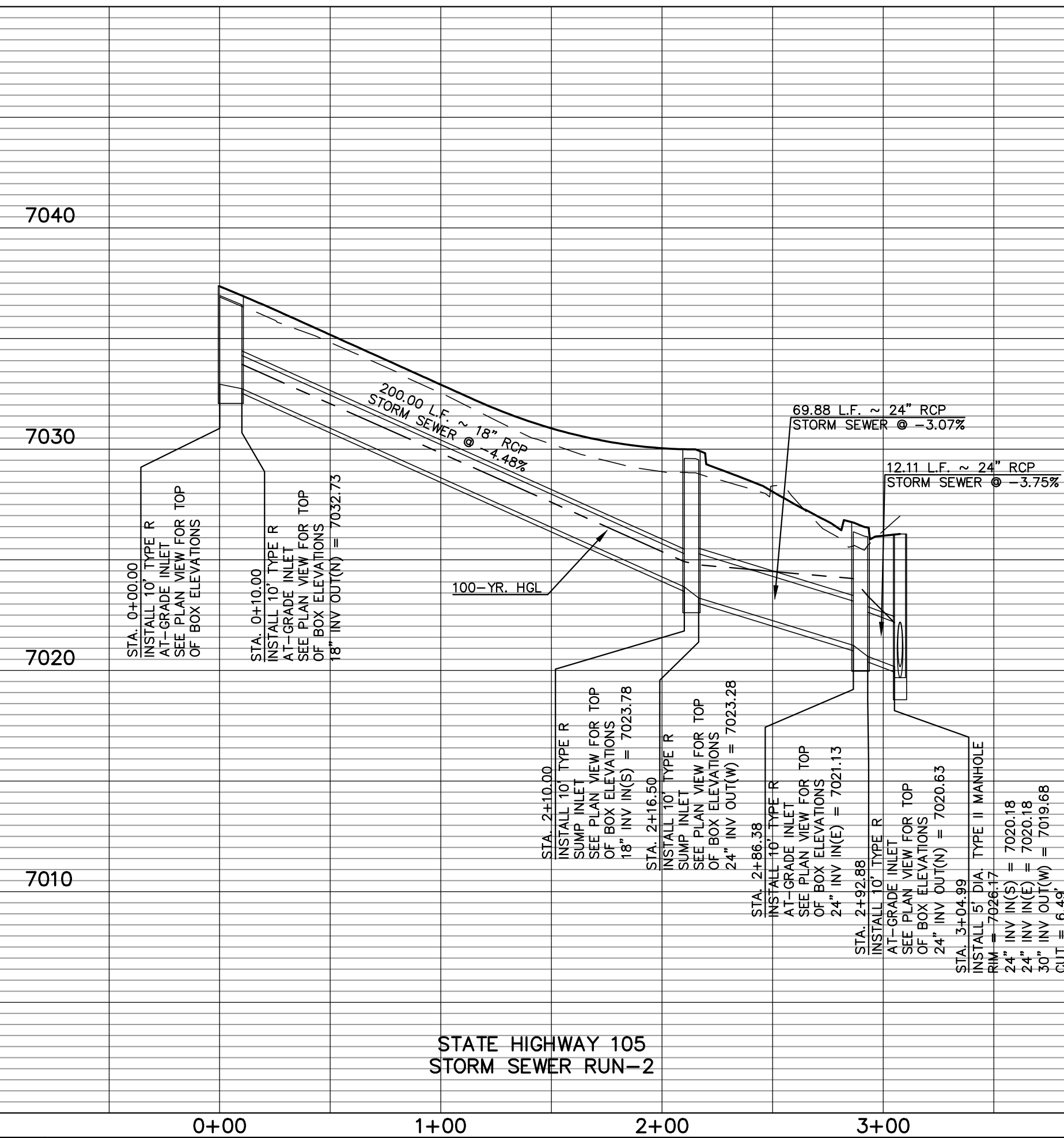
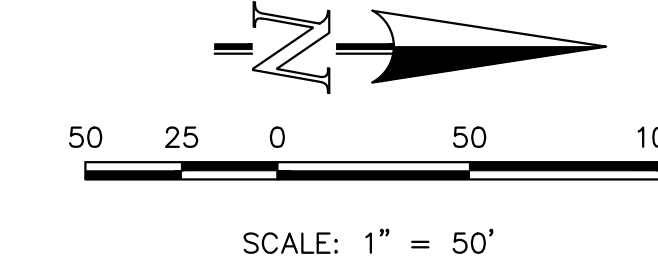
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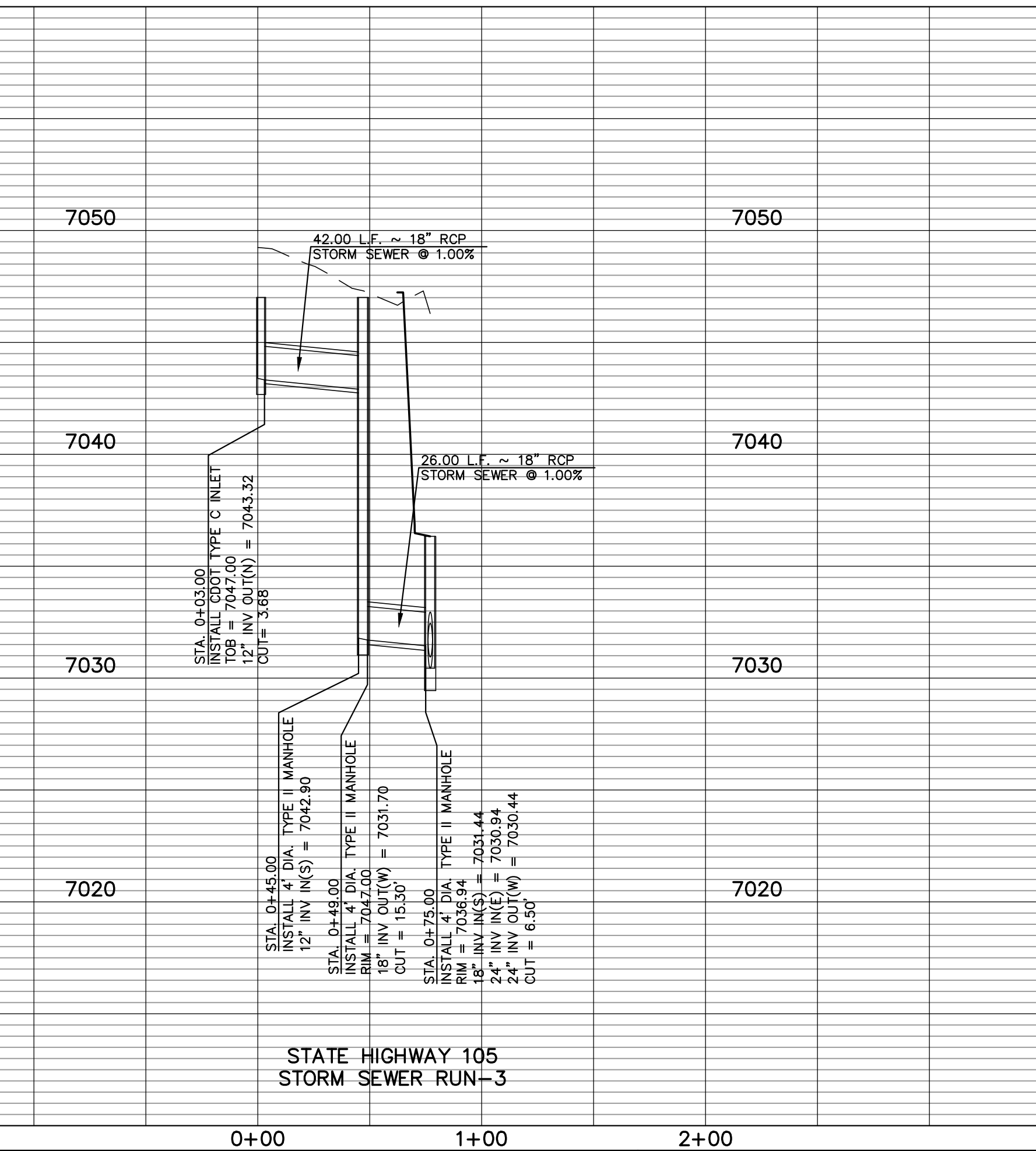
STATE HIGHWAY 105
STORM SEWER RUN-2



STATE HIGHWAY 105
STORM SEWER RUN-3



STATE HIGHWAY 105
STORM SEWER RUN-2



STATE HIGHWAY 105
STORM SEWER RUN-3

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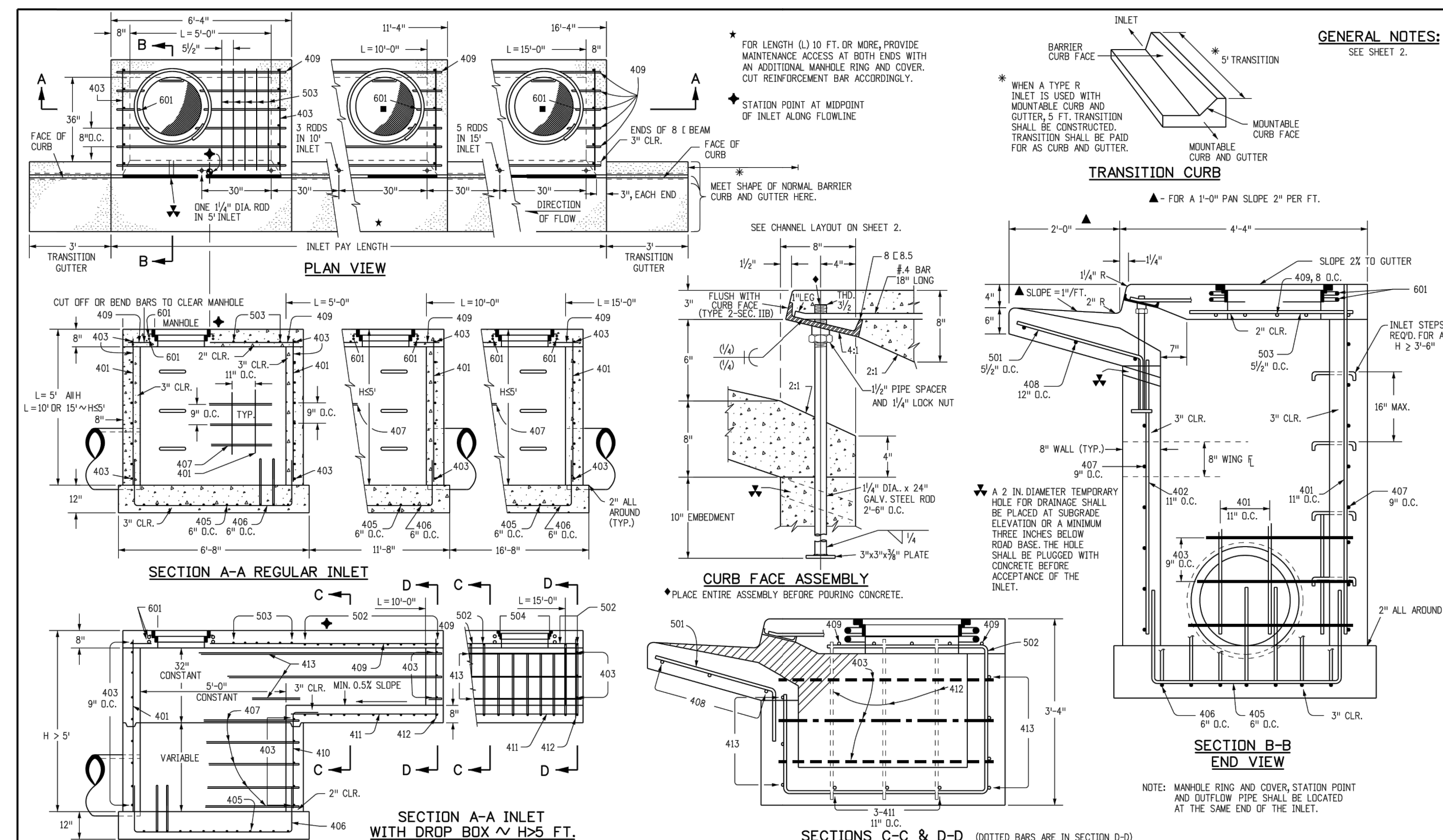
NO.	REVISION	DATE

