

STATE HIGHWAY 105 / JACKSON CREEK PARKWAY - PHASE 2

COUNTY OF EL PASO, TOWN OF MONUMENT, STATE OF COLORADO

CONSTRUCTION PLANS

FEBRUARY 2023

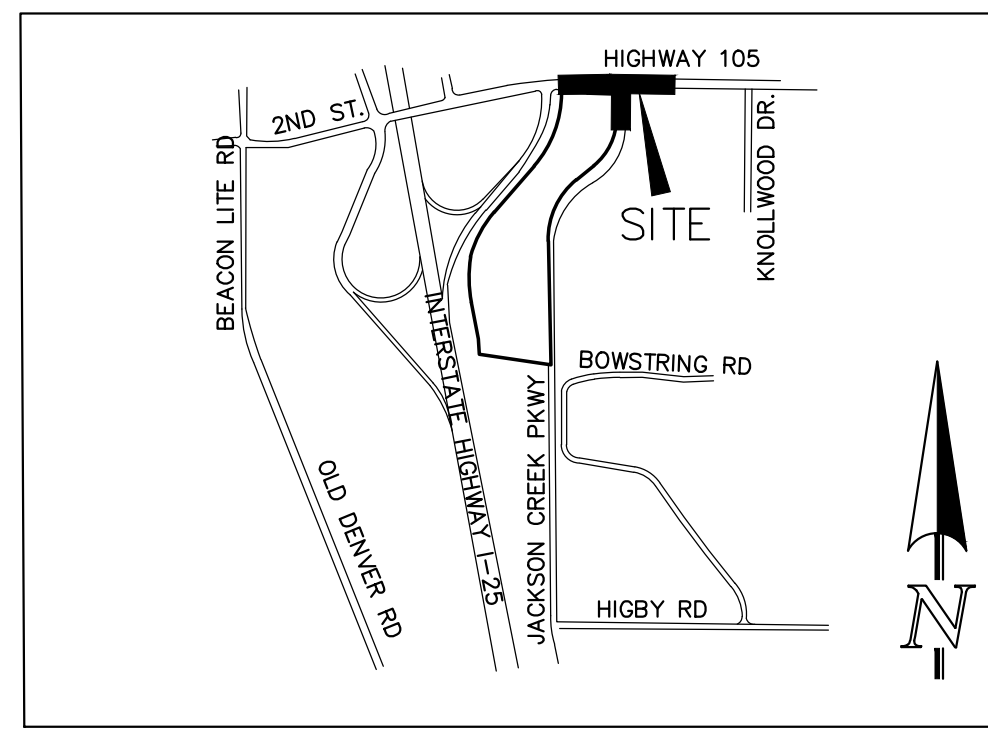
Comments provided by DPW Capital Projects

Comments provided by DPW Development Services

CCES RESPONSES

EL PASO COUNTY GENERAL CONSTRUCTION NOTES:

- THE LOCATION OF EXISTING UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND MAY NOT INCLUDE ALL UTILITIES. THE EXCAVATION CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES.
- BEFORE COMMENCING ANY EXCAVATION, CALL 1-800-922-1987 FOR EXISTING UTILITY LOCATIONS.
- THE CONTRACTOR WILL TAKE THE NECESSARY PRECAUTIONS TO PROTECT EXISTING UTILITIES FROM DAMAGE DUE TO THIS OPERATION. ANY DAMAGE TO THE UTILITIES WILL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, AND ANY SERVICE DISRUPTION WILL BE SETTLED BY THE CONTRACTOR.
- ALL BACKFILL, SUB-BASE AND/OR BASE COURSE (CLASS 6) MATERIAL SHALL BE COMPACTED TO THE SOILS ENGINEER'S RECOMMENDATIONS, AND APPROVED BY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (PCD).
- ALL STATIONING IS CENTERLINE UNLESS OTHERWISE INDICATED. ALL ELEVATIONS ARE CENTERLINE UNLESS OTHERWISE INDICATED.
- THE CONTRACTOR SHALL REVEGETATE ALL DISTURBED AREAS AS SOON AS POSSIBLE AND EROSION CONTROL SHALL BE INSTALLED AND MAINTAINED IN A FUNCTIONAL MANNER AT ALL TIMES. DEVELOPER RESPONSIBLE FOR MAINTAINING DISTURBED AREAS UNTIL REVEGETATION IS COMPLETE.
- ALL DISTURBED PAVEMENT EDGES SHALL BE CUT TO NEAT LINES. REPAIR SHALL CONFORM TO THE EPC ECM APPENDIX K - 1.2C.
- ADDITIONAL EROSION CONTROL STRUCTURES MAY BE REQUIRED AT THE TIME OF CONSTRUCTION.
- BUILDING CONTRACTORS WILL BE RESPONSIBLE FOR CONSTRUCTING POSITIVE DRAINAGE AWAY FROM ALL STRUCTURES.
- ASPHALT THICKNESS AND BASE COURSE THICKNESS (COMPACTED) FOR ROADS SHALL BE PER DESIGN REPORT BY OWNER'S GEOTECHNICAL ENGINEER. OWNER'S GEOTECHNICAL ENGINEER TO BE ON SITE AT TIME OF ROAD CONSTRUCTION TO EVALUATE SOIL CONDITIONS AND DETERMINE IF ADDITIONAL MEASURES ARE NECESSARY TO ASSURE STABILITY OF THE NEW ROADS. PAVEMENT DESIGN SHALL BE APPROVED BY PLANNING AND COMMUNITY DEVELOPMENT PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL REVEGETATE ALL DISTURBED AREAS WITHIN 21 DAYS OF SUBSTANTIAL GRADING COMPLETION. EROSION CONTROL SHALL BE INSTALLED AND MAINTAINED IN A FUNCTIONAL MANNER AT ALL TIMES. DEVELOPER IS RESPONSIBLE FOR MAINTAINING DISTURBED AREAS UNTIL REVEGETATION IS COMPLETE.
- TYPE M RIP-RAP WITH 4" OF TYPE II GRANULAR BEDDING AND MIRAFI 180N OR EQUAL MAY BE SUBSTITUTED WHERE TYPE L RIP-RAP WITH MIRAFI FW 700 OR EQUAL IS SPECIFIED
- ALL MATERIALS AND INSTALLATION PROCEDURES SHALL BE IN COMPLIANCE WITH ANY AND ALL APPLICABLE EL PASO COUNTY STANDARDS.



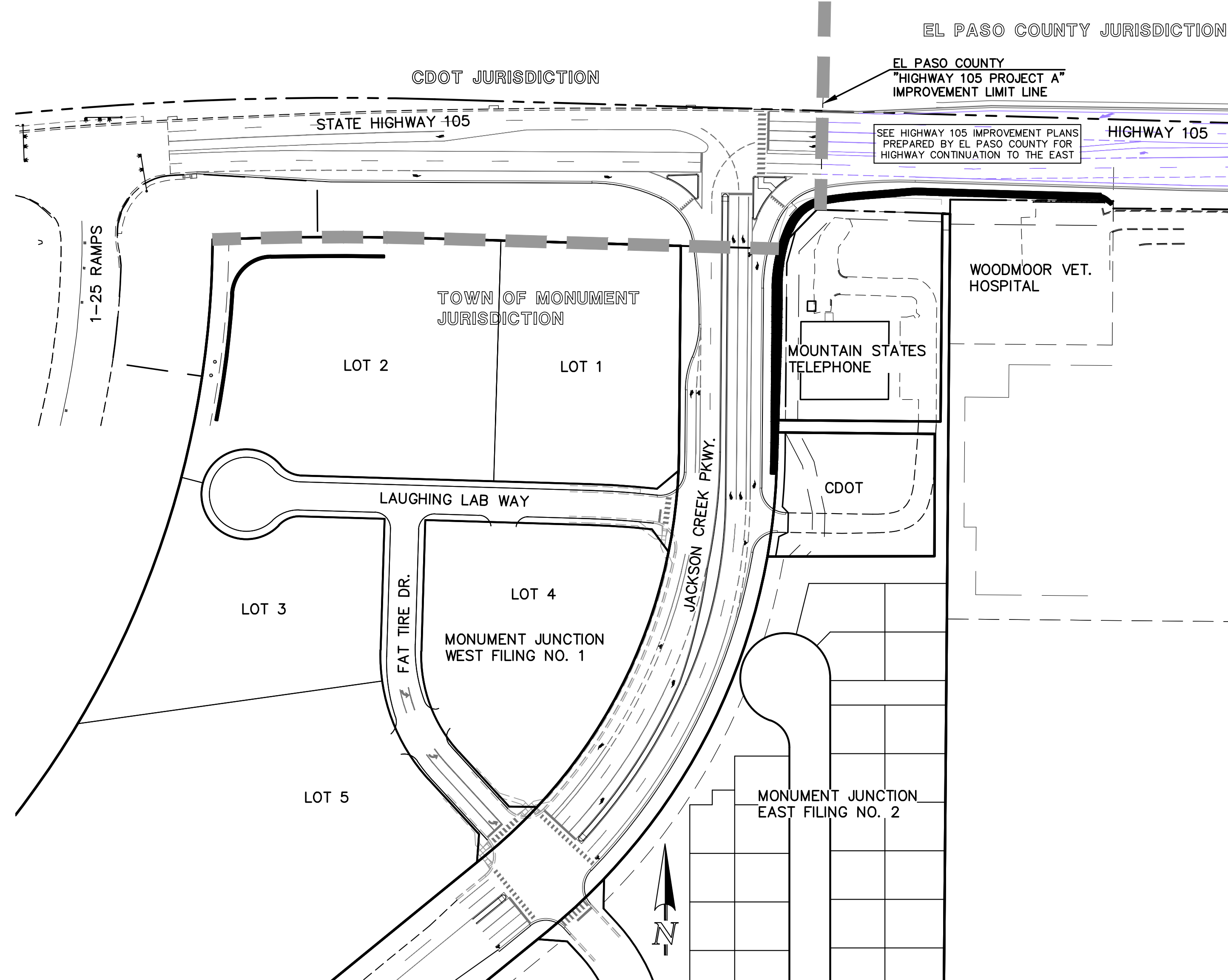
VICINITY MAP
N.T.S.

STANDARD NOTES FOR EL PASO COUNTY CONSTRUCTION PLANS:

- ALL DRAINAGE AND ROADWAY CONSTRUCTION SHALL MEET THE STANDARDS AND SPECIFICATIONS OF THE CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2, AND THE EL PASO COUNTY ENGINEERING CRITERIA MANUAL.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION AND FIELD NOTIFICATION OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, BEFORE BEGINNING CONSTRUCTION. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CALL 811 TO CONTACT THE UTILITY NOTIFICATION CENTER OF COLORADO (UNCC).
- CONTRACTOR SHALL KEEP A COPY OF THESE APPROVED PLANS, THE GRADING AND EROSION CONTROL PLAN, THE STORMWATER MANAGEMENT PLAN (SWMP), THE SOILS AND GEOTECHNICAL REPORT, AND THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES, INCLUDING THE FOLLOWING:
 - EL PASO COUNTY ENGINEERING CRITERIA MANUAL (ECM)
 - CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2
 - COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
 - CDOT M & S STANDARDS
- NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS FROM REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING. ANY MODIFICATIONS NECESSARY TO MEET CRITERIA AFTER-THE-FACT WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- IT IS THE DESIGN ENGINEER'S RESPONSIBILITY TO ACCURATELY SHOW EXISTING CONDITIONS, BOTH ONSITE AND OFFSITE, ON THE CONSTRUCTION PLANS. ANY MODIFICATIONS NECESSARY DUE TO CONFLICTS, OMISSIONS, OR CHANGED CONDITIONS WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT - INSPECTIONS, PRIOR TO STARTING CONSTRUCTION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE REQUIREMENTS OF ALL JURISDICTIONAL AGENCIES AND TO OBTAIN ALL REQUIRED PERMITS, INCLUDING BUT NOT LIMITED TO EL PASO COUNTY EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQCP), REGIONAL BUILDING FLOODPLAIN DEVELOPMENT PERMIT, U.S. ARMY CORPS OF ENGINEERS-ISSUED 401 AND/OR 404 PERMITS, AND COUNTY AND STATE FUGITIVE DUST PERMITS.
- CONTRACTOR SHALL NOT DEVIATE FROM THE PLANS WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE DESIGN ENGINEER AND PCD. CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY UPON DISCOVERY OF ANY ERRORS OR INCONSISTENCIES.
- ALL STORM DRAIN PIPE SHALL BE CLASS III RCP OR CLASS IV WITH WATER TIGHT JOINTS WHERE CALLED OUT, UNLESS OTHERWISE NOTED AND APPROVED BY PCD.
- CONTRACTOR SHALL COORDINATE GEOTECHNICAL TESTING PER ECM STANDARDS. PAVEMENT DESIGN SHALL BE APPROVED BY EL PASO COUNTY PCD PRIOR TO PLACEMENT OF CURB AND GUTTER AND PAVEMENT.
- ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE AT APPROVED CONSTRUCTION ACCESS POINTS.
- SIGHT VISIBILITY TRIANGLES AS IDENTIFIED IN THE PLANS SHALL BE PROVIDED AT ALL INTERSECTIONS. OBSTRUCTIONS GREATER THAN 18 INCHES ABOVE FLOWLINE ARE NOT ALLOWED WITHIN SIGHT TRIANGLES.
- SIGNING AND STRIPING SHALL COMPLY WITH EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS AND MUTCD CRITERIA.
- CONTRACTOR SHALL OBTAIN ANY PERMITS REQUIRED BY EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS, INCLUDING WORK WITHIN THE RIGHT-OF-WAY AND SPECIAL TRANSPORT PERMITS.
- THE LIMITS OF CONSTRUCTION SHALL REMAIN WITHIN THE PROPERTY LINE UNLESS OTHERWISE NOTED. THE OWNER/DEVELOPER SHALL OBTAIN WRITTEN PERMISSION AND EASEMENTS, WHERE REQUIRED, FROM ADJOINING PROPERTY OWNER(S) PRIOR TO ANY OFF-SITE DISTURBANCE, GRADING, OR CONSTRUCTION.

EL PASO COUNTY SIGNING AND STRIPING NOTES:

- ALL SIGNS AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY A METHOD THAT DOES NOT MATERIALLY DAMAGE THE PAVEMENT. THE PAVEMENT MARKINGS SHALL BE REMOVED TO THE EXTENT THAT THEY WILL NOT BE VISIBLE UNDER DAY OR NIGHT CONDITIONS. AT NO TIME WILL IT BE ACCEPTABLE TO PAINT OVER EXISTING PAVEMENT MARKINGS.
- ANY DEVIATION FROM THE STRIPING AND SIGNING PLAN SHALL BE APPROVED BY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT.
- ALL SIGNS SHOWN ON THE SIGNING AND STRIPING PLAN SHALL BE NEW SIGNS. EXISTING SIGNS MAY REMAIN OR BE REUSED IF THEY MEET CURRENT EL PASO COUNTY AND MUTCD STANDARDS.
- STREET NAME AND REGULATORY STOP SIGNS SHALL BE ON THE SAME POST AT INTERSECTIONS.
- ALL REMOVED SIGNS SHALL BE DISPOSED OF IN A PROPER MANNER BY THE CONTRACTOR.
- ALL STREET NAME SIGNS SHALL HAVE "D" SERIES LETTERS, WITH LOCAL ROADWAY SIGNS BEING 4" UPPER-LOWER CASE LETTERING ON 8" BLANK AND NON-LOCAL ROADWAY SIGNS BEING 6" LETTERING, UPPER-LOWER CASE ON 12" BLANK, WITH A WHITE BORDER THAT IS NOT RECESSED. MULTI-LANE ROADWAYS WITH SPEED LIMITS OF 40 MPH OR HIGHER SHALL HAVE 8" UPPER-LOWER CASE LETTERING ON 18" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. THE WIDTH OF THE NON-RECESSED WHITE BORDERS SHALL MATCH PAGE 255 OF THE 2012 MUTCD "STANDARD HIGHWAY SIGNS."
- ALL TRAFFIC SIGNS SHALL HAVE A MINIMUM HIGH INTENSITY PRISMATIC GRADE SHEETING.
- ALL LOCAL RESIDENTIAL STREET SIGNS SHALL BE MOUNTED ON A 1.75" X 1.75" SQUARE TUBE SIGN POST AND STUB POST BASE. FOR OTHER APPLICATIONS, REFER TO THE CDOT STANDARD S-614-8 REGARDING USE OF THE P2 TUBULAR STEEL POST SUBBASE DESIGN.
- ALL SIGNS SHALL BE SINGLE SHEET ALUMINUM WITH 0.100" MINIMUM THICKNESS.
- ALL LIMIT LINES/STOP LINES, CROSSWALK LINES, PAVEMENT LEGENDS, AND ARROWS SHALL BE A MINIMUM 125 MIL THICKNESS PREFORMED THERMOPLASTIC PAVEMENT MARKINGS WITH TAPERED LEADING EDGES PER CDOT STANDARD S-627-1. WORD AND SYMBOL MARKINGS SHALL BE THE NARROW TYPE. STOP BARS SHALL BE 24" IN WIDTH. CROSSWALKS LINES SHALL BE 24" WIDE AND 9' LONG.
- ALL LONGITUDINAL LINES SHALL BE A MINIMUM 15MIL THICKNESS EPOXY PAINT. ALL NON-LOCAL RESIDENTIAL ROADWAYS SHALL INCLUDE BOTH RIGHT AND LEFT EDGE LINE STRIPING AND ANY ADDITIONAL STRIPING AS REQUIRED BY CDOT S-627-1.
- THE CONTRACTOR SHALL NOTIFY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (719) 520-6819 PRIOR TO AND UPON COMPLETION OF SIGNING AND STRIPING.
- THE CONTRACTOR SHALL OBTAIN A WORK IN THE RIGHT OF WAY PERMIT FROM THE EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS (DPW) PRIOR TO ANY SIGNAGE OR STRIPING WORK WITHIN AN EXISTING EL PASO COUNTY ROADWAY.



KEY MAP
1" = 100'

EDARP PROJECT NUMBER: CDR237

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|--|------------------------------------|---------|---|
| 48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS 811 UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. | NO. REVISION | DATE | REVIEW: |
| | 1 REVISED PER CDOT/COUNTY COMMENTS | 5/11/23 | PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC |

MARC A. WHORTON, COLORADO P.E. #37155 DATE

CLASSIC CONSULTING

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|--|-------------|---------------|---------------|
| STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS | | | |
| ROADWAY IMPROVEMENT PLANS | | | |
| TITLE SHEET | | | |
| DESIGNED BY | PRA | SCALE | DATE 02-23-23 |
| DRAWN BY | PRA | (H) 1"=VARIES | SHEET 1 OF 26 |
| CHECKED BY | (V) 1"= N/A | JOB NO. | 1302.22 |

- AGENCIES**
- DEVELOPER: ELITE PROPERTIES OF AMERICA, INC.
2138 FLYING HORSE CLUB DR
COLORADO SPRINGS, CO 80921
MR. STEVE SCHLOSSER, (719) 592-9333
- CIVIL ENGINEER: CLASSIC CONSULTING
619 N. CASCADE AVENUE, SUITE 200
COLORADO SPRINGS, COLORADO 80903
MR. MARC A. WHORTON, P.E. (719) 785-2802
- COLORADO DEPARTMENT OF TRANSPORTATION, PERMIT REVIEW: REGION 2 - ACCESS
5615 WILLS BOULEVARD
PUEBLO, COLORADO 81008
MS. MICHELLE REGALDO (719) 546-5416
- COLORADO DEPARTMENT OF TRANSPORTATION, NORTH PROGRAM: REGION 2 - NORTH PROGRAM
1480 QUAIL LAKE LOOP ROAD, SUITE A
COLORADO SPRINGS, COLORADO 80906
MR. ANDY STECKLEIN, P.E. (719) 227-3264
- COLORADO DEPARTMENT OF TRANSPORTATION, ENVIRONMENTAL / ROW: REGION 2 - ACCESS
5615 WILLS BOULEVARD
PUEBLO, COLORADO 81008
MR. GABRIEL COSYLEON (719) 562-5528 (ENV. MANAGER)
MR. AMBER BILLINGS (719) 227-5725 (ROW MANAGER)
- COUNTY ENGINEERING: EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT
2880 INTERNATIONAL CIRCLE, SUITE 110
COLORADO SPRINGS, COLORADO 80910
MR. GILBERT LAFORCE (719) 520-7945
- GAS COMPANY: BLACKHILLS ENERGY
37 WIDEFIELD BOULEVARD
WIDEFIELD, COLORADO 80911
MR. GEORGE M. PETERSON, (719) 392-3491
- ELECTRIC COMPANY: MOUNTAIN VIEW ELECTRIC
P.O. BOX 1600
LIMON, COLORADO 80828
MR. LES ULFERS, (719) 495-2283
- TELEPHONE COMPANY: U.S. WEST COMMUNICATIONS (LOCATORS) (800) 922-1987

APPROVALS:

DESIGN ENGINEER'S STATEMENT:
THESE DETAILED PLANS AND SPECIFICATIONS WERE PREPARED UNDER MY DIRECTION AND SUPERVISION. SAID PLANS AND SPECIFICATIONS HAVE BEEN PREPARED ACCORDING TO THE CRITERIA ESTABLISHED BY THE COUNTY FOR DETAILED ROADWAY, DRAINAGE, GRADING AND EROSION CONTROL PLANS AND SPECIFICATIONS, AND SAID PLANS AND SPECIFICATIONS ARE IN CONFORMITY WITH APPLICABLE MASTER DRAINAGE PLANS AND MASTER TRANSPORTATION PLANS. SAID PLANS AND SPECIFICATIONS MEET THE PURPOSES FOR WHICH THE PARTICULAR ROADWAY AND DRAINAGE FACILITIES ARE DESIGNED AND ARE CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. I ACCEPT RESPONSIBILITY FOR ANY LIABILITY DIRECTLY CAUSED BY THE NEGLIGENCE, ACTS, ERRORS, OR OMISSIONS ON MY PART IN PREPARATION OF THESE DETAILED PLANS AND SPECIFICATIONS.

MARC A WHORTON, COLORADO P.E. #37155 DATE
FOR AND ON THE BEHALF OF CLASSIC CONSULTING ENGINEERS & SURVEYORS

OWNER/DEVELOPER'S STATEMENT:
I, THE OWNER/DEVELOPER HAVE READ AND WILL COMPLY WITH THE REQUIREMENTS OF THE GRADING AND EROSION CONTROL PLAN AND ALL OF THE REQUIREMENTS SPECIFIED IN THESE DETAILED PLANS AND SPECIFICATIONS.

STEVE SCHLOSSER DATE

TOWN OF MONUMENT APPROVAL:
THESE PLANS HAVE BEEN REVIEWED BY TOWN OF MONUMENT STAFF AND FOUND TO BE IN GENERAL COMPLIANCE WITH TOWN STANDARDS. IT IS THE RESPONSIBILITY OF THE SITE ENGINEER AND GENERAL CONTRACTOR TO ENSURE COMPLIANCE WITH THESE PLANS AND IN CONFORMANCE WITH THE TOWN OF MONUMENT ORDINANCES AND REGULATIONS. THE SPECIFICATIONS, STANDARDS, AND INTENT OF THE TOWN OF MONUMENT ORDINANCES, REGULATIONS AND THE CITY OF COLORADO SPRINGS DRAINAGE CRITERIA MANUALS VOLUMES 1 AND 2 SHALL PREVAIL IN ANY INSTANCES WHERE THESE PLANS DIFFER FROM THOSE REQUIREMENTS. THE DEVELOPMENT SERVICES DEPARTMENT SHALL BE NOTIFIED IF ANY CHANGES NEED TO BE MADE.

THIS REVIEW IS ONLY FOR THE PROPOSED CONSTRUCTION ON JACKSON CREEK PARKWAY.

SIGNED TOWN OF MONUMENT DATE

EL PASO COUNTY:
COUNTY PLAN REVIEW IS PROVIDED ONLY FOR GENERAL CONFORMANCE WITH COUNTY DESIGN CRITERIA. THE COUNTY IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND/OR ELEVATIONS WHICH SHALL BE CONFIRMED AT THE JOB SITE. THE COUNTY THROUGH THE APPROVAL OF THIS DOCUMENT ASSUMES NO RESPONSIBILITY FOR COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT.

FILED IN ACCORDANCE WITH THE REQUIREMENTS OF THE EL PASO COUNTY LAND DEVELOPMENT CODE, DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2 AND ENGINEERING CRITERIA MANUAL AS AMENDED.

IN ACCORDANCE WITH WITH ECM SECTION 1.12, THESE CONSTRUCTION DOCUMENTS WILL BE VALID FOR CONSTRUCTION FOR A PERIOD OF 2 YEARS FROM THE DATE SIGNED BY THE EL PASO COUNTY ENGINEER. IF CONSTRUCTION HAS NOT STARTED WITHIN THOSE 2 YEARS, THE PLANS WILL NEED TO BE RESUBMITTED FOR APPROVAL, INCLUDING PAYMENT OF REVIEW FEES AT THE PLANNING AND COMMUNITY DEVELOPMENT DIRECTOR'S DISCRETION.

THIS REVIEW IS ONLY FOR THE PROPOSED CONSTRUCTION IN HIGHWAY 105, EAST OF JACKSON CREEK PARKWAY.

JOSHUA PALMER, P.E. DATE
COUNTY ENGINEER / ECM ADMINISTRATOR

N:\130222\UPRAWINGS\CONSTRUCTION\DRAWINGS\01-SHEET-01-TITLE-SHEET.dwg, 7/14/2023, 11:13:19 AM, eraprop, 1:1

CDOT GENERAL NOTES

- ALL CONSTRUCTION MATERIALS, TECHNIQUES, AND PROCEDURES WITHIN THE HIGHWAY LIMITS SHALL BE IN CONFORMANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION AS SET FORTH IN THE LATEST "M & S" STANDARDS MANUAL.
- A FULLY EXECUTED COMPLETE COPY OF THE ACCESS PERMITS AND A VALID NOTICE TO PROCEED TO CONSTRUCTION MUST BE ON THE JOB SITE WITH THE CONTRACTOR AT ALL TIMES DURING CONSTRUCTION.
- ANY ADDITIONAL PERMITS AND CLEARANCES REQUIRED BY OTHER FEDERAL, STATE, AND LOCAL GOVERNMENT AGENCIES IS THE RESPONSIBILITY OF THE OWNER/DEVELOPER.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE EXISTENCE AND LOCATION OF ALL UNDERGROUND UTILITIES ALONG THE ROUTE OF HIGHWAY CONSTRUCTION. THE OMISSION FROM OR THE INCLUSION OF UTILITY LOCATIONS ON THE PLANS IS NOT TO BE CONSIDERED AS THE NONEXISTENCE OF OR A DEFINITE LOCATION OF EXISTING UNDERGROUND UTILITIES.
- THE CONTRACTOR WILL TAKE THE NECESSARY PRECAUTIONS TO PROTECT EXISTING UTILITIES FROM DAMAGE DUE TO THIS OPERATION. ANY DAMAGE TO THE UTILITIES WILL BE REPAIRED AT THE CONTRACTOR'S EXPENSE, AND ANY SERVICE DISRUPTION WILL BE SETTLED BY THE CONTRACTOR.
- ALL STATIONING IS CENTERLINE UNLESS OTHERWISE INDICATED. ALL ELEVATIONS ARE CENTERLINE UNLESS OTHERWISE INDICATED.
- FIVE WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, THE OWNER/CONTRACTOR MUST CONTACT MR. RON YOUNG (719) 289-8718, CDOT ACCESS/TRAFFIC MANAGER, TO COORDINATE CONSTRUCTION.
- WORK SHALL BEGIN AFTER 8:30 AM AND ALL EQUIPMENT SHALL BE OUT OF THE RIGHT-OF-WAY BEFORE 3:00 PM DURING SCHOOL HOURS/DAYS. NO WORK IS ALLOWED WITHIN THE HIGHWAY RIGHT-OF-WAY ON WEEKENDS OR STATE/FEDERAL HOLIDAYS. NO CONSTRUCTION VEHICLES SHALL BE PARKED, OR CONSTRUCTION MATERIALS STOCKPILED IN THE HIGHWAY RIGHT-OF-WAY OVERNIGHT. NO PRIVATE VEHICLES MAY BE PARKED IN THE HIGHWAY RIGHT-OF-WAY AT ANY TIME DURING CONSTRUCTION.
- TWO WAY TRAFFIC SHALL BE MAINTAINED THROUGHOUT THE WORK AREA AT ALL TIMES.
- ALL SIGNS SHALL BE MANUFACTURED IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.). THE SHEETING FOR THE SIGNS BE HIGHWAY INTENSITY SHEETING (ASTM TYPE IV RETRO REFLECTIVE SHEETING).
- ALL DISTURBED PAVEMENT EDGES SHALL BE CUT TO NEAT LINES 1' FROM EXISTING EDGE OF PAVEMENT. NEW SURFACING FOR THE ACCESS AND THE HIGHWAY WIDENING SHALL BE A FULL DEPTH TOTAL OF 7 INCHES OF COMPACTED HOT MIX ASPHALT (HMA), GRAD-S(100), PG. 58-28 OR PG. 64-22 (WITH 1% LIME) PLACED IN 3 LIFTS, OVER 12 INCHES OF CEMENT TREATED SUBGRADE, OVER COMPACTED RANDOM FILL SUBGRADE WITH A MINIMUM R-VALUE OF 17 CONFIRMED WITH R2 MATERIALS.
- PLACEMENT AND COMPACTION OF HOT MIX ASPHALT (HMA) SHALL COMPLY WITH SECTION 401 OF THE 2005 CDOT STANDARD SPECIFICATIONS AS WELL AS THE LATEST CDOT STANDARD SPECIAL PROVISIONS.
- PLACEMENT AND COMPACTION OF SUB-GRADE, EMBANKMENTS, AND BACKFILLS SHALL COMPLY WITH SECTION 203 OF THE CDOT STANDARD SPECIFICATIONS AS WELL AS THE LATEST CDOT STANDARD SPECIAL PROVISIONS.
- SOIL PREPARATION INCLUDING TOPSOIL, SEEDING, AND MULCHING IS REQUIRED WITHIN THE HIGHWAY RIGHT-OF-WAY ON ALL DISTURBED AREAS NOT SURFACED AND THOSE AREAS BEYOND THE HIGHWAY THAT MAY ERODE AND SEND DEBRIS INTO THE HIGHWAY RIGHT-OF-WAY.
- ADDITIONAL EROSION CONTROL STRUCTURES MAY BE REQUIRED AT THE TIME OF CONSTRUCTION.
- WIDENED AREAS OF THE ROADWAY SHALL BE A MINIMUM OF 20-YEAR DESIGN LIFE.
- OVERLAY OF EXISTING PAVEMENT SHALL ALSO BE A MINIMUM OF 20-YEAR DESIGN LIFE TO AVOID DIFFERING MAINTENANCE ISSUES BETWEEN THE OVERLAY SECTION AND THE NEW PAVEMENT WIDENING.
- WATER SHALL BE USED AS A DUST PALLIATIVE WHERE REQUIRED. COST OF WATER SHALL BE INCLUDED IN THE WORK.
- THIS DESIGN IS IN FULL COMPLIANCE WITH SECTION 4 OF THE STATE HIGHWAY ACCESS CODE, 2 COR 601-1 EXCEPT FOR THE FOLLOWING APPROVED VARIANCES: NONE
- THIS DESIGN IS IN FULL COMPLIANCE WITH TITLE II ADA ACCESSIBILITY REQUIREMENTS EXCEPT FOR THE FOLLOWING APPROVED VARIANCES: NONE

COLORADO DEPARTMENT OF TRANSPORTATION:

CDOT PLAN REVIEW IS PROVIDED ONLY FOR GENERAL CONFORMANCE WITH THE CDOT DESIGN CRITERIA. CDOT IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND/OR ELEVATIONS WHICH SHALL BE CONFIRMED AT THE JOB SITE. CDOT THROUGH THE APPROVAL OF THIS DOCUMENT ASSUMES NO RESPONSIBILITY FOR COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT.

EL PASO COUNTY TRAFFIC SIGNAL NOTES

- THE CONTRACTOR SHALL PROVIDE, FOR REVIEW, BY THE ENGINEER, A COMPLETE TRAFFIC SIGNAL MATERIAL SUBMITTAL PACKAGE THAT CONTAINS, ALL OF THE PROPOSED TRAFFIC SIGNAL EQUIPMENT, INCLUDING MATERIAL SPECIFICATIONS AND DESCRIPTIONS THAT WILL BE NECESSARY TO COMPLETE THE TRAFFIC SIGNAL WORK. THE CONTRACTOR SHALL ALLOW FOR A MINIMUM THREE-WEEK SUBMITTAL REVIEW PERIOD AND SHALL NOT ORDER ANY SIGNAL EQUIPMENT UNTIL AFTER A REVIEW OF ALL SUBMITTALS HAVE BEEN COMPLETED BY THE ENGINEER AND VERIFIED BY THE CONTRACTOR.
- FUNCTIONAL AND OPERATIONAL RESPONSIBILITY FOR ALL NEWLY INSTALLED AND EXISTING TRAFFIC SIGNAL EQUIPMENT WILL BECOME THE RESPONSIBILITY OF THE CONTRACTOR UNTIL FINAL ACCEPTANCE OF THE PROJECT. THE CONTRACTOR SHALL CONSIDER THIS WORK INCIDENTAL TO THE OVERALL WORK BEING PERFORMED AND SHALL BE INCLUDED AS PART OF THE PROJECT.
- SEE COLORADO DEPARTMENT OF TRANSPORTATION SIGNAL DETAILS FOR CONSTRUCTION/ INSTALLATION DETAILS NOT SHOWN ON THESE PLANS.
- ALL SIGNAL EQUIPMENT REMOVED BY THE CONTRACTOR SHALL BE SALVAGED AND BECOME THE PROPERTY OF EL PASO COUNTY. THE SALVAGED EQUIPMENT SHALL BE DELIVERED AS DIRECTED BY THE ENGINEER. DELIVERY OF THE SIGNAL EQUIPMENT WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE WORK FOR REMOVAL OF TRAFFIC SIGNAL EQUIPMENT.
- OVERHEAD STREET NAME SIGN DESIGN AND LAYOUT INFORMATION SHALL BE PER THE STREET NAME SIGN DETAIL CONTAINED IN THE PROJECT PLAN.
- TRAFFIC SIGNS MOUNTED ON SIGNAL POLES, MAST ARMS, AND PEDESTALS SHALL BE MOUNTED USING BANDING, ALUMINUM CHANNELS, AND BACKING ZEES PER APPLICABLE CDOT STANDARD PLANS, OR SIMILAR RIGID SIGN BRACING MOUNTING ASSEMBLY.
- ONCE THE PROFESSIONAL ENGINEERING CONSULTANT HAS COMPLETED ALL TRAFFIC SIGNAL CONTROLLER TIMING DEVELOPMENT AND CONTROLLER PROGRAMMING, THE CONTRACTOR WILL COORDINATE THE DELIVERY DATE OF THE PROGRAMMED TRAFFIC SIGNAL CONTROLLER FOR REVIEW OF EPC DEPARTMENT OF PUBLIC WORKS, HIGHWAY DIVISION SIGNAL SHOP AND ALLOW FOR A MINIMUM TWO-WEEK REVIEW PERIOD, AFTER WHICH TIME THE CONTRACTOR MAY MAKE ARRANGEMENTS FOR PICKING UP THE SIGNAL CONTROLLER.
- CONTROLLER CABINET SHALL BE FURNISHED WITH A "BEST" DOOR LOCK KIT LOCK AND CODE IS "BEST"; 5/6R LEFT AND RIGHT.
- CONDUIT IS TO BE REPLACED IN THE EVENT THAT EXISTING CONDUIT IS DAMAGED AND AS DIRECTED BY THE ENGINEER.
- ELECTRICAL SERVICE DISCONNECT BOXES SHALL BE LOCKABLE AND WEATHER PROOF WITH NEMA TYPE CIRCUIT BREAKER. ENCLOSURES SHALL BE PROVIDED AT THE CONNECTION POINT OF EACH POWER SOURCE OR POINT OF SERVICE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL FURNISH AND INSTALL ALL WIRING REQUIRED TO COMPLETE THE INSTALLATION AND ESTABLISH THE FUNCTIONALITY OF ALL TRAFFIC SIGNAL EQUIPMENT.
- ALL INCIDENTAL ITEMS NOT SHOWN IN THE SUMMARY OF APPROXIMATE QUANTITIES OR TABULATION OF SIGNAL EQUIPMENT SHALL BE CONSIDERED TO BE INCLUDED AS PART OF THE TRAFFIC SIGNAL INSTALLATION AND WILL NOT BE MEASURED AND PAID FOR SEPARATELY. ALL QUANTITIES ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL WORK NECESSARY TO COMPLETE THE CONSTRUCTION SHOWN ON THESE PLANS.
- THE SIGNAL SHALL NOT BE TURNED ON OR STARTED UNTIL DIRECTED BY THE ENGINEER. PRIOR TO SIGNAL ACTIVATION, THE ENGINEER SHALL CONFIRM THAT THE APPROPRIATE PAVEMENT MARKINGS AND SIGNING ARE IN PLACE AND THAT ALL WORK NECESSARY FOR PROPER SIGNAL OPERATION HAS BEEN COMPLETED.
- THE SIGNAL CONTROLLER SHALL BE A MACCRAIN 2070 FLEX ATC CONTROLLER AND THE CONFLICT MONITOR SHALL BE MODEL 2010 ECU W/ ETHERNET PORT (EDI). THE CONTROLLER CABINET SHALL BE A COOT SPECIFICATION MODEL 332D WITH BATTERY BACKUP AND AUX RACK AND SHALL CONTAIN ANTI-GRAFFITI SILVER FINISH COATING. THE CABINET SHALL BE MOUNTED ON A CAST-IN-PLACE CONCRETE FOUNDATION PER APPLICABLE COOT'S STANDARDS STANDARD PLAN AND THE CABINET SHALL BE POSITIONED SUCH THAT, WITH THE FRONT DOOR OPEN, BOTH THE CONTROLLER DISPLAY AND THE SIGNAL INSTALLATION BE VISIBLE.
- LUMINAIRES SHALL CONSIST OF AN ASSEMBLY THAT UTILIZES LEDS AS THE LIGHT SOURCE. IN ADDITION, A COMPLETE LUMINAIRE SHALL CONSIST OF A HOUSING, LED ARRAY, AND ELECTRONIC DRIVER (POWER SUPPLY). ALL LUMINAIRES SHALL BE WIRED 120 VOLTS AC WITH MULTI-TAP HEADS. THE LED FIXTURE MUST HAVE A COLOR TEMPERATURE OF 4100K (+/- 500K), MUST BE DESIGNED TO OPERATE AT A TEMPERATURE RANGE OF -40°F TO 105°F (-40°C TO 40°C), AND PROVIDE A MINIMUM OF 70,000 HOURS OF OPERATION. LUMINAIRES SHALL BE E-LIGHT-STAR LED STREET LIGHT, OR APPROVED EQUIVALENT. THE CONTRACTOR SHALL PROVIDE A RECOMMENDATION FOR TYPE OF THE STREET LIGHT BASED ON THE CONSTRUCTION PLANS AND MANUFACTURER'S SPECIFICATIONS, TO BE APPROVED BY THE ENGINEER. THE FIXTURE MUST BE CAST ALUMINUM, PROVIDED WITH FUSING, SURGE SUPPRESSION AND MUST BE UL LISTED FOR WET LOCATIONS. THE FIXTURE MUST HAVE AN INTERNAL WEATHER-TIGHT LED DRIVE. NO ACTIVE COOLING FEATURES (FANS, ETC.) WILL BE ALLOWED. THE FINISHED SHALL MATCH THE EXTENSION ARM SHAFTS. THE LUMINAIRES WILL BE INSTALLED ON 15 FOOT EXTENSION ARM SHAFTS AT NOMINAL HEIGHT OF 40 FEET AND SHALL BE WELDED TO THE SIGNAL POLE PER CDOT TYPICAL TRAFFIC SIGNAL INSTALLATION DETAILS STANDARD PLAN S-614-40. LUMINAIRE ARM SHAFT SHALL BE IN ACCORDANCE WITH THE PROJECT PLANS.
- THE INTERSECTION DETECTION SYSTEM (MICROWAVE RADAR) CONTRACT ITEM INCLUDES DEVICE INSTALLATION (I.E., DETECTOR UNIT, HARDWARE, WIRING, PROCESSOR MODULE, ETC.), AND VERIFICATION OF SUCCESSFUL IN-FIELD DETECTION ZONE OPERATION BASED ON SEVERAL VEHICLE ACTIONS IN ALL DETECTION ZONES.
- THE CONTRACTOR SHALL COORDINATE THE SCHEDULES OF THE CONTRACTED PROFESSIONAL ENGINEERING CONSULTANT AND THE EPC DEPARTMENT OF PUBLIC WORKS, HIGHWAY DIVISION TRAFFIC SIGNAL STAFF FOR SCHEDULING THE ON-SITE FIELD IMPLEMENTATION OF ALL TRAFFIC SIGNAL TIMING AND OPERATIONAL PROGRAMMING, VEHICLE DETECTION ZONE PLACEMENT, AND DETECTION EQUIPMENT POSITIONING. THIS WORK SHALL BE SCHEDULED NEAR THE END OF THE PROJECT, PRIOR TO PROJECT ACCEPTANCE, AND ONLY AFTER ALL FINAL PAVEMENT MARKINGS, SIGNING, AND TRAFFIC SIGNAL WORK HAS BEEN COMPLETED.
- THE MICROWAVE RADAR DETECTION SYSTEM SHALL UTILIZE MS SEDCO INTERSECTOR TC-K1-SBE WITH INTERFACE BOARD.
- PEDESTRIAN SIGNAL HEAD INSTALLATION SHALL INCLUDE ALUMINUM AND POWDER COATED GLOSS BLACK SIGNAL HEAD WITH APPROVED LED COUNTDOWN DISPLAY, ALUMINUM OPEN VISOR WITH THE OUTSIDE POWDER COATED GLOSS BLACK, PUSHBUTTON, AND INSTRUCTIONAL R10-3E COUNTDOWN PEDESTRIAN ACTUATION SIGN. PUSHBUTTONS SHALL BE POLERA NAVIGATOR S 2-WIRE PUSH BUTTONS, SPECIFIED AS INS2 5 UN 1-B-BD-ES, OR APPROVED EQUIVALENT. CUSTOM MESSAGING SHALL NOT INTERFERE WITH TRADITIONAL NON-VISUAL FORMATS SPECIFIED IN 4E OF THE MUTCD AND SHALL ONLY PROVIDE ADDITIONAL INFORMATION AS DIRECTED BY THE ENGINEER.
- ALL TRAFFIC SIGNAL POLES, MAST ARMS, PEDESTALS, AND LUMINAIRE ARMS SHALL HAVE A GLOSS BLACK COAT FINISH OVER HOT DIP GALVANIZED BASE COAT, INSTALLED IN ACCORDANCE WITH THE PAINT MANUFACTURER'S INSTRUCTIONS.
- ALL SIGNAL POLE AND CONTROLLER LOCATIONS SHOWN ARE APPROXIMATE ONLY. MAST ARMS SHALL BE OF SUFFICIENT LENGTH AND DESIGN TO ALLOW PROPER PLACEMENT OF SIGNAL HEADS AND OVERHEAD SIGNING PER THE PLANS. ACTUAL LOCATIONS SHALL BE STAKED IN THE FIELD AND FIELD VERIFIED BY THE ENGINEER PRIOR TO DRILLING, EXCAVATION, AND ORDERING THE SIGNAL EQUIPMENT AND MAST ARMS. THE LOCATION OF EACH SIGNAL POLE FOUNDATION SHALL BE POTHOLED PRIOR TO DRILLING TO CONFIRM WHETHER OR NOT ANY UTILITY CONFLICTS EXIST.
- LATERAL OFFSETS FROM THE NEAR EDGE OF TRAFFIC SIGNAL POLES, PEDESTALS, AND CABINETS TO THE FACE OF CURB OR EDGE OF PAVED SHOULDER SHOULD BE AT LEAST SIX FEET, HOWEVER, A MINIMUM LATERAL OFFSET OF AT LEAST FOUR FEET MAY BE PROVIDED FOR CURB OFFSETS. IF NO PAVED SHOULDER EXISTS, A MINIMUM LATERAL OFFSET OF AT LEAST EIGHT FEET SHOULD BE PROVIDED FROM THE EDGE OF PAVEMENT FOR AN AUXILIARY LANE AND A MINIMUM LATERAL OFFSET OF AT LEAST TWELVE FEET SHOULD BE PROVIDED FROM THE EDGE OF PAVEMENT FOR A THROUGH LANE.
- SHOULD THE CONTRACTOR ENCOUNTER WATER IN THE CAISSON, ANY DE-WATERING METHODS AND NECESSARY PERMITS SHALL BE INCLUDED IN THE COST OF THE CAISSON AND WILL BE CONSIDERED INCIDENTAL TO THE WORK.
- ALL TRAFFIC SIGNAL COMPONENT PULL BOXES SHALL BE PRE-CAST HIGH DENSITY POLYMER CONCRETE (HPCC) MATERIAL WITH THE FOLLOWING SIZES: 36 INCH X 48 INCH X 18 INCH FOR THE PULL BOX ADJACENT TO THE CONTROLLER CABINET FOUNDATION AND 24 INCH X 36 INCH X 18 INCH FOR THE REMAINING PULL BOXES.
- TRAFFIC PULL BOX LOCATIONS SHOWN IN THE PROJECT PLANS ARE APPROXIMATE. ACTUAL LOCATIONS SHALL BE VERIFIED IN THE FIELD BY THE ENGINEER. PULL BOXES SHALL BE FLUSH WITH THE FINISHED GROUND SURFACE AND SHALL NOT BE PLACED IN AREAS THAT ARE SUSCEPTIBLE TO WATER RUNOFF OR STANDING WATER. CONDUIT RUNS BETWEEN PULL BOXES SHALL NOT EXCEED APPROXIMATELY 200 FEET AND PULL BOXES SHALL NOT BE LOCATED IN HANDICAP RAMPS, PEDESTRIAN LANDING AREAS, SIDEWALKS, PRIMARY SIDEWALK PATHS, OR ROADWAY PAVEMENT AREAS.