REVIEW:
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MARC. A. WHORTON, COLORADO P.E. #37155 DATE

STATE HIGHWAY 105 /JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
STORM SEWER PLAN & PROFILE
DESIGNED BY PRA SCALE DATE 02-23-23
DRAWN BY PRA (H) 1"= 50' SHEET 19 OF 23
CHECKED BY (V) 1"= 5' JOB NO. 1302.22

CLASSIC CONSULTING ENGINEERS & SURVEYORS

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Computer File Information	Sheet Revisions	Colorado Department of Transportation	CURB INLET TYPE R	STANDARD PLAN NO. M-604-12
Creation Date: 07/31/19 Designer Initials: JBK Last Modification Date: 07/31/19 Detailer Initials: LTA CAD Ver: MicroStation V8 Scale: Not to Scale Units: English	Date: _____ Comments: _____	2829 West Howard Place COOT HQ, 3rd Floor Denver, CO 80204 Phone: 303-757-9921 FAX: 303-757-9868 Project Development Branch JBK	Issued by the Project Development Branch July 31, 2019 Project Sheet Number:	Standard Sheet No. 1 of 2

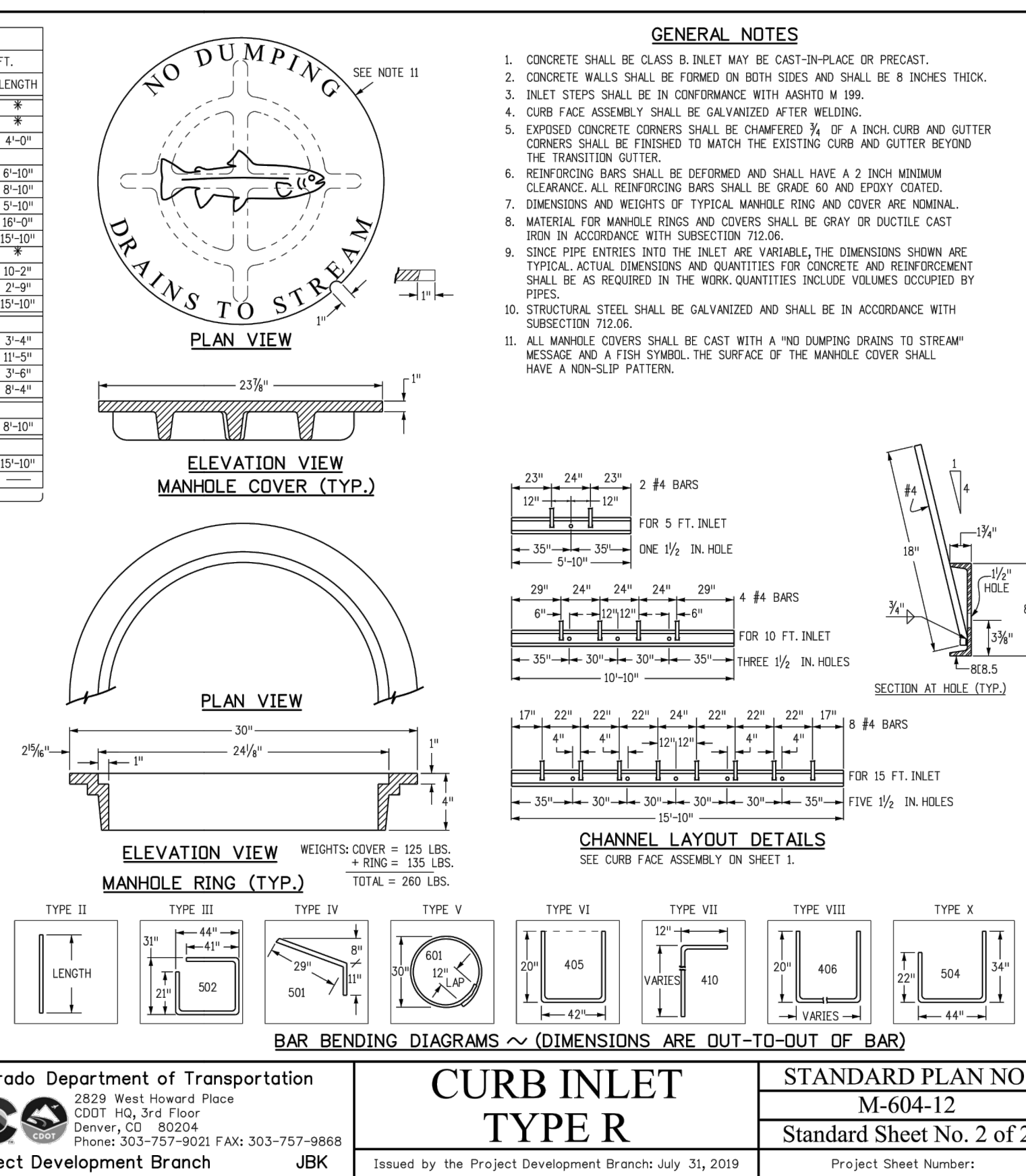
MARK	BAR #	O.C. SPACING	TYPE	ALL INLETS		INLETS H ≤ 5 FT.		INLETS H > 5 FT.	
				L = 5 FT.	L = 10 FT.	L = 5 FT.	L = 10 FT.	L = 5 FT.	L = 10 FT.
401	4	11"	II	15	21	26	31	11	11
402	4	11"	II	7	13	18	24	7	7
403	4	9"	II	7	13	18	24	7	7
404	4	9"	II	7	13	18	24	7	7
405	4	6"	VI	11	17	23	29	11	11
406	4	6"	VIII	7	13	19	25	7	7
407	4	9"	II	7	13	19	25	7	7
408	4	11"	II	3	5	7	9	3	3
409	4	8"	II	6	9	12	15	6	6
410	4	11"	VII	3	5	7	9	3	3
411	4	11"	II	3	5	7	9	3	3
412	4	11"	II	3	5	7	9	3	3
413	4	9"	II	3	5	7	9	3	3

TABLE ONE ~ BAR LIST FOR CURB INLETS, TYPE "R"

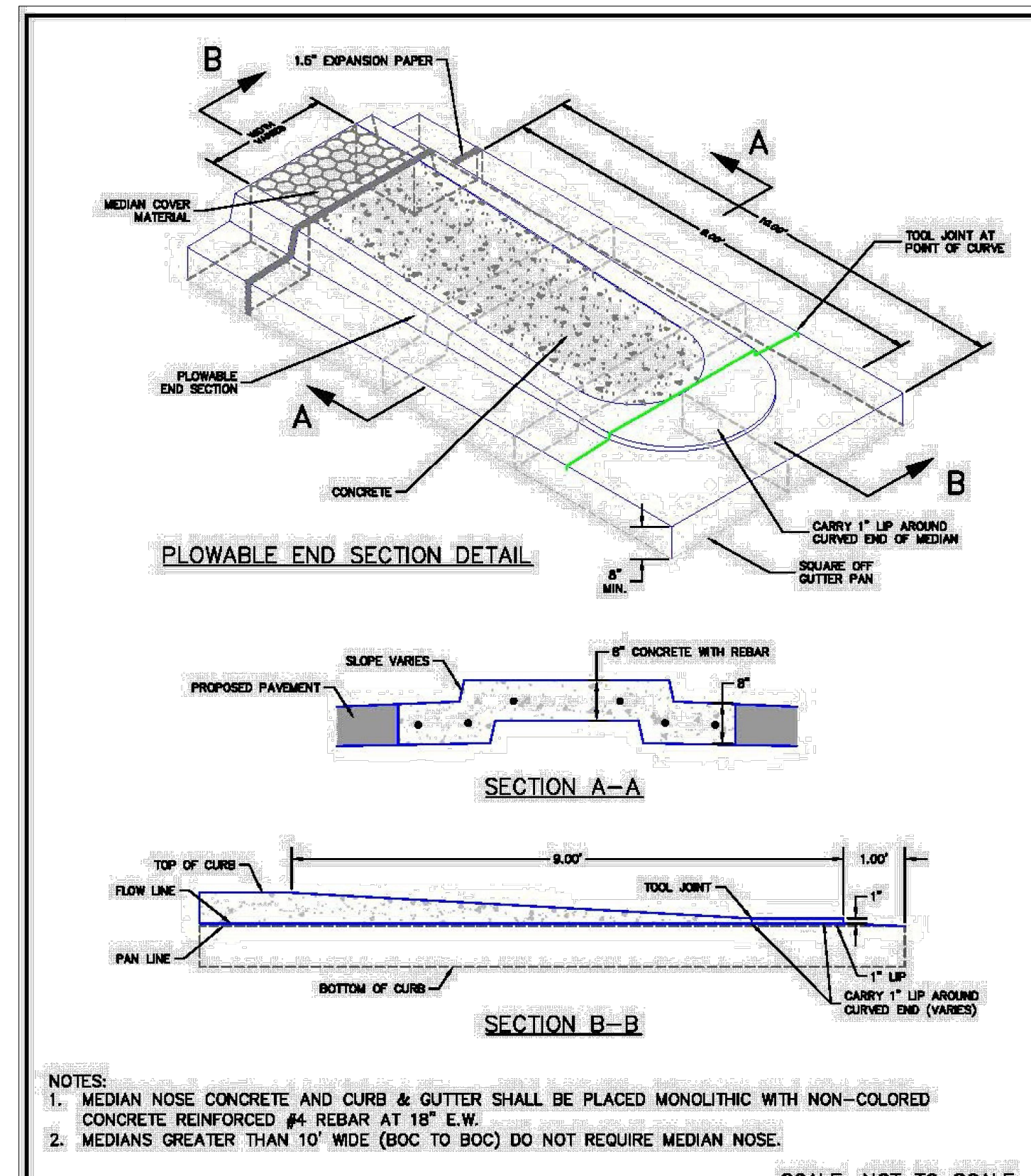
INLET	LENGTH	NO. REQ'D.		L = 5 FT.		L = 10 FT.		L = 15 FT.		
		REGULAR	DEEP BOX	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	
3'-0"	2'-8"	1-8"	10	7	3.2	285	5.3	497	7.4	706
3'-0"	3'-2"	2'-2"	10	7	3.4	305	5.7	528	7.9	747
4'-0"	3'-8"	2'-8"	12	9	3.7	326	6.0	559	8.4	786
4'-0"	4'-2"	3'-2"	12	9	3.9	334	6.4	571	8.8	803
5'-0"	4'-8"	3'-8"	14	11	4.1	354	6.7	602	9.3	844
5'-0"	5'-2"	4'-2"	16	13	4.4	375	7.0	627	9.7	850
6'-0"	5'-8"	4'-8"	18	15	4.6	382	7.2	636	9.9	860
6'-0"	6'-2"	5'-2"	18	15	4.8	402	7.4	637	10.1	880
7'-0"	6'-8"	5'-8"	20	17	5.0	423	7.6	654	10.3	897
7'-0"	7'-2"	6'-2"	20	17	5.3	430	7.8	664	10.5	907
8'-0"	7'-8"	6'-8"	22	19	5.5	451	8.0	684	10.7	927
8'-0"	8'-2"	7'-2"	24	21	5.7	471	8.2	702	10.9	944
9'-0"	8'-8"	7'-8"	24	21	6.0	479	8.4	711	11.1	954
9'-0"	9'-2"	8'-2"	26	23	6.2	499	8.6	732	11.3	974
10'-0"	9'-8"	8'-8"	28	25	6.4	520	8.8	749	11.5	992
10'-0"	10'-2"	9'-2"	28	25	6.7	527	9.0	759	11.7	1001
11'-0"	10'-8"	9'-8"	30	27	7.0	547	9.2	779	11.9	1022