EL PASO COUNTY TRAFFIC SIGNAL NOTES (CONT.)

- THE CONDUIT NUMBER AND SIZES FOR TYPICAL CONDUIT RUNS INCLUDE THE FOLLOWING FOR A PREEMINENT MAST ARM SIGNAL INSTALLATION.
 - BETWEEN THE SIGNAL POLE FOUNDATION AND ADJACENT SIGNAL POLE PULL BOX: TWO (2) 2 INCH AND ONE (1) 3 INCH.
 - BETWEEN SIGNAL PULL BOXES: TWO (2) 2 INCH AND THREE (3) 3 INCH.
 - BETWEEN THE CONTROLLER CABINET FOUNDATION AND ADJACENT PULL BOX: THREE (3) 2 INCH AND FOUR (4) 3 INCH.
 - BETWEEN THE SECONDARY SERVICE PEDESTAL METER FOUNDATION AND THE CONTROLLER CABINET FOUNDATION ONE (1) 2 INCH FOR THE ELECTRICAL SERVICE FEED.
- ALL CONDUIT AND FITTINGS SHALL BE SCHEDULE 80 PVC AND ALL CONDUIT SHALL HAVE A PULL ROPE LEFT IN THEM WHEN CONSTRUCTION IS COMPLETED. ALL CONDUIT ENTERING THE CABINET FOUNDATION AND PULL BOXES SHALL HAVE BELL END STYLE COUPLINGS ON ALL CONDUIT ENDS.
- ALL CONDUIT THAT IS DIRECTIONALLY BORED SHALL BE A MINIMUM OF THREE FEET BELOW THE EXISTING PAVEMENT. THIS WORK SHALL AVOID DISTURBING OR DAMAGING EXISTING FACILITIES AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROMPT RECONSTRUCTION, ALTERATION, REPAIR OR MAINTENANCE OF HIGHWAY PROPERTY, AS NECESSARY, TO REPAIR DAMAGE CAUSED BY THE ACCOMMODATION OF THE UTILITY, AND TO RESTORE THE HIGHWAY TO PRE-EXISTING OR BETTER CONDITIONS.
- ALL SIGNAL CABLE SHALL BE CONTINUOUS FROM CONNECTIONS MADE IN THE HANDHOLE COMPARTMENT OF THE SIGNAL POLE BASE TO THE TERMINAL COMPARTMENT OF THE CONTROLLER CABINET AND SHALL CONTAIN NO SPLICES. EACH SIGNAL HEAD SHALL CONTAIN SEPARATE AND CONTINUOUS SIGNAL CABLE FROM THE SIGNAL HEAD TO THE ABOVE GROUND HANDHOLE AT THE BASE OF THE SIGNAL POLE AND SHALL CONTAIN NO SPLICES.
- A SEPARATE AND CONTINUOUS 21 CONDUCTOR CABLE SHALL RUN FROM THE CONTROLLER CABINET TO THE HANDHOLE AT EACH SIGNAL POLE AND SHALL CONTAIN NO SPLICES.
- ALL SIGNS MOUNTED ON SIGNAL POLES, MAST ARMS, AND PEDESTALS SHALL BE MOUNTED USING BANDING ALUMINUM CHANNELS, AND BACKING ZEES PER CDOT TYPICAL POLE MOUNT SIGN INSTALLATIONS STANDARD PLAN S-614-20, OR SIMILAR RIGID SIGN BRACING MOUNTING ASSEMBLY, AS DIRECTED BY THE ENGINEER. MAST ARM SIGNS THAT REQUIRE Z-BRACKETS SHALL BE MOUNTED ON ASTRO-STYLE BRACKETS AND RISERS. THE COST OF ALL HARDWARE FITTINGS, TOOLS, AND EQUIPMENT NECESSARY FOR A COMPLETE INSTALLATION OF MAST ARM SIGNS WILL NOT BE MEASURED AND PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE WORK.
- ALL CONDUCTORS AND CABINET WIRING SHALL BE COLOR CODED AND PERMANENTLY TAGGED PER ENGINEER DIRECTION AND IN ACCORDANCE WITH THE SIGNAL PHASE NUMBERING AND DETECTION ZONE PHASE NUMBERING INFORMATION CONTAINED IN THE PROJECT PLANS.
- ALL VEHICLE SIGNAL HEADS SHALL HAVE APPROVED 12 INCH LED INDICATORS AND SHALL BE ALUMINUM WITH POWDER COATED GLOSS BLACK FINISH AND SHALL CONTAIN 12 INCH ALUMINUM TUNNEL VISORS WITH THE OUTSIDE POWDER COATED GLOSS BLACK. ALL VEHICLE SIGNAL HEADS SHALL HAVE ALUMINUM LOUVERED BACK PLATES WITH POWDER COATED GLOSS BLACK FINISH AND YELLOW RETRO REFLECTIVE BORDER. MAST ARM SIGNAL HEADS SHALL USE ASTRO-TYPE MOUNTING ASSEMBLIES AND SHALL BE INSTALLED APPROXIMATELY LEVEL WITH ONE ANOTHER AT A 17 TO 19 FOOT VERTICAL CLEARANCE ABOVE THE HIGH POINT OF THE PAVEMENT GRADE.
- FINAL VEHICLE DIRECTION ZONE PLACEMENT AND DIMENSIONS, IN ACCORDANCE WITH THE PROJECT PLAN, AND FINAL SIGNAL PROGRAMMING SHALL BE COMPLETED IN THE FIELD AND THE CONTRACTOR SHALL CONTACT THE ENGINEER FOR COORDINATING AND SCHEDULING THIS WORK.
- ALL VEHICLE AND PEDESTRIAN SIGNAL HEADS THAT HAVE NOT BEEN PLACED IN SERVICE SHALL BE COVERED WITH PREFABRICATED WEATHER RESISTANT NYLON FORM FITTING SIGNAL FACE COVER MATERIAL. THE SIGNAL FACE SHALL REMAIN COMPLETELY COVERED UNTIL THE SIGNAL HEAD IS PLACED IN SERVICE AND IS FULLY FUNCTIONAL AND OPERATIONAL.
- ALL DETECTION EQUIPMENT, DETECTION ZONES, AND SIGNAL TIMING OPERATION SHALL BE CONFIRMED IN THE FIELD BY THE PROFESSIONAL ENGINEERING CONSULTANT TO BE ACHIEVING SATISFACTORY TRAFFIC SIGNAL OPERATION.
- COMMUNICATION SYSTEM SHALL BE A DYMEC ETHERNET SWITCH KY-3170EMX AND CELLULAR MODEM MICROHARD BULLET LTE, WITH CITEL SURGE SUPPRESSION ON ALL THESE COMPONENTS.
- THE CONTRACTED PROFESSIONAL ENGINEERING TRAFFIC SIGNAL TIMING, CONTROLLER PROGRAMMING AND OPERATION AND OVERALL TRAFFIC SIGNAL OPERATIONAL CONSULTANT SERVICES THAT ARE TO BE RETAINED BY THE OWNER/ DEVELOPER/ DISTRICT SHALL INCLUDE, BUT NOT BE LIMITED TO:
 - DEVELOPING ALL TRAFFIC SIGNAL TIMING AND ALL OPERATIONAL PARAMETERS FOR ACHIEVING ISOLATED, FULL-ACTUATED VEHICLE AND PEDESTRIAN INTERSECTION OPERATION AND, WHEN DOCUMENTED TO BE NECESSARY, COORDINATED SIGNAL SYSTEM TIMING PLAN OPERATION DURING VARIOUS TIMES OF THE DAY.
 - PROGRAMMING ALL SIGNAL TIMING PARAMETERS INTO THE TRAFFIC SIGNAL CONTROLLER.
 - FIELD IMPLEMENTING AND FINE-TUNING/ ADJUSTING ALL TRAFFIC SIGNAL TIMING PARAMETERS, INCLUDING FOLLOW-UP FIELD REVIEWS AS MAY BE NECESSARY.
 - DEVELOPING, PROGRAMMING, FIELD IMPLEMENTING, AND FINE-TUNING ALL VEHICLE DETECTION ZONE DIMENSIONS, ZONE LOCATIONS, AND OPERATIONAL PARAMETERS.


ALL OF THE AFOREMENTIONED CONTRACTED PROFESSIONAL ENGINEERING TRAFFIC SIGNAL OPERATIONAL CONSULTANT SERVICED DELIVERABLES SHALL BE CONSISTENT WITH NATIONAL PUBLICATIONS, INCLUDING BUT NOT LIMITED TO: MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) LATEST EDITION/ REVISION, FHWA-HOP-08-024 "TRAFFIC SIGNAL TIMING MANUAL" (JUNE 2008), FHWA-HRT-04-091 "SIGNALIZED INTERSECTIONS: INFORMATION GUIDE" (AUGUST 2004), FHWA-HOP-06-006 "TRAFFIC CONTROL SYSTEMS HANDBOOK" (OCTOBER 2005), FHWA-DTH61-01-C-00183 "SIGNAL TIMING PROCESS FINAL REPORT" (DECEMBER 2003), NCHRP REPORT 731 "GUIDELINES FOR TIMING YELLOW AND ALL-RED INTERVALS AT SIGNALIZED INTERSECTIONS" (2012), NCHRP REPORT 812 "SIGNAL TIMING MANUAL SECOND EDITION" (2015)

TOWN OF MONUMENT GENERAL NOTES:

- ALL MATERIALS, WORKMANSHIP, AND CONSTRUCTION OF SITE IMPROVEMENTS SHALL MEET OR EXCEED THE SITE WORK STANDARDS AND SPECIFICATIONS AND THE STANDARDS AND SPECIFICATIONS SET FORTH IN THE TOWN OF MONUMENT MUNICIPAL CODE CRITERIA AND APPLICABLE STANDARDS, AND APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS. WHERE THERE IS CONFLICT BETWEEN THESE PLANS AND THE SPECIFICATIONS, OR ANY APPLICABLE STANDARDS, THE HIGHER QUALITY STANDARD SHALL APPLY. ALL WORK WITHIN PUBLIC R.O.W. OR EASEMENTS SHALL BE INSPECTED AND APPROVED BY THE TOWN OF MONUMENT INSPECTOR. THE TOWN WILL ALSO INSPECT ALL WORK ON PRIVATE PROPERTY.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES, AS SHOWN ON THESE PLANS, IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED UPON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL VERIFY PERTINENT LOCATIONS AND ELEVATIONS, ESPECIALLY AT THE CONNECTION POINTS AND AT POTENTIAL UTILITY CONFLICTS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FROM THE TOWN OF MONUMENT AND ALL APPLICABLE AGENCIES. THE CONTRACTOR SHALL NOTIFY THE TOWN INSPECTOR AT LEAST 48 HOURS PRIOR TO THE START OF ANY EARTH DISTURBING ACTIVITY, OR CONSTRUCTION ON ANY AND ALL PUBLIC IMPROVEMENTS.
- THE CONTRACTOR SHALL COORDINATE AND COOPERATE WITH THE TOWN AND ALL UTILITY COMPANIES INVOLVED WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION AND TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION AND WITH THE MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY UTILITY SERVICE.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE APPROVED PLANS, ONE (1) COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS, AND A COPY OF ALL PERMITS NEEDED FOR THE JOB, ON-SITE AT ALL TIMES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ASPECTS OF SAFETY INCLUDING, BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, AND SECURITY.
- IF, DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER OF RECORD AND THE TOWN INSPECTOR IMMEDIATELY.
- ALL REFERENCES TO ANY PUBLISHED STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARD, UNLESS SPECIFICALLY STATED OTHERWISE.
- THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN, IN ACCORDANCE WITH M.U.T.C.D. TO THE TOWN OF MONUMENT FOR APPROVAL, PRIOR TO ANY CONSTRUCTION ACTIVITIES WITHIN, OR AFFECTING, THE RIGHT-OF-WAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY AND ALL TRAFFIC CONTROL DEVICES AS MAY BE REQUIRED BY THE CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF ANY INTENDED IMPROVEMENTS SHOWN ON THESE DRAWINGS OR DESIGNATED TO BE PROVIDED, INSTALLED, OR CONSTRUCTED, UNLESS SPECIFICALLY NOTED OTHERWISE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING AS-BUILT INFORMATION ON A SET OF RECORD DRAWINGS KEPT AT THE CONSTRUCTION SITE. THE TOWN OF MONUMENT DEVELOPMENT SERVICES DEPARTMENT INSPECTOR AT ALL TIMES. A REPRODUCIBLE SET OF AS-BUILT DRAWINGS MUST BE FURNISHED TO THE TOWN OF MONUMENT AT THE COMPLETION OF THE PROJECT, PRIOR TO FINAL APPROVAL BY THE TOWN AND AS A CONDITION FOR OBTAINING A CERTIFICATE OF OCCUPANCY.
- DIMENSIONS FOR LAYOUT AND CONSTRUCTION ARE NOT TO BE SCALED FROM ANY DRAWING. IF PERTINENT DIMENSIONS ARE NOT SHOWN, CONTACT THE ENGINEER-OF-RECORD FOR CLARIFICATION, AND ANNOTATE THE DIMENSION ON THE AS-BUILT RECORD DRAWINGS.
- ALL STRUCTURAL EROSION CONTROL MEASURES SHALL BE INSTALLED, AT THE LIMITS OF CONSTRUCTION, PRIOR TO ANY OTHER GROUND DISTURBING ACTIVITY. ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED IN GOOD REPAIR BY THE CONTRACTOR, UNTIL SUCH TIME AS THE ENTIRE DISTURBED AREAS ARE STABILIZED WITH HARD SURFACE OR LANDSCAPING.
- THE CONTRACTOR SHALL SEQUENCE THE INSTALLATION OF UTILITIES IN SUCH A MANNER AS TO MINIMIZE POTENTIAL UTILITY CONFLICTS. IN GENERAL, STORM SEWER AND SANITARY SEWER SHOULD BE CONSTRUCTED PRIOR TO INSTALLATION OF WATER LINES AND DRY UTILITIES.
- NO SITE-RELATED IMPROVEMENTS MAY COMMENCE UNTIL A PRE-CONSTRUCTION MEETING IS HELD WITH THE TOWN OF MONUMENT AND ALL APPLICABLE PERMITS ARE OBTAINED.
- THE DEVELOPER MUST IDENTIFY TO THE TOWN OF MONUMENT, PRIOR TO THE START OF ANY WORK, A QUALIFIED PERSON RESPONSIBLE FOR REVIEWING AND MONITORING ALL OPERATIONS IN ORDER TO PREVENT OR MINIMIZE THE IMPACT OF VIBRATION, NOISE, DUST, DRAINAGE, AND EROSION DAMAGE, AND OTHER FORMS OF POLLUTION ON NEARBY PROPERTY AND THE PUBLIC AS A WHOLE. THE DEVELOPER MUST WRITE TO THE OWNERS/OCCUPANTS OF PROPERTIES WITHIN AT LEAST 100 YARDS OF THE WORKSITE, INFORMING THEM OF THE NATURE AND TIMING OF THE PROJECT AND PROVIDING CONTACT DETAILS FOR COMPLAINTS. THE TOWN, PRIOR TO THE COMMENCEMENT OF THE PROJECT, MUST APPROVE A COPY OF THE LETTER, MAILING LIST, AND DELIVERY DATES.
- ***TOWN OF MONUMENT DISCLAIMER*****
THE APPROVAL OF THESE SITE CONSTRUCTION PLANS PRIOR TO SITE PLAN APPROVAL DOES NOT IN ANY WAY OBLIGATE THE TOWN TO APPROVE SUBSEQUENT SUBMITTALS (I.E., SITE PLANS AND PLATS), AND THE TOWN HAS NO LIABILITY IN ANY FORM DUE TO ITS ACTIONS IN THE APPROVAL OF THESE SITE CONSTRUCTION PLANS.

TOWN OF MONUMENT CONSTRUCTION NOTES:

- THE TOWN OF MONUMENT DESIGN CRITERIA & CONSTRUCTION SPECIFICATIONS MANUAL IS CONSIDERED PART OF THIS CONSTRUCTION DRAWING SET. THIS DESIGN AND PLAN SET IS INCOMPLETE WITHOUT THIS SPECIFICATIONS MANUAL. THE CONTRACTOR SHALL OBTAIN A COPY OF THIS MANUAL AND BE FAMILIAR WITH IT FOR ALL CONSTRUCTION ACTIVITIES. A COPY CAN BE OBTAINED FROM THE TOWN BY CALLING (719)-499-3375.
- ALL MATERIALS AND WORKMANSHIP SHALL BE IN COMPLIANCE WITH THE TOWN CRITERIA AND CONSTRUCTION SPECIFICATION MANUAL AND TOWN OF MONUMENT MUNICIPAL CODE AND SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF MONUMENT INSPECTOR.
- THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING UTILITIES PRIOR TO ACTUAL CONSTRUCTION. ALL EXISTING UTILITIES SHOWN ARE BASED ON INFORMATION OF RECORD. THE CONTRACTOR IS RESPONSIBLE TO TAKE PRECAUTIONARY MEASURES TO PROTECT THE EXISTING UTILITIES SHOWN HEREON AND ANY OTHER EXISTING UTILITIES NOT OF RECORD OR NOT SHOWN ON THESE PLANS AND AGREES TO ACCEPT FULL RESPONSIBILITY FOR FAILURE TO LOCATE AND PRESERVE ANY AND ALL EXISTING UTILITIES.
- THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING IMPROVEMENTS TO BE MET BY THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS SHALL BE CONFIRMED BY FIELD MEASUREMENTS PRIOR TO CONSTRUCTION. ANY SIGNIFICANT DISCREPANCIES FOUND BETWEEN THIS PLAN SET AND ACTUAL FIELD CONDITIONS SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER OF RECORD FOR APPROPRIATE ACTION.
- THE CONTRACTOR IS ADVISED THAT ALL EXISTING CONDITIONS OUTSIDE THE AREA OF WORK SHALL BE PROTECTED, IF DAMAGE OCCURS DURING CONSTRUCTION, IT WILL BE REPLACED IN THE ORIGINAL EXISTING CONDITION AT THE CONTRACTOR'S EXPENSE.
- CONCRETE USED FOR CONSTRUCTION OF CURB AND GUTTER, SIDEWALK, AND CROSSPANS SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 P.S.I. AFTER 28 DAYS.
- ALL DISTURBED PAVEMENT EDGES WILL BE CUT TO NEAT LINES. THE THICKNESS OF ANY REPLACED ASPHALT AND BASE COURSE SHALL EQUAL OR EXCEED THE EXISTING THICKNESS.
- WHEN ABUTTING NEW PAVEMENT TO EXISTING, OR TO REMOVE ANY BROKEN OR CRACKED PAVEMENT, SAWCUT EXISTING PAVEMENT TO A STRAIGHT EDGE AND AT A RIGHT ANGLE, OR AS APPROVED BY THE TOWN INSPECTOR.
- FINAL GRADING OF CURBS AND PAVING SHALL PROVIDE POSITIVE DRAINAGE. STANDING WATER POCKETS OR PONDING WILL NOT BE ACCEPTABLE.
- WHERE REMOVAL OF EXISTING CURB, GUTTER, SIDEWALK, OR PAVEMENT IS REQUIRED, THE CONTRACTOR SHALL SAWCUT AND/OR REMOVE TO THE NEAREST JOINT. CURB, GUTTER, AND SIDEWALK SHOWN AS EXISTING ON THESE PLANS IS NOT TO BE REMOVED UNLESS OTHERWISE NOTED. IF ANY OF THE EXISTING CURB, GUTTER, OR SIDEWALK IS DAMAGED BY THE CONTRACTOR, THEN IT SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL AND ROUTING DURING CONSTRUCTION, IF REQUIRED. TWO-WAY TRAFFIC SHALL BE MAINTAINED THROUGH THE WORK AREA AT ALL TIMES.
- ALL DISTURBED AREAS THAT ARE TO REMAIN UNCOVERED FOR A PERIOD GREATER THAN 2 MONTHS SHALL BE RESEDED AND WATERED UNTIL STABLE VEGETATION IS ESTABLISHED.
- AT LEAST ONE SIGNED AND STAMPED SET OF THESE CONSTRUCTION DRAWINGS SHALL BE KEPT ON-SITE AT ALL TIMES.

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| <p>48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS</p> <p>811</p> <p>UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW</p> <p>THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</p> | <p>NO. REVISION</p> <p>1 REVISED PER CDOT/COUNTY COMMENTS</p> | <p>DATE</p> <p>5/12/23</p> | <p>REVIEW:</p> <p>PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC</p> |  <p>STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS</p> <p>ROADWAY IMPROVEMENT PLANS</p> <p>STANDARD NOTES</p> <table border="1"> <tr> <td>DESIGNED BY</td> <td>PRA</td> <td>SCALE</td> <td>DATE</td> <td>02-23-23</td> </tr> <tr> <td>DRAWN BY</td> <td>PRA</td> <td>(H) 1"=VARIES</td> <td>SHEET</td> <td>2 OF 26</td> </tr> <tr> <td>CHECKED BY</td> <td>(V) 1"= N/A</td> <td>JOB NO.</td> <td colspan="2">1.302.22</td> </tr> </table> | DESIGNED BY | PRA | SCALE | DATE | 02-23-23 | DRAWN BY | PRA | (H) 1"=VARIES | SHEET | 2 OF 26 | CHECKED BY | (V) 1"= N/A | JOB NO. | 1.302.22 | |
| | DESIGNED BY | PRA | SCALE | | DATE | 02-23-23 | | | | | | | | | | | | | |
| DRAWN BY | PRA | (H) 1"=VARIES | SHEET | 2 OF 26 | | | | | | | | | | | | | | | |
| CHECKED BY | (V) 1"= N/A | JOB NO. | 1.302.22 | | | | | | | | | | | | | | | | |
| <p>MARC A. WHORTON, COLORADO P.E. #37155 DATE</p> | | | <p>619 N. Cascade Avenue, Suite 200 Colorado Springs, Colorado 80903 (719)785-0790 (719)785-0799(Fax)</p> | | | | | | | | | | | | | | | | |

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| <input checked="" type="checkbox"/> M-203-12 | SUPERELEVATION STREETS (2 SHEETS) | 13-14 |
| <input checked="" type="checkbox"/> M-206-1 | EXCAVATION AND BACKFILL FOR STRUCTURES (2 SHEETS) | 15-16 |
| <input type="checkbox"/> M-206-2 | EXCAVATION AND BACKFILL FOR BRIDGES (2 SHEETS) | 17-18 |
| <input checked="" type="checkbox"/> M-208-1 | TEMPORARY EROSION CONTROL (11 SHEETS) | 19-29 |
| <input type="checkbox"/> M-210-1 | MAILBOX SUPPORTS (2 SHEETS) | 30-31 |
| <input type="checkbox"/> M-214-1 | NURSERY STOCK DETAILS | 32 |
| <input checked="" type="checkbox"/> M-216-1 | SOIL RETENTION COVERING (2 SHEETS) | 33-34 |
| <input type="checkbox"/> M-412-1 | CONCRETE PAVEMENT JOINTS (9 SHEETS) | 35-39 |
| <input type="checkbox"/> M-412-2 | CONCRETE PAVEMENT CRACK REPAIR (6 SHEETS) | (REVISED ON SEPTEMBER 6, 2022) |
| <input type="checkbox"/> M-510-1 | STRUCTURAL PLATE PIPE H-20 LOADING | 40 |
| <input type="checkbox"/> M-601-1 | SINGLE CONCRETE BOX CULVERT (CAST-IN-PLACE) (2 SHEETS) | 41-42 |
| <input type="checkbox"/> M-601-2 | DOUBLE CONCRETE BOX CULVERT (CAST-IN-PLACE) (2 SHEETS) | 43-44 |
| <input type="checkbox"/> M-601-3 | TRIPLE CONCRETE BOX CULVERT (CAST-IN-PLACE) (2 SHEETS) | 45-46 |
| <input checked="" type="checkbox"/> M-601-10 | HEADWALL FOR PIPES | 47 |
| <input type="checkbox"/> M-601-11 | TYPE "S" SADDLE HEADWALLS FOR PIPE | 48 |
| <input type="checkbox"/> M-601-12 | HEADWALLS AND PIPE OUTLET PAVING | 49 |
| <input type="checkbox"/> M-601-20 | WINGWALLS FOR PIPE OR BOX CULVERTS (2 SHEETS) | 50-51 |
| <input type="checkbox"/> M-603-1 | METAL PIPE (4 SHEETS) | 52-55 |
| <input checked="" type="checkbox"/> M-603-2 | REINFORCED CONCRETE PIPE | 56 |
| <input type="checkbox"/> M-603-3 | PRECAST CONCRETE BOX CULVERT | 57 |
| <input type="checkbox"/> M-603-4 | CORRUGATED POLYETHYLENE PIPE (AASHTO M294) AND CORRUGATED POLYPROPYLENE PIPE (AASHTO M330) (2 sheets) | 58 |
| <input type="checkbox"/> M-603-5 | POLYVINYL CHLORIDE (PVC) PIPE (AASHTO M304) | 59 |
| <input type="checkbox"/> M-603-6 | STEEL REINFORCED POLYETHYLENE RIBBED PIPE (AASHTO MP 20) | 60 |
| <input type="checkbox"/> M-603-10 | CONCRETE AND METAL END SECTIONS | 61 |
| <input type="checkbox"/> M-603-12 | TRAVERSABLE END SECTIONS AND SAFETY GRATES (3 SHEETS) | 62-64 |
| <input checked="" type="checkbox"/> M-604-10 | INLET, TYPE C | 65 |
| <input type="checkbox"/> M-604-11 | INLET, TYPE D | 66 |
| <input checked="" type="checkbox"/> M-604-12 | CURB INLET TYPE R (2 SHEETS) | 67-68 |
| <input type="checkbox"/> M-604-13 | CONCRETE INLET TYPE 13 | 69 |
| <input checked="" type="checkbox"/> M-604-20 | MANHOLES (3 SHEETS) | 70-72 |
| <input type="checkbox"/> M-604-25 | VANE GRATE INLET (5 SHEETS) | 73-77 |
| <input checked="" type="checkbox"/> M-605-1 | SUBSURFACE DRAINS | 78 |

| PLAN NUMBER | M STANDARD TITLE | PAGE NUMBER |
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| <input type="checkbox"/> M-606-1 | MIDWEST GUARDRAIL SYSTEM TYPE 3 W-BEAM 31 INCHES (19 SHEETS) | 79-97 |
| <input type="checkbox"/> M-606-13 | GUARDRAIL TYPE 7 F-SHAPE BARRIER (4 SHEETS) | 98-101 |
| <input type="checkbox"/> M-606-14 | PRECAST TYPE 7 CONCRETE BARRIER (4 SHEETS) | 102-104 |
| <input type="checkbox"/> M-606-15 | GUARDRAIL TYPE 9 SINGLE SLOPE BARRIER (11 SHEETS) | 105-115 |
| <input type="checkbox"/> M-607-1 | WIRE FENCES AND GATES (3 SHEETS) | 116-118 |
| <input type="checkbox"/> M-607-2 | CHAIN LINK FENCE (3 SHEETS) | 119-121 |
| <input type="checkbox"/> M-607-3 | BARRIER FENCE | 122 |
| <input type="checkbox"/> M-607-4 | DEER FENCE, GATES, AND GAME RAMPS (7 SHEETS) | 123-127 |
| <input type="checkbox"/> M-607-10 | PICKET SNOW FENCE | 128 |
| <input type="checkbox"/> M-607-15 | ROAD CLOSURE GATE (9 SHEETS) | 129-137 |
| <input checked="" type="checkbox"/> M-608-1 | CURB RAMPS (10 SHEETS) | 138-147 |
| <input checked="" type="checkbox"/> M-609-1 | CURBS, GUTTERS, AND SIDEWALKS (4 SHEETS) | 148-151 |
| <input type="checkbox"/> M-611-1 | CATTLE GUARD (2 SHEETS) | 152-153 |
| <input type="checkbox"/> M-611-2 | DEER GUARD (2 SHEETS) | 154-155 |
| <input type="checkbox"/> M-614-1 | RUMBLE STRIPS (3 SHEETS) | 156-158 |
| <input type="checkbox"/> M-614-2 | SAND BARREL ARRAYS (2 SHEETS) | 159-160 |
| <input type="checkbox"/> M-615-1 | EMBANKMENT PROTECTOR TYPE 3 | 161 |
| <input type="checkbox"/> M-615-2 | EMBANKMENT PROTECTOR TYPE 5 | 162 |
| <input type="checkbox"/> M-616-1 | INVERTED SIPHON | 163 |
| <input type="checkbox"/> M-620-1 | FIELD LABORATORY CLASS 1 | 164 |
| <input type="checkbox"/> M-620-2 | FIELD LABORATORY CLASS 2 (2 SHEETS) | 165-166 |
| <input type="checkbox"/> M-620-11 | FIELD OFFICE CLASS 1 | 167 |
| <input type="checkbox"/> M-620-12 | FIELD OFFICE CLASS 2 | 168 |
| <input type="checkbox"/> M-629-1 | SURVEY MONUMENTS (2 SHEETS) | 169-170 |

| PLAN NUMBER | S STANDARD TITLE | PAGE NUMBER |
|---|---|--|
| <input type="checkbox"/> S-612-1 | DELINEATOR INSTALLATIONS (8 SHEETS) | 171-178 |
| <input type="checkbox"/> S-613-1 | ROADWAY LIGHTING (6 SHEETS) | 179-186 |
| <input type="checkbox"/> S-613-2 | ALTERNATIVE ROADWAY LIGHTING (4 SHEETS) | (NEW, ISSUED ON SEPTEMBER 30, 2020) |
| <input checked="" type="checkbox"/> S-614-1 | GROUND SIGN PLACEMENT (2 SHEETS) | 187-188 |
| <input checked="" type="checkbox"/> S-614-2 | CLASS I SIGNS | 189 |
| <input checked="" type="checkbox"/> S-614-3 | CLASS II SIGNS | 190 |
| <input type="checkbox"/> S-614-4 | CLASS III SIGNS (3 SHEETS) | 191-193 |
| <input type="checkbox"/> S-614-5 | BREAK-AWAY SIGN SUPPORT DETAILS FOR CLASS III SIGNS (2 SHEETS) | 194-195 |
| <input type="checkbox"/> S-614-6 | CONCRETE FOOTINGS AND SIGN ISLANDS FOR CLASS III SIGNS (2 SHEETS) | 196-197 |
| <input checked="" type="checkbox"/> S-614-8 | TUBULAR STEEL SIGN SUPPORT DETAILS (7 SHEETS) | 198-204 |
| S-614-9 | PEDESTRIAN PUSH BUTTON POST ASSEMBLY (2 SHEETS) | 205-206 |
| <input type="checkbox"/> S-614-10 | MARKER ASSEMBLY INSTALLATIONS | 207 |
| <input type="checkbox"/> S-614-11 | MILEPOST SIGN DETAIL FOR HIGH SNOW AREAS | 208 |
| <input type="checkbox"/> S-614-12 | STRUCTURE NUMBER INSTALLATION (2 SHEETS) | 209-210 |
| <input type="checkbox"/> S-614-14 | FLASHING BEACON AND SIGN INSTALLATIONS (4 SHEETS) | 211-214 |
| <input checked="" type="checkbox"/> S-614-20 | TYPICAL POLE MOUNT SIGN INSTALLATIONS | 215 |
| <input type="checkbox"/> S-614-21 | CONCRETE BARRIER SIGN POST INSTALLATIONS (2 SHEETS) | 216-217 |
| <input checked="" type="checkbox"/> S-614-22 | TYPICAL MULTI-SIGN INSTALLATIONS | 218 |
| <input type="checkbox"/> S-614-40 | TYPICAL TRAFFIC SIGNAL 30'-75' DOUBLE MAST ARMS 65'-75' SINGLE MAST ARMS (5 SHEETS) | 219-223 |
| <input checked="" type="checkbox"/> S-614-40A | ALTERNATIVE TRAFFIC SIGNAL 25'-55' SINGLE MAST ARMS (4 SHEETS) | 224-227 |
| <input type="checkbox"/> S-614-41 | TEMPORARY SPAN WIRE SIGNALS (13 SHEETS) | 228-240 |
| <input checked="" type="checkbox"/> S-614-42 | CABINET FOUNDATION DETAIL (4 SHEETS) | 241-244 |
| <input type="checkbox"/> S-614-43 | TRAFFIC LOOP AND MISCELLANEOUS SIGNAL DETAILS (8 SHEETS) | 245-252 |
| <input type="checkbox"/> S-614-44 | PEDESTAL POLE SIGNALS (2 SHEETS) | 253-254 |
| <input checked="" type="checkbox"/> S-614-45 | PEDESTRIAN PUSH BUTTON POST ASSEMBLY DETAILS (6 SHEETS) | 255-266 |
| <input type="checkbox"/> S-614-50 | STATIC SIGN MONOTUBE STRUCTURES (12 SHEETS) | 267-280 |
| <input type="checkbox"/> S-614-60 | DYNAMIC SIGN MONOTUBE STRUCTURES (14 SHEETS) | 281-289 |
| <input checked="" type="checkbox"/> S-630-2 | PAVEMENT MARKINGS (11 SHEETS) | 290-313 |
| <input type="checkbox"/> S-630-3 | TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION (26 SHEETS) | (REVISED ON JANUARY 20, 2023) |
| <input type="checkbox"/> S-630-4 | BARRICADES, DRUMS, CONCRETE BARRIERS (TEMP.) AND VERTICAL PANELS | 314 |
| <input type="checkbox"/> S-630-5 | FLASHING BEACON (PORTABLE) DETAILS | 315 |
| <input type="checkbox"/> S-630-6 | STEEL SIGN SUPPORT (TEMPORARY) INSTALLATION DETAILS (2 SHEETS) | 316-317 |
| <input type="checkbox"/> S-630-7 | PORTABLE RUMBLE STRIPS (TEMPORARY) (2 SHEETS) | 318-319 |
| <input type="checkbox"/> S-630-8 | EMERGENCY PULL-OFF AREA (TEMPORARY) | 320 |
| <input type="checkbox"/> S-630-9 | ROLLING ROADBLOCKS FOR TRAFFIC CONTROL (3 SHEETS) | 321-323 |

COLORADO
DEPARTMENT OF TRANSPORTATION
M&S STANDARDS PLANS LIST
 July 31, 2019
 Revised on April 14, 2023

UPDATE THIS SHEET AS APPLICABLE

UPDATED AND THIS IS THE LATEST CDOT SHEET

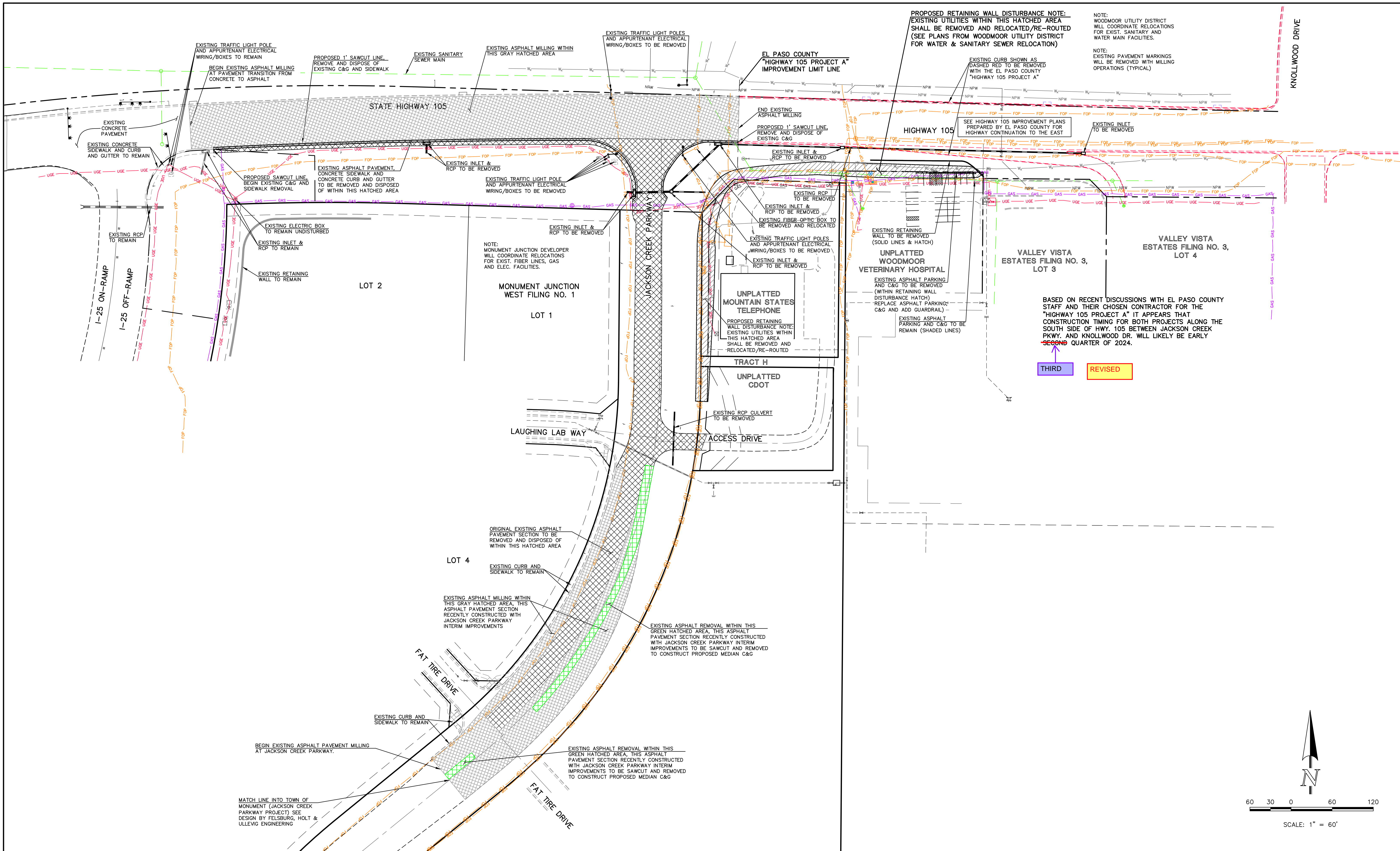
ALL OF THE M&S STANDARD PLANS, AS SUPPLEMENTED AND REVISED, APPLY TO THIS PROJECT WHEN USED BY DESIGNATED PAY ITEM OR SUBSIDIARY ITEM.

THE M&S STANDARD PLANS USED TO DESIGN THIS PROJECT ARE INDICATED BY A MARKED BOX , AND WILL BE ATTACHED TO THE PLANS. ALL OTHER M&S STANDARD PLANS ARE STILL ELIGIBLE FOR USE IN CONSTRUCTION IF APPROVED BY AN APPROPRIATE CDOT ENGINEER.

| Computer File Information Creation Date: 07/31/19 Designer Initials: JBK Last Modification Date: 04/14/23 Detailer Initials: LTA CAD Ver.: MicroStation V8 Scale: Not to Scale Units: English | Sheet Revisions <table border="1"> <tr><th>Date:</th><th>Comments</th></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table> | Date: | Comments | | | | | | | Colorado Department of Transportation 2829 West Howard Place CDOT HQ, 3rd Floor Denver, CO 80204 Phone: 303-757-9021 FAX: 303-757-9868 Construction Engineering Services JBK | <h1 style="margin: 0;">STANDARD PLANS LIST</h1> <p>Issued by the Project Development Branch: July 31, 2019</p> | STANDARD PLAN NO. STANDARDS PLANS LIST Standard Sheet No. 1 of 1 Project Sheet Number: |
|---|--|-------|----------|--|--|--|--|--|--|---|--|---|
| Date: | Comments | | | | | | | | | | | |
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| 48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS 811 UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW <small>THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</small> | <table border="1"> <tr><th>NO. REVISION</th><th>DATE</th></tr> <tr><td>1 REVISED PER CDOT COMMENTS</td><td>5/18/23</td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> <tr><td> </td><td> </td></tr> </table> | NO. REVISION | DATE | 1 REVISED PER CDOT COMMENTS | 5/18/23 | | | | | | | REVIEW: PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC MARC A. WHORTON, COLORADO P.E. #37155 DATE | 619 N. Cascade Avenue, Suite 200 Colorado Springs, Colorado 80903 (719) 785-0790 (719) 785-0799 (fax) | STATE HIGHWAY 105 / JACKSON CREEK PRWY. - PHASE 2 CONSTRUCTION PLANS CDOT STANDARD PLAN SHEET <table border="1"> <tr><td>DESIGNED BY</td><td>PRA</td><td>SCALE</td><td>DATE</td><td>02-23-23</td></tr> <tr><td>DRAWN BY</td><td>PRA</td><td>(H) 1"= N/A</td><td>SHEET</td><td>3 OF 26</td></tr> <tr><td>CHECKED BY</td><td></td><td>(V) 1"= N/A</td><td>JOB NO.</td><td>1302.22</td></tr> </table> | DESIGNED BY | PRA | SCALE | DATE | 02-23-23 | DRAWN BY | PRA | (H) 1"= N/A | SHEET | 3 OF 26 | CHECKED BY | | (V) 1"= N/A | JOB NO. | 1302.22 |
|---|---|--------------|---------|-----------------------------|---------|--|--|--|--|--|--|--|--|--|-------------|-----|-------|------|----------|----------|-----|-------------|-------|---------|------------|--|-------------|---------|---------|
| NO. REVISION | DATE | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 1 REVISED PER CDOT COMMENTS | 5/18/23 | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| DESIGNED BY | PRA | SCALE | DATE | 02-23-23 | | | | | | | | | | | | | | | | | | | | | | | | | |
| DRAWN BY | PRA | (H) 1"= N/A | SHEET | 3 OF 26 | | | | | | | | | | | | | | | | | | | | | | | | | |
| CHECKED BY | | (V) 1"= N/A | JOB NO. | 1302.22 | | | | | | | | | | | | | | | | | | | | | | | | | |

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PROPOSED RETAINING WALL DISTURBANCE NOTE:
 EXISTING UTILITIES WITHIN THIS HATCHED AREA SHALL BE REMOVED AND RELOCATED/RE-ROUTED (SEE PLANS FROM WOODMOOR UTILITY DISTRICT FOR WATER & SANITARY SEWER RELOCATION)

NOTE:
 WOODMOOR UTILITY DISTRICT WILL COORDINATE RELOCATIONS FOR EXIST. SANITARY AND WATER MAIN FACILITIES.

NOTE:
 EXISTING PAVEMENT MARKINGS WILL BE REMOVED WITH MILLING OPERATIONS (TYPICAL)

NOTE:
 MONUMENT JUNCTION DEVELOPER WILL COORDINATE RELOCATIONS FOR EXIST. FIBER LINES, GAS AND ELEC. FACILITIES.

BASED ON RECENT DISCUSSIONS WITH EL PASO COUNTY STAFF AND THEIR CHOSEN CONTRACTOR FOR THE "HIGHWAY 105 PROJECT A" IT APPEARS THAT CONSTRUCTION TIMING FOR BOTH PROJECTS ALONG THE SOUTH SIDE OF HWY. 105 BETWEEN JACKSON CREEK PKWY. AND KNOLLWOOD DR. WILL LIKELY BE EARLY ~~SECOND~~ **THIRD** QUARTER OF 2024.

THIRD REVISÉ

MATCH LINE INTO TOWN OF MONUMENT (JACKSON CREEK PARKWAY PROJECT) SEE DESIGN BY FELSBURG, HOLT & ULLEVG ENGINEERING

48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS
811
 UTILITY NOTIFICATION CENTER OF COLORADO
 IT'S THE LAW

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

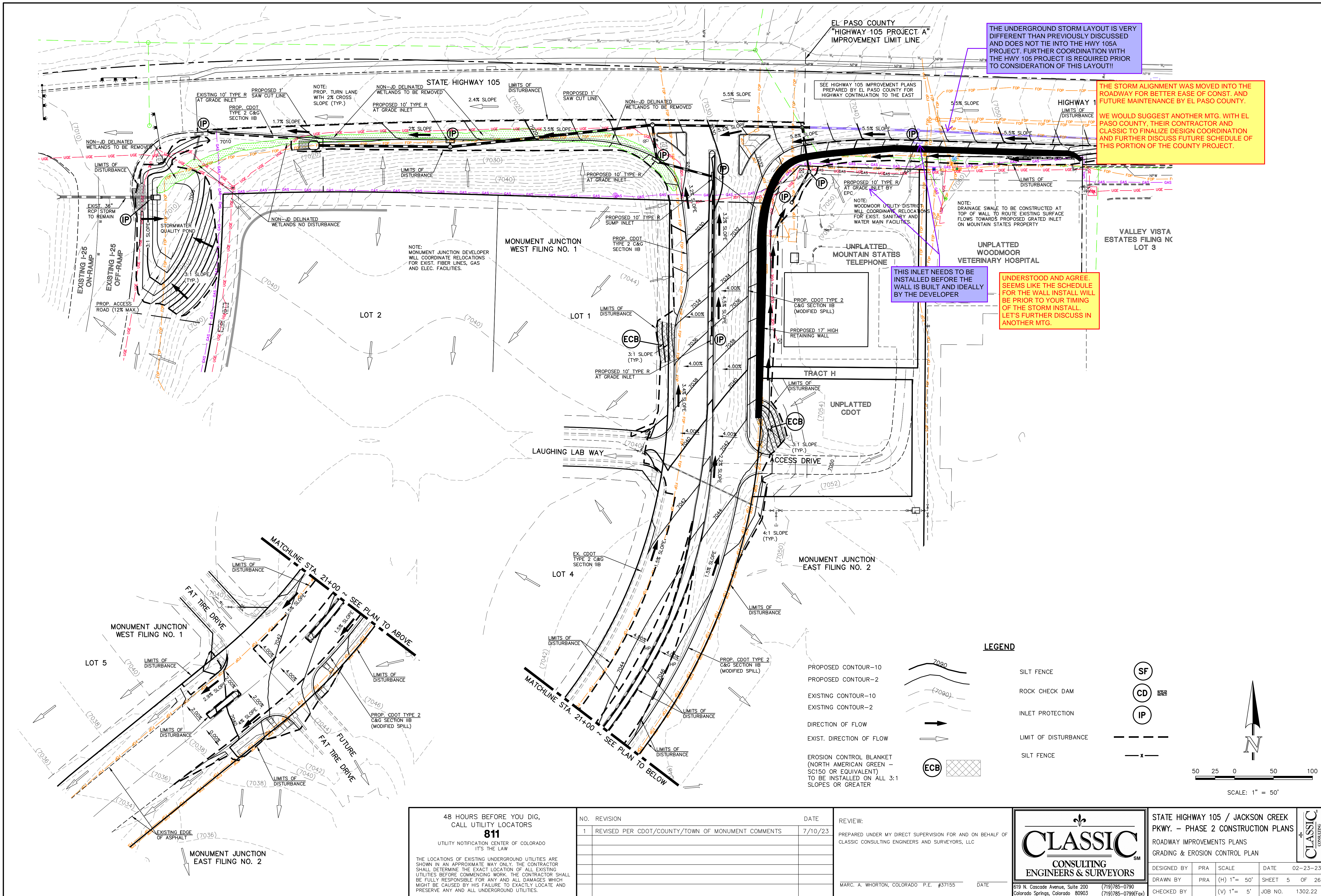
| NO. | REVISION | DATE |
|-----|---|---------|
| 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 |

REVIEW:
 PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC. A. WHORTON, COLORADO P.E. #37155 DATE

| | | | |
|--|--------------|--------------|---------------|
| STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS | | | |
| ROADWAY IMPROVEMENT PLANS | | | |
| DEMOLITION PLAN | | | |
| DESIGNED BY | PRA | SCALE | DATE 02-23-23 |
| DRAWN BY | PRA | (H) 1" = 60' | SHEET 4 OF 26 |
| CHECKED BY | (V) 1" = N/A | JOB NO. | 1302.22 |

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EL PASO COUNTY
"HIGHWAY 105 PROJECT A"
IMPROVEMENT LIMIT LINE

THE UNDERGROUND STORM LAYOUT IS VERY DIFFERENT THAN PREVIOUSLY DISCUSSED AND DOES NOT TIE INTO THE HWY 105A PROJECT. FURTHER COORDINATION WITH THE HWY 105 PROJECT IS REQUIRED PRIOR TO CONSIDERATION OF THIS LAYOUT!!

THE STORM ALIGNMENT WAS MOVED INTO THE ROADWAY FOR BETTER EASE OF CONST. AND FUTURE MAINTENANCE BY EL PASO COUNTY.

WE WOULD SUGGEST ANOTHER MTG. WITH EL PASO COUNTY, THEIR CONTRACTOR AND CLASSIC TO FINALIZE DESIGN COORDINATION AND FURTHER DISCUSS FUTURE SCHEDULE OF THIS PORTION OF THE COUNTY PROJECT.

SEE HIGHWAY 105 IMPROVEMENT PLANS PREPARED BY EL PASO COUNTY FOR HIGHWAY CONTINUATION TO THE EAST

NOTE: WOODMOOR UTILITY DISTRICT WILL COORDINATE RELOCATIONS FOR EXIST. SANITARY AND WATER MAIN FACILITIES.

NOTE: DRAINAGE SWALE TO BE CONSTRUCTED AT TOP OF WALL TO ROUTE EXISTING SURFACE FLOWS TOWARDS PROPOSED GRATED INLET ON MOUNTAIN STATES PROPERTY

THIS INLET NEEDS TO BE INSTALLED BEFORE THE WALL IS BUILT AND IDEALLY BY THE DEVELOPER

UNDERSTOOD AND AGREE. SEEMS LIKE THE SCHEDULE FOR THE WALL INSTALL WILL BE PRIOR TO YOUR TIMING OF THE STORM INSTALL. LET'S FURTHER DISCUSS IN ANOTHER MTG.

48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS
811
UTILITY NOTIFICATION CENTER OF COLORADO
IT'S THE LAW

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| NO. | REVISION | DATE |
|-----|---|---------|
| 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 |

REVIEW:
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC. A. WHORTON, COLORADO P.E. #37155 DATE

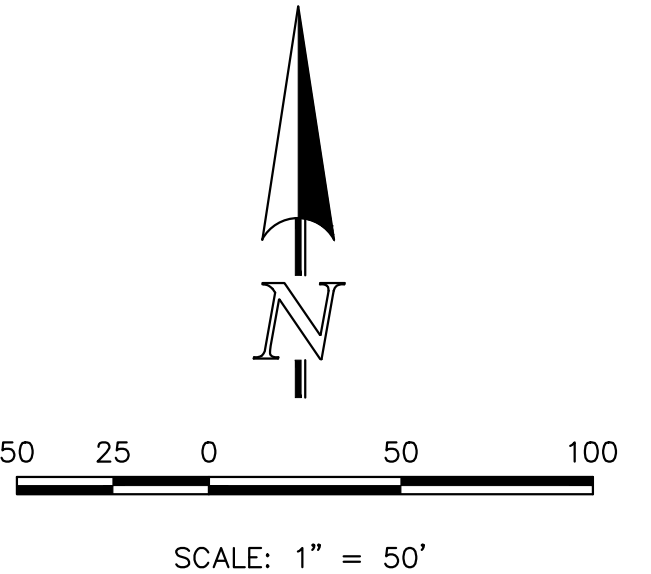
CLASSIC
CONSULTING ENGINEERS & SURVEYORS

619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903
(719) 785-0790
(719) 785-0799 (fax)

| | | | |
|--|-------------|--------------|---------------|
| STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS | | | |
| ROADWAY IMPROVEMENTS PLANS | | | |
| GRADING & EROSION CONTROL PLAN | | | |
| DESIGNED BY | PRA | SCALE | DATE 02-23-23 |
| DRAWN BY | PRA | (H) 1" = 50' | SHEET 5 OF 26 |
| CHECKED BY | (V) 1" = 5' | JOB NO. | 1302.22 |

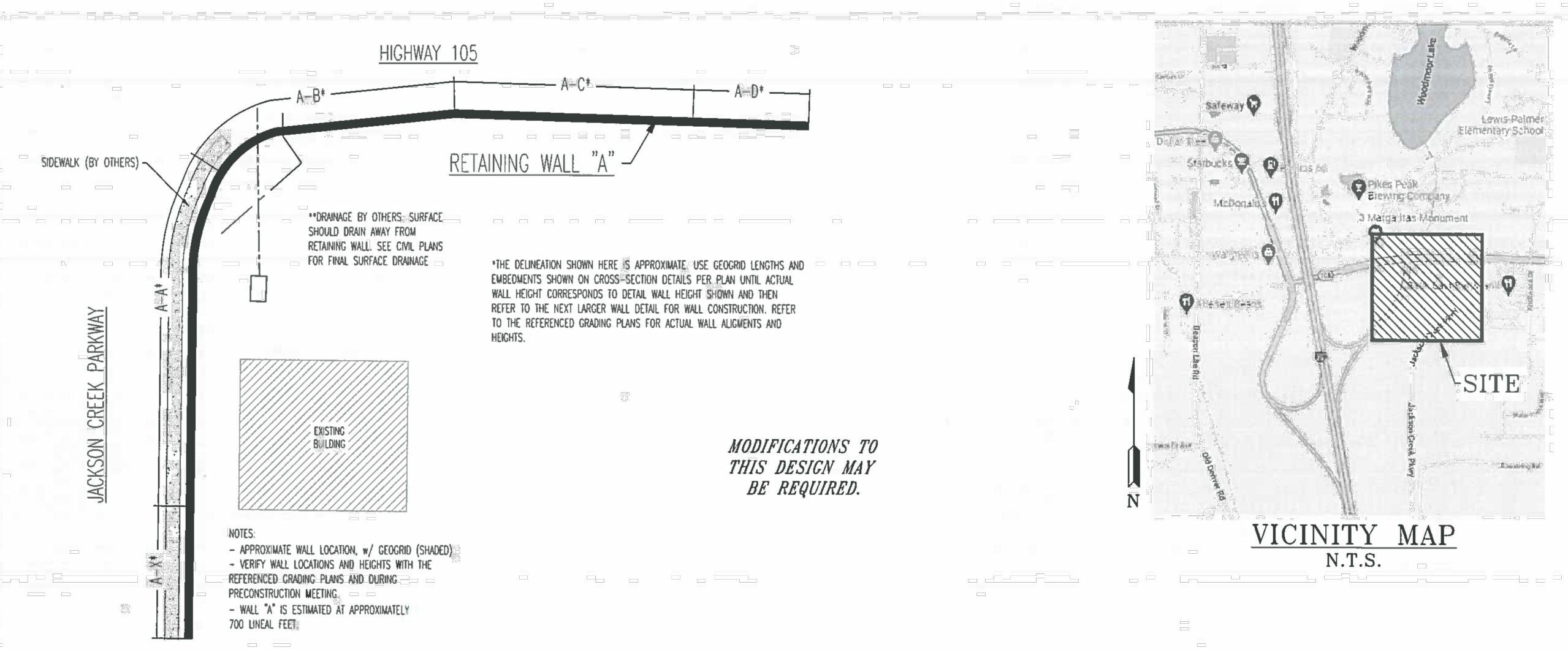
LEGEND

- PROPOSED CONTOUR-10
- PROPOSED CONTOUR-2
- EXISTING CONTOUR-10
- EXISTING CONTOUR-2
- DIRECTION OF FLOW
- EXIST. DIRECTION OF FLOW
- EROSION CONTROL BLANKET (NORTH AMERICAN GREEN - SC150 OR EQUIVALENT) TO BE INSTALLED ON ALL 3:1 SLOPES OR GREATER
- SILT FENCE
- ROCK CHECK DAM
- INLET PROTECTION
- LIMIT OF DISTURBANCE
- SILT FENCE



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REDI-ROCK RETAINING WALL JACKSON CREEK PARKWAY & STATE HIGHWAY 105



CONCEPTUAL PLAN VIEW
N.T.S.

INDEX OF SHEETS
PLAN & COVER SHEET
GENERAL NOTES & DETAILS
WALL "A" ELEVATION & SECTIONS
WALL "A" SECTIONS

| PLAN SHEET NO. | INDEX OF SHEETS |
|----------------|-------------------------------|
| 1 | PLAN & COVER SHEET |
| 2 | GENERAL NOTES & DETAILS |
| 3 | WALL "A" ELEVATION & SECTIONS |
| 4 | WALL "A" SECTIONS |

NOTE: THIS DESIGN WAS BASED ON CONSTRUCTION PLANS BY CLASSIC CONSULTING ENGINEERS & SURVEYORS, AND THE SUBSURFACE SOILS INVESTIGATION BY ENTECH ENGINEERING, INC., DATED OCTOBER 20, 2022, ENTECH JOB NO. 221459. CONTRACTOR TO VERIFY RETAINING WALL DESIGN TO FINAL PLANS. IF THE RETAINED SOILS DIFFER FROM OUR ASSUMED SOIL TYPES, THEN THE ENGINEER MUST BE NOTIFIED AND MODIFICATIONS TO THIS DESIGN MAY BE REQUIRED.

Unresolved: Include calculation in drainage report to show that drainage is sized to adequately handle flow. **NOW ADDED TO REPORT**

ENTECH ENGINEERING, INC.
11-6-22
PROFESSIONAL ENGINEER
STATE OF COLORADO P.E.

REVISIONS

| NO. | REVISION | DATE |
|-----|----------|------|
| 1 | | |
| 2 | | |
| 3 | | |
| 4 | | |

GENERAL NOTES

- These notes shall be read in conjunction with the drawings. In the event of a conflict, notify the Engineer for clarification.
- Before excavating anything herein shown, examine actual job conditions. Report any discrepancy, dimensional or otherwise, and any other error, omission, or difficulty affecting the work to the Engineer for review.
- These retaining wall plans have been prepared to represent specific design parameters. It is the responsibility of the Project Owner/General Contractor to ensure construction problems due to changed conditions encountered during the progress of any portion of the work. The Project Owner/General Contractor is responsible for contacting the Engineer prior to making any changes to this design. This design addresses local retaining wall stability including lateral and external stability and does not include a slope or global stability analysis for areas beyond the retaining wall.
- Groundwater or footing level was not considered in the design of this retaining wall. If groundwater is encountered, contact the Engineer immediately.
- The Owner or his representative reserves the right to inspect any material, fabrication, or workmanship of any type in the field or shop for conformance to the specifications, general notes, and drawings.
- All details and sections are intended to be typical and shall be construed to apply to any similar situation elsewhere, except where a different detail is shown.
- The Contractor shall make substitutions as noted herein to the Engineer in a timely manner so as to allow a ten business day review period. Quantities and dimensions are the responsibility of the Contractor. The Contractor shall make substitutions as necessary until the concurrence of the Engineer and his Consultants is obtained. Substitutions will not be considered by the Engineer or his Consultants unless submitted two weeks prior to time of installation, and complete documentation is provided substantiating compliance with the Contract Documents. Substituted quantities and samples as required. Substitutions will not be considered when acceptance will require substantial revision of the Contract Documents. The Engineer and his Consultants will determine acceptability of proposed substitution.
- The Contractor shall obtain a copy of and understand all applicable manufacturer's specifications. If discrepancies occur between manufacturer's specifications and those shown on the details, contact the Engineer immediately. Backfill areas shall be sloped to avoid ponding water and to allow for surface drainage to flow away from the wall.
- This design is valid only for the proposed retaining wall at the specified address. This retaining wall has been designed according to the grading plans by Classic Consulting Engineers & Surveyors, and the Subsurface Soils Investigation by Entech Engineering, Inc., dated October 20, 2022, Entech Job No. 221459.

BLOCK UNITS

- Redi-Rock PC. It shall be the contractor's responsibility to ensure that all units are erected in undamaged conditions. Units with cracks or other imperfections which, in the opinion of the Engineer, are unacceptable shall not be placed in the wall. Rejected units shall be removed from the site with no additional compensation.

WALL LAYOUT AND BASE PREPARATION

- Excavation to sound soil material shall be made at the base of the wall. Expansive Material may be encountered. Based on information obtained in the Subsurface Soils Investigation, expansive material should be removed and replaced per the recommendations provided in the Subsurface Soils Investigation. Unless shown otherwise, benches shall be made horizontal in the line of the wall.
- Contractor shall provide sufficient notice to the Engineer for observations, testing, and approval of the excavation prior to placing the granular bedding. If structural fill is used to obtain base elevation, the base soil shall be compacted to a minimum 95% of the Modified Proctor Dry Density (ASTM D 1585).
- Finished base excavation shall be within 0.5 inches of the base elevation and shall include vertical steps where the base of the wall is to step.

LEVELING PAD

- After approval of a base soil, a continuous gravel pad (see plans) shall be placed below the first course of the concrete block to be used as a leveling pad. This pad shall consist of a minimum of 6-inch deep, U.M.C., COOT No. 57 or 67 coarse aggregate. (COOT Standard Specifications Table 703-2). The wall is to be placed by placing the first course of concrete block directly onto the leveling pad.

WALL UNIT INSTALLATION

- The units in the base course shall be checked for level alignment.
- Capstones used on the top course shall be attached with adhesive on a clean, dry surface as specified by the Manufacturer.

DRAINAGE MATERIAL PLACEMENT

- Place and compact drainage fill material behind wall units.
- Material shall consist of No. 57 or 67 coarse aggregate (COOT Standard Specifications Table 703-2).
- Minimum stacked vertical height of wall units prior to fill placement shall not exceed 2 courses.

BACKFILL MATERIAL

- Backfill materials used behind the wall shall consist of select granular structural backfill, compliant with COOT Class 1 backfill requirements or COOT 57 or 67 Aggregate per plan.
- Backfill materials shall be placed in lifts not exceeding 8 inches in loose thickness and compacted to a minimum of 95% of the Modified Proctor Dry Density (ASTM D 1585).

LOW PERMEABLE SOIL CAP

- Soil used for low permeable soils cap such as styrofoam (SC) soils or import approved by the engineer. Materials shall be placed in lifts not exceeding 8 inches in loose thickness and compacted to a minimum of 95% of the Modified Proctor Dry Density (ASTM D 1585).
- 4" of gravel is to be placed on top of the impermeable layer. The top 6" from the surface down shall be tamped or mixed to ensure cohesion of the impermeable and topsoil layers.

CONSTRUCTION PRECAUTIONS

- During construction, Contractor shall slope back backfill (if) at each day away from wall units and shall not allow runoff from adjacent areas to enter construction area.
- Construction excavation shall comply with OSHA standards and regulations.

PRECONSTRUCTION

- Contractor must schedule a preconstruction meeting with the Engineer prior to the initiation of any excavation, with at least 24 hours of advanced notice.

WALL GEOMETRY

| | |
|------------------------------------|-----------|
| 1. Design Wall Height (ft) | See Plans |
| 2. Embedment Wall Height (ft) | See Plans |
| 3. Exposed Wall Design Height (ft) | See Plans |
| 4. Wall Inclination (Deg.) | See Plans |

SEGMENTAL UNIT DATA - VERIFY w/ MANUFACTURER SPECS.

| Segmental Unit Name | Redi-Rock 28" | Redi-Rock 41" | Redi-Rock 60" |
|--|---------------|---------------|---------------|
| 1. Segmental Unit Name | Redi-Rock 28" | Redi-Rock 41" | Redi-Rock 60" |
| 2. Cap Height (inches) | 18 | 18 | 18 |
| 3. Unit Height (H _u) (inches) | 40.75 | 40.75 | 40.75 |
| 4. Unit Width (W _u) (inches) | 16 | 16 | 16 |
| 5. Unit Length (inches) | 46 | 46 | 46 |
| 6. Setback (inches) | 9.75 | 9.75 | 9.75 |
| 7. Weight (pcf) | 130 | 130 | 130 |
| 8. Segmental Unit Name | Redi-Rock 41" | | |
| 9. Cap Height (inches) | 18 | | |
| 10. Unit Height (H _u) (inches) | 18 | | |
| 11. Unit Width (W _u) (inches) | 18 | | |
| 12. Unit Length (inches) | 46 | | |
| 13. Setback (inches) | 9.75 | | |
| 14. Weight (pcf) | 130 | | |
| 15. Segmental Unit Name | Redi-Rock 60" | | |
| 16. Cap Height (inches) | 18 | | |
| 17. Unit Height (H _u) (inches) | 18 | | |
| 18. Unit Width (W _u) (inches) | 18 | | |
| 19. Unit Length (inches) | 46 | | |
| 20. Setback (inches) | 9.75 | | |
| 21. Weight (pcf) | 130 | | |

WALL GRADES

- Top Slope: See Plans
- Toe Slope: See Plans

RETAINING WALL DESIGN PARAMETERS

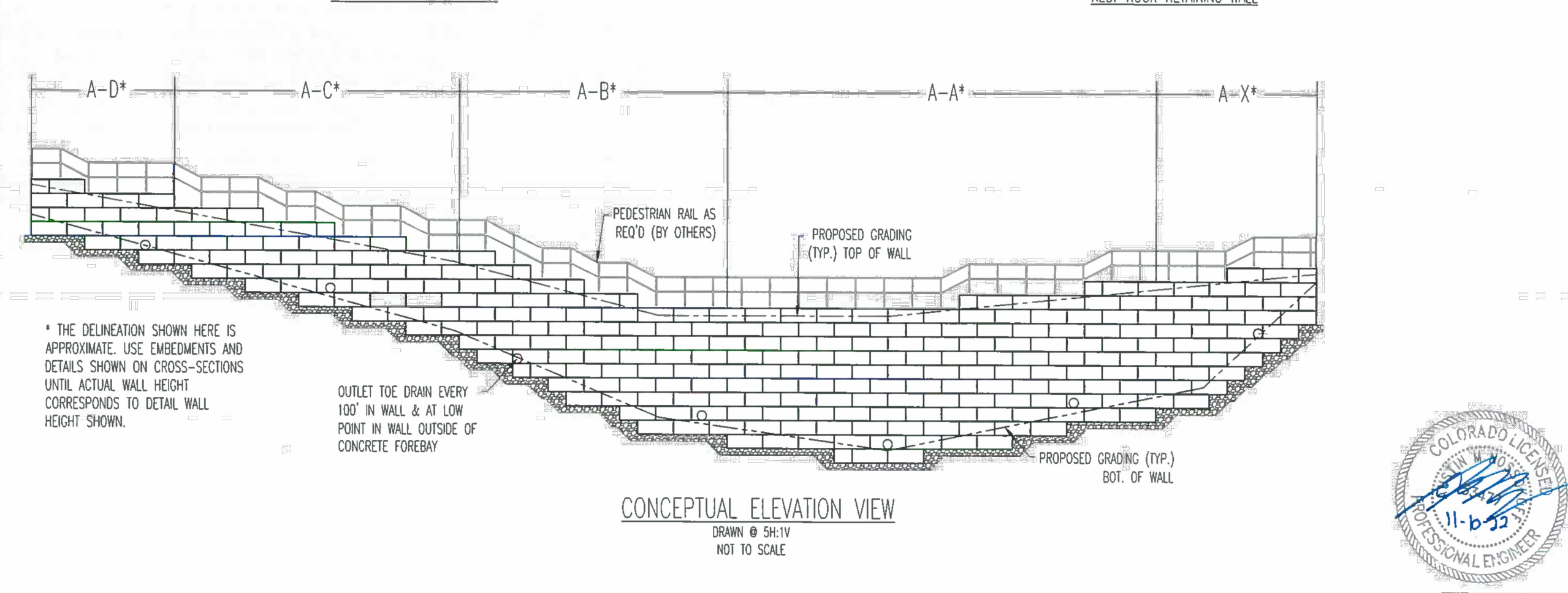
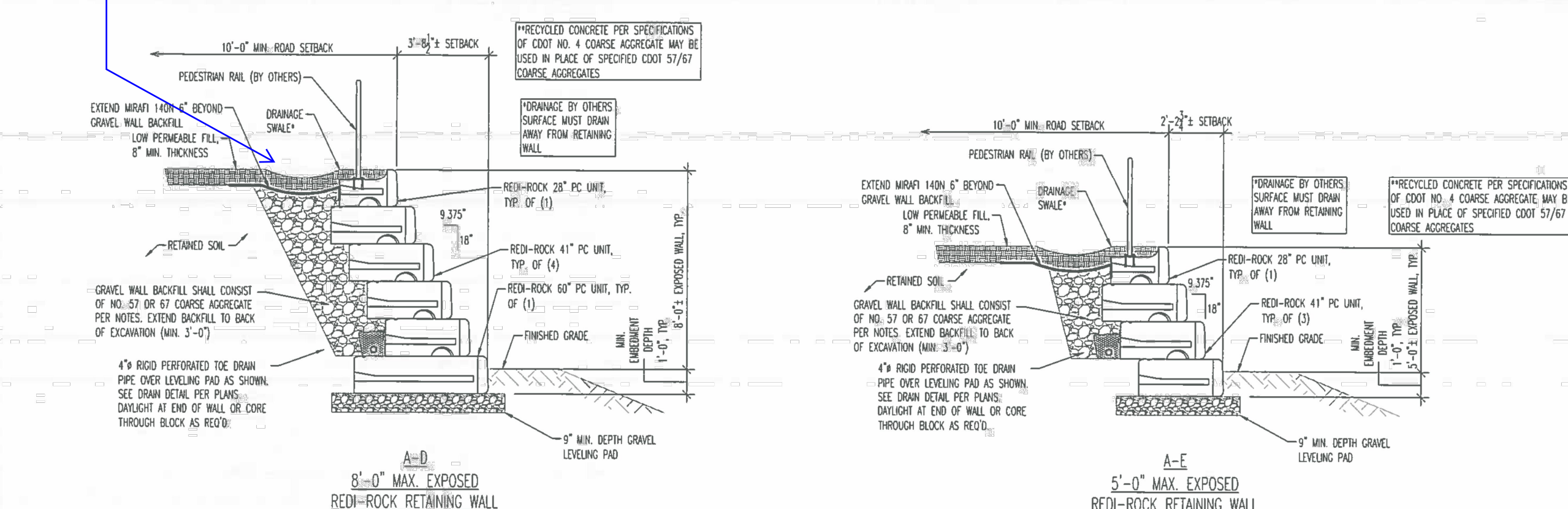
- Distributed Surcharge: 100 Pedestrian (As Req'd)
- Minimum Factors of Safety:
 - a. External Stability: 1.5
 - b. Overturning: 2.0
 - c. Bearing Capacity: 2.0
 - d. Internal Stability: 1.5
 - e. Pullout: 1.5

DRAIN DETAIL

ENTECH ENGINEERING, INC.
11-6-22
PROFESSIONAL ENGINEER
STATE OF COLORADO P.E.

REVISIONS

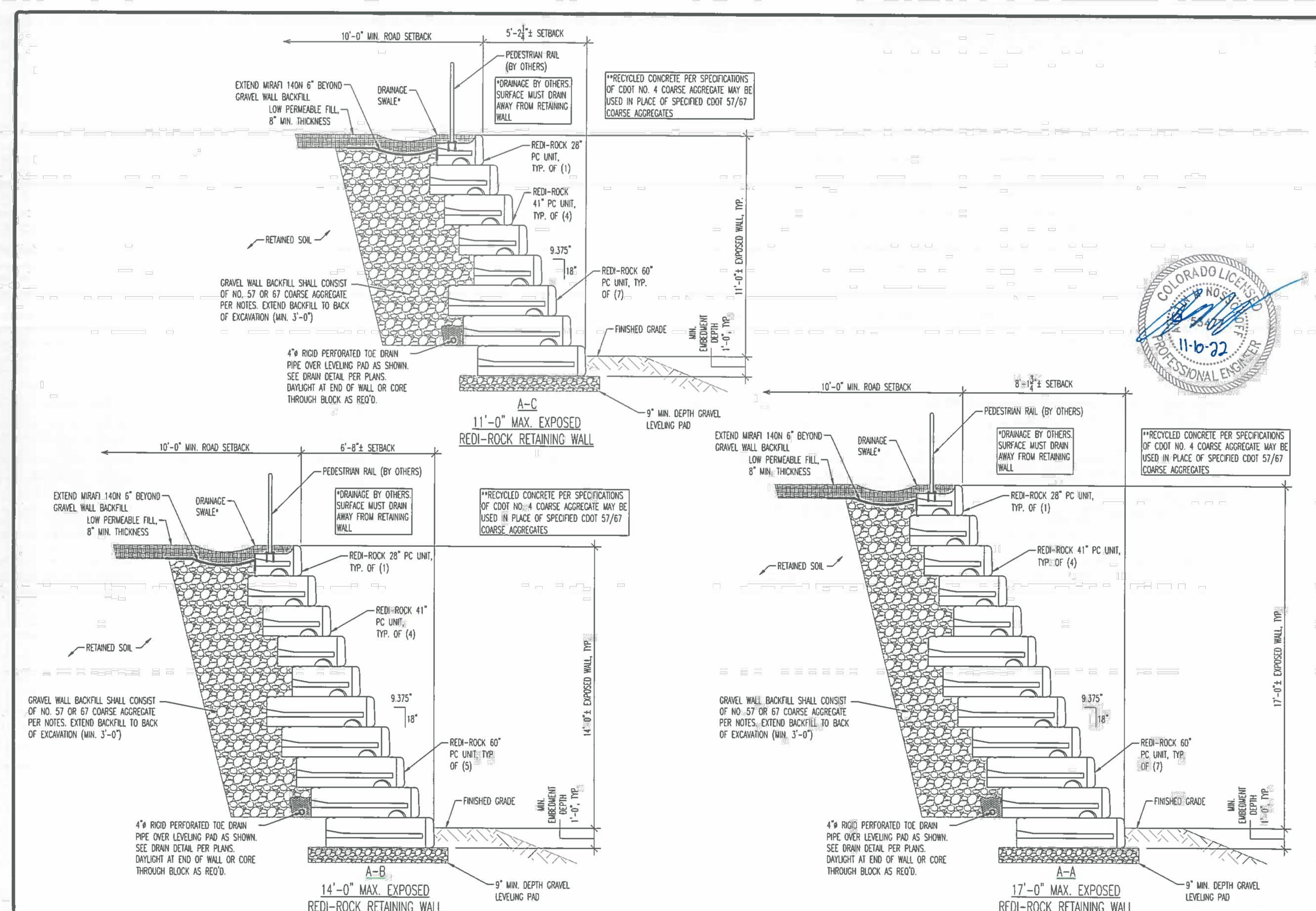
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ENTECH ENGINEERING, INC.
11-6-22
PROFESSIONAL ENGINEER
STATE OF COLORADO P.E.

REVISIONS

| NO. | REVISION | DATE |
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ENTECH ENGINEERING, INC.
11-6-22
PROFESSIONAL ENGINEER
STATE OF COLORADO P.E.