TABLE TWO ~ BARS AND QUANTITIES VARIABLE WITH "H"

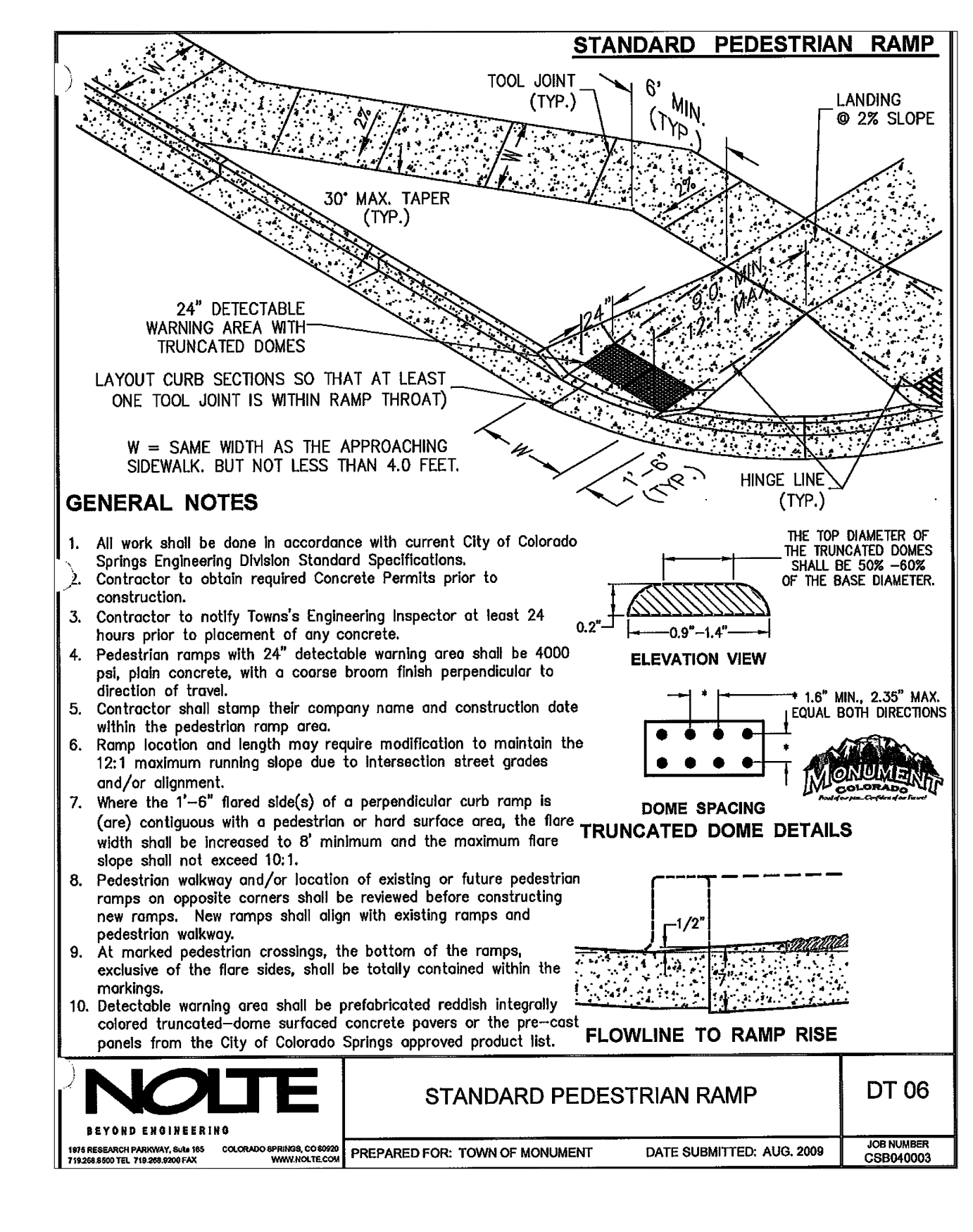
INLET	LENGTH	NO. REQ'D.		L = 5 FT.		L = 10 FT.		L = 15 FT.		
		REGULAR	DEEP BOX	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	CONC. STEEL (CU. YDS.)	
3'-0"	2'-8"	1-8"	10	7	3.2	285	5.3	497	7.4	706
3'-0"	3'-2"	2'-2"	10	7	3.4	305	5.7	528	7.9	747
4'-0"	3'-8"	2'-8"	12	9	3.7	326	6.0	559	8.4	786
4'-0"	4'-2"	3'-2"	12	9	3.9	334	6.4	571	8.8	803
5'-0"	4'-8"	3'-8"	14	11	4.1	354	6.7	602	9.3	844
5'-0"	5'-2"	4'-2"	16	13	4.4	375	7.0	627	9.7	850
6'-0"	5'-8"	4'-8"	18	15	4.6	382	7.2	636	9.9	860
6'-0"	6'-2"	5'-2"	18	15	4.8	402	7.4	637	10.1	880
7'-0"	6'-8"	5'-8"	20	17	5.0	423	7.6	654	10.3	897
7'-0"	7'-2"	6'-2"	20	17	5.3	430	7.8	664	10.5	907
8'-0"	7'-8"	6'-8"	22	19	5.5	451	8.0	684	10.7	927
8'-0"	8'-2"	7'-2"	24	21	5.7	471	8.2	702	10.9	944
9'-0"	8'-8"	7'-8"	24	21	6.0	479	8.4	711	11.1	954
9'-0"	9'-2"	8'-2"	26	23	6.2	499	8.6	732	11.3	974
10'-0"	9'-8"	8'-8"	28	25	6.4	520	8.8	749	11.5	992
10'-0"	10'-2"	9'-2"	28	25	6.7	527	9.0	759	11.7	1001
11'-0"	10'-8"	9'-8"	30	27	7.0	547	9.2	779	11.9	1022