REVISIONS

| NO. | REVISION | DATE |
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| 1 | | |
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N:\110222\DRAWINGS\CONSTRUCT\CON\SH-1-06-07-RETAINING WALL PLANS.DWG, 7/10/2023 4:16:22 PM, eragon, 1:1

48 HOURS BEFORE YOU DIG,
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IT'S THE LAW

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

NO. REVISION

| | | |
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| | | |
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| | | |

DATE

REVIEW:

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

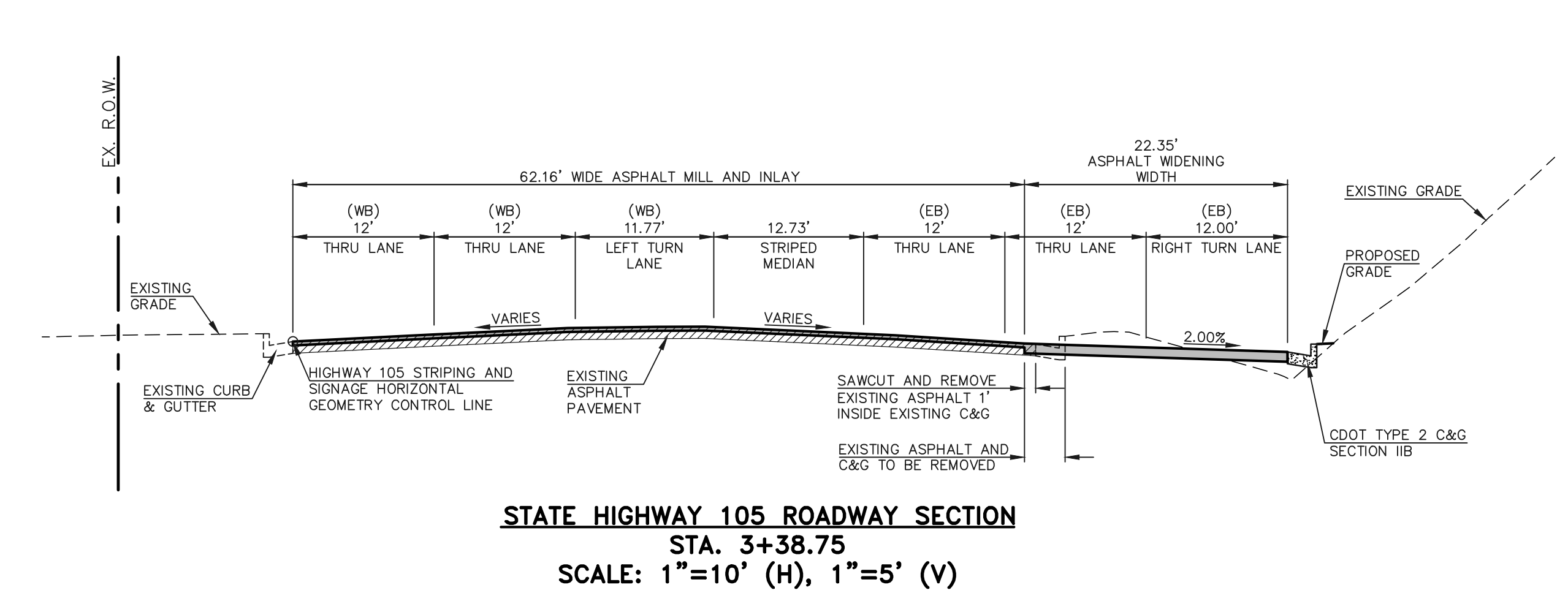
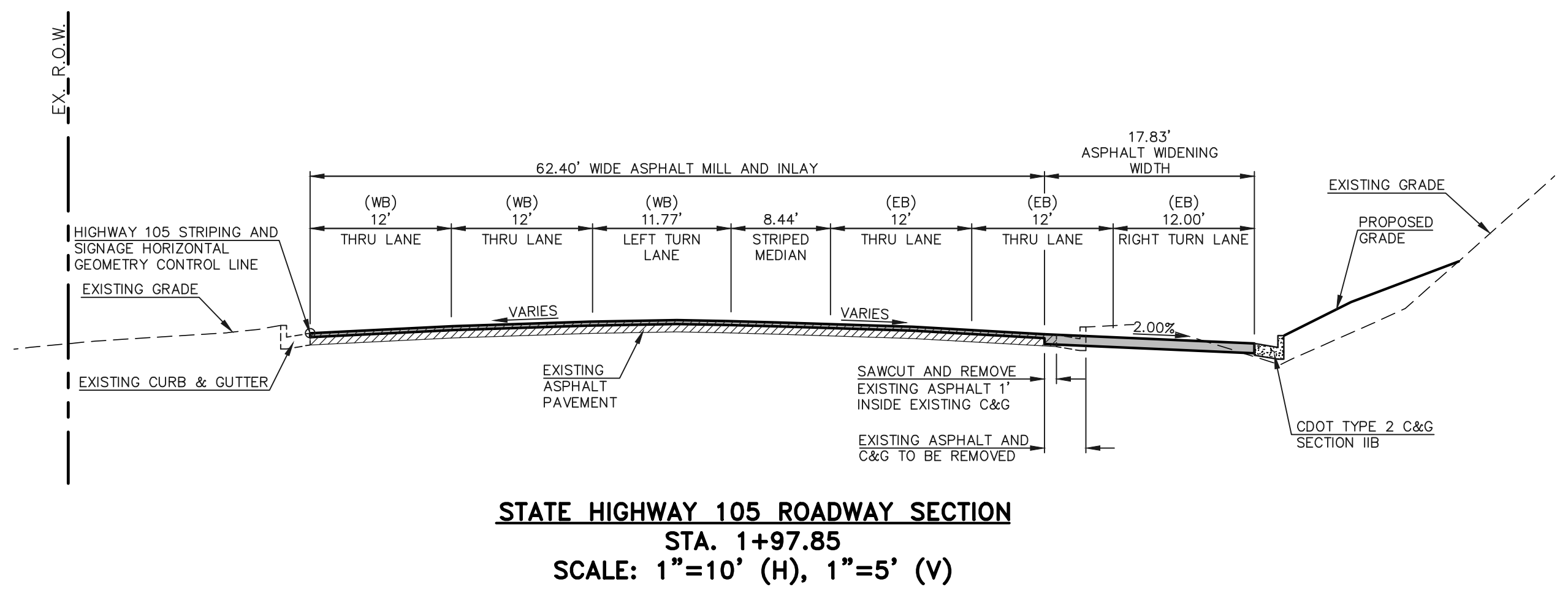
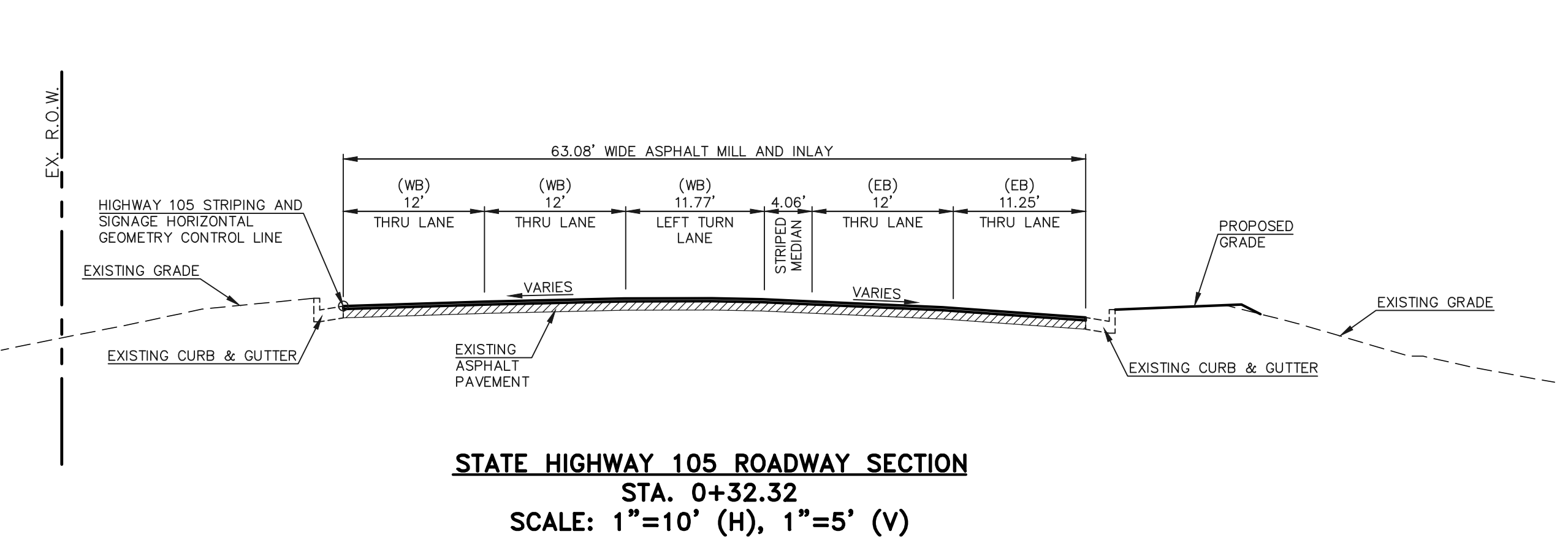
MARC. A. WHORTON, COLORADO P.E. #37155 DATE

CLASSIC
CONSULTING
ENGINEERS & SURVEYORS

STATE HIGHWAY 105 / JACKSON CREEK
PKWY. - PHASE 2 CONSTRUCTION PLANS
ROADWAY IMPROVEMENT PLANS
RETAINING WALL DETAILS

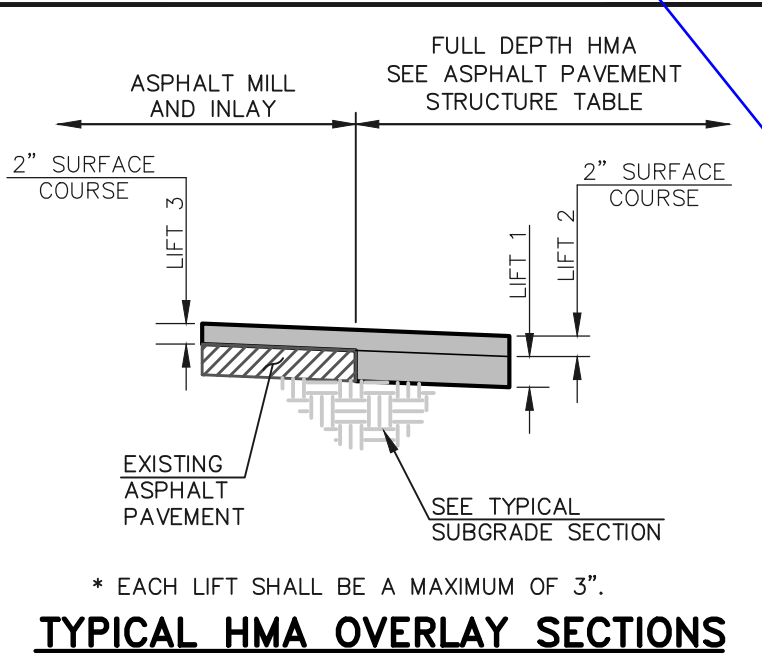
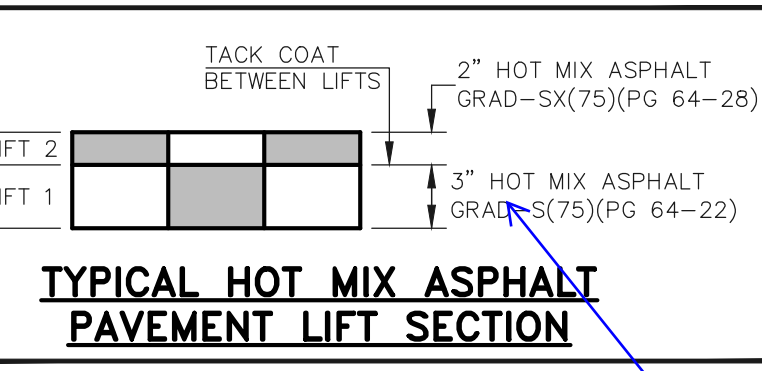
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|-------------|-------------|--------------|---------|----------|
| DESIGNED BY | PRA | SCALE | DATE | 02-23-23 |
| DRAWN BY | PRA | (H) 1" = 50' | SHEET | 7 OF 26 |
| CHECKED BY | (V) 1" = 5' | JOB NO. | 1302.22 | |

619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903
(719) 785-0790
(719) 785-0799(Fax)

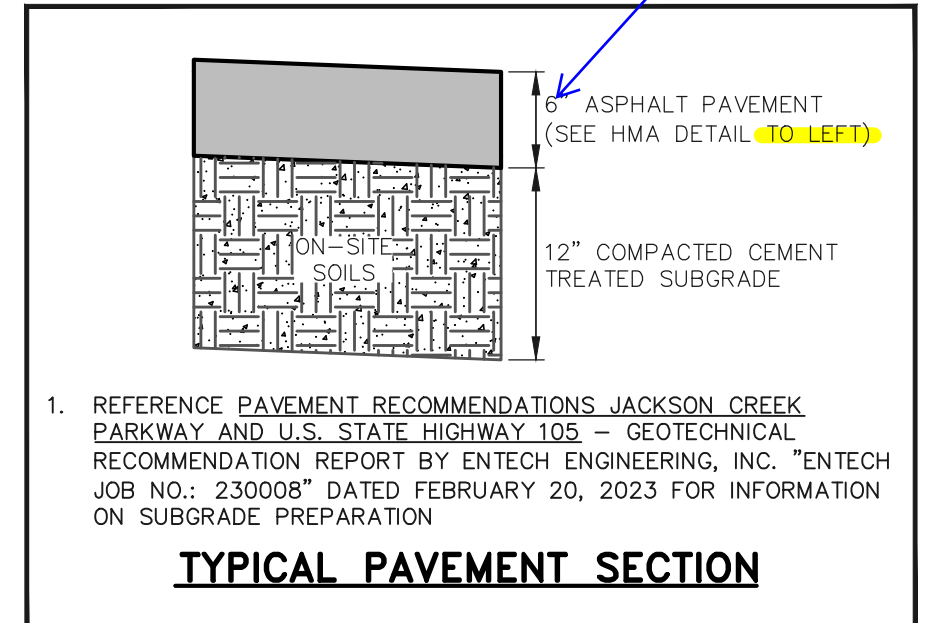


HOT MIX ASPHALT PAVEMENT STRUCTURE TABLE

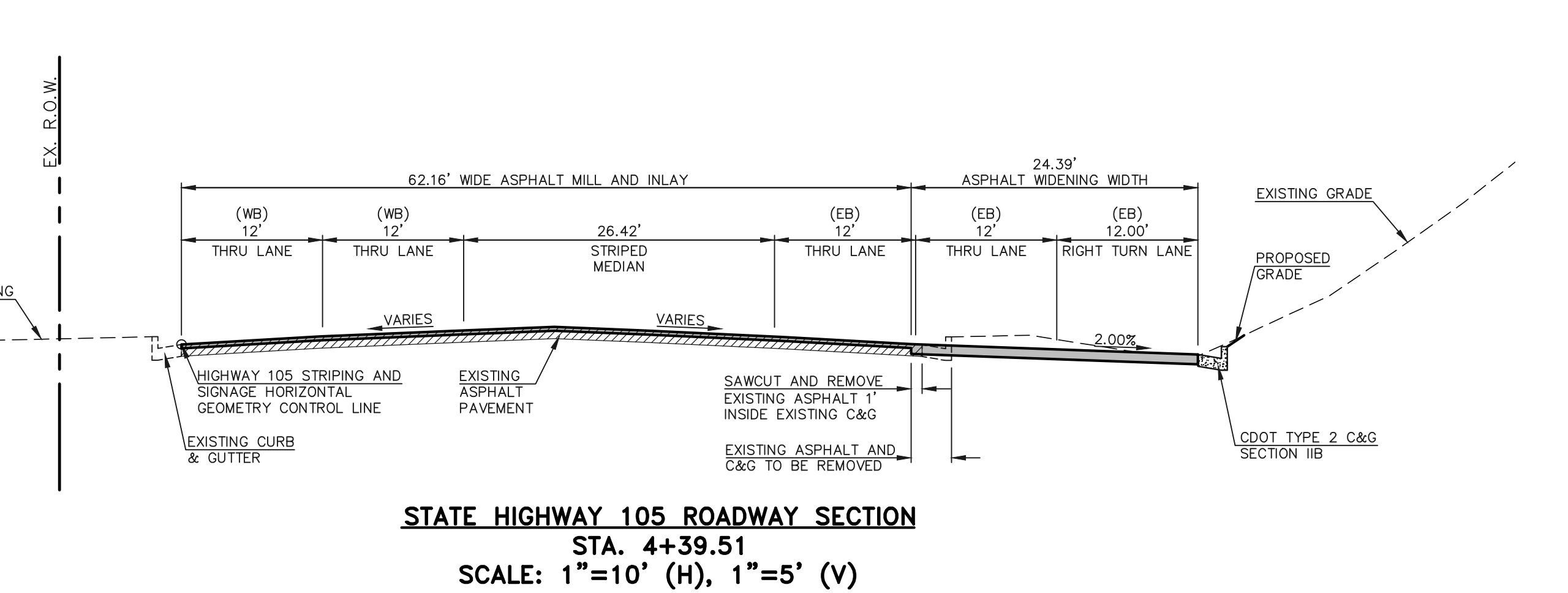
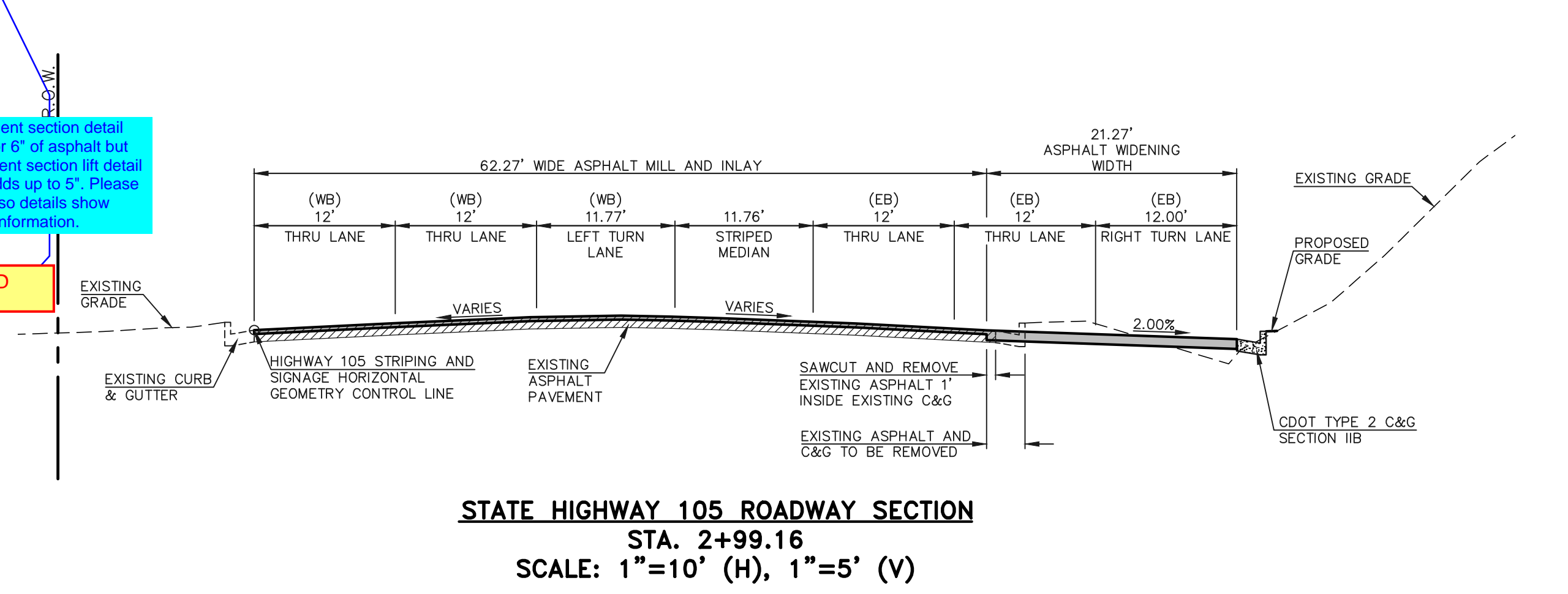
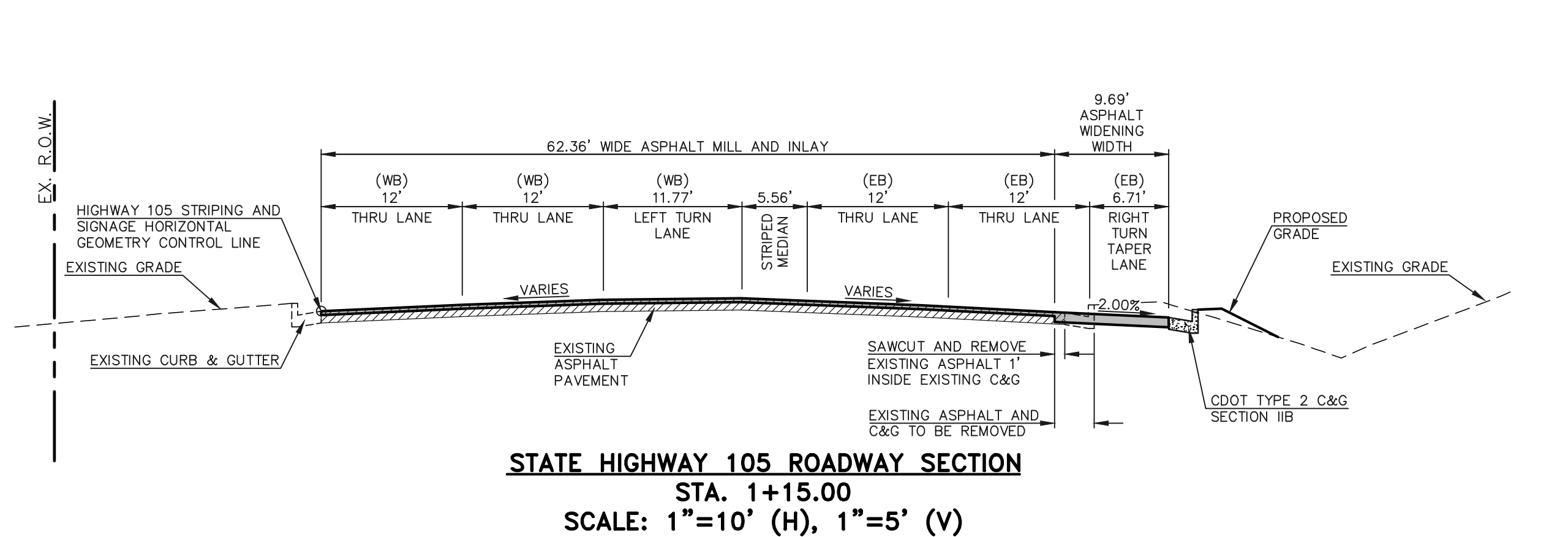
| | |
|---|-------------|
| TOTAL HOT MIX ASPHALT DEPTH | 5.0" |
| # OF LIFTS | 2 |
| HOT MIX ASPHALT BINDER COURSE GRAD-S(75) (PG 64-22) | LIFT 1 3.0" |
| HOT MIX ASPHALT SURFACE COURSE GRAD-SX(75) (PG 64-28) | LIFT 2 2.0" |



SUBGRADE NOTES:
SUBGRADE FOR CUT CONDITIONS:
 IF SUBGRADE IS R >= 17, THEN PERFORM CEMENT TREATED SUBGRADE ON TOP 12 INCHES.
 IF SUBGRADE IS R < 17, THEN OVER EXCAVATE 24 INCHES AND REPLACE WITH R >= 17 MATERIAL AND PERFORM CEMENT TREATED SUBGRADE ON TOP 12 INCHES.
 [RECONDITIONING IS SPECIFIED IN SECTION 306 OF THE CDOT STANDARD SPECIFICATIONS].
SUBGRADE FOR FILL CONDITIONS:
 ALL FILL SHALL BE R >= 17 MATERIAL WITH 12 INCHES OF CEMENT TREATED SUBGRADE ON TOP OF FILL MATERIAL.



- HIGHWAY 105 TYPICAL SECTION NOTES**
- BREAK POINTS IN SLOPES SHALL BE ROUNDED BY THE CONTRACTOR FOR A PLEASING APPEARANCE DURING CONSTRUCTION.
 - SEE HIGHWAY 105 IMPROVEMENT PLANS (SHEETS 9, 10 AND 17) FOR ADDITIONAL INFORMATION ON THE ROADWAY SECTION DIMENSIONS.
 - A TWO-INCH OVERLAY OF THE ROADWAY IN AREAS OF RECONSTRUCTION AND/OR RE-STRIPING IS REQUIRED. THE WIDTH OF THE OVERLAY SHALL ENCOMPASS ALL LANES CONTIGUOUS TO AREAS OF RECONSTRUCTION AND/OR RE-STRIPING.
 - SEE PAVEMENT RECOMMENDATIONS JACKSON CREEK PARKWAY AND U.S. STATE HIGHWAY 105 - GEOTECHNICAL RECOMMENDATION REPORT BY ENTECH ENGINEERING, INC. "ENTECH JOB NO.: 230008" DATED FEBRUARY 20, 2023 FOR INFORMATION ON SUBGRADE PREPARATION.
 - THE CONTRACTOR SHALL VERIFY ALL SUBGRADE CONDITIONS IN CUT AREAS PRIOR TO PREPARATION AND PAVING.
 - FOR FILL CONDITIONS, THE CONTRACTOR SHALL VERIFY SUBGRADE CONDITIONS THROUGH COMPACTION AND DENSITY TESTING PRIOR TO PAVING.
 - SMOOTHNESS CATEGORY SHALL BE CATEGORY 2.



Pavement section detail calls for 6" of asphalt but pavement section lift detail only adds up to 5". Please revise so details show same information.

REVISED

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| NO. | REVISION | DATE |
|-----|---------------------------------------|---------|
| 1 | ADDED SECTION SHEET PER CDOT COMMENTS | 7/10/23 |
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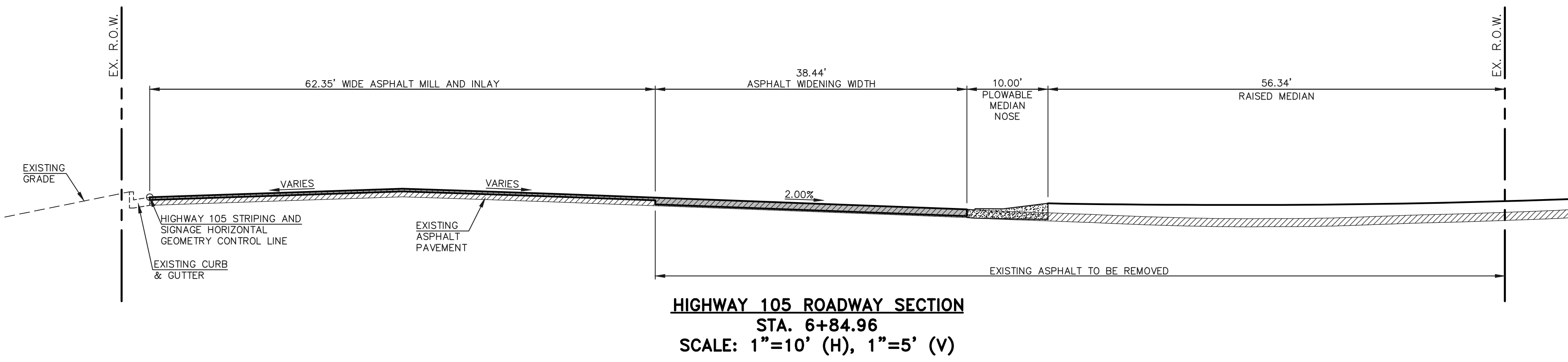
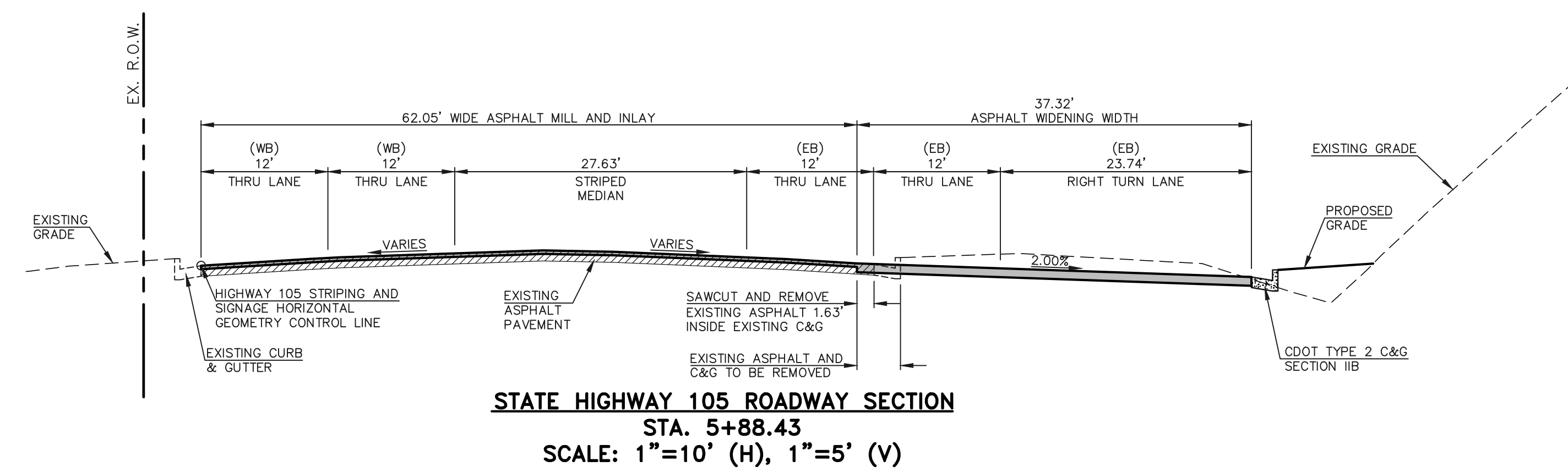
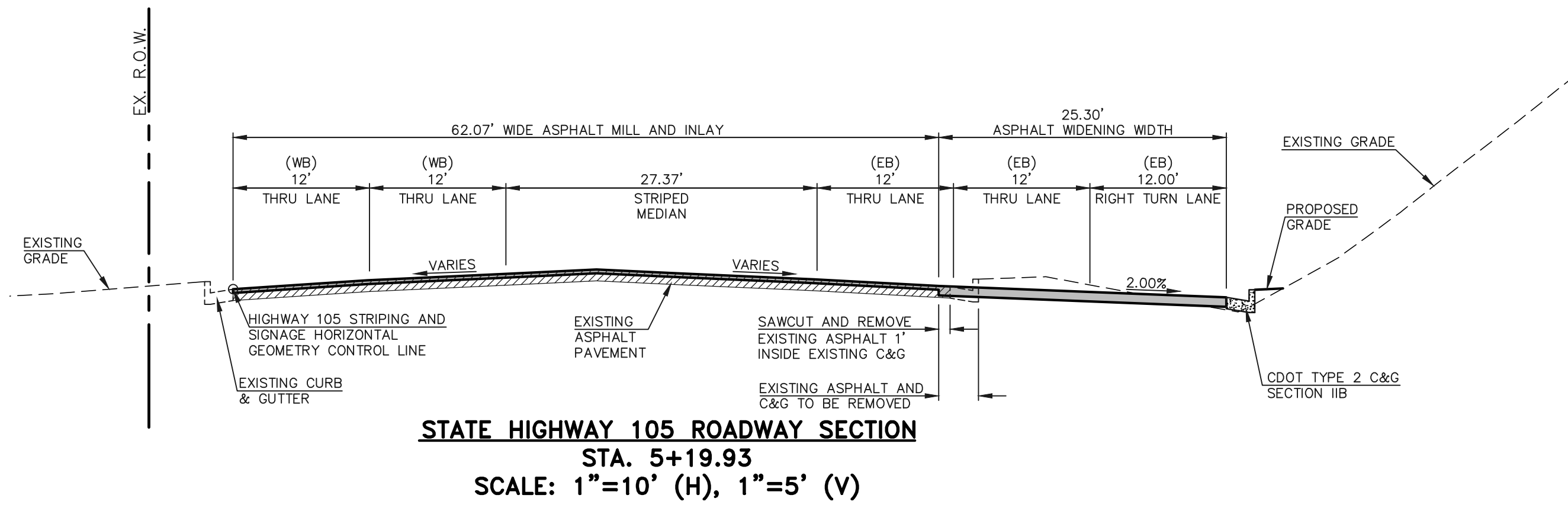
REVIEW:
 PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC A. WHORTON, COLORADO P.E. #37155 DATE



STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
 ROADWAY IMPROVEMENT PLANS
 HIGHWAY 105 ROADWAY SECTIONS

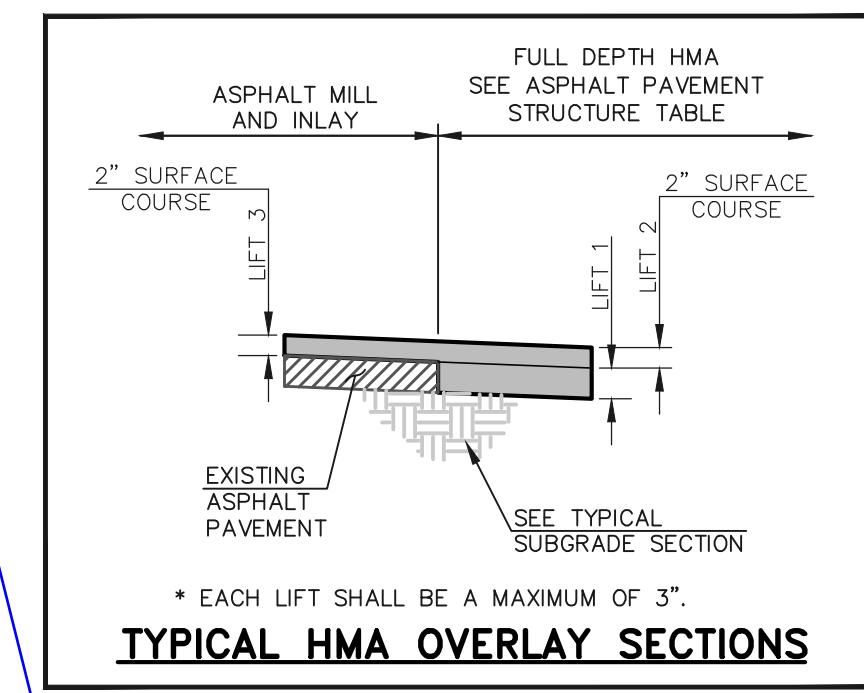
DESIGNED BY PRA SCALE DATE 02-23-23
 DRAWN BY PRA (H) 1"= 50' SHEET 8 OF 26
 CHECKED BY (V) 1"= N/A JOB NO. 1302.22



HOT MIX ASPHALT PAVEMENT STRUCTURE TABLE

| | |
|---|-------------|
| TOTAL HOT MIX ASPHALT DEPTH | 5.0" |
| # OF LIFTS | 2 |
| HOT MIX ASPHALT BINDER COURSE GRAD-S(75) (PG 64-22) | LIFT 1 3.0" |
| HOT MIX ASPHALT SURFACE COURSE GRAD-SX(75) (PG 64-28) | LIFT 2 2.0" |

TYPICAL HOT MIX ASPHALT PAVEMENT LIFT SECTION



SUBGRADE NOTES:

SUBGRADE FOR CUT CONDITIONS:
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SUBGRADE FOR FILL CONDITIONS:
 IF SUBGRADE IS R < 17, THEN OVER EXCAVATE 24 INCHES AND REPLACE WITH R >= 17 MATERIAL AND PERFORM CEMENT TREATED SUBGRADE ON TOP 12 INCHES. [RECONDITIONING IS SPECIFIED IN SECTION 306 OF THE CDOT STANDARD SPECIFICATIONS].

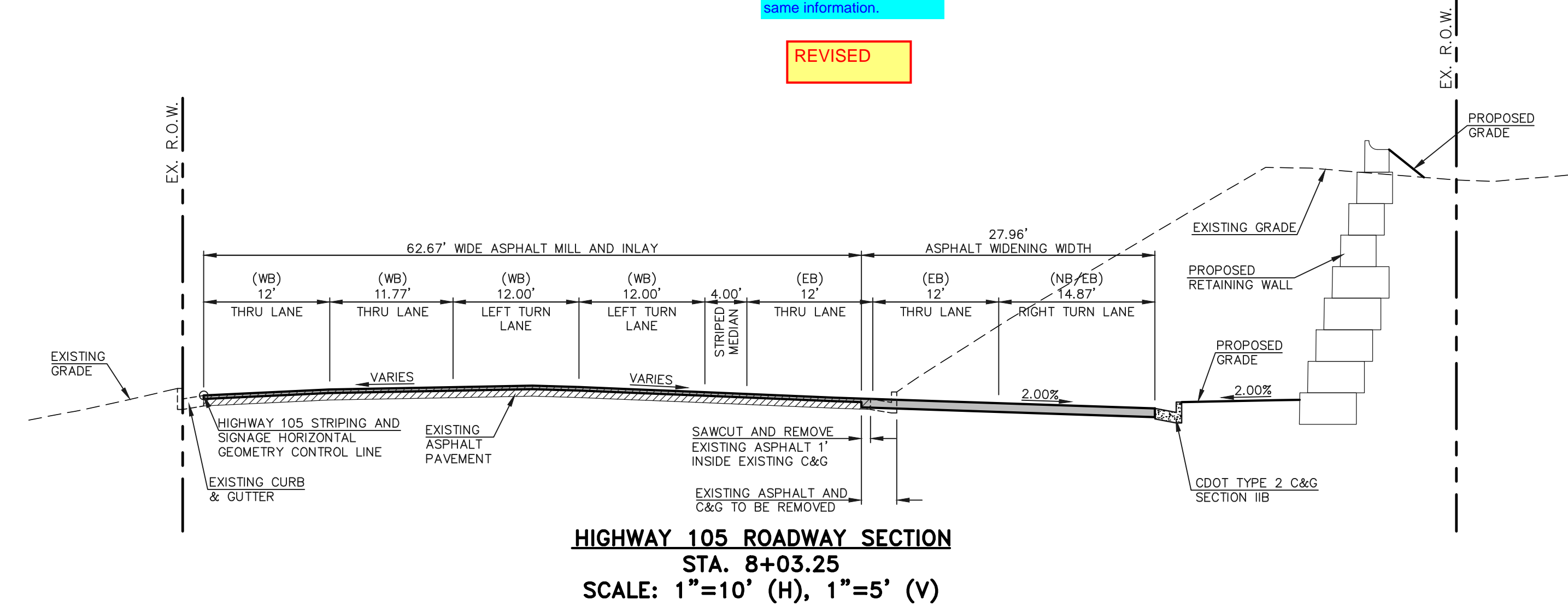
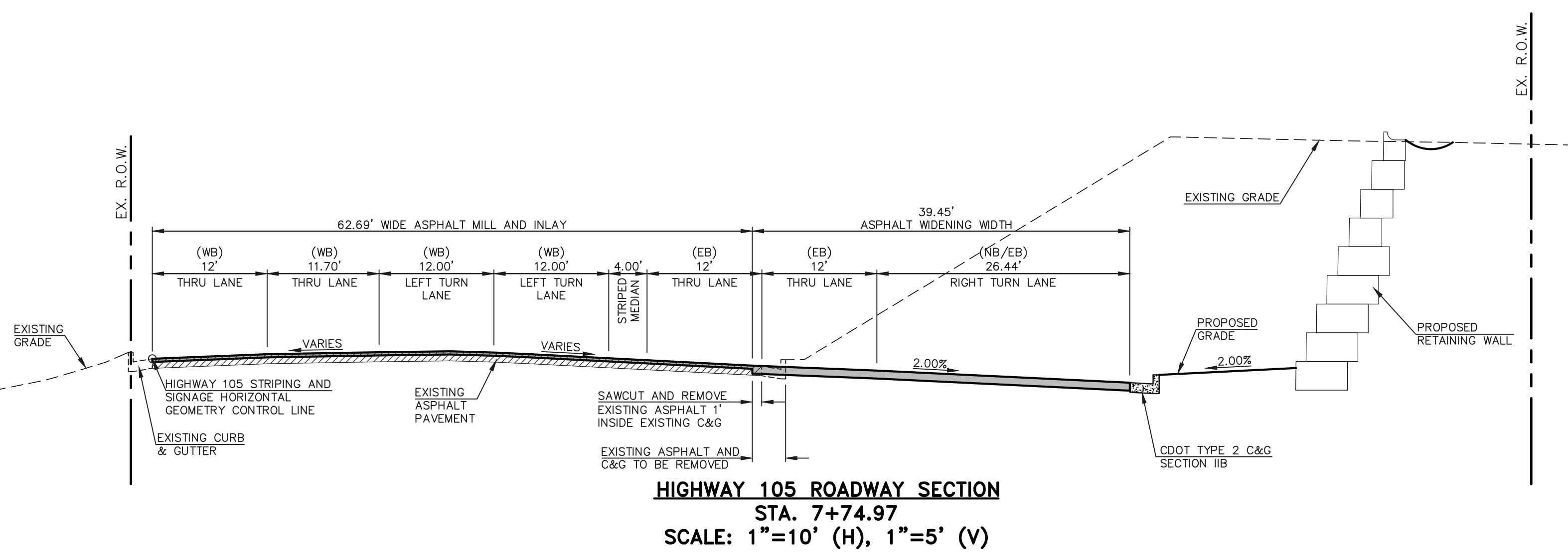
ALL FILL SHALL BE R >= 17 MATERIAL WITH 12 INCHES OF CEMENT TREATED SUBGRADE ON TOP OF FILL MATERIAL.

TYPICAL PAVEMENT SECTION

1. REFERENCE PAVEMENT RECOMMENDATIONS JACKSON CREEK PARKWAY AND U.S. STATE HIGHWAY 105 - GEOTECHNICAL RECOMMENDATION REPORT BY ENTECH ENGINEERING, INC. "ENTECH JOB NO.: 230008" DATED FEBRUARY 20, 2023 FOR INFORMATION ON SUBGRADE PREPARATION

Pavement section detail calls for 6" of asphalt but pavement section lift detail only adds up to 5". Please revise so details show same information.

REVISED



- HIGHWAY 105 TYPICAL SECTION NOTES**
- BREAK POINTS IN SLOPES SHALL BE ROUNDED BY THE CONTRACTOR FOR A PLEASING APPEARANCE DURING CONSTRUCTION.
 - SEE HIGHWAY 105 IMPROVEMENT PLANS (SHEETS 9, 10 AND 17) FOR ADDITIONAL INFORMATION ON THE ROADWAY SECTION DIMENSIONS.
 - A TWO-INCH OVERLAY OF THE ROADWAY IN AREAS OF RECONSTRUCTION AND/OR RE-STRIPING IS REQUIRED. THE WIDTH OF THE OVERLAY SHALL ENCOMPASS ALL LANES CONTIGUOUS TO AREAS OF RECONSTRUCTION AND/OR RE-STRIPING.
 - SEE PAVEMENT RECOMMENDATIONS JACKSON CREEK PARKWAY AND U.S. STATE HIGHWAY 105 - GEOTECHNICAL RECOMMENDATION REPORT BY ENTECH ENGINEERING, INC. "ENTECH JOB NO.: 230008" DATED FEBRUARY 20, 2023 FOR INFORMATION ON SUBGRADE PREPARATION.
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 - FOR FILL CONDITIONS, THE CONTRACTOR SHALL VERIFY SUBGRADE CONDITIONS THROUGH COMPACTION AND DENSITY TESTING PRIOR TO PAVING.
 - SMOOTHNESS CATEGORY SHALL BE CATEGORY 2.

FIX PAGE NUMBERS
 UPDATED

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| NO. | REVISION | DATE |
|-----|---------------------------------------|---------|
| 1 | ADDED SECTION SHEET PER CDOT COMMENTS | 7/10/23 |
| | | |
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REVIEW:

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

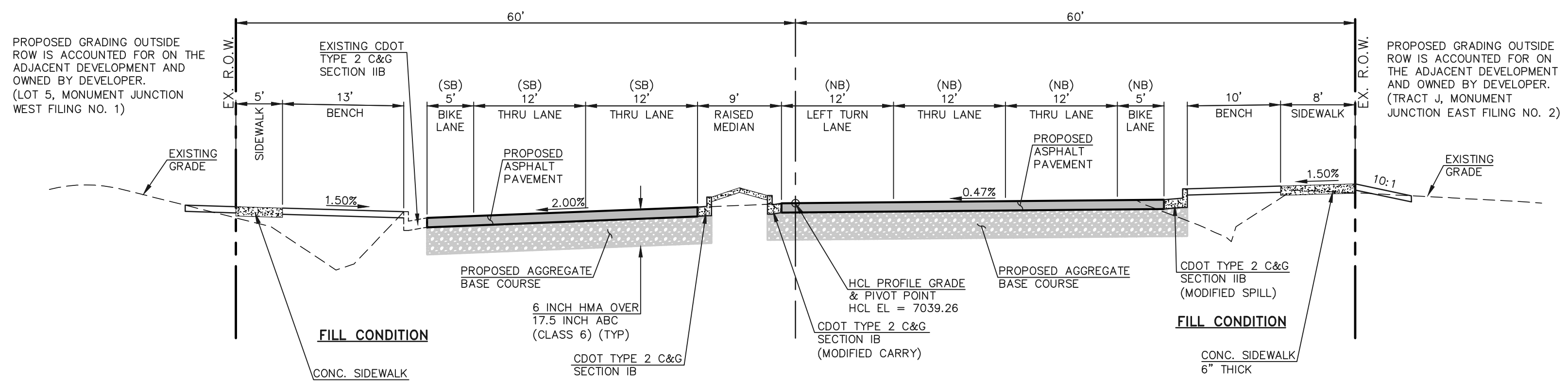
MARC A. WHORTON, COLORADO P.E. #37155 DATE



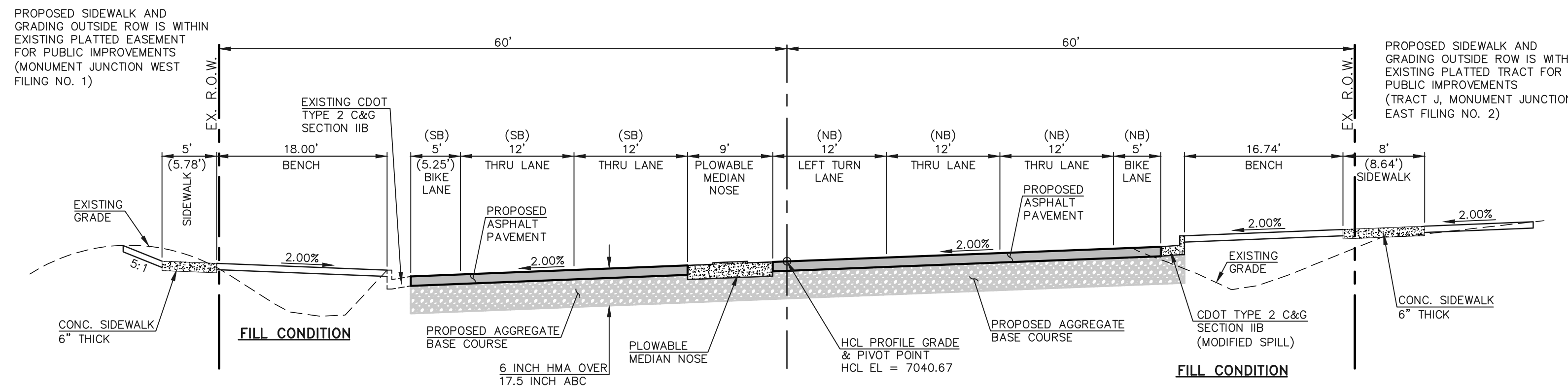
STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
 ROADWAY IMPROVEMENT PLANS
 HIGHWAY 105 ROADWAY SECTIONS

| | | | | |
|-------------|-----|-------------|---------|----------|
| DESIGNED BY | PRA | SCALE | DATE | 02-23-23 |
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| CHECKED BY | | (V) 1"= N/A | JOB NO. | 1302.22 |

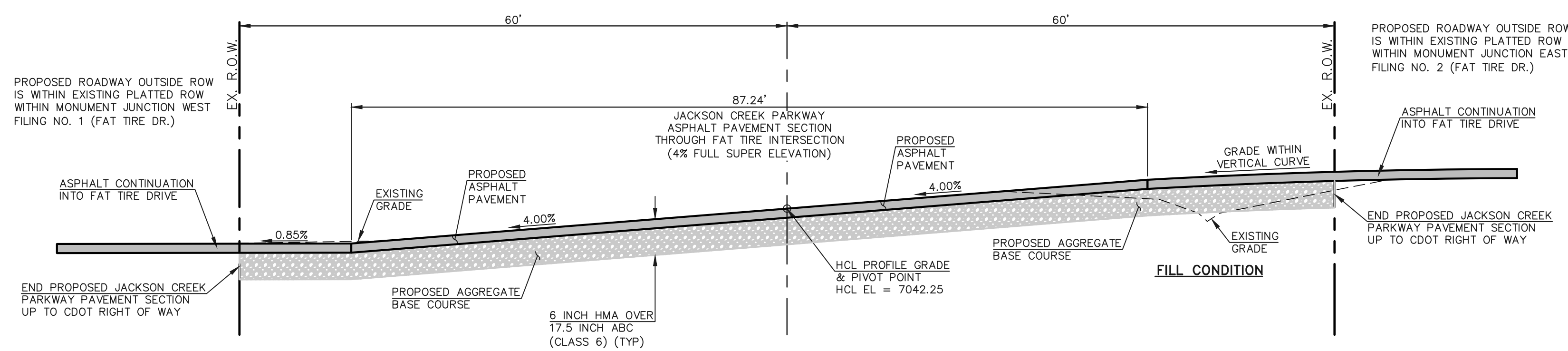
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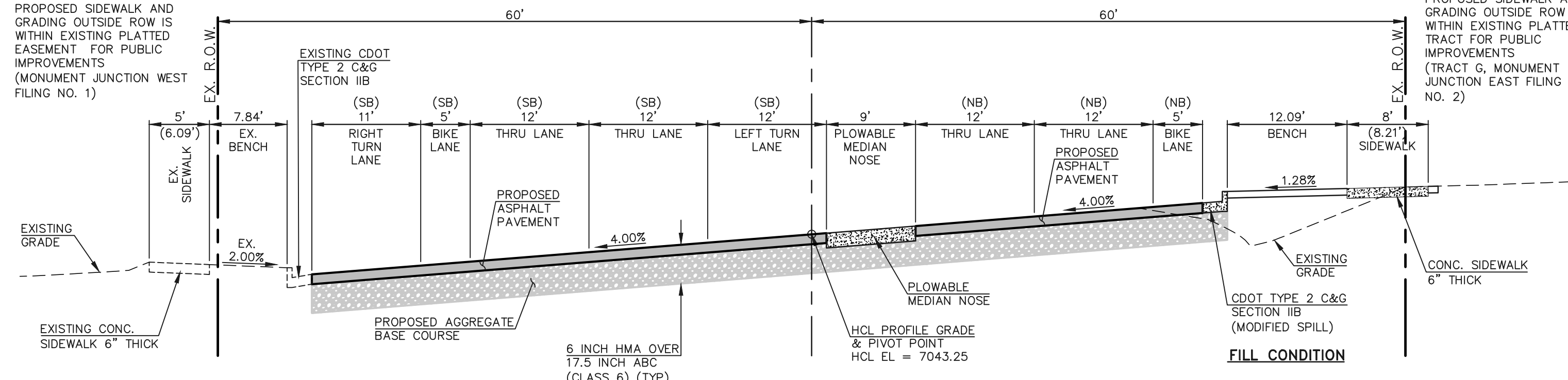
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STA. 18+73.48
SCALE: 1"=10' (H), 1"=5' (V)



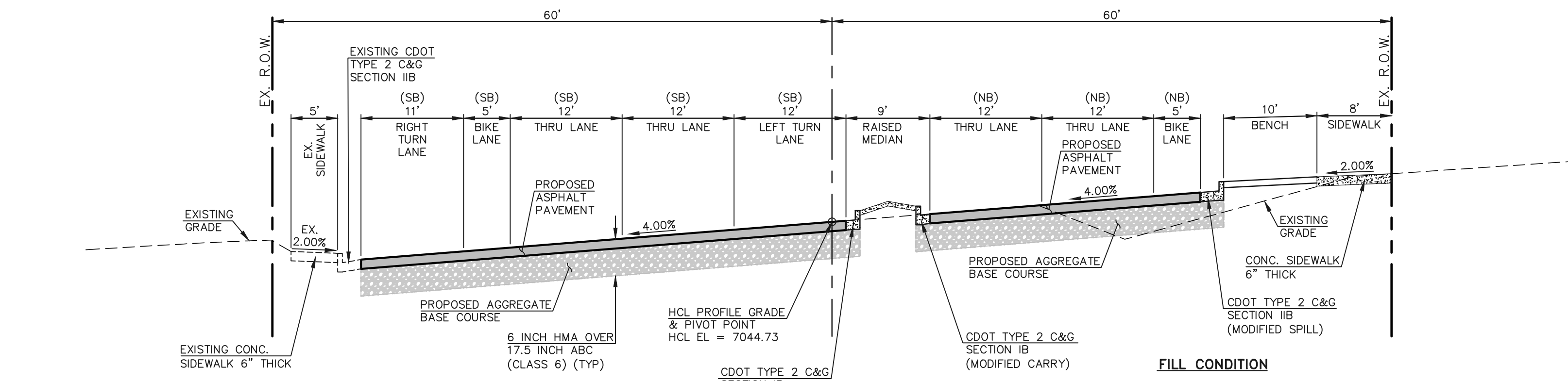
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STA. 19+22.22
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 19+84.20
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 20+34.01
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 22+63.43
SCALE: 1"=10' (H), 1"=5' (V)

TYPICAL SECTION NOTES

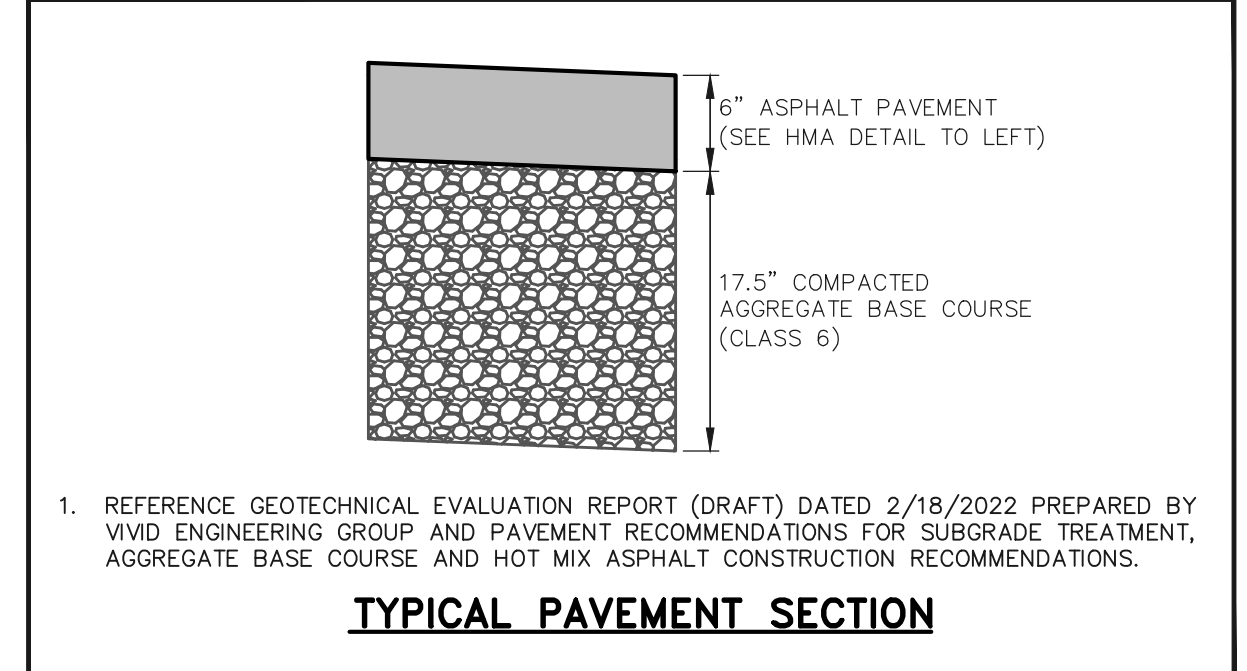
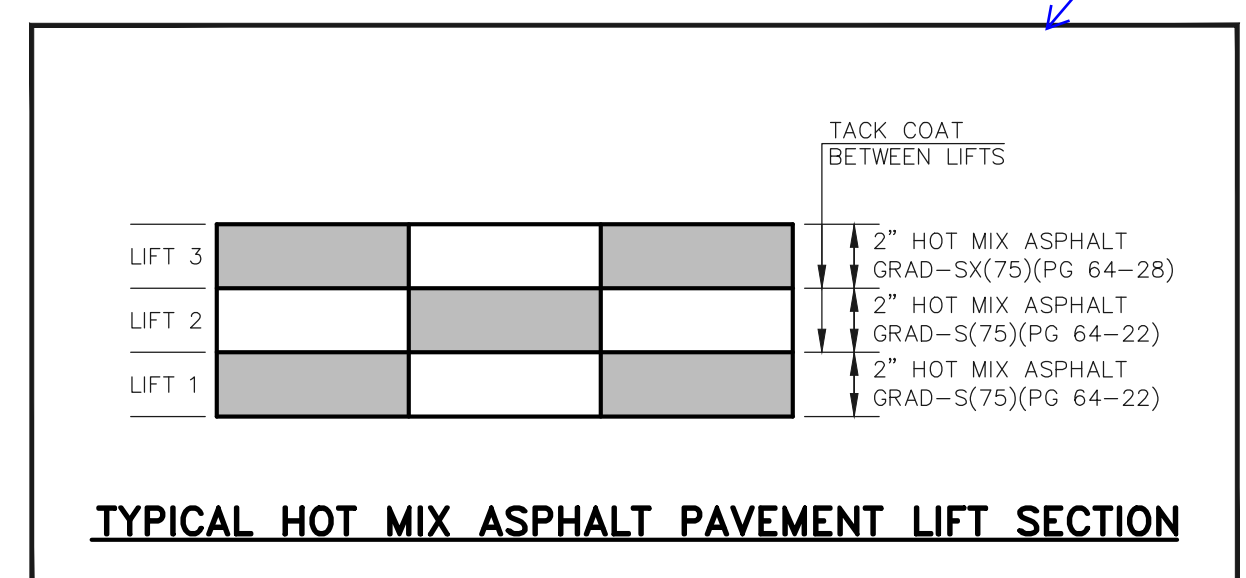
- BREAK POINTS IN SLOPES SHALL BE ROUNDED BY THE CONTRACTOR FOR A PLEASING APPEARANCE DURING CONSTRUCTION.
- SEE JACKSON CREEK PARKWAY IMPROVEMENT PLANS (SHEETS 12, 13 AND 17) FOR ADDITIONAL INFORMATION ON THE ROADWAY SECTION DIMENSIONS.
- A TWO-INCH OVERLAY OF THE ROADWAY IN AREAS OF RECONSTRUCTION AND/OR RE-STRIPING IS REQUIRED. THE WIDTH OF THE OVERLAY SHALL ENCOMPASS ALL LANES CONTIGUOUS TO AREAS OF RECONSTRUCTION AND/OR RE-STRIPING.
- SEE PROPOSED JACKSON CREEK PARKWAY WIDENING, HIGHWAY 105 TO HIGBY ROAD - GEOTECHNICAL EVALUATION REPORT (DRAFT) BY VIVID ENGINEERING GROUP "VIVID PROJECT NO.: D21-2-456" DATED 2-18-2022 FOR INFORMATION ON SUBGRADE PREPARATION.
- THE CONTRACTOR SHALL VERIFY ALL SUBGRADE CONDITIONS IN CUT AREAS PRIOR TO PREPARATION AND PAVING.
- FOR FILL CONDITIONS, THE CONTRACTOR SHALL VERIFY SUBGRADE CONDITIONS THROUGH COMPACTION AND DENSITY TESTING PRIOR TO PAVING.
- SMOOTHNESS CATEGORY SHALL BE CATEGORY 2.

HOT MIX ASPHALT PAVEMENT STRUCTURE TABLE

| | |
|--------------------------------|-------------|
| TOTAL HOT MIX ASPHALT DEPTH | 6.0" |
| # OF LIFTS | 3 |
| HOT MIX ASPHALT BINDER COURSE | LIFT 1 2.0" |
| GRAD-S(75) (PG 64-22) | LIFT 2 2.0" |
| HOT MIX ASPHALT SURFACE COURSE | LIFT 3 2.0" |
| GRAD-SX(75) (PG 64-28) | |

Asphalt lift is different than previous sheets. Please revise to same detail or clearly state where different lift sections are happening within plan.

THERE IS A DIFFERENT PAVEMENT DESIGN FOR JCP VS. HWY. 105. NOTE NOW ADDED TO EXPLAIN.



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| NO. | REVISION | DATE |
|-----|-----------------------------|---------|
| 1 | REVISED PER JACOBS COMMENTS | 5/17/23 |

REVIEW:
 PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

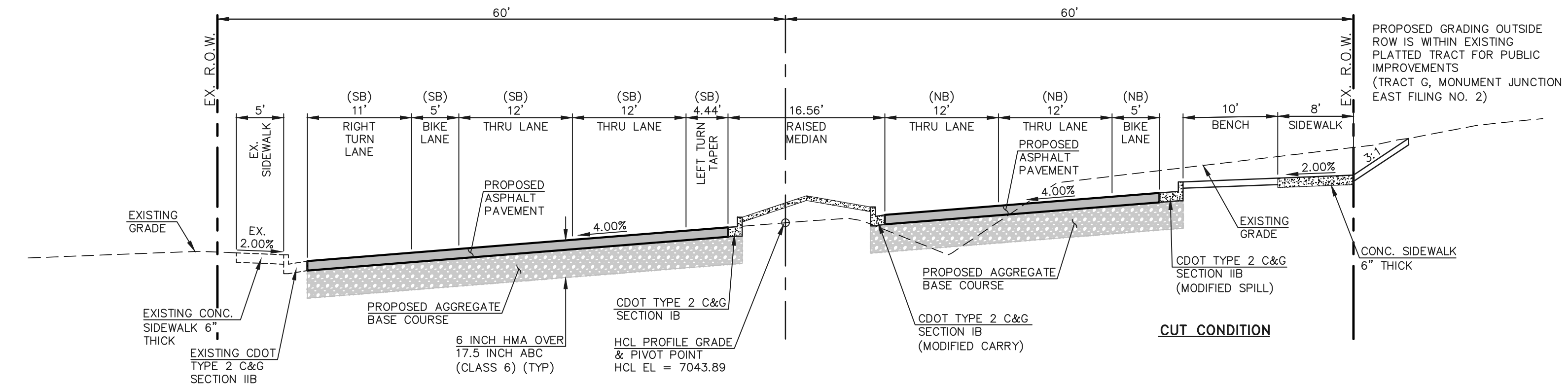
MARC A. WHORTON, COLORADO P.E. #37155 DATE



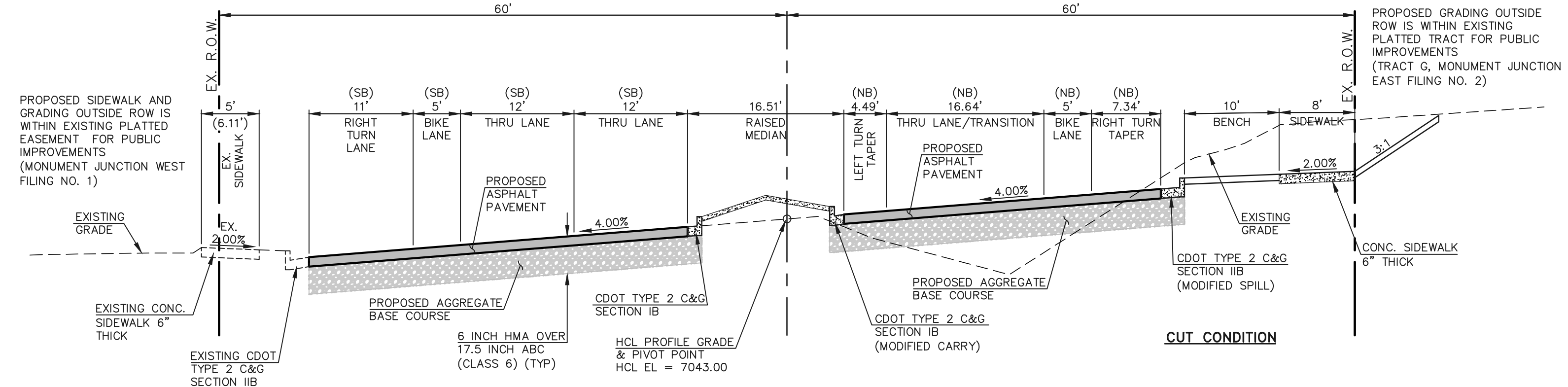
STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
 ROADWAY IMPROVEMENT PLANS
 JACKSON CREEK PKWY. ROADWAY SECTIONS

DESIGNED BY PRA SCALE DATE 02-23-23
 DRAWN BY PRA (H) 1"= 50' SHEET 10 OF 26
 CHECKED BY (V) 1"= N/A JOB NO. 1302.22

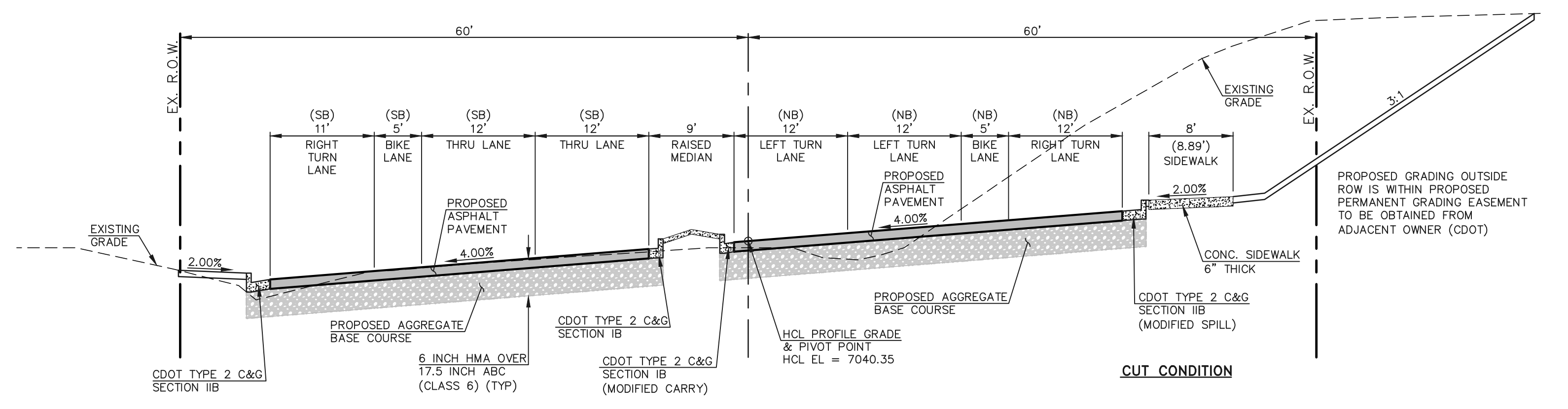




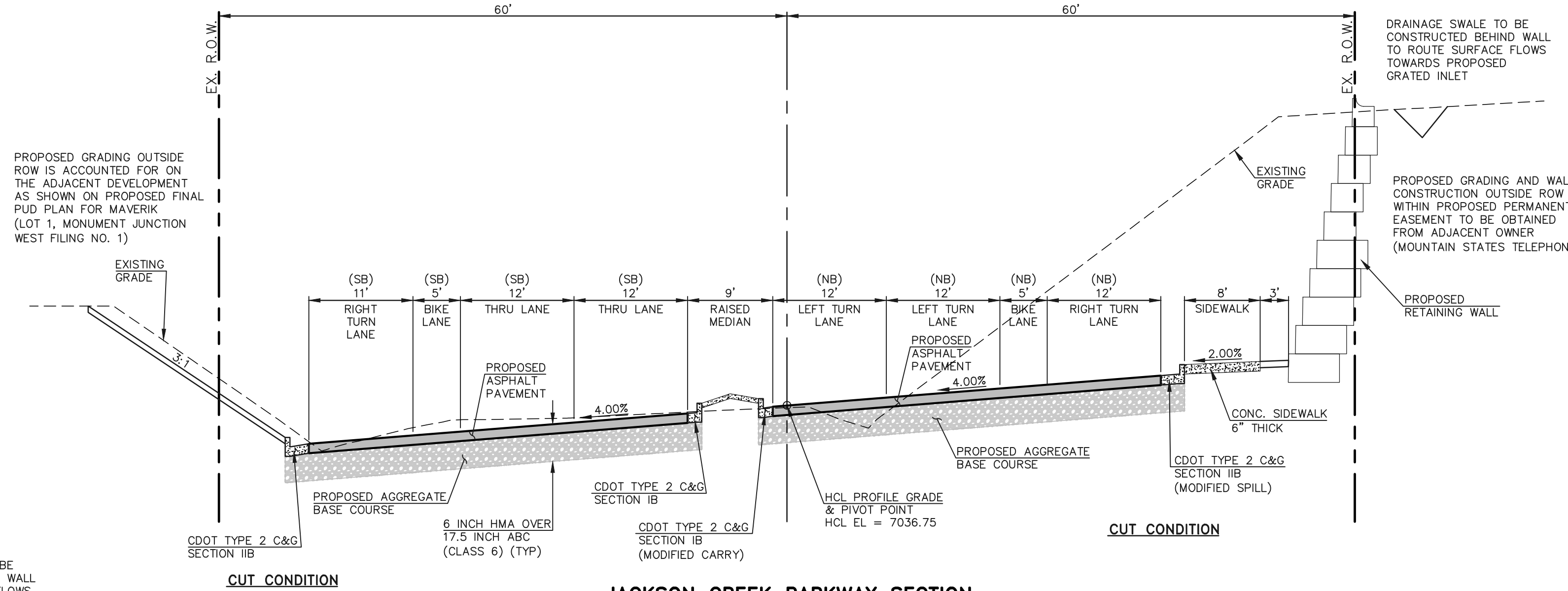
JACKSON CREEK PARKWAY SECTION
STA. 23+55.45
SCALE: 1"=10' (H), 1"=5' (V)



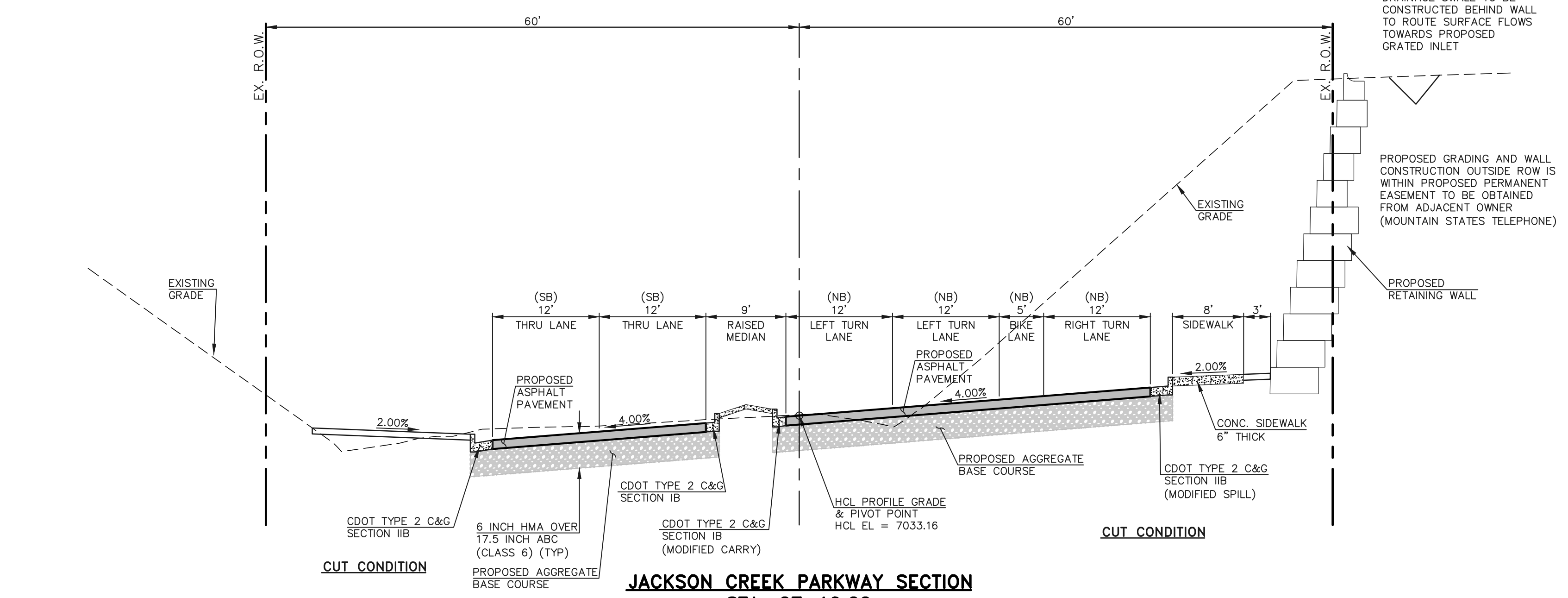
JACKSON CREEK PARKWAY SECTION
STA. 24+09.60
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 25+26.86
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 26+29.82
SCALE: 1"=10' (H), 1"=5' (V)



JACKSON CREEK PARKWAY SECTION
STA. 27+10.22
SCALE: 1"=10' (H), 1"=5' (V)

TYPICAL SECTION NOTES

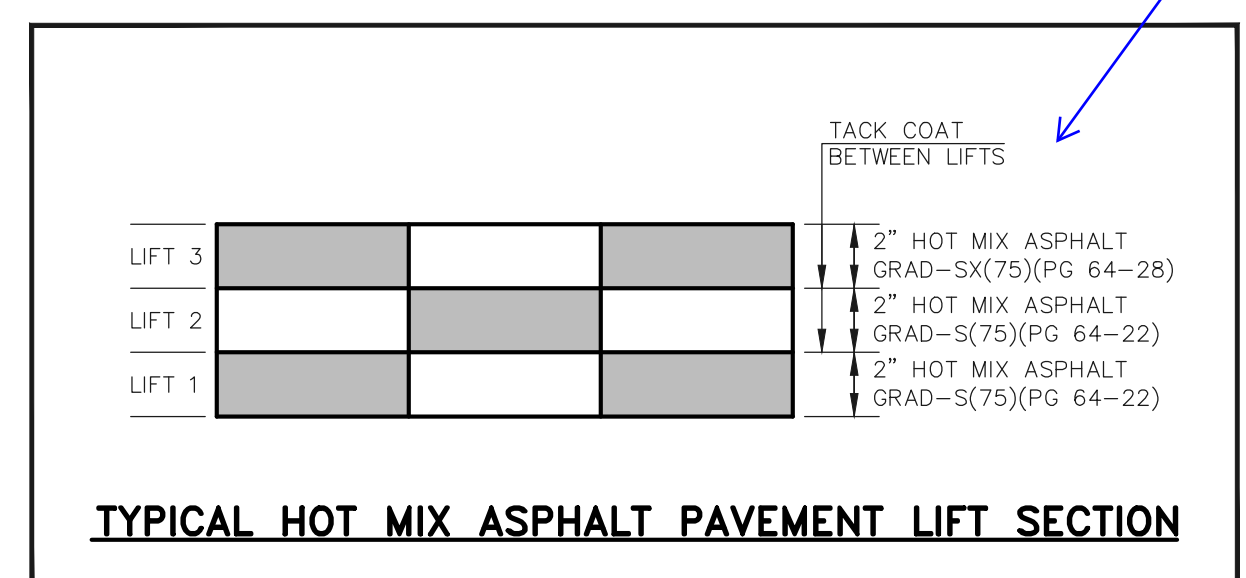
- BREAK POINTS IN SLOPES SHALL BE ROUNDED BY THE CONTRACTOR FOR A PLEASING APPEARANCE DURING CONSTRUCTION.
- SEE JACKSON CREEK PARKWAY IMPROVEMENT PLANS (SHEETS 12, 13 AND 17) FOR ADDITIONAL INFORMATION ON THE ROADWAY SECTION DIMENSIONS.
- A TWO-INCH OVERLAY OF THE ROADWAY IN AREAS OF RECONSTRUCTION AND/OR RE-STRIPING IS REQUIRED. THE WIDTH OF THE OVERLAY SHALL ENCOMPASS ALL LANES CONTIGUOUS TO AREAS OF RECONSTRUCTION AND/OR RE-STRIPING.
- SEE PROPOSED JACKSON CREEK PARKWAY WIDENING, HIGHWAY 105 TO HIGBY ROAD - GEOTECHNICAL EVALUATION REPORT (DRAFT) BY VIVID ENGINEERING GROUP "VIVID PROJECT NO.: D21-2-456" DATED 2-18-2022 FOR INFORMATION ON SUBGRADE PREPARATION.
- THE CONTRACTOR SHALL VERIFY ALL SUBGRADE CONDITIONS IN CUT AREAS PRIOR TO PREPARATION AND PAVING.
- FOR FILL CONDITIONS, THE CONTRACTOR SHALL VERIFY SUBGRADE CONDITIONS THROUGH COMPACTION AND DENSITY TESTING PRIOR TO PAVING.
- SMOOTHNESS CATEGORY SHALL BE CATEGORY 2.

HOT MIX ASPHALT PAVEMENT STRUCTURE TABLE

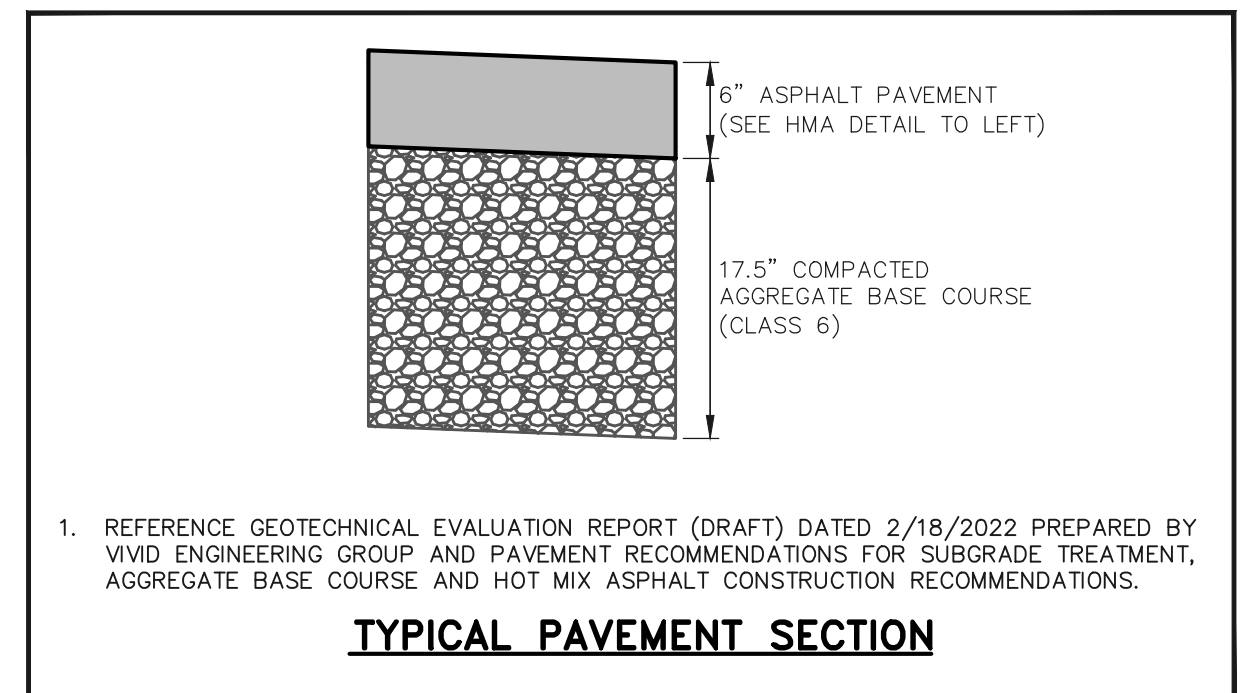
| | |
|---|-------------|
| TOTAL HOT MIX ASPHALT DEPTH | 6.0" |
| # OF LIFTS | 3 |
| HOT MIX ASPHALT BINDER COURSE GRAD-S(75) (PG 64-22) | LIFT 1 2.0" |
| HOT MIX ASPHALT SURFACE COURSE GRAD-SX(75) (PG 64-28) | LIFT 2 2.0" |
| HOT MIX ASPHALT SURFACE COURSE GRAD-S(75) (PG 64-22) | LIFT 3 2.0" |

Asphalt lift is different than previous sheets. Please revise to same detail or clearly state where different lift sections are happening within plan.

THERE IS A DIFFERENT PAVEMENT DESIGN FOR JCP VS. HWY. 105. NOTE NOW ADDED TO EXPLAIN.