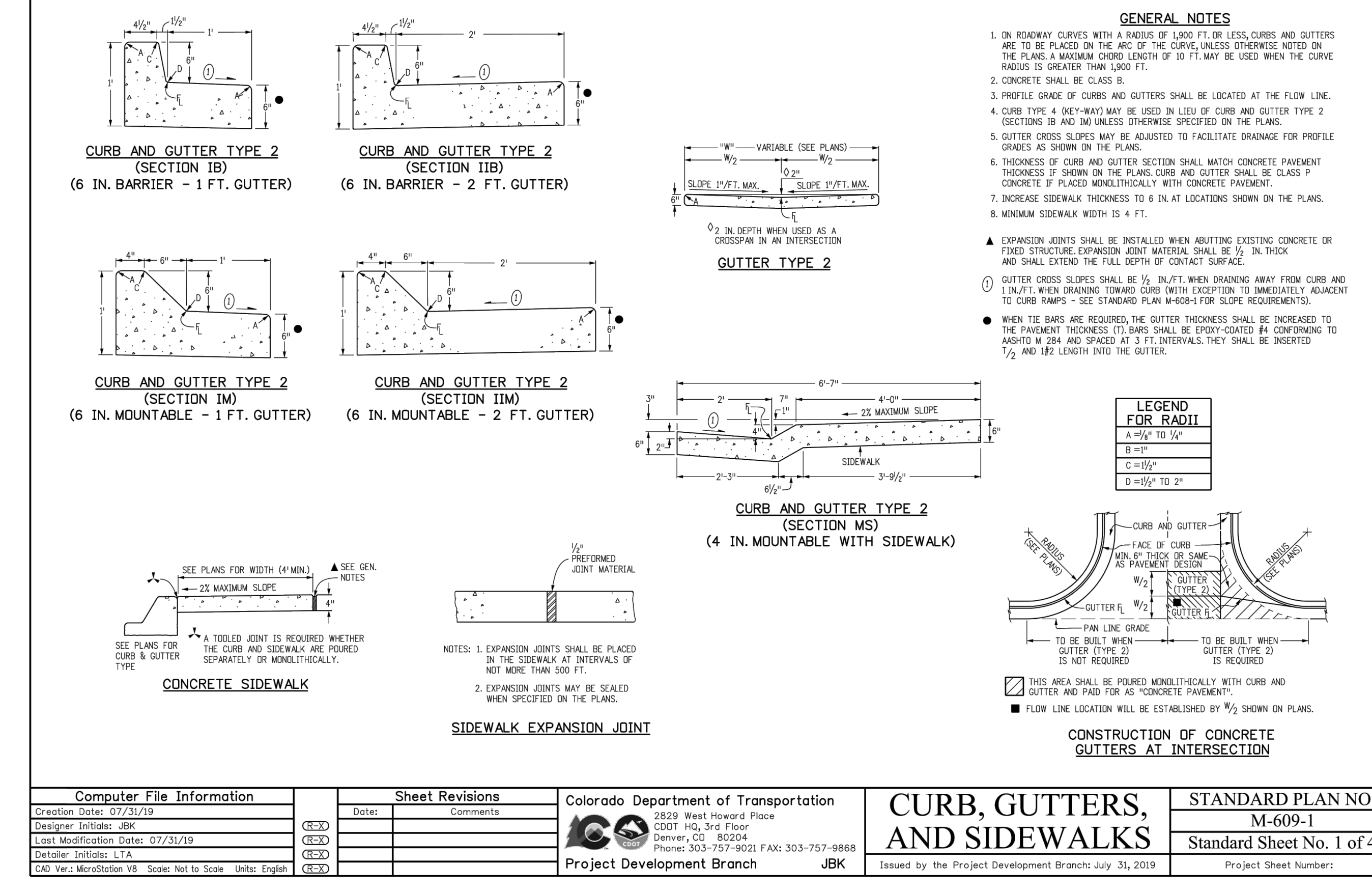
Computer File Information	Sheet Revisions	Colorado Department of Transportation	CURB INLET TYPE R	STANDARD PLAN NO. M-604-12
Creation Date: 07/31/19 Designer Initials: JBK Last Modification Date: 07/31/19 Detailer Initials: LTA CAD Ver: MicroStation V8 Scale: Not to Scale Units: English	Date: _____ Comments: _____	2829 West Howard Place COOT HQ, 3rd Floor Denver, CO 80204 Phone: 303-757-9921 FAX: 303-757-9868 Project Development Branch JBK	Issued by the Project Development Branch July 31, 2019 Project Sheet Number:	Standard Sheet No. 2 of 2



1/12/16	Plowable Median Nose Detail	EL PASO COUNTY
DATE APPROVED: _____ André P. Brackin	DESIGNED BY: PRA SCALE: (H) 1" = 50' DRAWN BY: PRA CHECKED BY: (V) 1" = 5'	DEPARTMENT OF TRANSPORTATION 1/12/16 FILE NO: SD_2-22



NOTES	STANDARD PEDESTRIAN RAMP	DT 06
REVISIONS: _____	PREPARED FOR: TOWN OF MONUMENT DATE SUBMITTED: AUG. 2009 JOB NUMBER: CS8040003	



Computer File Information	Sheet Revisions	Colorado Department of Transportation	CURB, GUTTERS, AND SIDEWALKS	STANDARD PLAN NO. M-609-1
Creation Date: 07/31/19 Designer Initials: JBK Last Modification Date: 07/31/19 Detailer Initials: LTA CAD Ver: MicroStation V8 Scale: Not to Scale Units: English	Date: _____ Comments: _____	2829 West Howard Place COOT HQ, 3rd Floor Denver, CO 80204 Phone: 303-757-9921 FAX: 303-757-9868 Project Development Branch JBK	Issued by the Project Development Branch July 31, 2019 Project Sheet Number:	Standard Sheet No. 1 of 4

48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS	NO. REVISION	DATE
811 UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW		
THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.		

REVIEW:
 PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC
 MARC. A. WHORTON, COLORADO P.E. #37155

CLASSIC CONSULTING ENGINEERS & SURVEYORS
 STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
 ROADWAY IMPROVEMENT PLANS
 DETAIL SHEET
 DESIGNED BY: PRA
 DRAWN BY: PRA
 CHECKED BY: (V)
 SCALE: (H) 1" = 50'
 (V) 1" = 5'
 DATE: 02-23-23
 SHEET 20 OF 23
 JOB NO. 1302.22

S:\121234-01 - Jackson Creek Parkway\04_CIVIL\CADD\Traffic\ITS\Drawings\105_Signal\T121234-01SIG-TAB01_10/27/2022 9:49:44 AM - Ben.Harms

TABULATION OF TRAFFIC SIGNAL ITEMS

CDOT ITEM NO.	ITEM DESCRIPTION	UNITS	TOTAL	NOTES
202-00828	REMOVAL OF TRAFFIC SIGNAL EQUIPMENT	L S	1	REFER TO THE REMOVAL ITEMS AND QUANTITIES BELOW
FOR INFORMATION ONLY	REMOVAL OF TRAFFIC SIGNAL POLE	EACH	4	RETURN TO EL PASO COUNTY
	REMOVAL OF TRAFFIC SIGNAL HEAD	EACH	12	
	REMOVAL OF TRAFFIC SIGNAL CONTROLLER AND CABINET	EACH	1	
	REMOVAL OF LUMINAIRE	EACH	4	
	REMOVAL OF STREET NAME SIGN	EACH	3	
	REMOVAL OF LANE USE/OPERATION SIGN	EACH	2	
	REMOVAL OF SPAN WIRE CABLE	LF	320	
503-00036	DRILLED SHAFT (36 INCH)	LF	60	CDOT STD. S-614-40A FOOTING
613-00206	2 INCH ELECTRICAL CONDUIT (BORED)	LF	405	SCHEDULE 80
613-00306	3 INCH ELECTRICAL CONDUIT (BORED)	LF	910	SCHEDULE 80
613-01200	2 INCH ELECTRICAL CONDUIT (PLASTIC)	LF	155	SCHEDULE 80
613-01300	3 INCH ELECTRICAL CONDUIT (PLASTIC)	LF	175	SCHEDULE 80
613-07003	TYPE THREE PULL BOX	EACH	4	SIGNAL POLE AND CONTROLLER PULL BOXES
613-07004	TYPE FOUR PULL BOX	EACH	1	HOME RUN PULL BOX
613-10000	WIRING	L S	1	SIGNAL AND LIGHTING
613-13004	LUMINAIRE (LED) (4,000 LUMENS)	EACH	4	
613-50109	METER POWER PEDESTAL	EACH	1	
614-10160	SIGNAL HEAD BACKPLATES	EACH	9	BACKPLATES SHALL HAVE YELLOW RETROREFLECTIVE BORDER
614-70150	PEDESTRIAN SIGNAL FACE (16) (COUNTDOWN)	EACH	2	LED TYPE, POLYCARBONATE, YELLOW INCOLOR
614-70336	TRAFFIC SIGNAL FACE (12-12-12)	EACH	15	LED TYPE, POLYCARBONATE, YELLOW INCOLOR
614-72854	TRAFFIC CONTROLLER CABINET	EACH	1	
614-72860	PEDESTRIAN PUSH BUTTON	EACH	1	PUSH BUTTON STATION, R10-3e SIGN AND ANY NECESSARY EXTENDER
614-72863	PEDESTRIAN PUSH BUTTON POST ASSEMBLY	EACH	1	CDOT STD. S-614-45, INCLUDES R10-3e SIGNS
614-72886	INTERSECTION DETECTION SYSTEM (CAMERA)	EACH	2	
614-81000	TRAFFIC SIGNAL-LIGHT POLE STEEL	EACH	4	
614-81125	TRAFFIC SIGNAL-LIGHT POLE STEEL (1-25 FOOT MAST ARM)	EACH	1	CDOT STD. S-614-40A
614-81145	TRAFFIC SIGNAL-LIGHT POLE STEEL (1-45 FOOT MAST ARM)	EACH	1	CDOT STD. S-614-40A
614-81155	TRAFFIC SIGNAL-LIGHT POLE STEEL (1-55 FOOT MAST ARM)	EACH	1	CDOT STD. S-614-40A
614-86800	UNINTERRUPTED POWER SUPPLY	EACH	1	

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 Colorado Springs, CO 80903
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SHEET REVISIONS		
DATE	COMMENTS	INITIALS

AS CONSTRUCTED
NO. REVISIONS:
REVISED:
VOID:

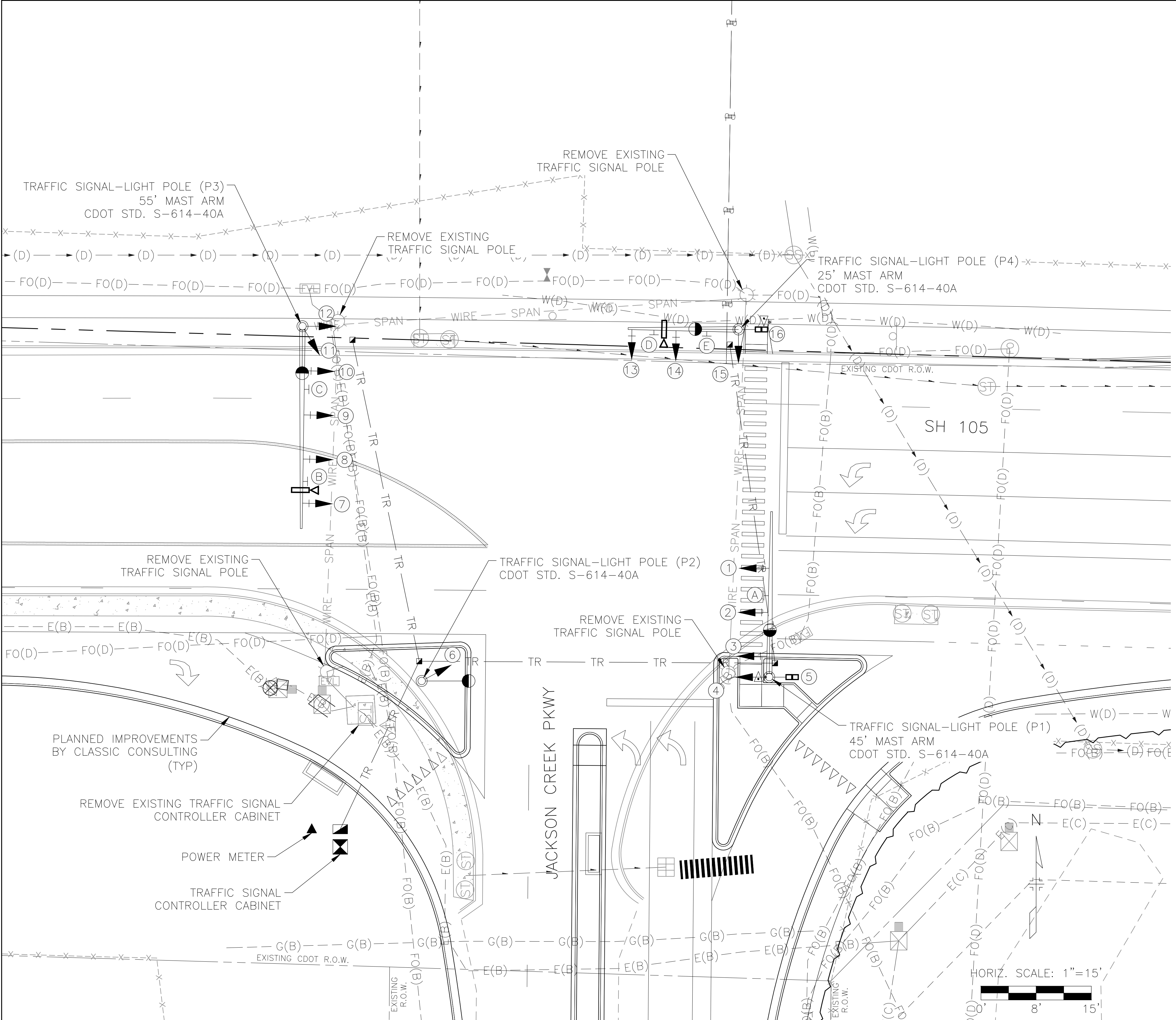
JACKSON CREEK PARKWAY
 TRAFFIC SIGNAL PLAN
 STATE HIGHWAY 105

DESIGNER: BJH
 DETAILER: BEN.HARMS

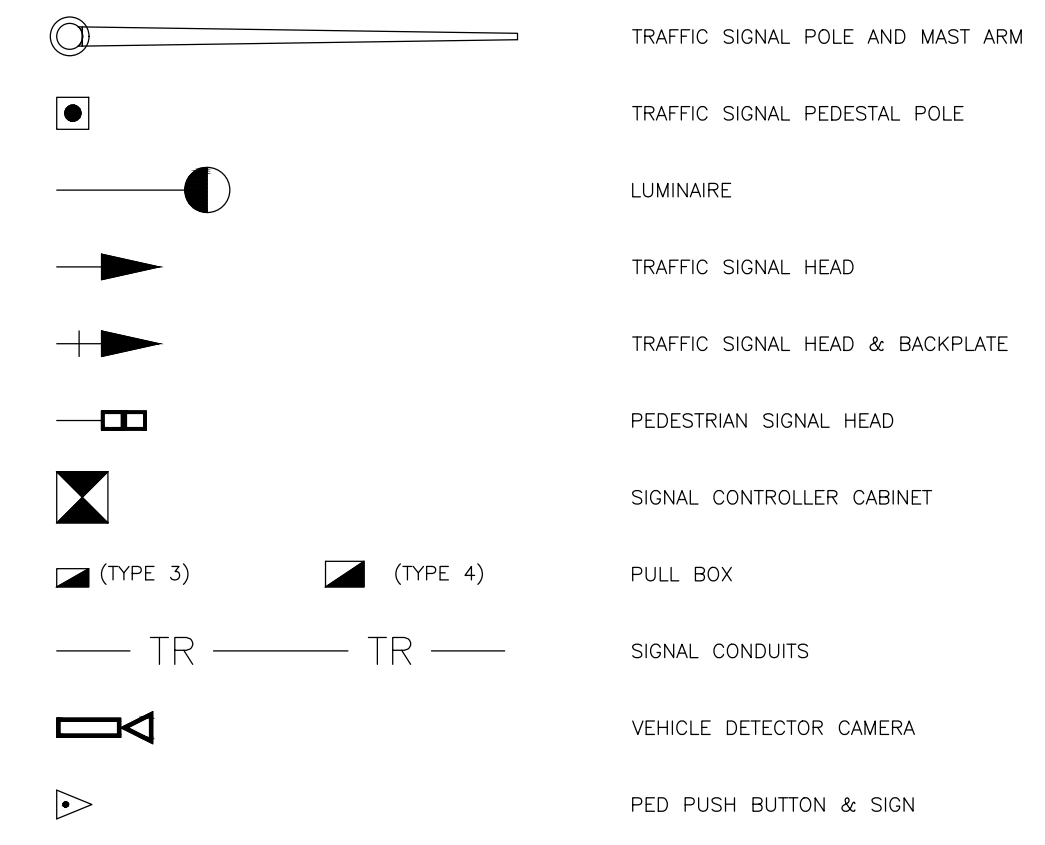
SHEET SUBSET: TRAFFIC

PROJECT NO./CODE	121234-01
SHEET 21 OF 23	

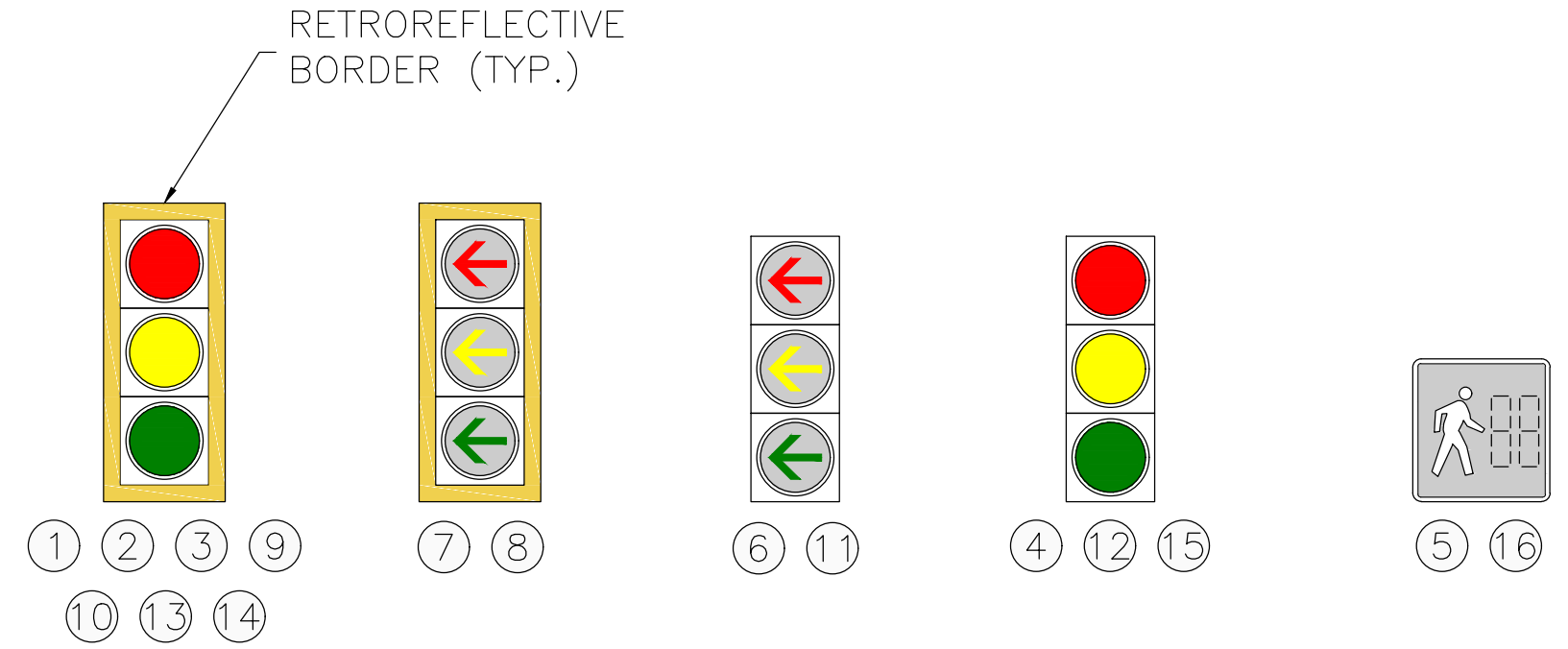
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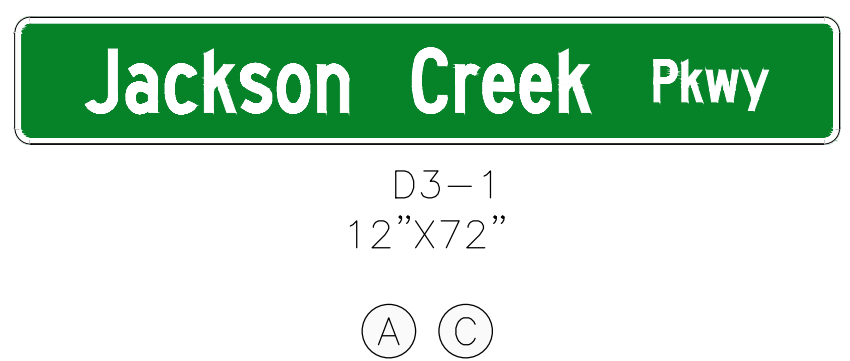
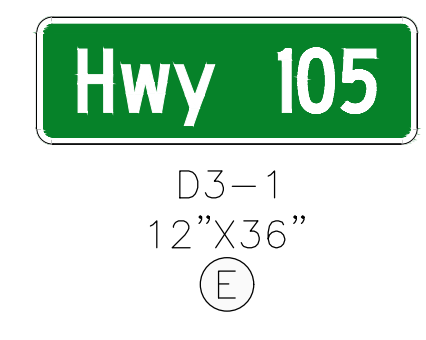
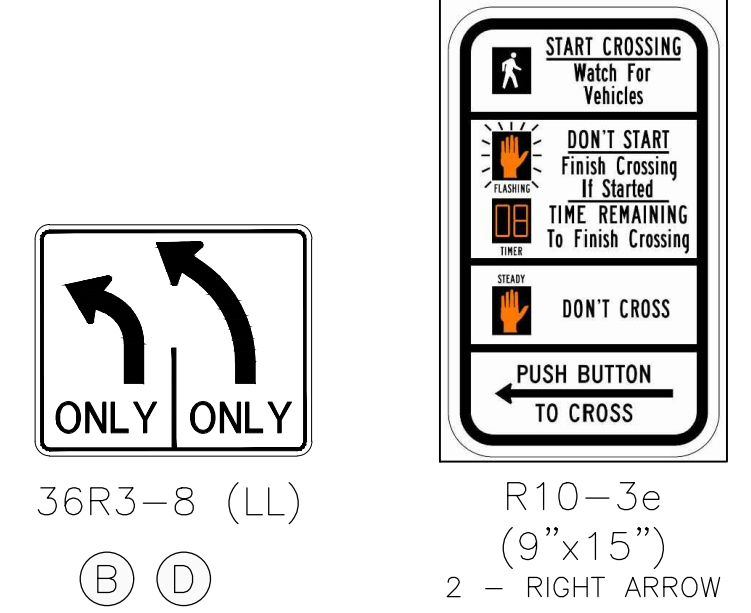
TRAFFIC SIGNAL SYMBOLS