TYPICAL HOT MIX ASPHALT PAVEMENT LIFT SECTION



TYPICAL PAVEMENT SECTION

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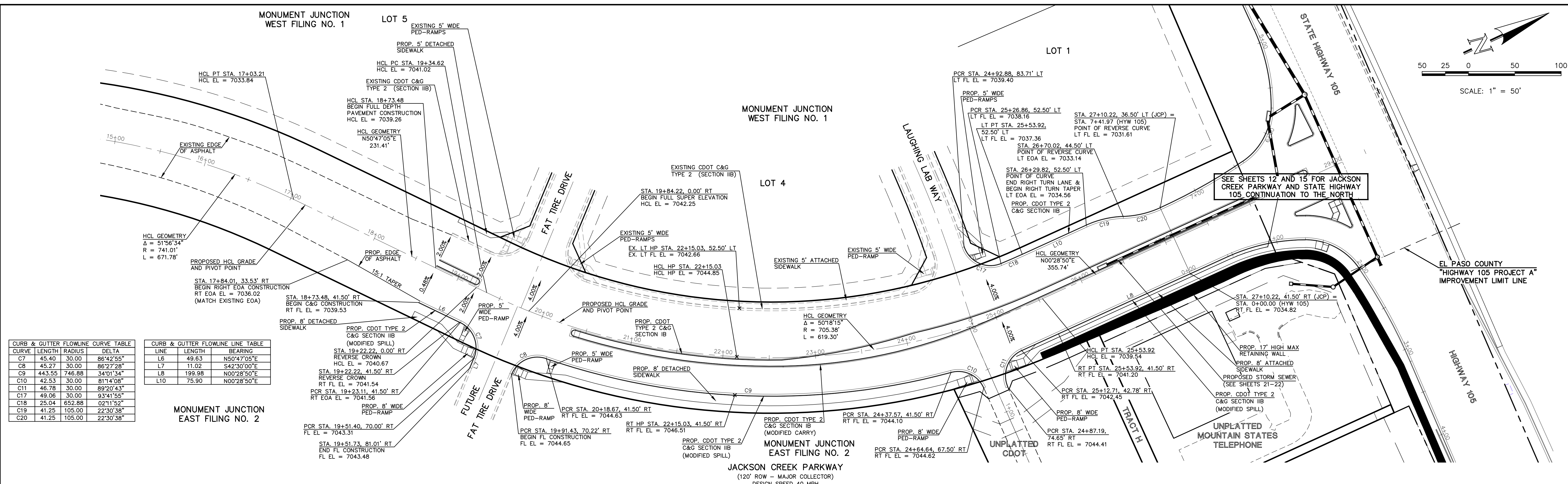
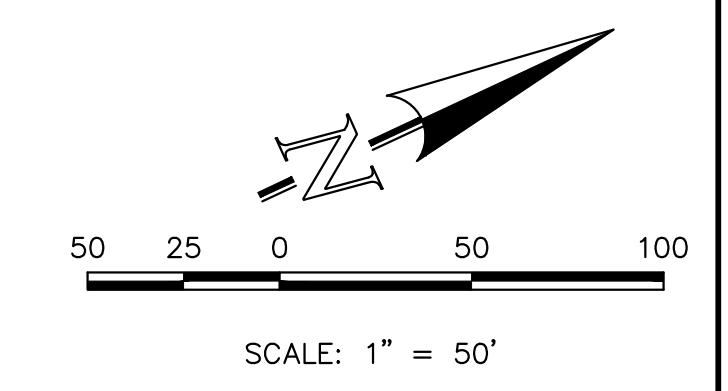
| NO. | REVISION | DATE | REVIEW: |
|-----|-----------------------------|---------|---|
| 1 | REVISED PER JACOBS COMMENTS | 5/17/23 | PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC |
| | | | MARC A. WHORTON, COLORADO P.E. #37155 |



STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
 ROADWAY IMPROVEMENT PLANS
 JACKSON CREEK PKWY. ROADWAY SECTIONS

DESIGNED BY: PRA SCALE: DATE: 02-23-23
 DRAWN BY: PRA (H) 1"= 50' SHEET 11 OF 26
 CHECKED BY: (V) 1"= N/A JOB NO. 1302.22





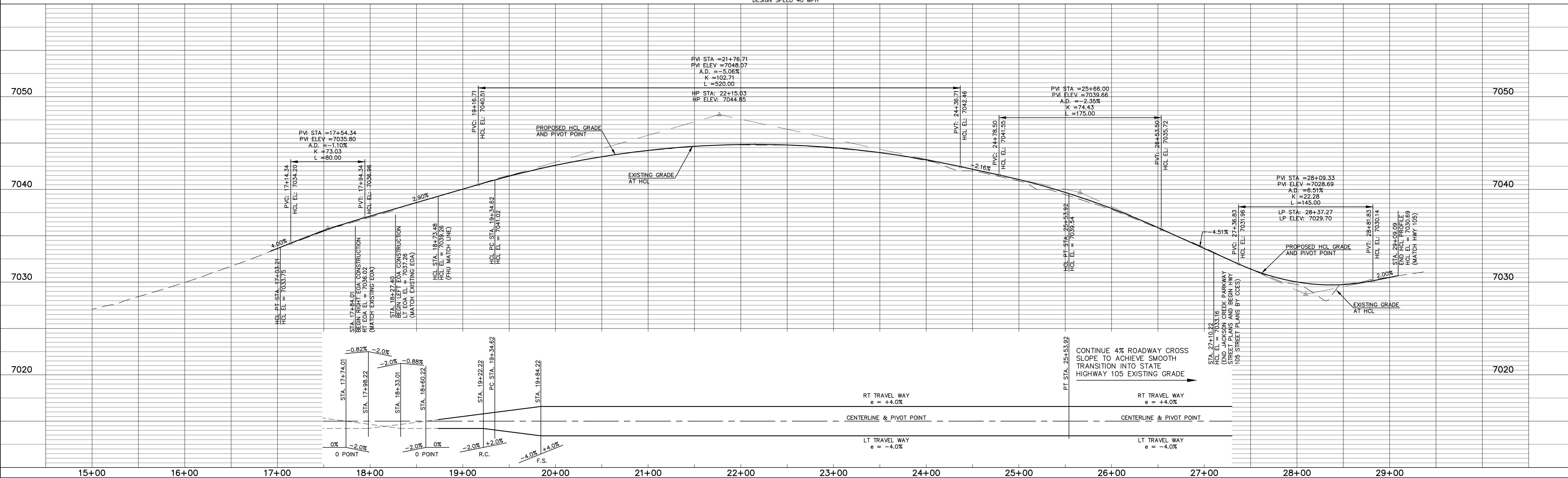
CURB & GUTTER FLOWLINE CURVE TABLE

| CURVE | LENGTH | RADIUS | DELTA |
|-------|--------|--------|-----------|
| C7 | 45.40 | 30.00 | 86°42'55" |
| C8 | 45.27 | 30.00 | 86°27'28" |
| C9 | 443.55 | 746.88 | 34°01'34" |
| C10 | 42.53 | 30.00 | 81°14'08" |
| C11 | 46.78 | 30.00 | 89°20'43" |
| C17 | 49.06 | 30.00 | 93°41'55" |
| C18 | 25.04 | 652.88 | 02°11'52" |
| C19 | 41.25 | 105.00 | 22°30'38" |
| C20 | 41.25 | 105.00 | 22°30'38" |

CURB & GUTTER FLOWLINE LINE TABLE

| LINE | LENGTH | BEARING |
|------|--------|-------------|
| L6 | 49.63 | N50°47'05"E |
| L7 | 11.02 | S42°30'00"E |
| L8 | 199.98 | N00°28'50"E |
| L10 | 75.90 | N00°28'50"E |

MONUMENT JUNCTION EAST FILING NO. 2



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| NO. | REVISION | DATE |
|-----|---|---------|
| 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 |

REVIEW:
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC. A. WHORTON, COLORADO P.E. #37155 DATE

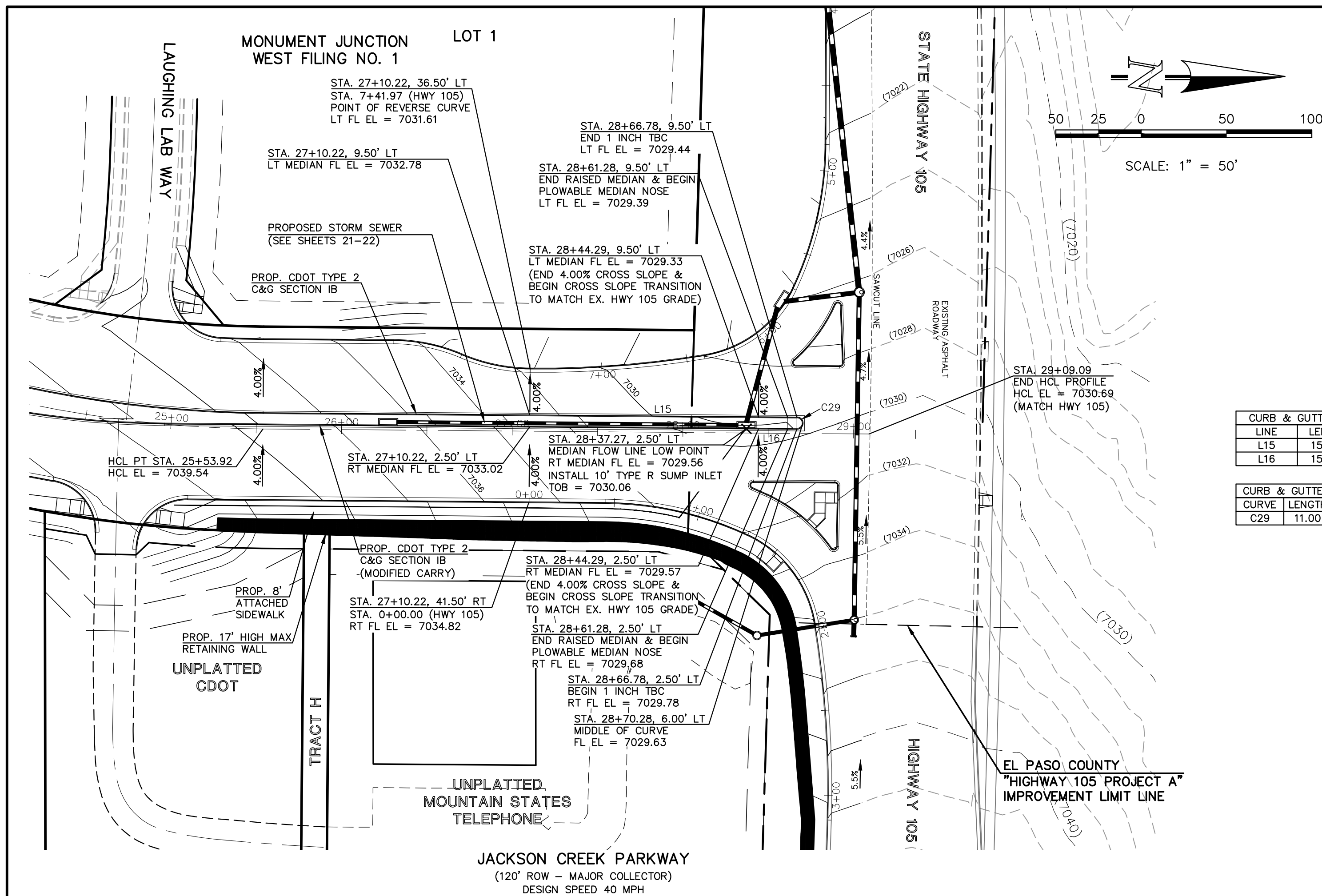
CLASSIC
CONSULTING ENGINEERS & SURVEYORS

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
ROADWAY IMPROVEMENT PLANS
JACKSON CREEK PKWY. PLAN & PROFILE

| | | | | |
|-------------|-------------|--------------|---------|----------|
| DESIGNED BY | PRA | SCALE | DATE | 02-23-23 |
| DRAWN BY | PRA | (H) 1" = 50' | SHEET | 13 OF 26 |
| CHECKED BY | (V) 1" = 5' | JOB NO. | 1302.21 | |

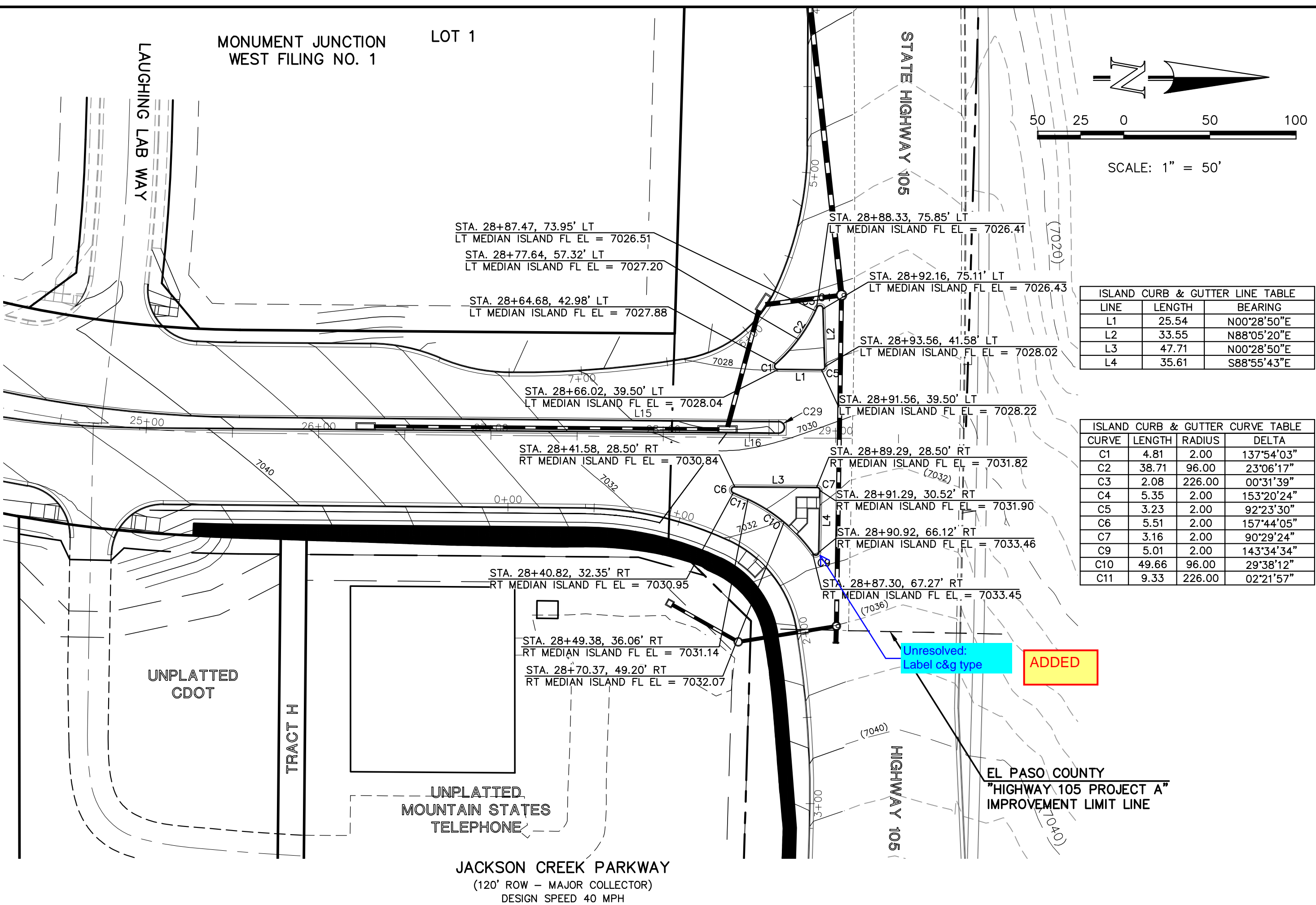
619 N. Cascade Avenue, Suite 200
Colorado Springs, Colorado 80903 (719) 785-0790
(719) 785-0799 (fax)

V:\130222\PRAMING\CONSTRUCTION\DWG\13-01-02.dwg, 7/19/2023 3:27:29 PM, cwhorton, 1:1



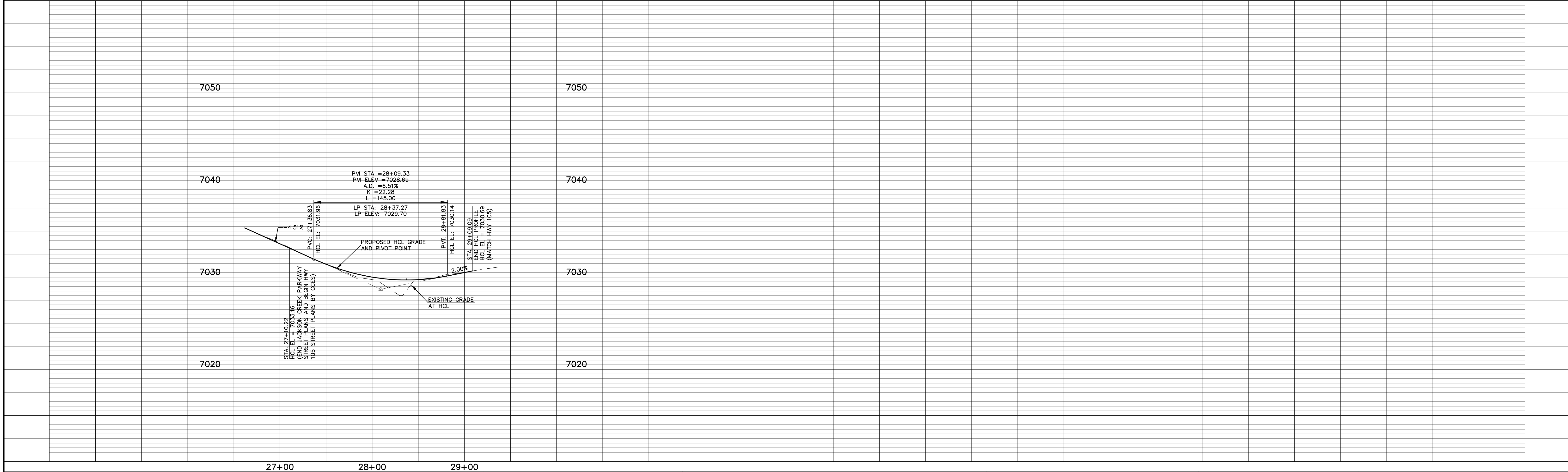
| LINE | LENGTH | BEARING |
|------|--------|-------------|
| L15 | 156.56 | N00°28'50"E |
| L16 | 156.56 | N00°28'50"E |

| CURVE | LENGTH | RADIUS | DELTA |
|-------|--------|--------|------------|
| C29 | 11.00 | 3.50 | 180°00'00" |



| LINE | LENGTH | BEARING |
|------|--------|-------------|
| L1 | 25.54 | N00°28'50"E |
| L2 | 33.55 | N88°05'20"E |
| L3 | 47.71 | N00°28'50"E |
| L4 | 35.61 | S88°55'43"E |

| CURVE | LENGTH | RADIUS | DELTA |
|-------|--------|--------|------------|
| C1 | 4.81 | 2.00 | 137°54'03" |
| C2 | 38.71 | 96.00 | 23°06'17" |
| C3 | 2.08 | 226.00 | 00°31'39" |
| C4 | 5.35 | 2.00 | 153°20'24" |
| C5 | 3.23 | 2.00 | 92°23'30" |
| C6 | 5.51 | 2.00 | 157°44'05" |
| C7 | 3.16 | 2.00 | 90°29'24" |
| C9 | 5.01 | 2.00 | 143°34'34" |
| C10 | 49.66 | 96.00 | 29°38'12" |
| C11 | 9.33 | 226.00 | 02°21'57" |



| NO. | REVISION | DATE |
|-----|---|---------|
| 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 |

REVIEW: PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC. A. WHORTON, COLORADO P.E. #37155 DATE

STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
ROADWAY IMPROVEMENT PLANS
INTERSECTION RAISED MEDIAN PLAN

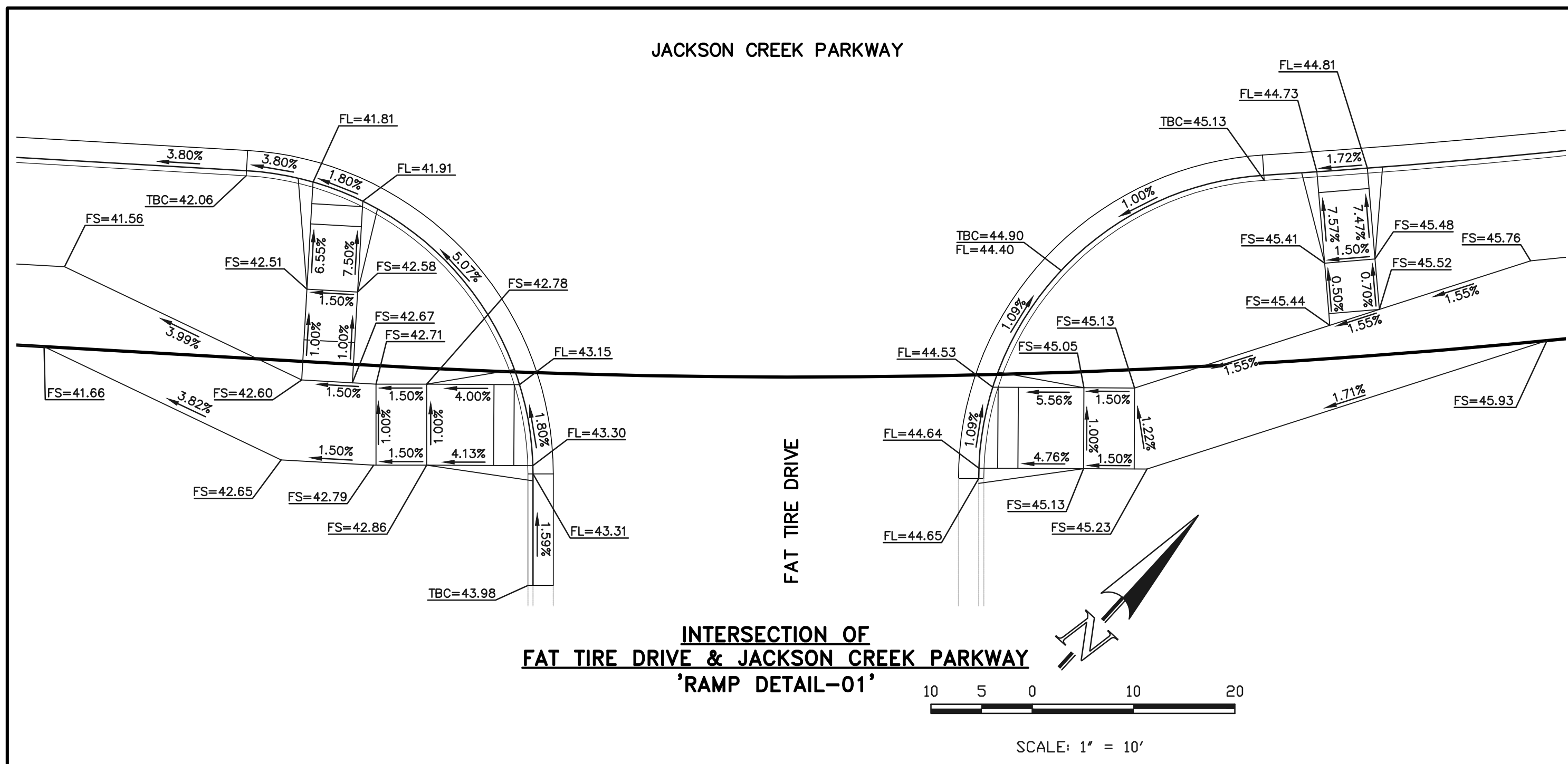
DESIGNED BY PRA SCALE DATE 02-23-23
DRAWN BY PRA (H) 1" = 50' SHEET 15 OF 26
CHECKED BY (V) 1" = 5' JOB NO. 1302.22

619 N. Cascade Avenue, Suite 200 Colorado Springs, Colorado 80903 (719) 785-0790 (719) 785-0799(fax)

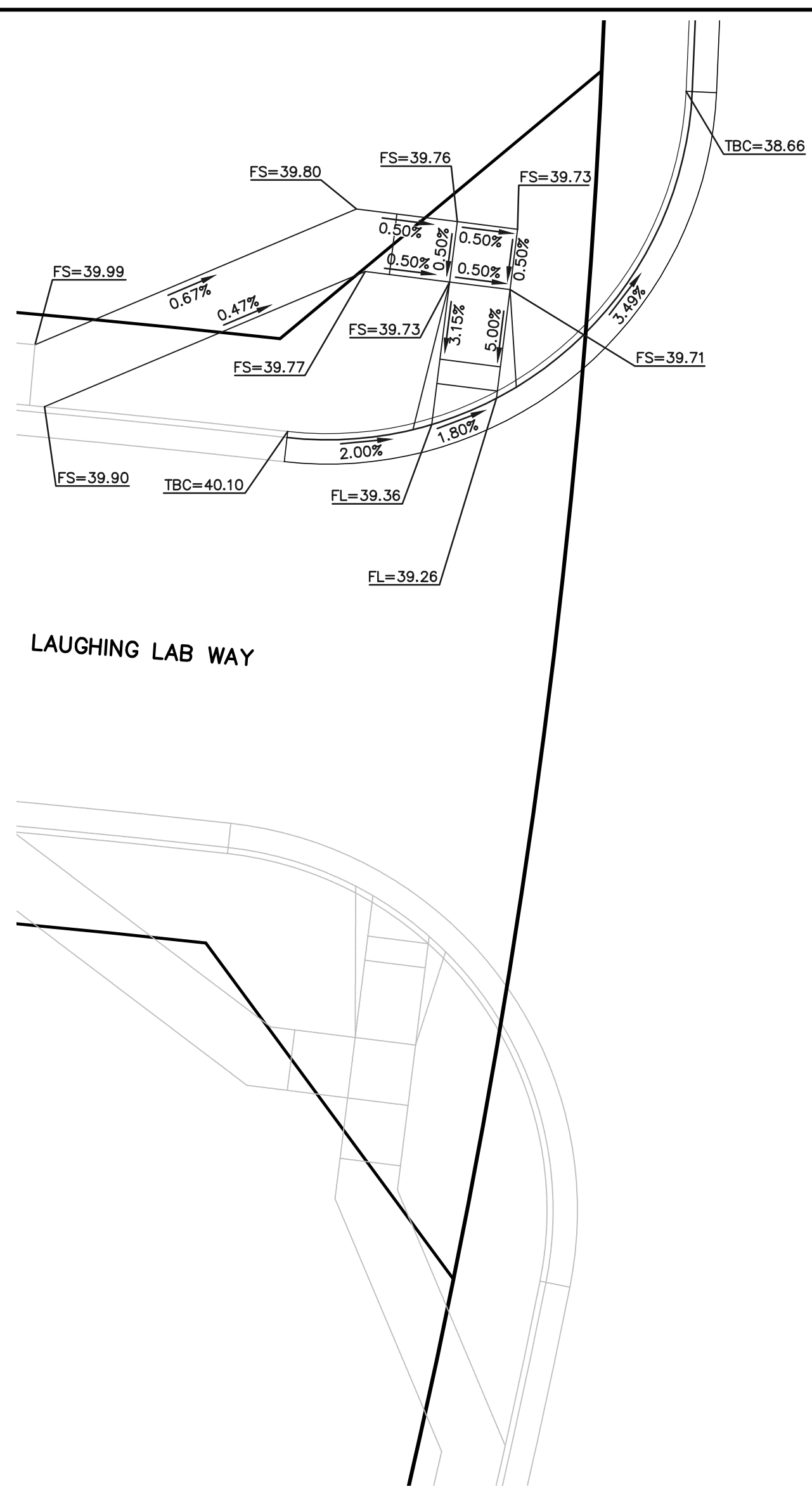
48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS
811
UTILITY NOTIFICATION CENTER OF COLORADO ITS THE LAW

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

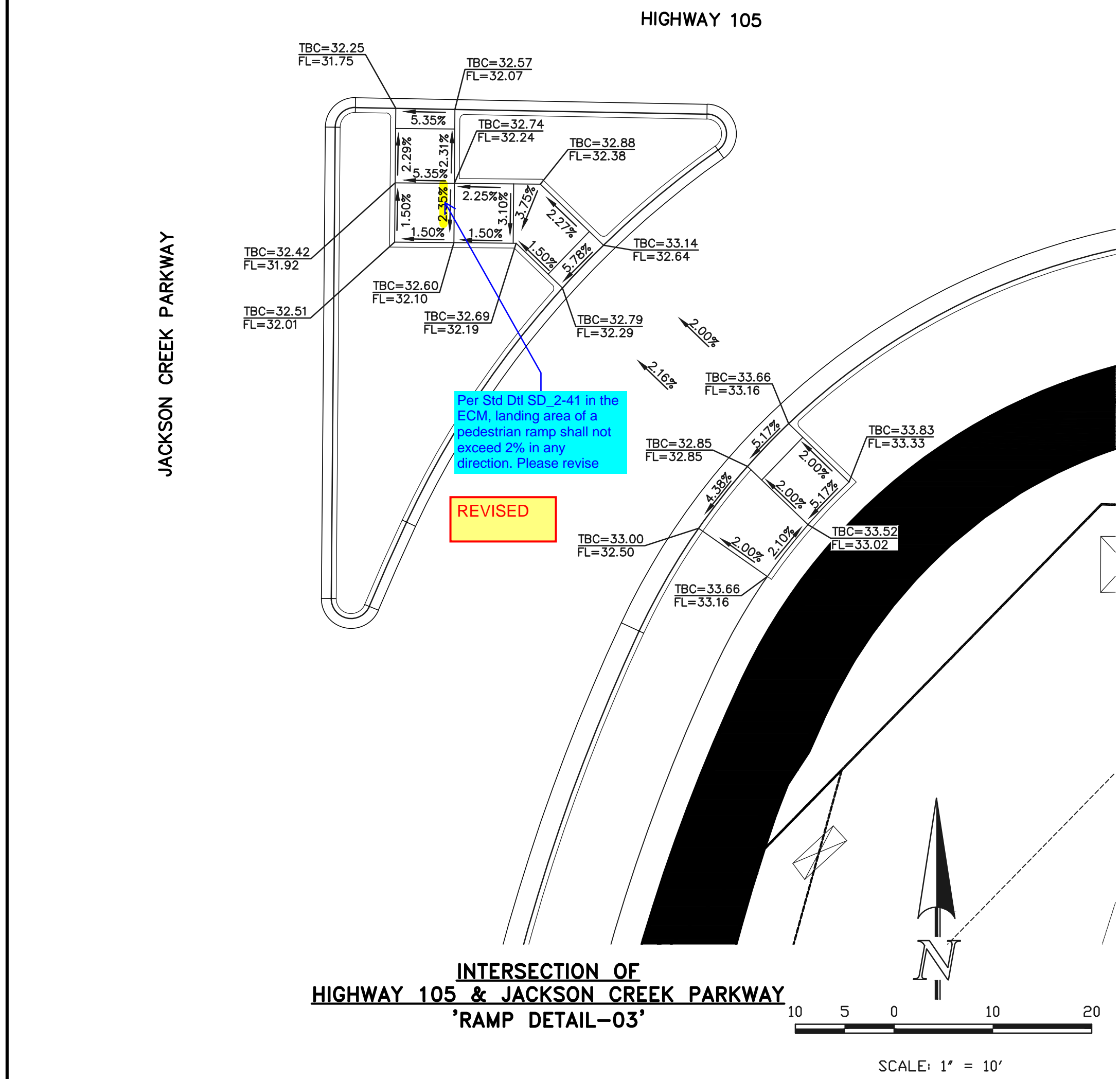
V:\130222\DRAWINGS\CD\STRUCT\CON\SPHET-15-91-04.dwg, 7/19/2023 3:32:47 PM, cwhorton, 1:1



**INTERSECTION OF
FAT TIRE DRIVE & JACKSON CREEK PARKWAY
'RAMP DETAIL-01'**



**INTERSECTION OF
LAUGHING LAB WAY & JACKSON CREEK
PARKWAY
'RAMP DETAIL-02'**



**INTERSECTION OF
HIGHWAY 105 & JACKSON CREEK PARKWAY
'RAMP DETAIL-03'**

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| NO. | REVISION | DATE |
|-----|------------------------------------|---------|
| 1 | ADDED PEDRAMP DETAIL SHEET TO PLAN | 6-23-23 |
| | | |
| | | |
| | | |
| | | |

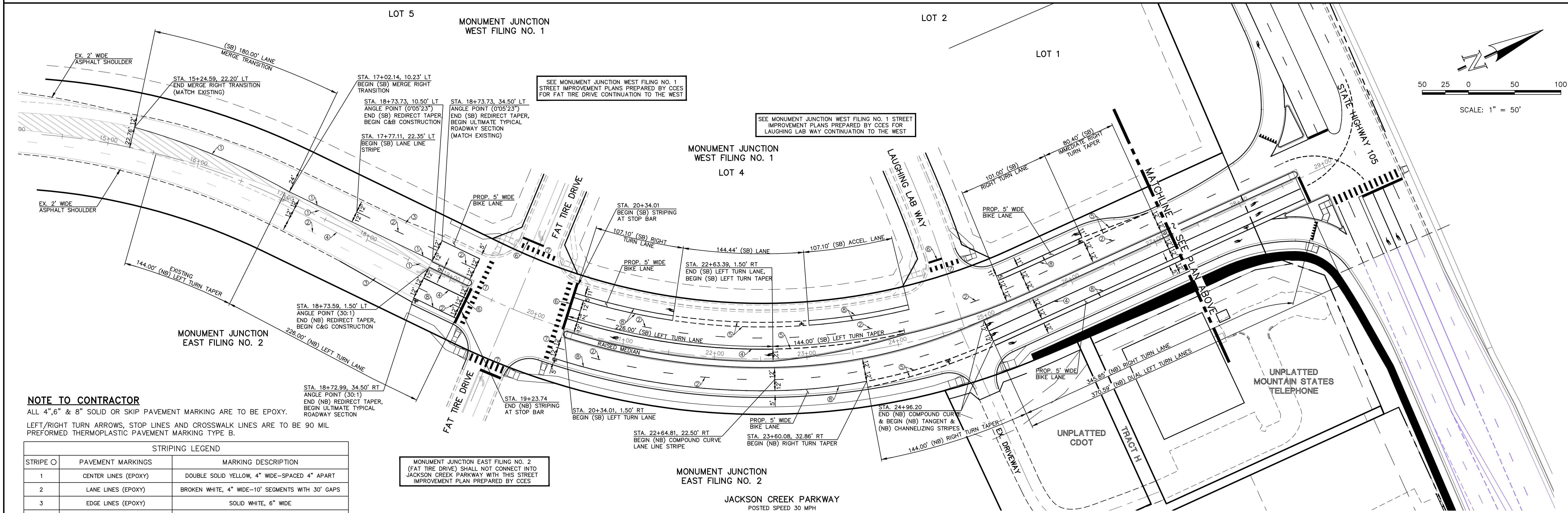
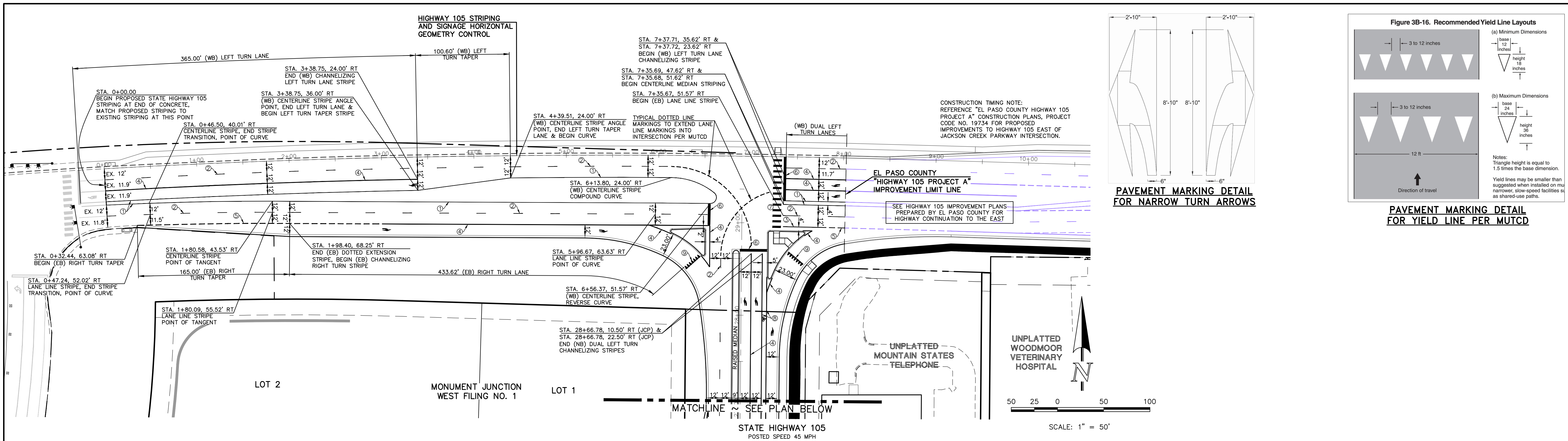
REVIEW:
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC A. WHORTON, COLORADO P.E. #37155 DATE



| | | | |
|---|--------------|--------------|----------------|
| STATE HIGHWAY/ JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS | | | |
| ROADWAY IMPROVEMENT PLANS | | | |
| PEDESTRIAN RAMP DETAILS | | | |
| DESIGNED BY | PRA | SCALE | DATE 6-23-23 |
| DRAWN BY | PRA | (H) 1" = 10' | SHEET 16 OF 26 |
| CHECKED BY | (V) 1" = N/A | JOB NO. | 1302.22 |

N:\130222\DRAWINGS\CONSTRUCT\CON\SHET-16-PEDRAMP-DETAILS.dwg, 7/12/2023 9:46:40 AM, rrrrrrrrrrr, 1:1



NOTE TO CONTRACTOR
 ALL 4", 6" & 8" SOLID OR SKIP PAVEMENT MARKING ARE TO BE EPOXY.
 LEFT/RIGHT TURN ARROWS, STOP LINES AND CROSSWALK LINES ARE TO BE 90 MIL PERFORMED THERMOPLASTIC PAVEMENT MARKING TYPE B.

| STRIPE | PAVEMENT MARKINGS | MARKING DESCRIPTION |
|--------|--------------------------------------|--|
| 1 | CENTER LINES (EPOXY) | DOUBLE SOLID YELLOW, 4" WIDE-SPACED 4" APART |
| 2 | LANE LINES (EPOXY) | BROKEN WHITE, 4" WIDE-10" SEGMENTS WITH 30" GAPS |
| 3 | EDGE LINES (EPOXY) | SOLID WHITE, 6" WIDE |
| 4 | CHANNELIZING LINES (EPOXY) | SOLID WHITE, 8" WIDE |
| 5 | DOTTED LANE LINES (EPOXY) | BROKEN WHITE, 6" WIDE-3" SEGMENTS WITH 12" GAPS |
| 6 | STOP LINES (THERMO PLASTIC) | SOLID WHITE, 24" WIDE |
| 7 | CROSSWALK LINES (THERMO PLASTIC) | SOLID WHITE, 24" WIDE, 9' LONG, 48" GAPS |
| 8 | BIKE LANE CHANNELIZING LINES (EPOXY) | SOLID WHITE, 6" WIDE |
| 9 | YIELD PAVEMENT MARKINGS (EPOXY) | SEE DETAIL AT TOP RIGHT OF PLAN |

MONUMENT JUNCTION EAST FILING NO. 2 (FAT TIRE DRIVE) SHALL NOT CONNECT INTO JACKSON CREEK PARKWAY WITH THIS STREET IMPROVEMENT PLAN PREPARED BY CDES

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| NO. | REVISION | DATE |
|-----|---|---------|
| 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 |

REVIEW:
 PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC. A. WHORTON, COLORADO P.E. #37155 DATE

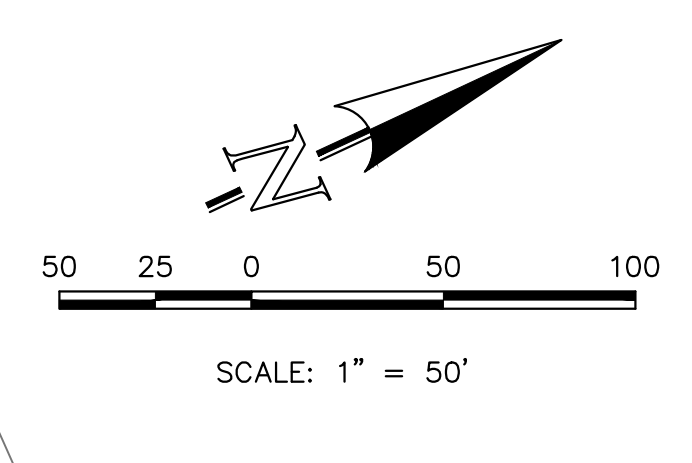
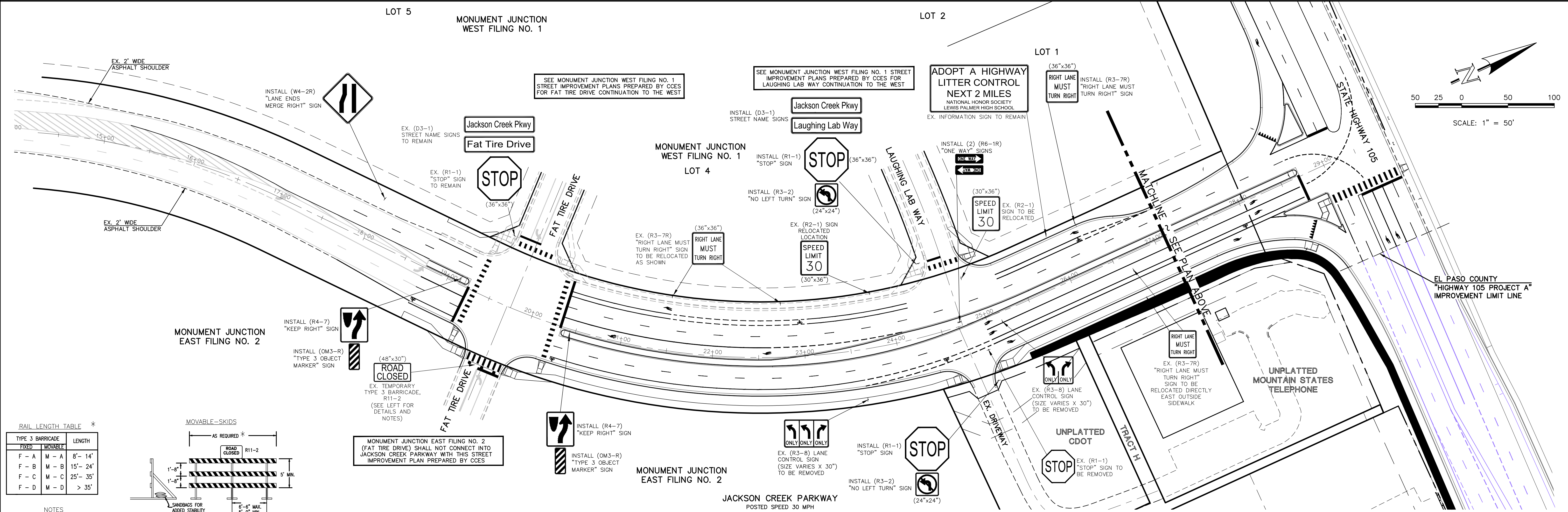
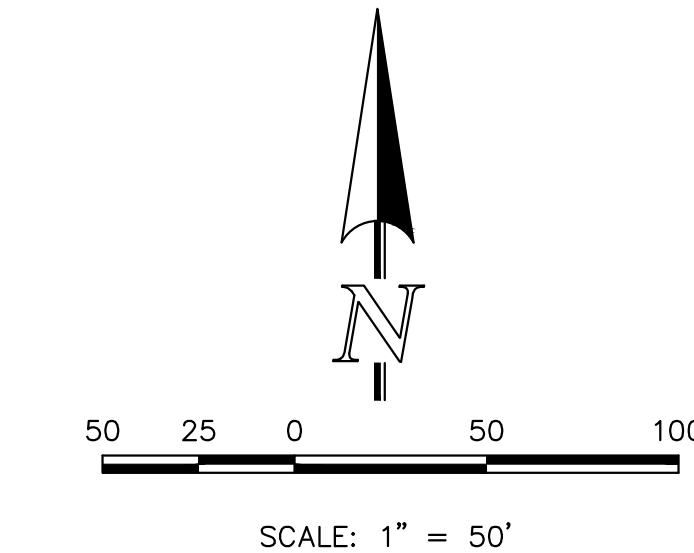
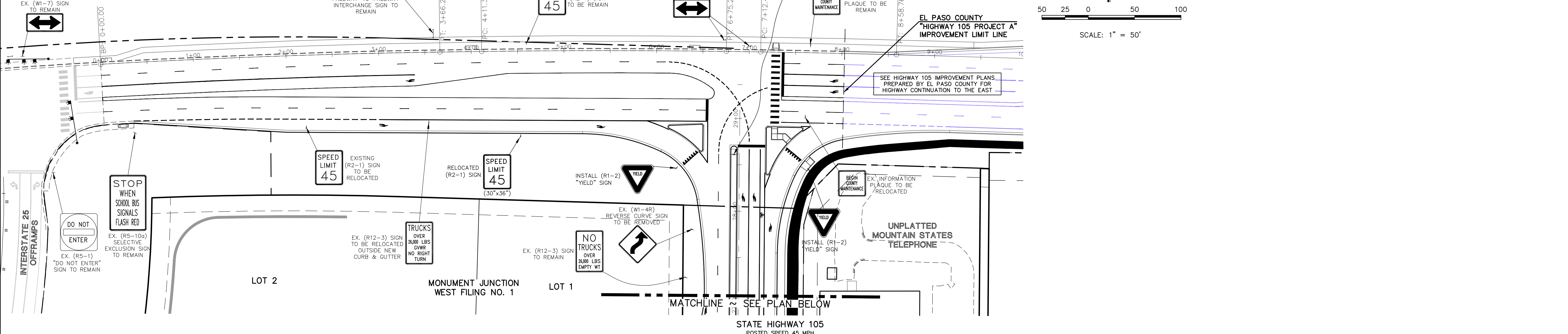
CLASSIC
 CONSULTING ENGINEERS & SURVEYORS

619 N. Cascade Avenue, Suite 200
 Colorado Springs, Colorado 80903
 (719)785-0790
 (719)785-0799(fax)

| STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS | | | |
|--|--------------|--------------|----------------|
| ROADWAY IMPROVEMENT PLANS | | | |
| STRIPING PLAN | | | |
| DESIGNED BY | PRA | SCALE | DATE 02-23-23 |
| DRAWN BY | PRA | (H) 1" = 50' | SHEET 17 OF 26 |
| CHECKED BY | (V) 1" = N/A | JOB NO. | 1302.22 |

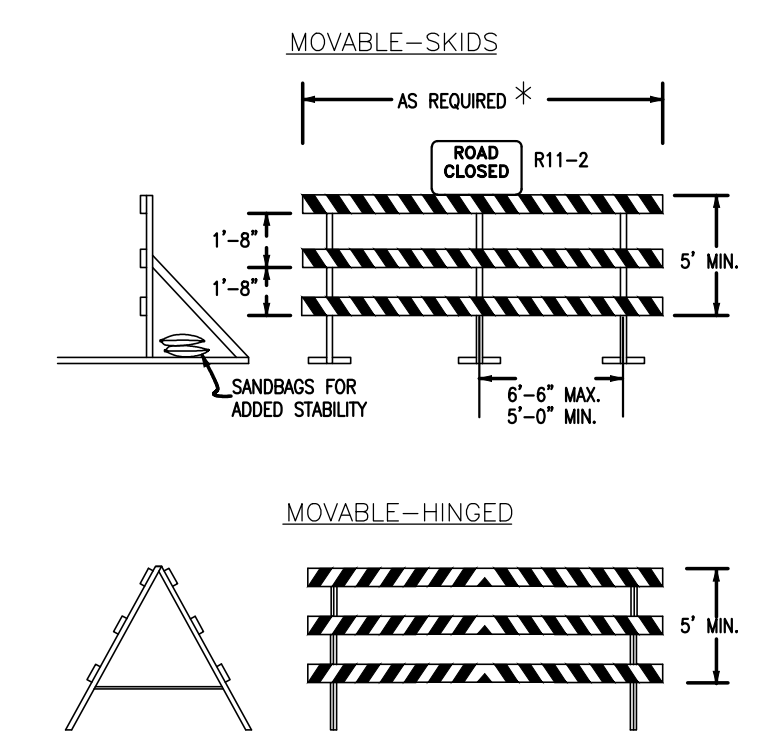
V:\130222\ROADWAYS\CONSTRUCTION\DRAWINGS\17-STRIPING-01.dwg, 7/20/2023 10:38:39 AM, Whorton, 1:1

NOTE TO CONTRACTOR
 SIGNS AND POSTS SHALL BE PER CDOT STANDARDS
 S-614-8, S-614-2, AND S-614-3, LATEST REVISION.
 ALL SIGNAGE INSTALLATION IS TO BE IN COMPLIANCE WITH
 THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
NOTE:
 ALL INTERNAL STREET NAME SIGNS SHALL BE
 4" FONT LETTER SIZE.
 6" FONT LETTER SIZE ON ALL STREETS POSTED
 30MPH OR GREATER



RAIL LENGTH TABLE *

| TYPE | BARRICADE | LENGTH |
|-------|-----------|-----------|
| F - A | M - A | 8' - 14' |
| F - B | M - B | 15' - 24' |
| F - C | M - C | 25' - 35' |
| F - D | M - D | > 35' |



NOTES

- TYPE 3 BARRICADES HAVE 3 REFLECTORIZED RAIL FACES IF FACING TRAFFIC IN ONE DIRECTION AND 6 IF FACING TRAFFIC IN TWO DIRECTIONS.
- THE PORTION OF THE POST ABOVE THE GROUND LINE SHALL BE PAINTED IN ACCORDANCE WITH THE APPROPRIATE GENERAL NOTE.
- DETACHABLE EXTENSION WING RAILS FOR BYPASSING OF CONSTRUCTION EQUIPMENT ARE PERMITTED, WHEN NECESSARY, ON FIXED OR MOVABLE TYPE 3 BARRICADES. THE LENGTH SHALL BE ADEQUATE TO CLOSE THE SHOULDER AS REQUIRED.

TYPICAL TYPE 3 BARRICADES

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| NO. | REVISION | DATE |
|-----|---|---------|
| 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 |

REVIEW:
 PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

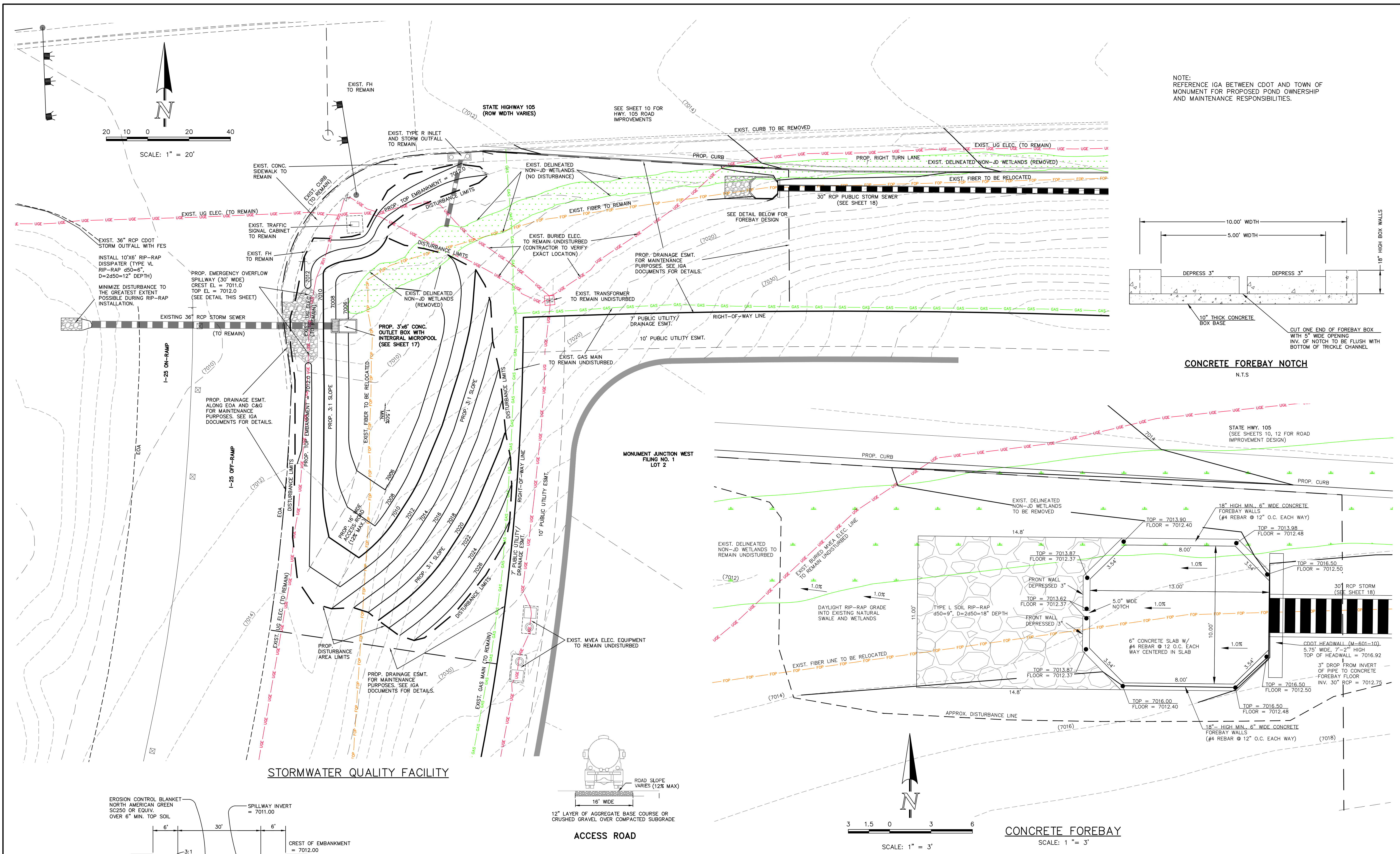
MARC. A. WHORTON, COLORADO P.E. #37155 DATE

CLASSIC
 CONSULTING ENGINEERS & SURVEYORS

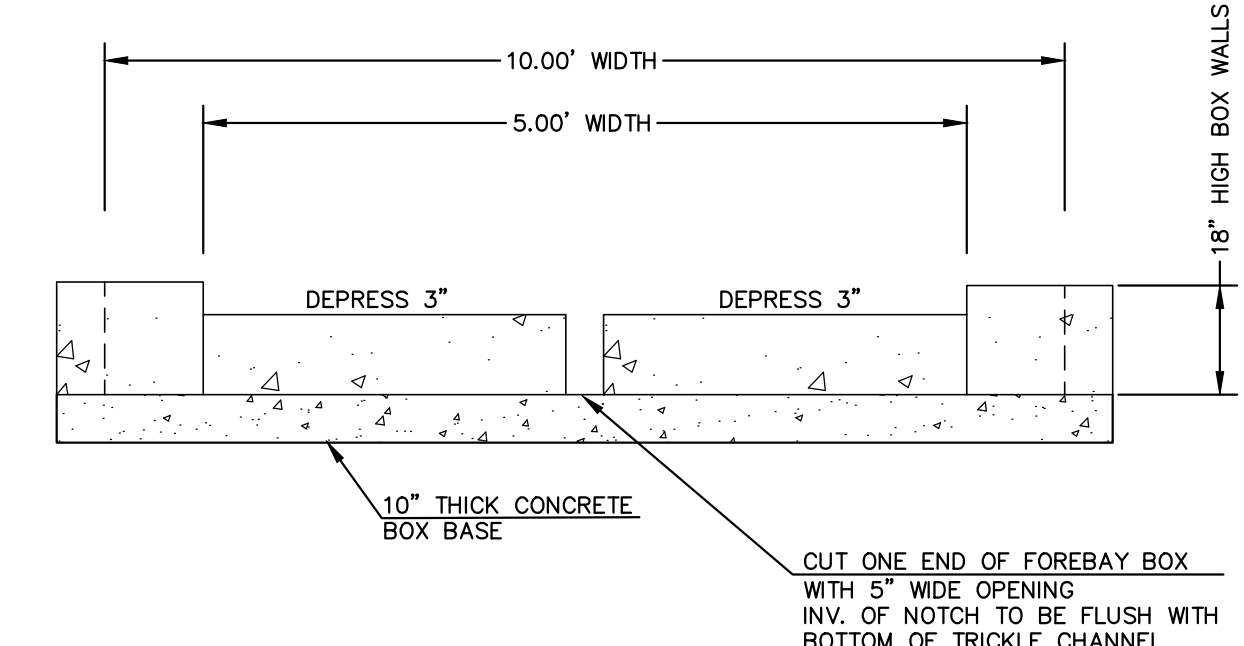
619 N. Cascade Avenue, Suite 200
 Colorado Springs, Colorado 80903
 (719) 785-0790
 (719) 785-0799 (fax)

| STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS | | | |
|--|--------------|--------------|----------------|
| ROADWAY IMPROVEMENT PLANS SIGNAGE PLAN | | | |
| DESIGNED BY | PRA | SCALE | DATE 02-23-23 |
| DRAWN BY | PRA | (H) 1" = 50' | SHEET 18 OF 26 |
| CHECKED BY | (V) 1" = N/A | JOB NO. | 1302.22 |

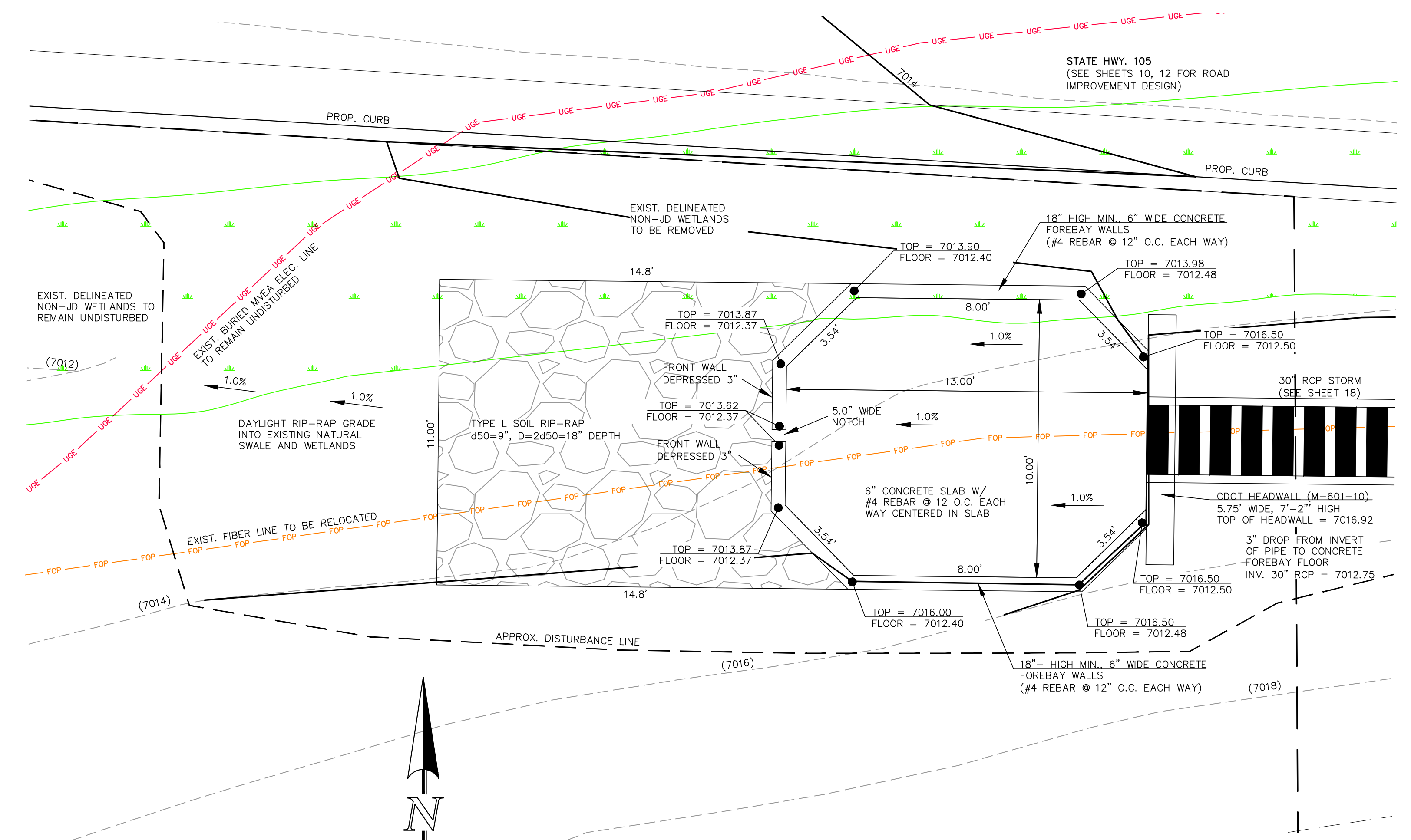
N:\130222\DRAWINGS\CONSTRUCTION\DWG\18-SIGNAGE-01.dwg, 7/14/2023 10:46:29 AM, mrogon, 11



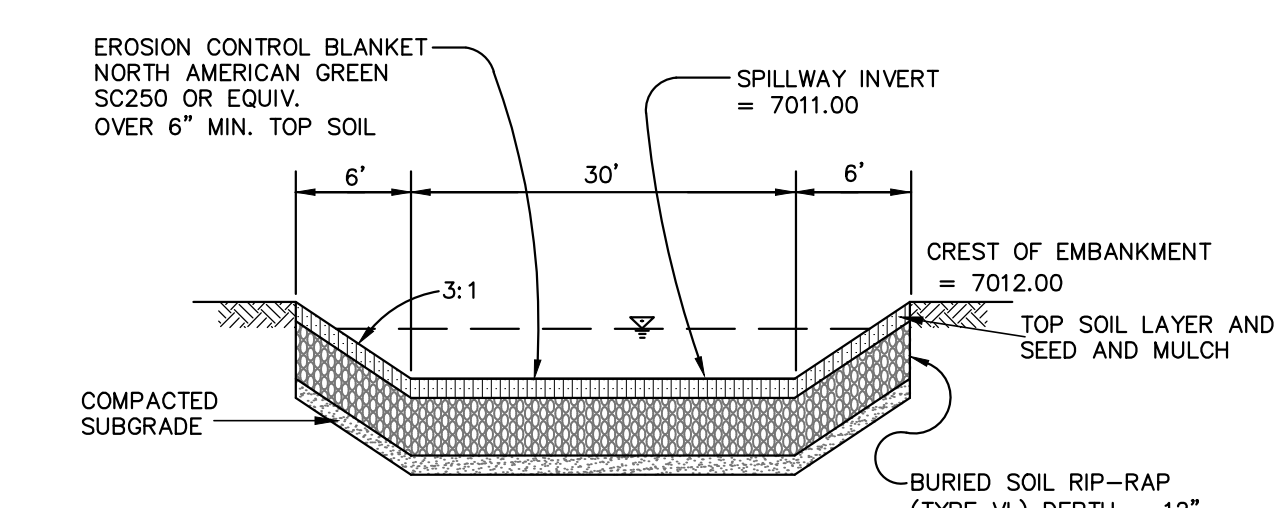
NOTE:
REFERENCE IGA BETWEEN CDOT AND TOWN OF
MONUMENT FOR PROPOSED POND OWNERSHIP
AND MAINTENANCE RESPONSIBILITIES.



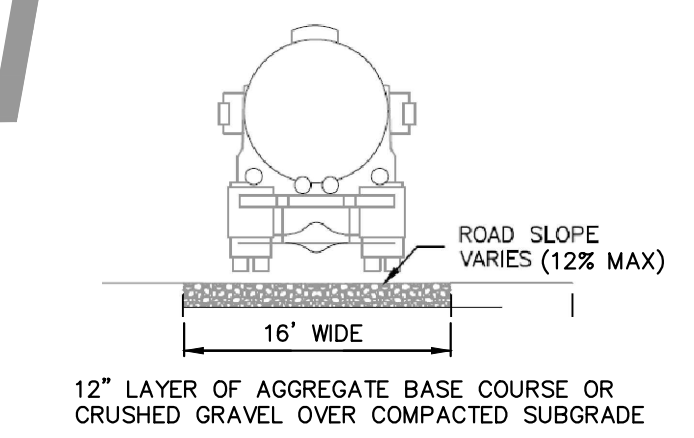
CONCRETE FOREBAY NOTCH
N.T.S.



CONCRETE FOREBAY
SCALE: 1" = 3'



EMERGENCY SPILLWAY CROSS SECTION
SCALE: N.T.S.



ACCESS ROAD

| | | | | | |
|--|--|--------------|---|---------------------------------------|---|
| 48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS 811 UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW | | NO. REVISION | DATE | REVIEW: | |
| THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. | | 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 | PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC |
| | | | | MARC A. WHORTON, COLORADO P.E. #37155 | |

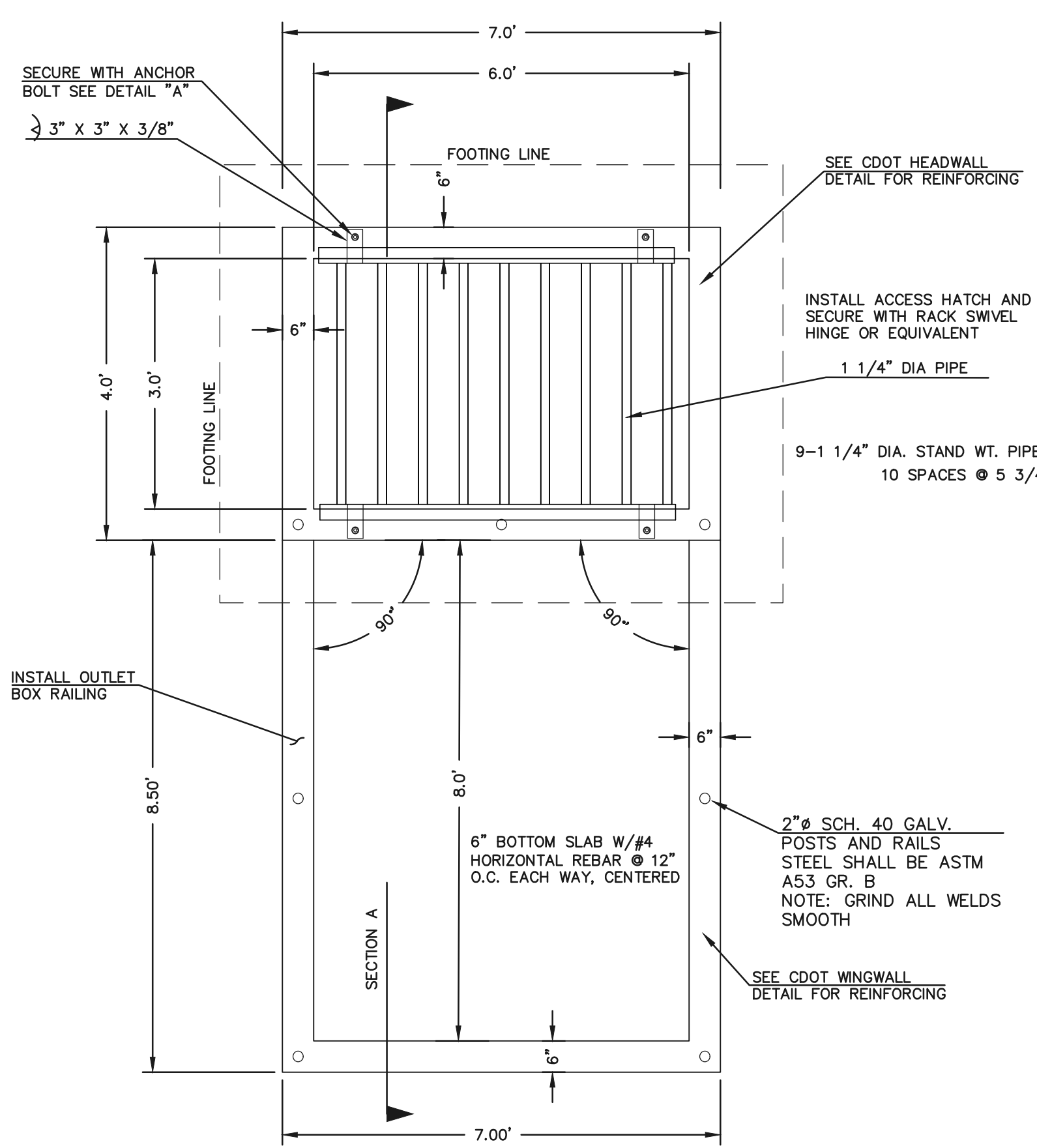
CLASSIC
CONSULTING
ENGINEERS & SURVEYORS

STATE HIGHWAY 105 / JACKSON CREEK
PKWY. - PHASE 2 CONSTRUCTION PLANS
STORMWATER QUALITY FACILITY
POND PLAN AND FOREBAY DETAIL

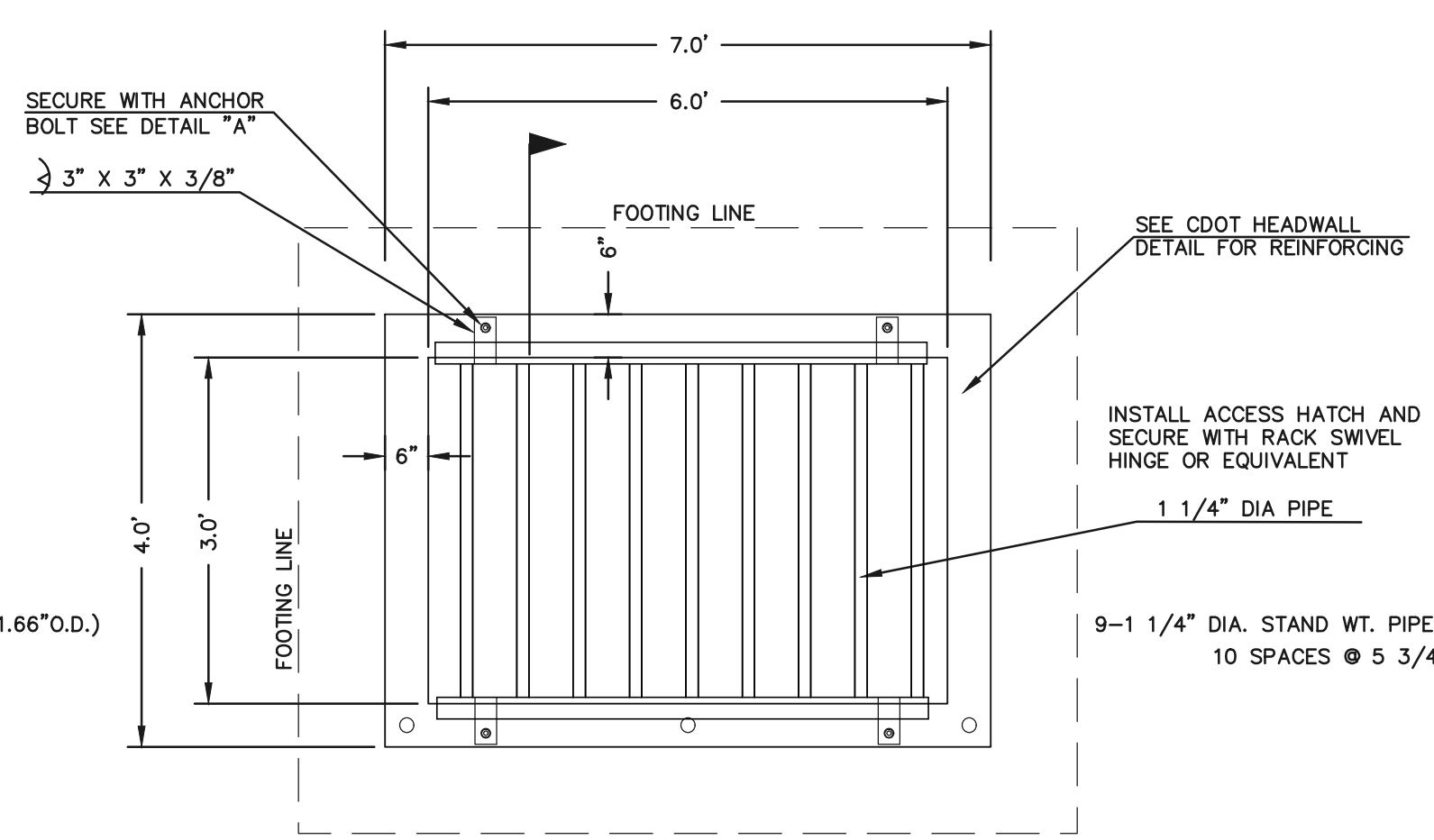
| | | | | |
|-------------|--------------|--------------|---------|----------|
| DESIGNED BY | MAW | SCALE | DATE | 02-23-23 |
| DRAWN BY | MAW | (H) 1" = 20' | SHEET | 19 OF 26 |
| CHECKED BY | (V) 1" = N/A | JOB NO. | 1302.22 | |

619 N. Cascade Avenue, Suite 200 (719) 785-0790
Colorado Springs, Colorado 80903 (719) 785-0799 (Fax)

V:\13022\DRAWINGS\CDSTRUCT\CON\SHW1-19-2023-01.dwg, 7/20/2023 11:19:00 AM, MWhorton, 1:1



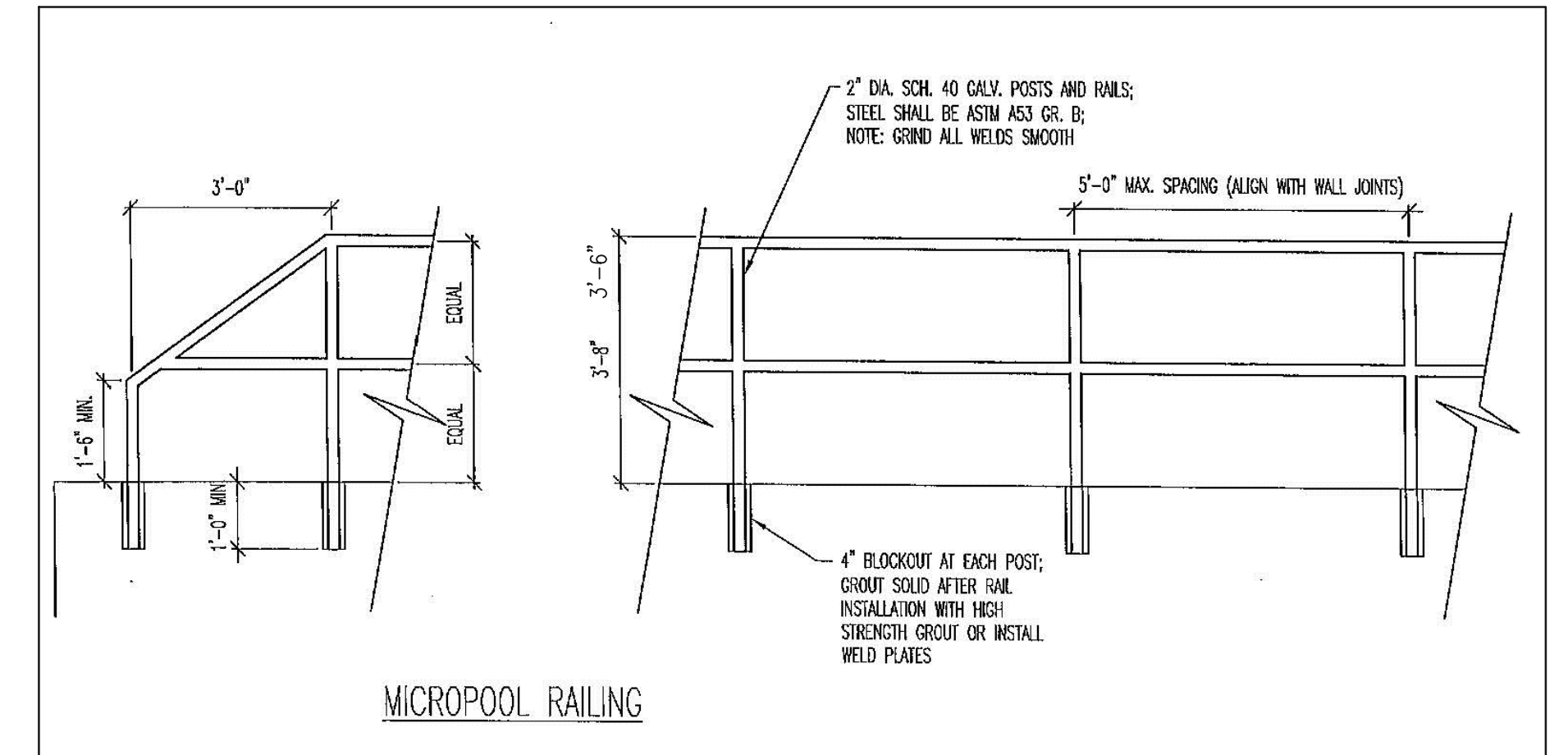
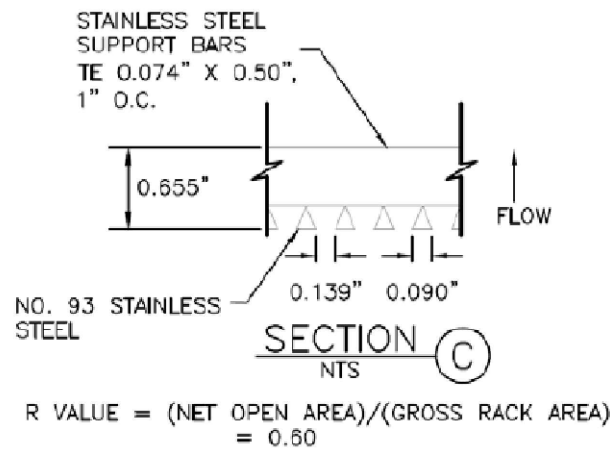
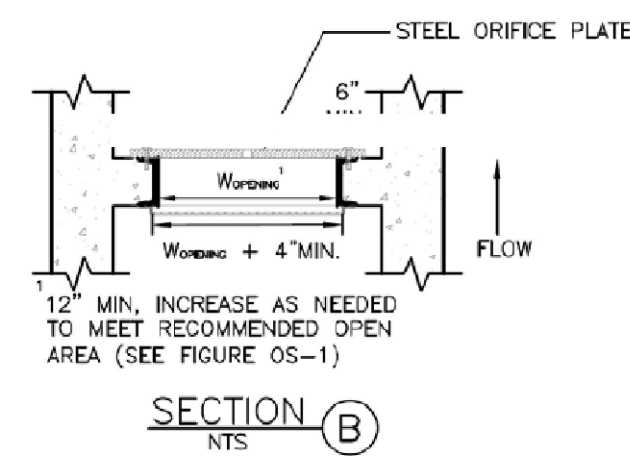
CONCRETE MICROPOOL
SCALE 1" = 2'



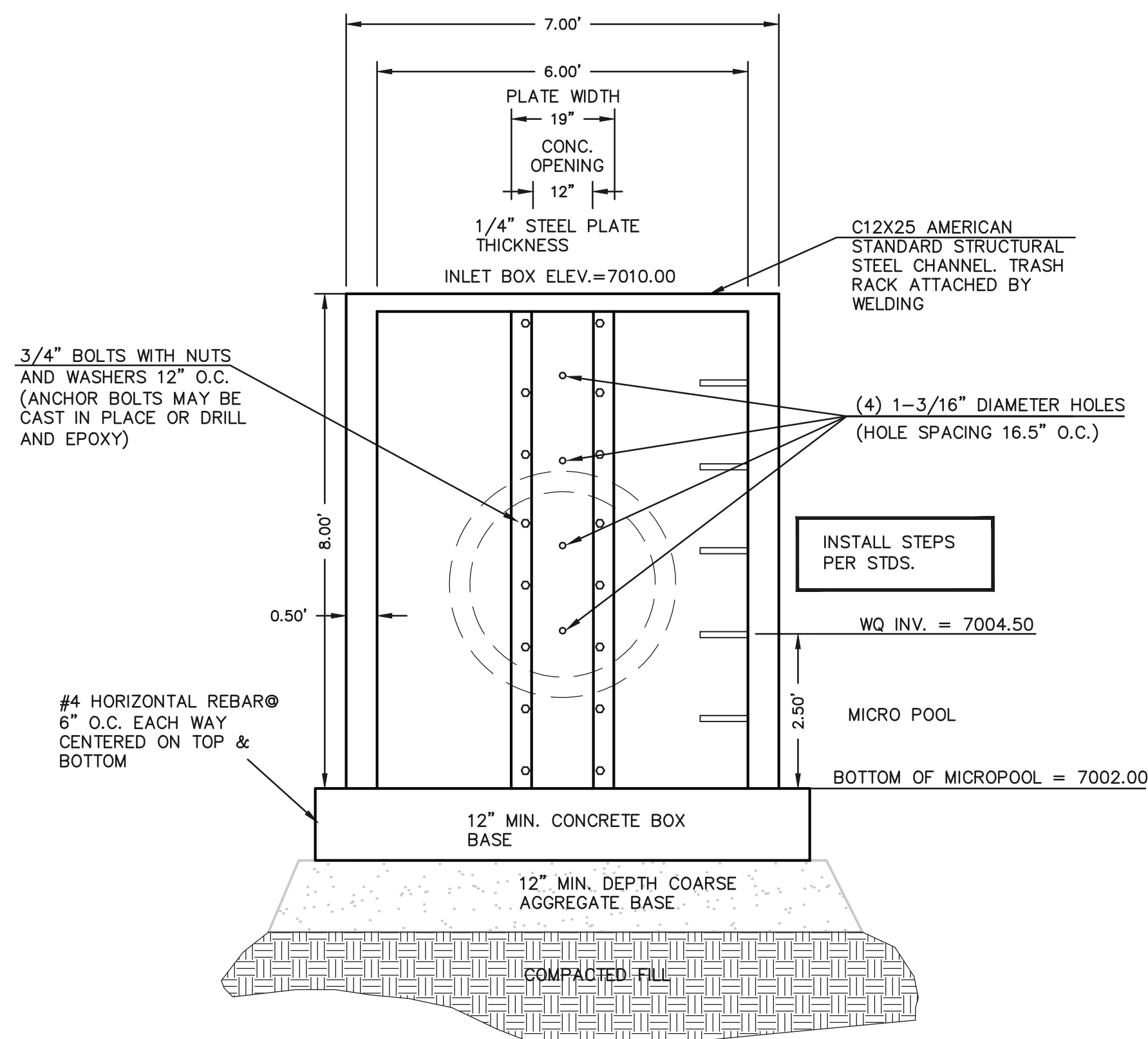
3'X6' OUTLET BOX OVERFLOW TRASH RACK
SCALE 1" = 2'

NOTES:

1. WELD PLATES EMBEDS MAY BE SUBSTITUTED. DESIGN CRITERIA SHALL BE IN ACCORDANCE WITH AASHTO STANDARDS.
2. HANDRAIL DESIGN SHALL BE COMPATIBLE WITH THE DESIGN OF THE WINGWALLS AND HEADWALLS. RAILING POSTS SHALL BE SET TO NORMAL TO GRADE. RAILS SHALL RUN PARALLEL TO THE SLOPES OF TOPS OF THE WALLS.
3. ALL RAILS SHALL HAVE EXPANSION JOINTS SPACED AT 40'-0" MAX. JOINT ENDS SHALL BE FREE OF ANY SHARP EDGES OR CORNERS.



OUTLET BOX RAILING
N.T.S.



3'X6' OUTLET BOX ORIFICE PLATE
SCALE 1" = 2'

(ALL MATERIALS PER CDOT SPECIFICATIONS)

ORIFICE PLATE NOTES:

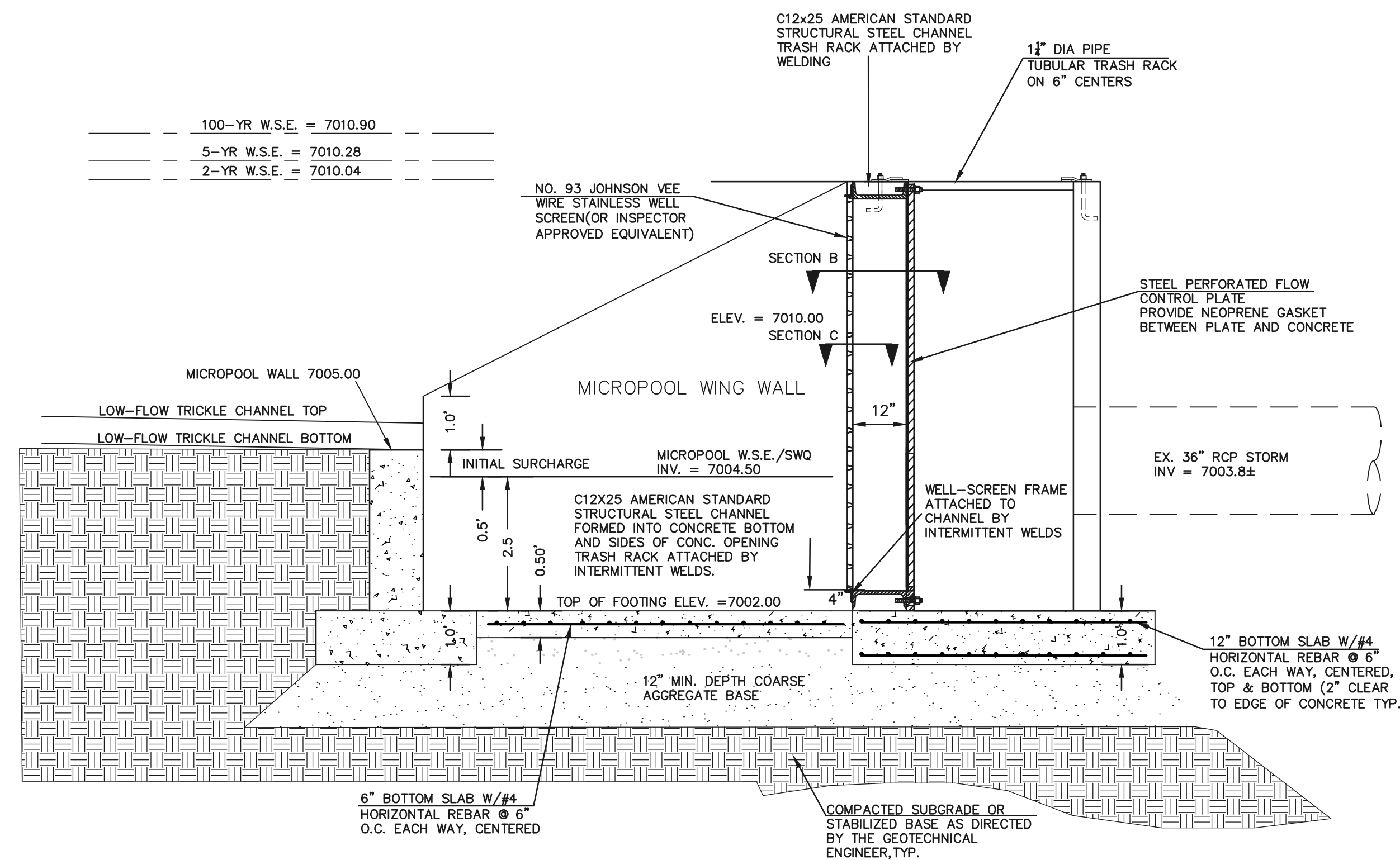
1. INSTALL HOLES AS SHOWN ON DETAIL TO LEFT.
2. PROVIDE GASKET MATERIAL BETWEEN THE ORIFICE PLATE AND CONCRETE
3. ALL STAINLESS STEEL USED TO BE GALVANIZED.