PROPOSED SIGNAL HEADS



PROPOSED SIGNS



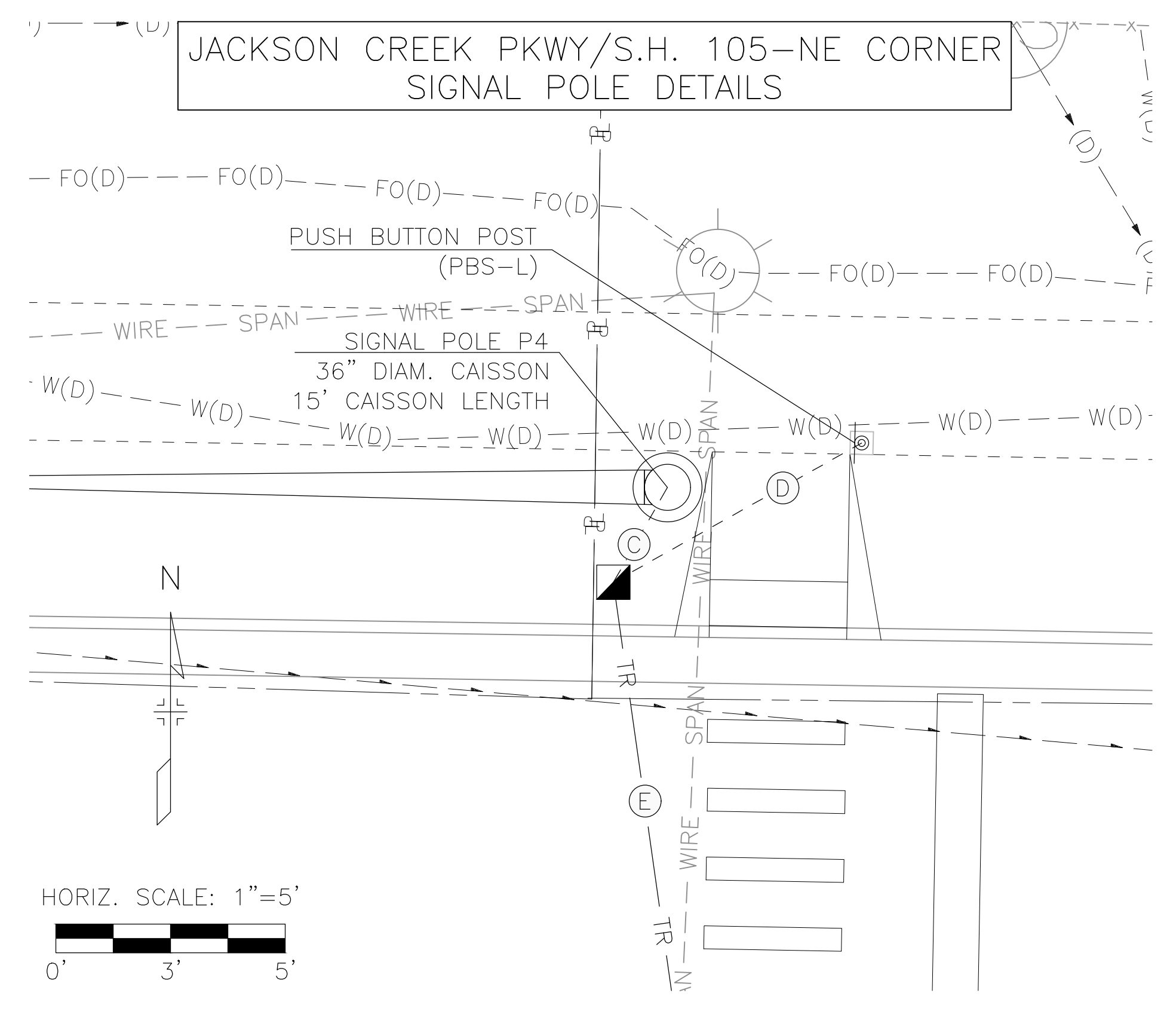
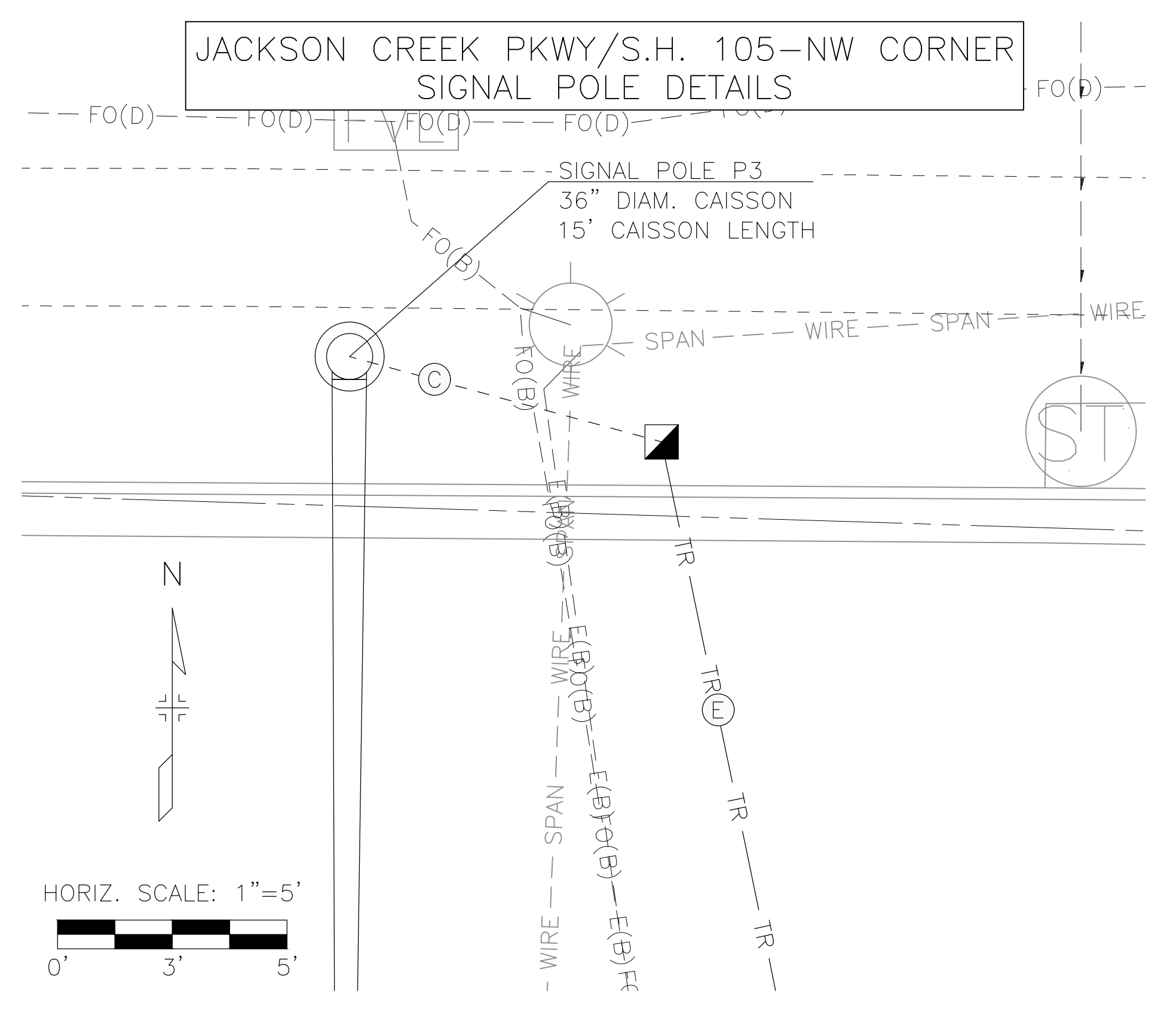
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SHEET REVISIONS			
DATE	COMMENTS	INITIALS	



AS CONSTRUCTED	JACKSON CREEK PARKWAY TRAFFIC SIGNAL PLAN STATE HIGHWAY 105	PROJECT NO./CODE
NO. REVISIONS:	DESIGNER: BJH	121234-01
REVISED:	DETAILER: BEN.HARMS	
VOID:	SHEET SUBSET: TRAFFIC	SHEET SUBSET: TS-01 OF 2



LEGEND

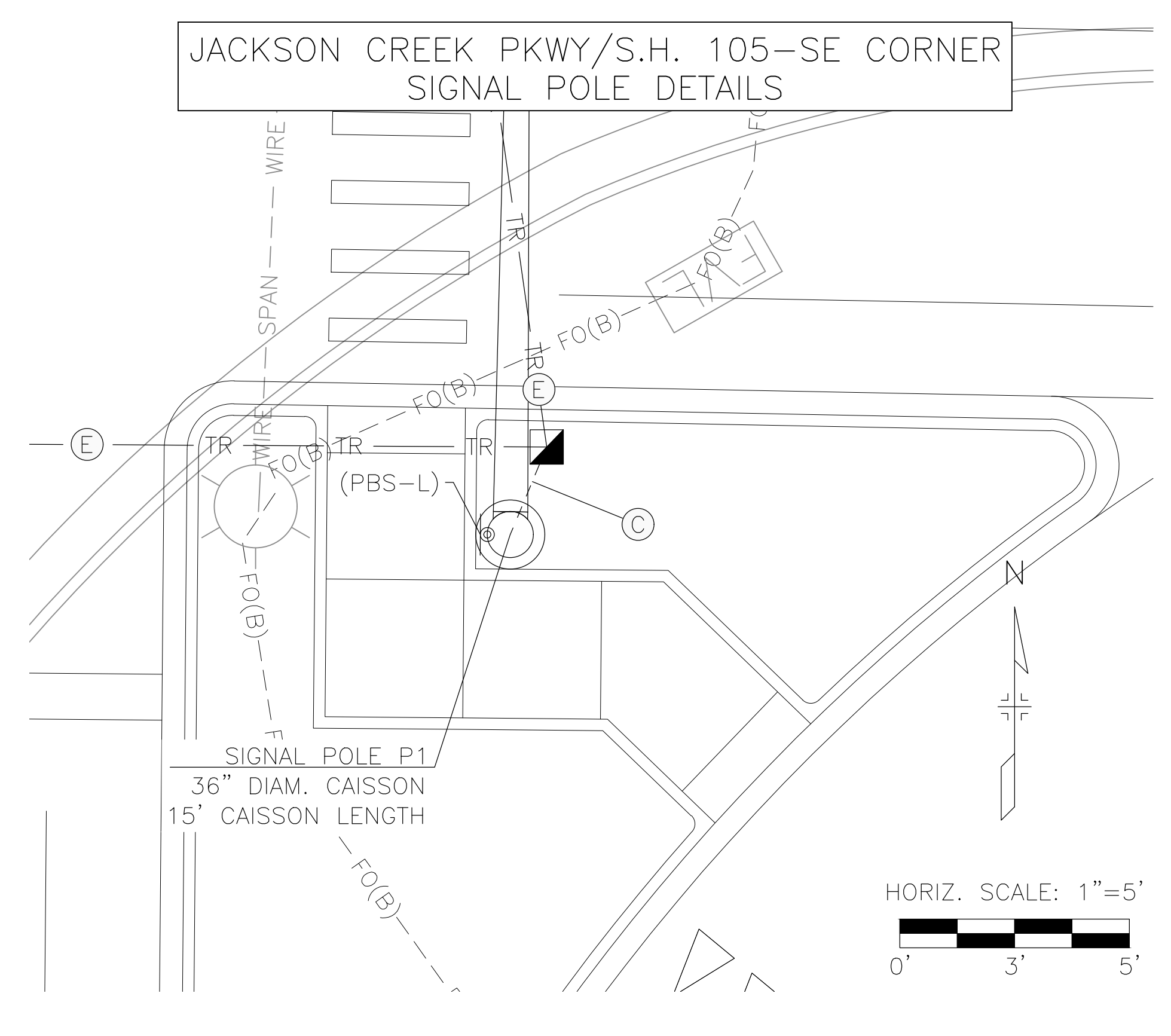
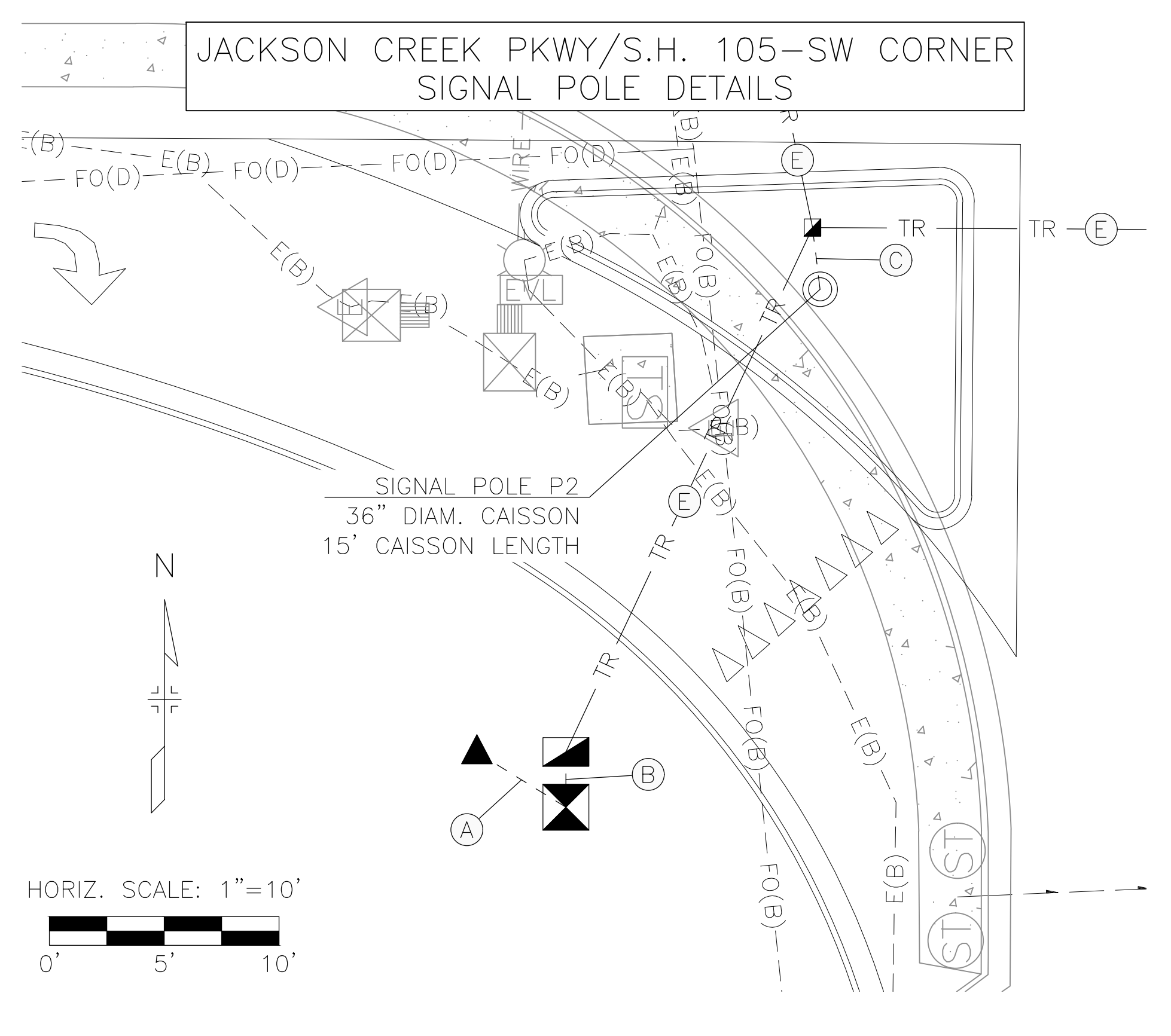
(PBS-R/L = PED PUSH BUTTON STATION WITH R10-3e RIGHT OR LEFT SIGN)

- PULL BOX (TYPE 4) (24"x36"x24")
- PULL BOX (TYPE 3) (17"x30"x12")
- TR — TRAFFIC SIGNAL CONDUIT STREET CROSSINGS
- TRAFFIC SIGNAL CONDUIT PULL BOX CONNECTIONS

SIGNAL CONDUITS

(A)	POWER METER:	1-2"	(TRENCHED)
(B)	CONTROLLER	2-2" AND 2-3"	(TRENCHED)
(C)	SIGNAL POLES	1-2" AND 2-3"	(TRENCHED)
(D)	PUSH BUTTON POST	1-2"	(TRENCHED)
(E)	STREET CROSSINGS	1-2" AND 2-3"	(BORED)

NOTE:
 PULL BOX AND CONDUIT LOCATIONS ARE APPROXIMATE ONLY. TO THE EXTENT POSSIBLE, THE CONTRACTOR SHALL INSTALL PULL BOXES OUTSIDE OF SIDEWALK AREAS; HOWEVER, IF PULL BOXES NEED TO BE PLACED WITHIN SIDEWALKS, THEY SHALL BE INSTALLED FLUSH WITH THE FINISHED SURFACE AND THE LIDS SHALL HAVE AN ANTI-SKID SURFACE TREATMENT. PULL BOXES SHALL NOT BE INSTALLED IN ANY CURB RAMPS.



SCHEDULE OF TRAFFIC SIGNAL POLES

POLE ID	TRAFFIC SIGNAL POLE		STAKING LOCATION AND CAISSON DETAILS			
	MAST	ARM	NORTHING	EASTING	DIAMETER	DEPTH
P1	30 FT	15 FT ARM	1459419.60	3183115.91	36 IN	15 FT
P2	-	15 FT ARM	1459418.55	3183021.45	36 IN	15 FT
P3	50 FT	15 FT ARM	1459515.01	3182989.04	36 IN	15 FT
P4	25 FT	15 FT ARM	1459514.13	3183107.67	36 IN	15 FT

Mast arm lengths do not match with lengths given on previous page

S:\121234-01 - Jackson Creek Parkway\04_CIVIL\CADD\Traffic\ITS\Drawings\105_Signal\T121234-01SIG-PLN02.dwg, 10/27/2022 9:46:19 AM, Ben.Harms

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SHEET REVISIONS		
DATE	COMMENTS	INITIALS



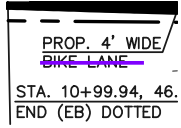
AS CONSTRUCTED
NO. REVISIONS:
REVISED:
VOID:

JACKSON CREEK PARKWAY TRAFFIC SIGNAL POLE DETAILS STATE HIGHWAY 105	
DESIGNER: BJH	STRUCTURE NUMBERS
DETAILER: BEN.HARMS	
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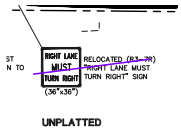
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SHEET 23 OF 23	

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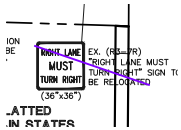
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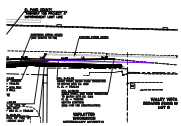
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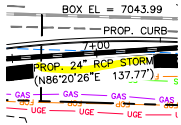


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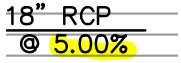


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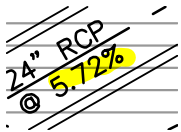
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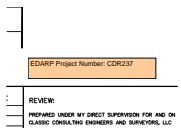


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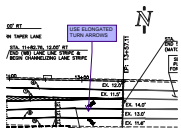
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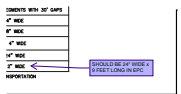
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EDARP Project Number: CDR237



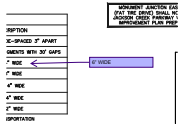
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USE ELONGATED TURN ARROWS



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SHOULD BE 24" WIDE x 9 FEET LONG IN EPC



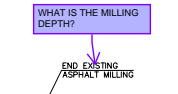
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6" WIDE



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CONCERNED ABOUT CLASSIC ENDING THEIR STORM HERE. IF THEY CONSTRUCT THE WALL BEFORE THE HWY 105 PROJECT STORM IS CONSTRUCTED, THERE IS POTENTIAL FOR THE HWY105 PROJECT TO IMPACT THE INTEGRITY OF THE WALL. PLEASE SHOW THE STORM EXTENDING BEYOND THE LIMITS OF THE WALL TO TIE INTO THE HWY 105 STORM. THIS IS WORST CASE SCENERIO AND MAY NOT BE REQUIRED IF STORM FOR HWY 105 PROJECT COMES FIRST. ADDITIONAL COORDINATION WITH EPC FOR THE STORM ELEVATIONS IS REQUIRED.



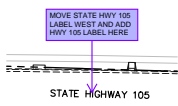
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WHAT IS THE MILLING DEPTH?



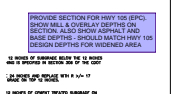
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DELETE "STATE" HERE. CDOT'S STATE HWY 105 IS ON WEST SIDE OF JCP



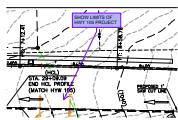
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MOVE STATE HWY 105 LABEL WEST AND ADD HWY 105 LABEL HERE



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PROVIDE SECTION FOR HWY 105 (EPC). SHOW MILL & OVERLAY DEPTHS ON SECTION. ALSO SHOW ASPHALT AND BASE DEPTHS - SHOULD MATCH HWY 105 DESIGN DEPTHS FOR WIDENED AREA



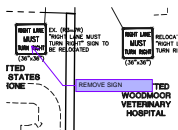
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SHOW LIMITS OF HWY 105 PROJECT



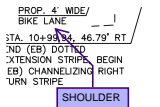
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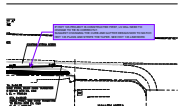
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REMOVE SIGN



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SHOULDER



Subject: Callout
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Author: EPCDPW-Werre
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IF HWY 105 PROJECT IS CONSTRUCTED FIRST, L5 WILL NEED TO CHANGE TO TIE IN CORRECTLY. SUGGEST CHANGING THE CURB AND GUTTER DESIGN NOW TO MATCH HWY 105 PLANS AND STRIPE THE TAPER. SEE HWY 105 LINEWORK



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Author: EPCDPW-Werre
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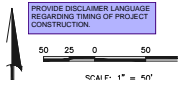
STRIPING WILL NEED TO BE ADJUSTED IF HWY 105 PROJECT GOES FIRST. ALSO ADJUST/ADD FOR NEW C&G ALIGNMENT AT SOUTHEAST TIE IN.

PROVIDE DISCLAIMER LANGUAGE REGARDING TIMING OF PROJECT CONSTRUCTION.

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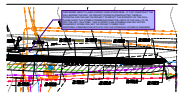
PROVIDE DISCLAIMER LANGUAGE REGARDING TIMING OF PROJECT CONSTRUCTION.

PROVIDE DISCLAIMER LANGUAGE REGARDING TIMING OF PROJECT CONSTRUCTION.



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PROVIDE DISCLAIMER LANGUAGE REGARDING TIMING OF PROJECT CONSTRUCTION.



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Space:

CONCERNED ABOUT CLASSIC ENDING THEIR STORM HERE. IF THEY CONSTRUCT THE WALL BEFORE THE HWY 105 PROJECT STORM IS CONSTRUCTED, THERE IS POTENTIAL FOR THE HWY105 PROJECT TO IMPACT THE INTEGRITY OF THE WALL. PLEASE SHOW THE STORM EXTENDING BEYOND THE LIMITS OF THE WALL TO TIE INTO THE HWY 105 STORM. THIS IS WORST CASE SCENERIO AND MAY NOT BE REQUIRED IF STORM FOR HWY 105 PROJECT COMES FIRST. ADDITIONAL COORDINATION WITH EPC FOR THE STORM ELEVATIONS IS REQUIRED.



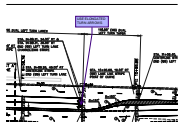
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Author: EPCDPW-Werre
Date: 4/3/2023 9:33:16 AM
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CONCERNED ABOUT CLASSIC ENDING THEIR STORM HERE. IF THEY CONSTRUCT THE WALL BEFORE THE HWY 105 PROJECT STORM IS CONSTRUCTED, THERE IS POTENTIAL FOR THE HWY105 PROJECT TO IMPACT THE INTEGRITY OF THE WALL. PLEASE SHOW THE STORM EXTENDING BEYOND THE LIMITS OF THE WALL TO TIE INTO THE HWY 105 STORM. THIS IS WORST CASE SCENERIO AND MAY NOT BE REQUIRED IF STORM FOR HWY 105 PROJECT COMES FIRST. ADDITIONAL COORDINATION WITH EPC FOR THE STORM ELEVATIONS IS REQUIRED.



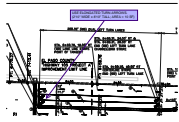
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Page Label: [1] LAYOUT1
Author: EPCDPW-Werre
Date: 4/3/2023 9:47:08 AM
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SHOW 24 IN x 9 FT AND SPACING ON STRIPES AND CENTER OF LANE



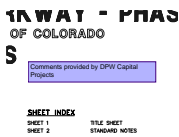
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Author: EPCDPW-Werre
Date: 4/3/2023 9:58:26 AM
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USE ELONGATED TURN ARROWS



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Author: EPCDPW-Werre
Date: 4/3/2023 9:59:12 AM
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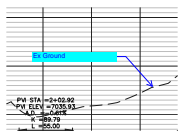
USE ELONGATED TURN ARROWS. (2'10" WIDE x 8'10" TALL; AREA = 10 SF)



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Page Label: [1] Layout1
Author: CDurham
Date: 4/5/2023 10:21:37 AM
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Space:

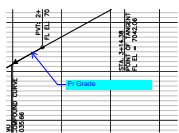
Comments provided by DPW Capital Projects

#0000FF (26)



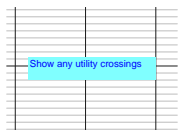
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Author: CDurham
Date: 3/29/2023 10:35:57 AM
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Layer:
Space:

Ex Ground



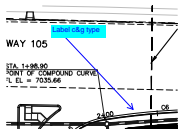
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Author: CDurham
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Pr Grade



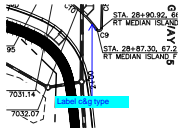
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Page Label: [1] SI-01
Author: CDurham
Date: 3/29/2023 10:36:19 AM
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Color: ■
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Space:

Show any utility crossings



Subject: Callout
Page Label: [1] SI-01
Author: CDurham
Date: 3/29/2023 10:40:51 AM
Status:
Color: ■
Layer:
Space:

Label c&g type



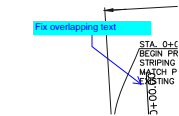
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Author: CDurham
Date: 3/29/2023 10:41:13 AM
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Label c&g type



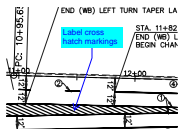
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Page Label: [1] SI-04
Author: CDurham
Date: 3/29/2023 10:43:17 AM
Status:
Color: ■
Layer:
Space:

Provide slopes for pedestrian ramps and through "cross walk" areas to ensure meeting 2% slope requirement



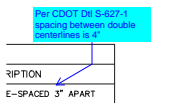
Subject: Callout
Page Label: [1] LAYOUT1
Author: CDurham
Date: 3/29/2023 10:46:49 AM
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Color: ■
Layer:
Space:

Fix overlapping text



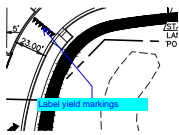
Subject: Callout
Page Label: [1] LAYOUT1
Author: CDurham
Date: 3/29/2023 11:21:19 AM
Status:
Color: ■
Layer:
Space:

Label cross hatch markings



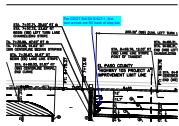
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Page Label: [1] LAYOUT1
Author: CDurham
Date: 3/29/2023 11:24:51 AM
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Per CDOT Dtl S-627-1 spacing between double centerlines is 4"



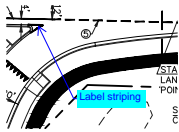
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Author: CDurham
Date: 3/29/2023 11:31:43 AM
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Layer:
Space:

Label yield markings



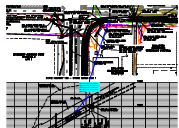
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Page Label: [1] LAYOUT1
Author: CDurham
Date: 3/29/2023 11:37:14 AM
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Per CDOT Std Dtl S-627-1, first turn arrows are 50' back of stop bar



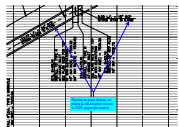
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Page Label: [1] LAYOUT1
Author: CDurham
Date: 3/29/2023 11:44:45 AM
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Layer:
Space:

Label striping



Subject: Callout
Page Label: [1] STORM-1
Author: CDurham
Date: 3/29/2023 2:11:24 PM
Status:
Color: ■
Layer:
Space:

Pipe size does not match with calculations shown in FDR appendix. Revise so documents all show same pipe size



Subject: Callout
Page Label: [1] STORM-1
Author: CDurham
Date: 3/29/2023 2:12:38 PM
Status:
Color: ■
Layer:
Space:

Revise so pipe slopes on plans & information shown in FDR appendix match

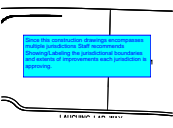


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Author: dsdlaforce
Date: 3/29/2023 3:02:10 PM
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Layer:
Space:



Subject: Callout
Page Label: [1] Layout1
Author: dsdlaforce
Date: 3/29/2023 3:24:12 PM
Status:
Color: ■
Layer:
Space:

Include the following signal notes. For higher resolution see the approved signal plans under CDR2010 for Trails at Aspen signal design.



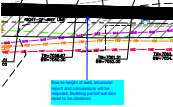
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Date: 3/29/2023 5:23:00 PM
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Since this construction drawings encompasses multiple jurisdictions Staff recommends Showing/Labeling the jurisdictional boundaries and extents of improvements each jurisdiction is approving.



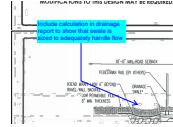
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This review is only for the proposed construction in Highway 105, east of Jackson Creek Parkway.



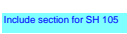
Subject: Callout
Page Label: [1] LAYOUT1
Author: CDurham
Date: 3/29/2023 9:31:26 AM
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Due to height of wall, structural report and calculations will be required. Building permit will also need to be obtained.



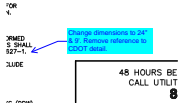
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Author: CDurham
Date: 3/29/2023 9:37:07 AM
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Include calculation in drainage report to show that swale is sized to adequately handle flow



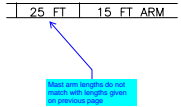
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Page Label: [1] Layout1
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Include section for SH 105



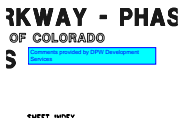
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Change dimensions to 24" & 9'. Remove reference to CDOT detail.



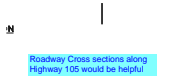
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Mast arm lengths do not match with lengths given on previous page



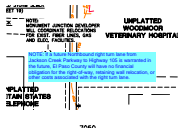
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Comments provided by DPW Development Services



Subject: Text Box
Page Label: [1] Layout1
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Date: 4/5/2023 10:23:12 AM
Status:
Color: ■
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Space:

Roadway Cross sections along Highway 105 would be helpful



Subject: Text Box
Page Label: [1] STORM-1
Author: CDurham
Date: 4/5/2023 11:18:44 AM
Status:
Color: ■
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NOTE: If a future Northbound right turn lane from Jackson Creek Parkway to Highway 105 is warranted in the future, El Paso County will have no financial obligation for the right-of-way, retaining wall relocation, or other costs associated with the right turn lane.