EURV AND WQCV TRASH RACKS:

4. WELL-SCREEN TRASH RACKS SHALL BE STAINLESS STEEL AND SHALL BE ATTACHED BY INTERMITTENT WELDS ALONG THE EDGE OF THE MOUNTING FRAME.
5. BAR GRATE TRASH RACKS SHALL BE ALUMINUM AND SHALL BE BOLTED USING STAINLESS STEEL HARDWARE.
6. STRUCTURAL DESIGN OF TRASH RACKS SHALL BE BASED ON FULL HYDROSTATIC HEAD WITH ZERO HEAD DOWNSTREAM OF RACK

OVERFLOW TRASH RACKS:

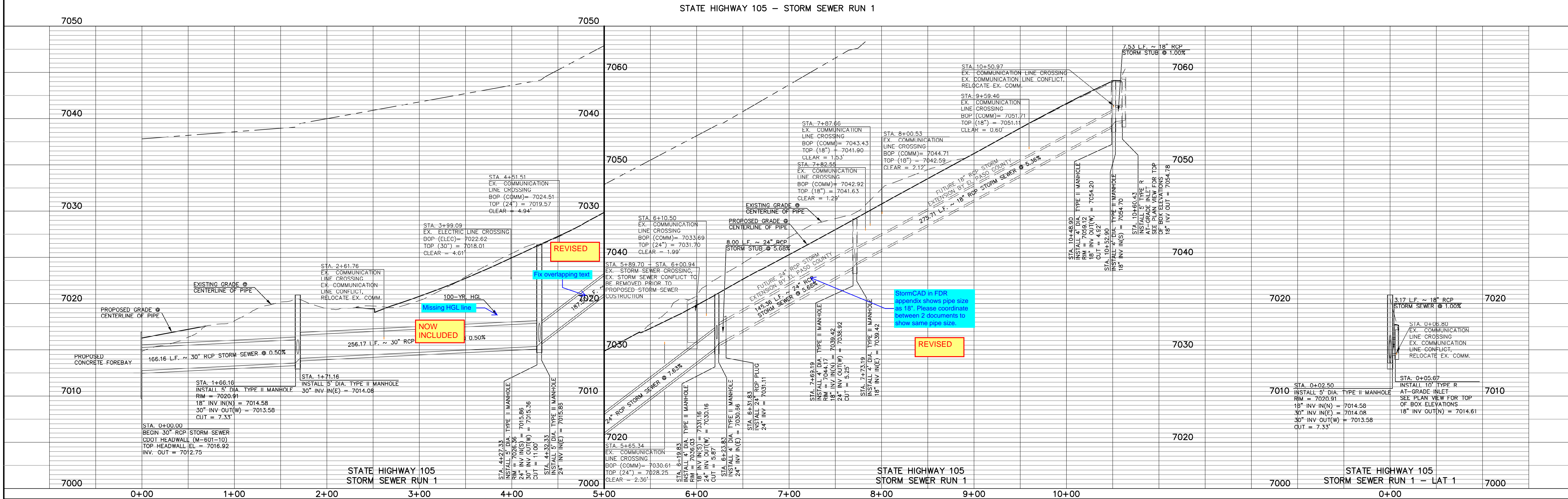
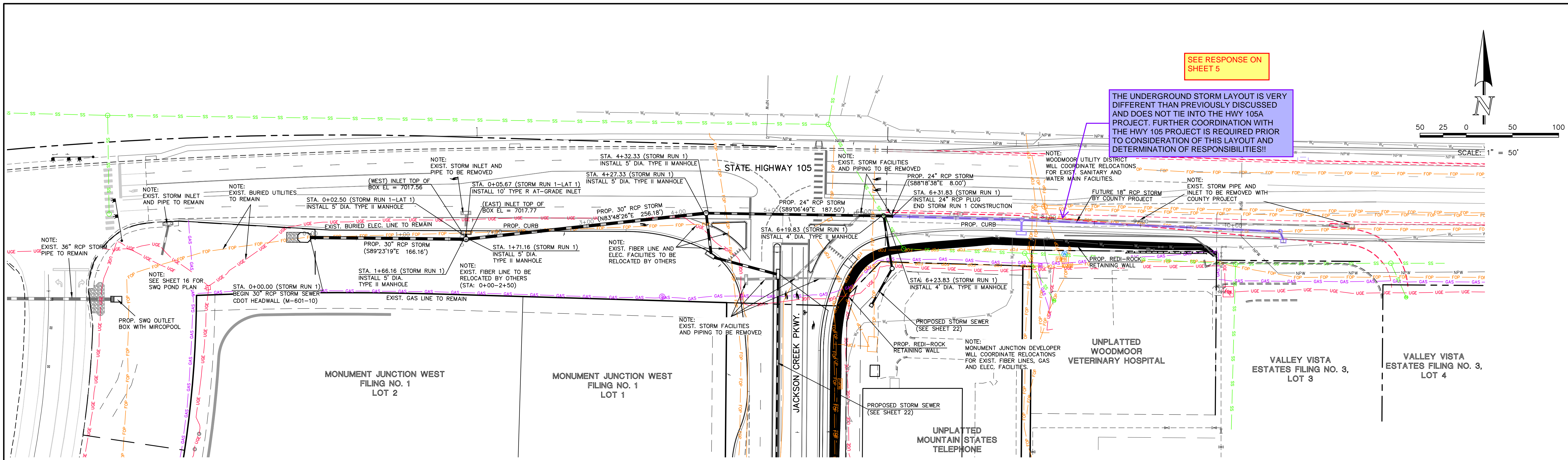
1. ALL TRASH RACKS SHALL BE MOUNTED USING STAINLESS STEEL HARDWARE AND PROVIDED WITH HINGED AND LOCKABLE OR BOLTABLE ACCESS PANELS.
2. TRASH RACKS SHALL BE STAINLESS STEEL, ALUMINUM, OR STEEL. TRASH RACKS SHALL BE HOT DIP GALVANIZED AND MAY BE HOT POWDER COATED AFTER GALVANIZING.
3. TRASH RACKS SHALL BE DESIGNED SUCH THAT THE DIAGONAL DIMENSION OF EACH OPENING IS SMALLER THAN THE DIAMETER OF THE OUTLET PIPE.
4. STRUCTURAL DESIGN OF THE TRASH RACKS SHALL BE BASED ON FULL HYDROSTATIC HEAD WITH ZERO HEAD DOWNSTREAM OF THE RACK.



3'X6' OUTLET BOX MICRO POOL SECTION A
SCALE 1" = 2'

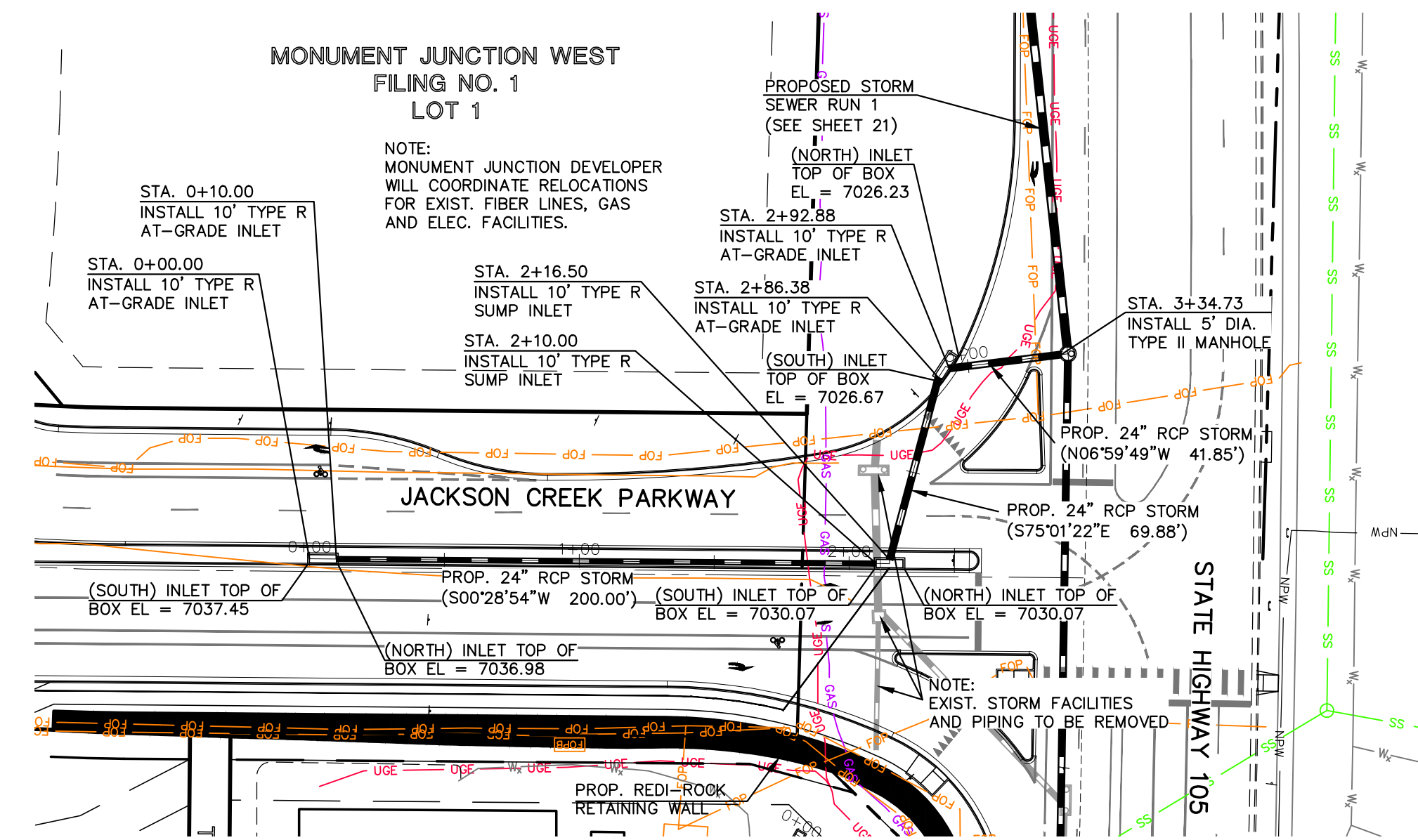
| | | | | |
|---|---|----------------------------|--|--|
| <p>48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS</p> <p>811</p> <p>UTILITY NOTIFICATION CENTER OF COLORADO IT'S THE LAW</p> <p>THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</p> | <p>NO. REVISION</p> <p>1 REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS</p> | <p>DATE</p> <p>7/10/23</p> | <p>REVIEW:</p> <p>PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC</p> <p>MARC A. WHORTON, COLORADO P.E. #37155</p> | <p>STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS</p> <p>STORMWATER QUALITY FACILITY</p> <p>OUTLET BOX DETAILS</p> <p>DESIGNED BY MAW SCALE DATE 02-23-23</p> <p>DRAWN BY PRA (H) 1"= N/A SHEET 20 OF 26</p> <p>CHECKED BY (V) 1"= N/A JOB NO. 1302.22</p> |
| | <p>CLASSIC CONSULTING ENGINEERS & SURVEYORS</p> <p>619 N. Cascade Avenue, Suite 200 Colorado Springs, Colorado 80903 (719)785-0790 (719)785-0799(Fax)</p> | | | |

V:\130222\DRAWINGS\CONSTRUCT\CON\SH-105-20-000-02.dwg, 7/20/2023 11:20:50 AM, Whorton, 1:1

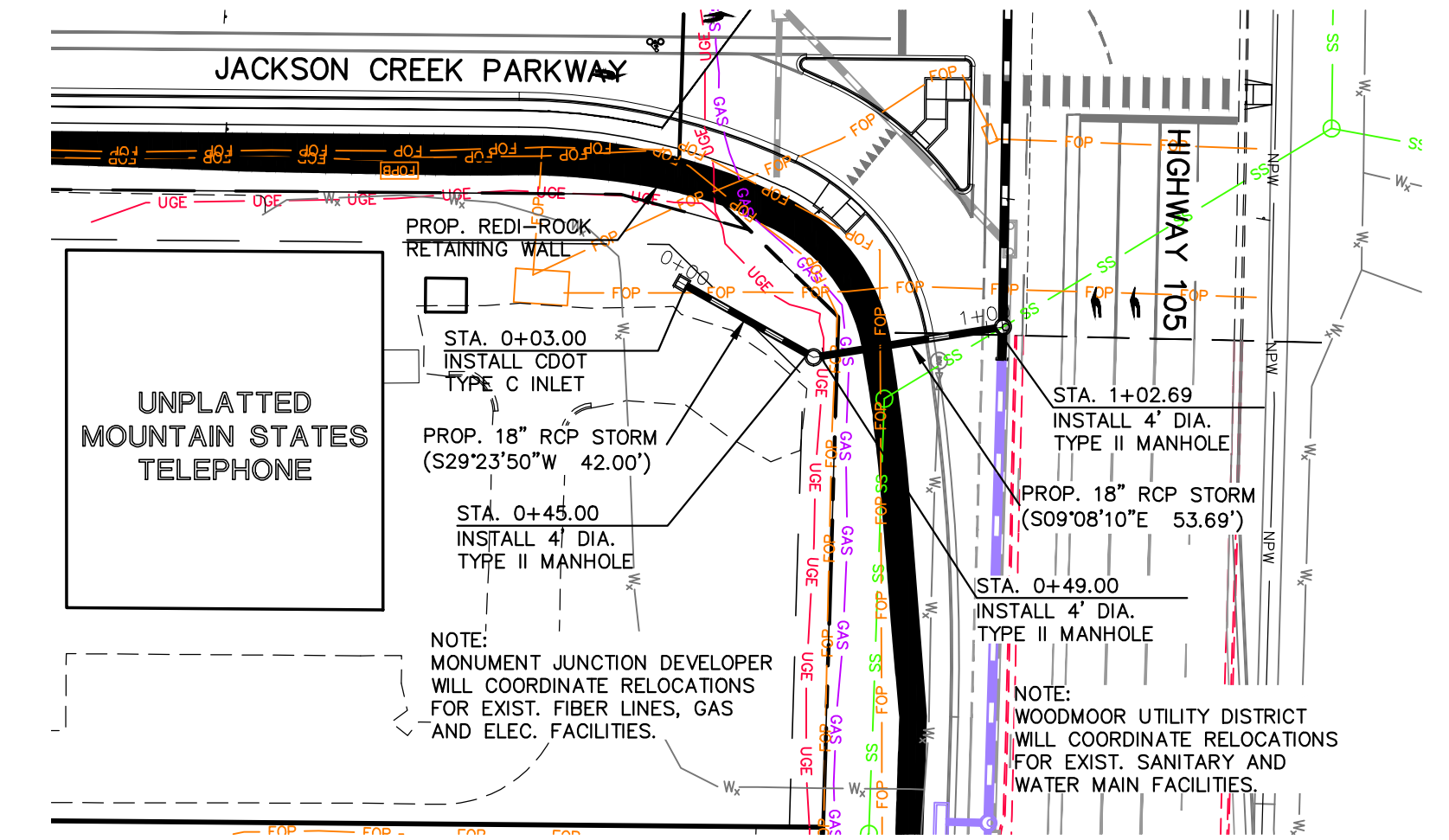
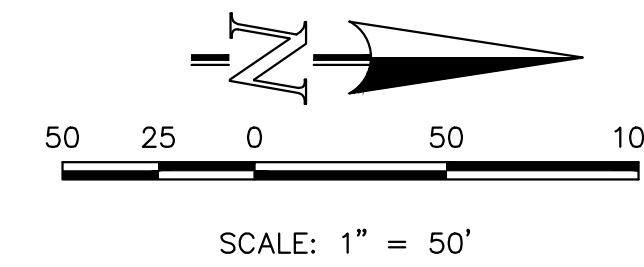


| | | | | |
|--|---|----------------------------|--|--|
| <p>48 HOURS BEFORE YOU DIG, CALL UTILITY LOCATORS</p> <p>811</p> <p>UTILITY NOTIFICATION CENTER OF COLORADO ITS THE LAW</p> <p>THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.</p> | <p>NO. REVISION</p> <p>1 REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS 7/10/23</p> | <p>DATE</p> <p>7/10/23</p> | <p>REVIEW:</p> <p>PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC</p> <p>MARC. A. WHORTON, COLORADO P.E. #37155 DATE</p> | <p>STATE HIGHWAY 105 / JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS</p> <p>STORM SEWER PLAN & PROFILE</p> <p>DESIGNED BY PRA SCALE DATE 02-23-23</p> <p>DRAWN BY PRA (H) 1" = 50' SHEET 21 OF 26</p> <p>CHECKED BY (V) 1" = 5' JOB NO. 1302.22</p> |
| | <p>CLASSIC CONSULTING ENGINEERS & SURVEYORS</p> <p>619 N. Cascade Avenue, Suite 200 Colorado Springs, Colorado 80903 (719)785-0790 (719)785-0799(fax)</p> | | | <p>CLASSIC CONSULTING ENGINEERS & SURVEYORS</p> |

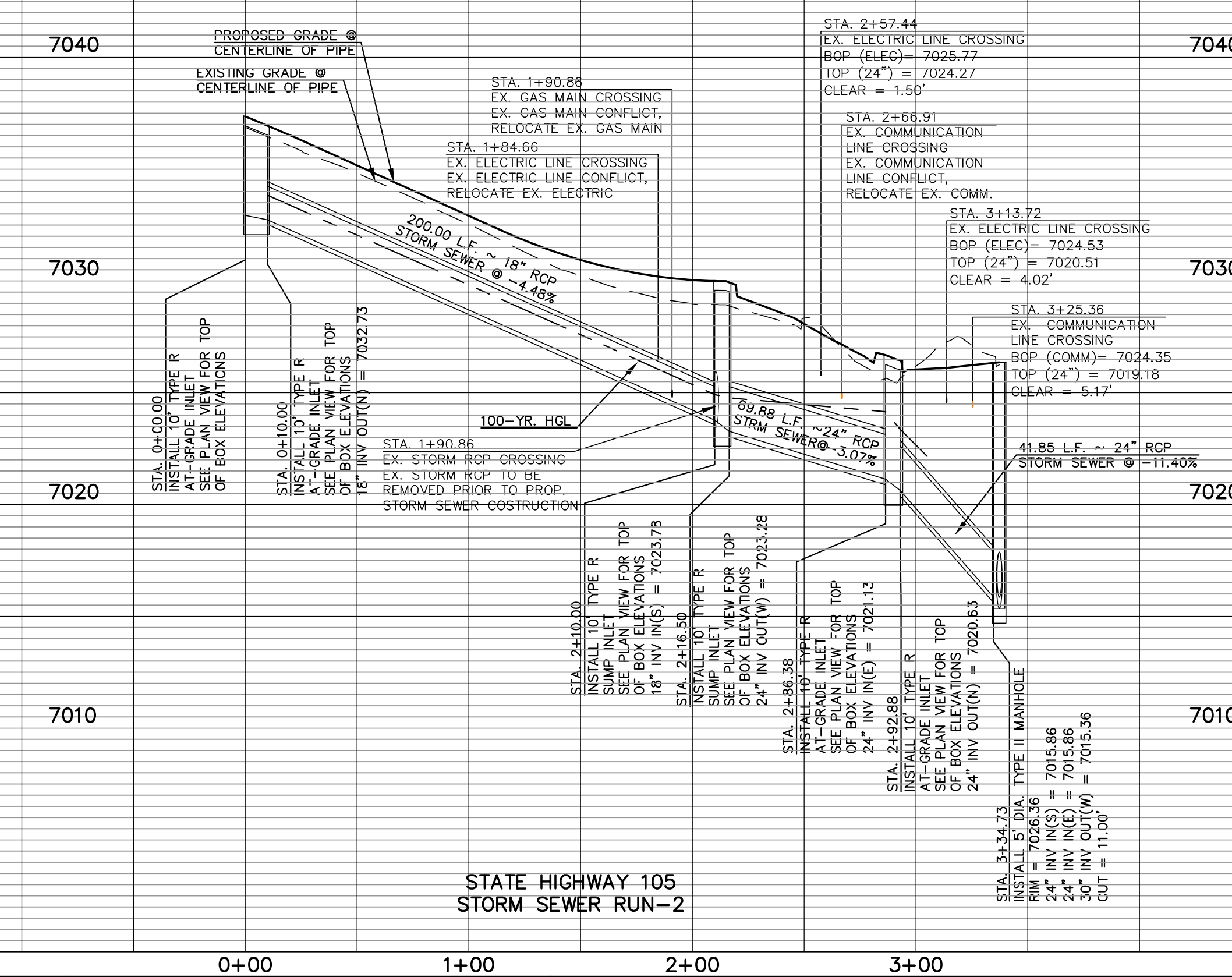
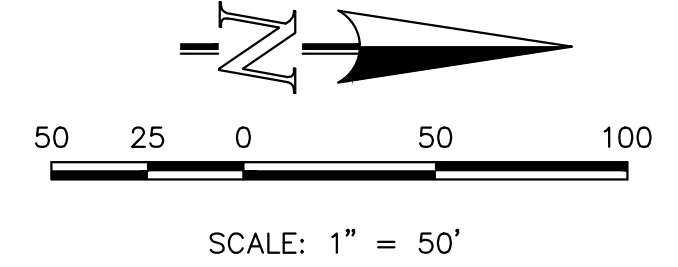
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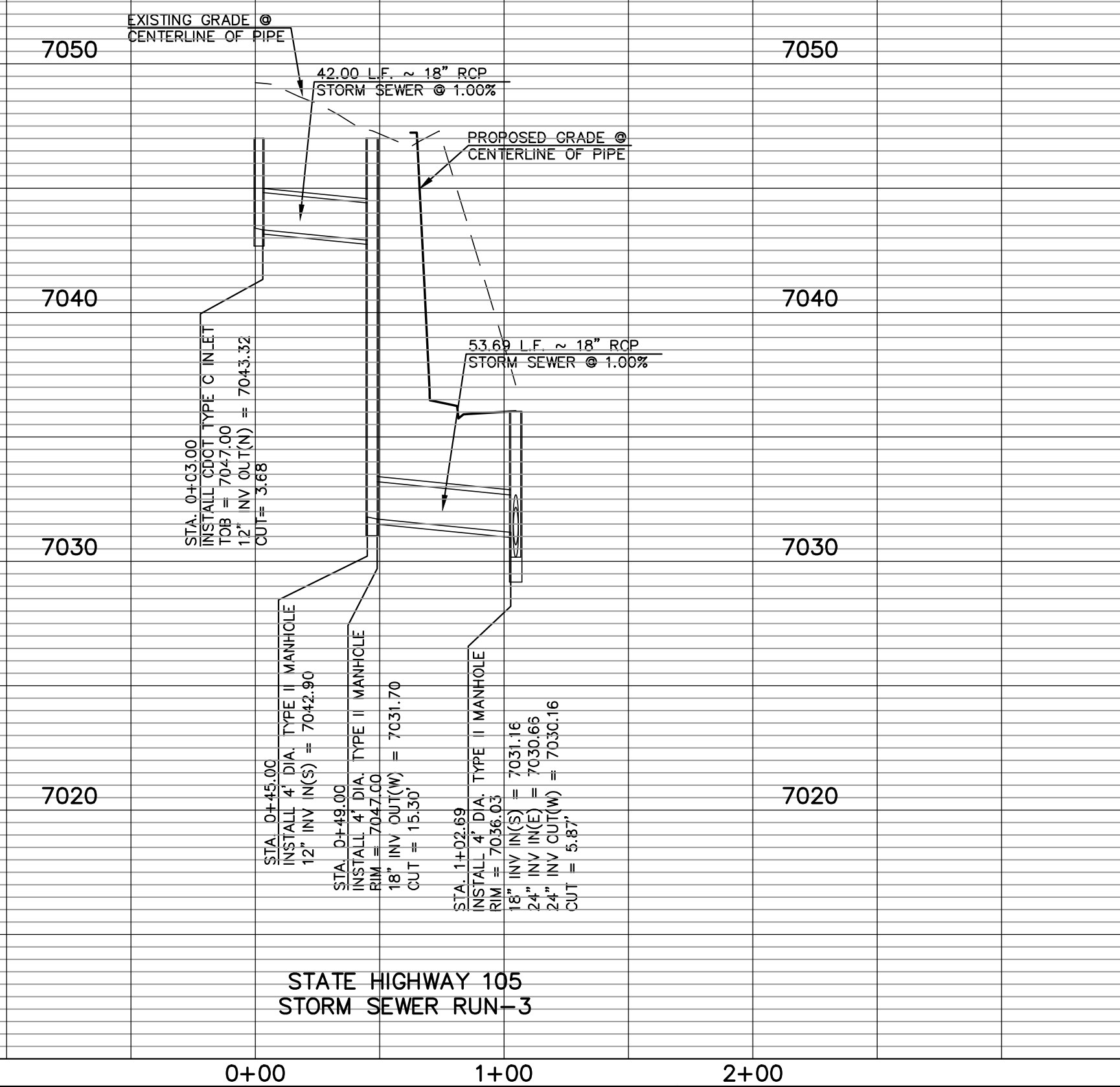
STATE HIGHWAY 105
STORM SEWER RUN-2



STATE HIGHWAY 105
STORM SEWER RUN-3



STATE HIGHWAY 105
STORM SEWER RUN-2



STATE HIGHWAY 105
STORM SEWER RUN-3

0+00 1+00 2+00 3+00 0+00 1+00 2+00

| NO. | REVISION | DATE |
|-----|---|---------|
| 1 | REVISED PER CDOT/COUNTY/TOWN OF MONUMENT COMMENTS | 7/10/23 |

REVIEW:
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF
CLASSIC CONSULTING ENGINEERS AND SURVEYORS, LLC

MARC. A. WHORTON, COLORADO P.E. #37155 DATE

48 HOURS BEFORE YOU DIG,
CALL UTILITY LOCATORS
811
UTILITY NOTIFICATION CENTER OF COLORADO
IT'S THE LAW

THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY HIS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.

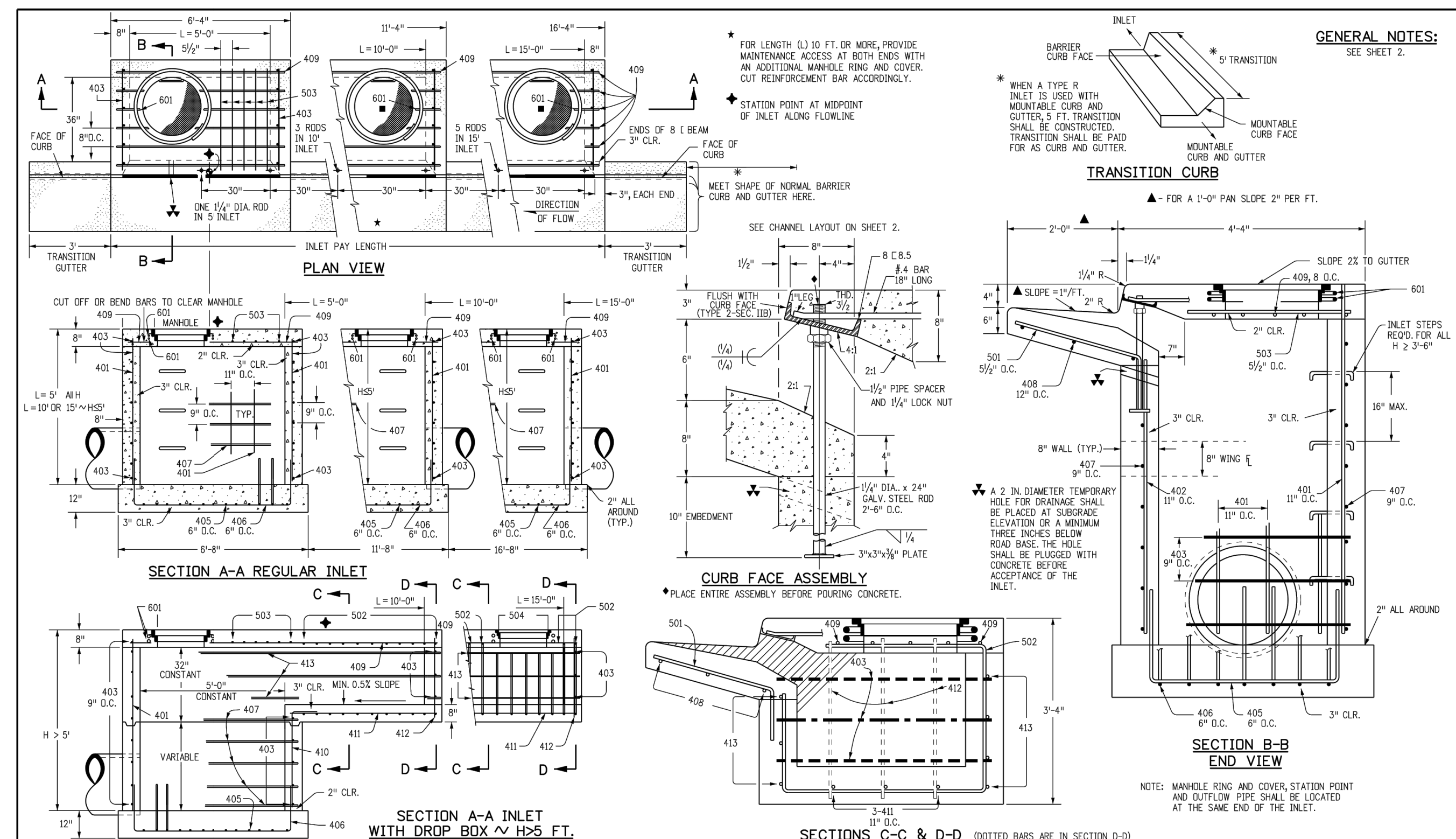
CLASSIC
CONSULTING
ENGINEERS & SURVEYORS

STATE HIGHWAY 105 /JACKSON CREEK PKWY. - PHASE 2 CONSTRUCTION PLANS
STORM SEWER PLAN & PROFILE

| | | | | |
|-------------|------------|-------------|---------|----------|
| DESIGNED BY | PRA | SCALE | DATE | 02-23-23 |
| DRAWN BY | PRA | (H) 1"= 50' | SHEET | 22 OF 26 |
| CHECKED BY | (V) 1"= 5' | JOB NO. | 1302.22 | |

619 N. Cascade Avenue, Suite 200 Colorado Springs, Colorado 80903 (719)785-0790 (719)785-0799(fax)

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| | | | | |
|---|--------------------------------|---|--|-----------------------------------|
| Computer File Information | Sheet Revisions | Colorado Department of Transportation | CURB INLET TYPE R | STANDARD PLAN NO. M-604-12 |
| Creation Date: 07/31/19 Designer: JBK Last Modification Date: 07/31/19 Detailer: LTA CAD Ver: MicroStation V8 | Date: _____ Comments: _____ | 2829 West Howard Place CDOT HQ, 3rd Floor Denver, CO 80204 Phone: 303-757-9921 FAX: 303-757-9868 | Issued by the Project Development Branch July 31, 2019 | Project Sheet Number: _____ |

| MARK | BAR # OR SIZE | O.C. SPACING | TYPE | ALL INLETS | | INLETS H ≤ 5 FT. | | INLETS H > 5 FT. | |
|------|---------------|--------------|------|------------|------------|------------------|------------|------------------|------------|
| | | | | L = 5 FT. | L = 10 FT. | L = 5 FT. | L = 10 FT. | L = 5 FT. | L = 10 FT. |
| 401 | 4 | 12" | II | 15 | 21 | 26 | 31 | 11 | 11 |
| 402 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 403 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 404 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 405 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 406 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 407 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 408 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 409 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 410 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 411 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 412 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 413 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 414 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 415 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 416 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 417 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 418 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 419 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 420 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 421 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 422 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 423 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 424 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 425 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 426 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 427 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 428 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 429 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 430 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 431 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 432 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 433 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 434 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 435 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 436 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 437 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 438 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 439 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 440 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 441 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 442 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 443 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 444 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 445 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 446 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 447 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 448 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 449 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 450 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 451 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 452 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 453 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 454 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 455 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 456 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 457 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 458 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 459 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 460 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 461 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 462 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 463 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 464 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 465 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 466 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 467 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 468 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 469 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 470 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 471 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 472 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 473 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 474 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 475 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 476 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 477 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 478 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 479 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 480 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 481 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 482 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 483 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 484 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 485 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 486 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 487 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 488 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 489 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 490 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 491 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 492 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 493 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 494 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 495 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 496 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 497 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 498 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 499 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |
| 500 | 4 | 12" | II | 7 | 13 | 18 | 24 | 7 | 7 |

TABLE ONE ~ BAR LIST FOR CURB INLETS, TYPE "R"

| TYPE | LENGTH | NO. REQ'D. | | L = 5 FT. | | L = 10 FT. | | L = 15 FT. | |
|--------|--------|------------|----------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | REGULAR | DEEP BOX | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) |
| 3'-0" | 2'-8" | 1 | 0 | 1.2 | 285 | 5.3 | 497 | 7.4 | 706 |
| 3'-0" | 3'-2" | 1 | 0 | 1.3 | 305 | 5.7 | 528 | 7.9 | 747 |
| 4'-0" | 3'-8" | 1 | 0 | 1.7 | 376 | 6.0 | 559 | 8.4 | 786 |
| 4'-0" | 4'-2" | 1 | 0 | 1.9 | 414 | 6.4 | 571 | 8.8 | 803 |
| 5'-0" | 4'-8" | 1 | 0 | 2.1 | 452 | 6.7 | 602 | 9.3 | 844 |
| 5'-0" | 5'-2" | 1 | 0 | 2.3 | 490 | 7.0 | 633 | 9.8 | 885 |
| 6'-0" | 5'-8" | 1 | 0 | 2.7 | 561 | 7.4 | 684 | 10.3 | 926 |
| 6'-0" | 6'-2" | 1 | 0 | 2.9 | 600 | 7.7 | 715 | 10.8 | 967 |
| 7'-0" | 7'-2" | 1 | 0 | 3.3 | 671 | 8.1 | 766 | 11.3 | 1008 |
| 7'-0" | 7'-6" | 1 | 0 | 3.5 | 710 | 8.4 | 797 | 11.8 | 1049 |
| 8'-0" | 8'-2" | 1 | 0 | 3.9 | 782 | 8.8 | 848 | 12.3 | 1090 |
| 8'-0" | 8'-6" | 1 | 0 | 4.1 | 821 | 9.1 | 879 | 12.8 | 1131 |
| 9'-0" | 9'-2" | 1 | 0 | 4.5 | 893 | 9.5 | 930 | 13.3 | 1172 |
| 9'-0" | 9'-6" | 1 | 0 | 4.7 | 932 | 9.8 | 961 | 13.8 | 1213 |
| 10'-0" | 10'-2" | 1 | 0 | 5.1 | 1004 | 10.2 | 1012 | 14.3 | 1254 |
| 10'-0" | 10'-6" | 1 | 0 | 5.3 | 1043 | 10.5 | 1043 | 14.8 | 1295 |
| 11'-0" | 11'-2" | 1 | 0 | 5.7 | 1115 | 10.9 | 1114 | 15.3 | 1336 |
| 11'-0" | 11'-6" | 1 | 0 | 5.9 | 1154 | 11.2 | 1145 | 15.8 | 1377 |

TABLE TWO ~ BARS AND QUANTITIES VARIABLE WITH "H"

| TYPE | LENGTH | NO. REQ'D. | | L = 5 FT. | | L = 10 FT. | | L = 15 FT. | |
|-------|--------|------------|----------|------------------------|------------------------|------------------------|------------------------|------------------------|------------------------|
| | | REGULAR | DEEP BOX | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) | CONC. STEEL (CU. YDS.) |
| 3'-0" | 2'-8" | 1 | 0 | 1.2 | 285 | 5.3 | 497 | 7.4 | 706 |
| 3'-0" | 3'-2" | 1 | 0 | 1.3 | 305 | 5.7 | 528 | 7.9 | 747 |
| 4'-0" | 3'-8" | 1 | 0 | 1.7 | 376 | 6.0 | 559 | 8.4 | 786 |
| 4'-0" | 4'-2" | 1 | 0 | 1.9 | 414 | 6.4 | 571 | 8.8 | 803 |
| 5'-0" | 4'-8" | 1 | 0 | 2.1 | 452 | 6.7 | 602 | 9.3 | 844 |
| 5'-0" | 5'-2" | 1 | 0 | 2.3 | 490 | 7.0 | 633 | 9.8 | 885 |
| 6'-0" | 5'-8" | 1 | 0 | 2.7 | 561 | 7.4 | 684 | 10.3 | 926 |
| 6'-0" | 6'-2" | 1 | 0 | 2.9 | 600 | 7.7 | 715 | 10.8 | 967 |
| 7'-0" | 7'-2" | 1 | 0 | 3.3 | 671 | 8.1 | 766 | 11.3 | 1008 |
| 7'-0" | 7'-6" | 1 | 0 | 3.5 | 710 | 8.4 | 797 | 11.8 | 1049 |

S:\121234-01 - Jackson Creek Parkway\04_CIVIL\CADD\Traffic\ITS\Drawings\105_Signal\T121234-01SIG-TAB01_5/5/2023 11:26:27 AM_Ben.Harms

TABULATION OF TRAFFIC SIGNAL ITEMS

| CDOT ITEM NO. | ITEM DESCRIPTION | UNITS | TOTAL | NOTES |
|----------------------|--|-------|-------|---|
| 202-00828 | REMOVAL OF TRAFFIC SIGNAL EQUIPMENT | L S | 1 | REFER TO THE REMOVAL ITEMS AND QUANTITIES BELOW |
| FOR INFORMATION ONLY | REMOVAL OF TRAFFIC SIGNAL POLE | EACH | 4 | RETURN TO EL PASO COUNTY |
| | REMOVAL OF TRAFFIC SIGNAL HEAD | EACH | 12 | |
| | REMOVAL OF TRAFFIC SIGNAL CONTROLLER AND CABINET | EACH | 1 | |
| | REMOVAL OF LUMINAIRE | EACH | 4 | |
| | REMOVAL OF STREET NAME SIGN | EACH | 3 | |
| | REMOVAL OF LANE USE/OPERATION SIGN | EACH | 2 | |
| | REMOVAL OF SPAN WIRE CABLE | LF | 320 | |
| 210-00479 | RESET RADIO COMMUNICATION ANTENNA | EACH | 1 | |
| 503-00036 | DRILLED SHAFT (36 INCH) | LF | 60 | CDOT STD. S-614-40A FOOTING |
| 613-00206 | 2 INCH ELECTRICAL CONDUIT (BORED) | LF | 400 | SCHEDULE 80 |
| 613-00306 | 3 INCH ELECTRICAL CONDUIT (BORED) | LF | 800 | SCHEDULE 80 |
| 613-01200 | 2 INCH ELECTRICAL CONDUIT (PLASTIC) | LF | 155 | SCHEDULE 80 |
| 613-01300 | 3 INCH ELECTRICAL CONDUIT (PLASTIC) | LF | 190 | SCHEDULE 80 |
| 613-07003 | TYPE THREE PULL BOX | EACH | 4 | SIGNAL POLE AND CONTROLLER PULL BOXES |
| 613-07004 | TYPE FOUR PULL BOX | EACH | 1 | HOME RUN PULL BOX |
| 613-10000 | WIRING | L S | 1 | SIGNAL AND LIGHTING |
| 613-13004 | LUMINAIRE (LED) (4,000 LUMENS) | EACH | 4 | |
| 613-50109 | METER POWER PEDESTAL | EACH | 1 | |
| 614-00011 | SIGN PANEL (CLASS I) | SF | 40 | |
| 614-70150 | PEDESTRIAN SIGNAL FACE (16) (COUNTDOWN) | EACH | 2 | LED TYPE, POLYCARBONATE, YELLOW INCOLOR |
| 614-70336 | TRAFFIC SIGNAL FACE (12-12-12) | EACH | 15 | LED TYPE, POLYCARBONATE, YELLOW INCOLOR |
| 614-72855 | TRAFFIC SIGNAL CONTROLLER CABINET | EACH | 1 | |
| 614-72860 | PEDESTRIAN PUSH BUTTON | EACH | 1 | PUSH BUTTON STATION, R10-3e SIGN AND ANY NECESSARY EXTENDER |
| 614-72863 | PEDESTRIAN PUSH BUTTON POST ASSEMBLY | EACH | 2 | CDOT STD. S-614-45, INCLUDES R10-3e SIGNS |
| 614-72866 | FIRE PREEMPTION UNIT AND TIMER | EACH | 1 | INCLUDES 2 PREEMPTION UNITS |
| 614-72886 | INTERSECTION DETECTION SYSTEM (CAMERA) | EACH | 2 | |
| 614-81000 | TRAFFIC SIGNAL-LIGHT POLE STEEL | EACH | 1 | |
| 614-81125 | TRAFFIC SIGNAL-LIGHT POLE STEEL (1-25 FOOT MAST ARM) | EACH | 1 | CDOT STD. S-614-40A |
| 614-81145 | TRAFFIC SIGNAL-LIGHT POLE STEEL (1-45 FOOT MAST ARM) | EACH | 1 | CDOT STD. S-614-40A |
| 614-81155 | TRAFFIC SIGNAL-LIGHT POLE STEEL (1-55 FOOT MAST ARM) | EACH | 1 | CDOT STD. S-614-40A |
| 614-86248 | TRAFFIC SIGNAL CONTROLLER (TYPE 2070LC) | EACH | 1 | |

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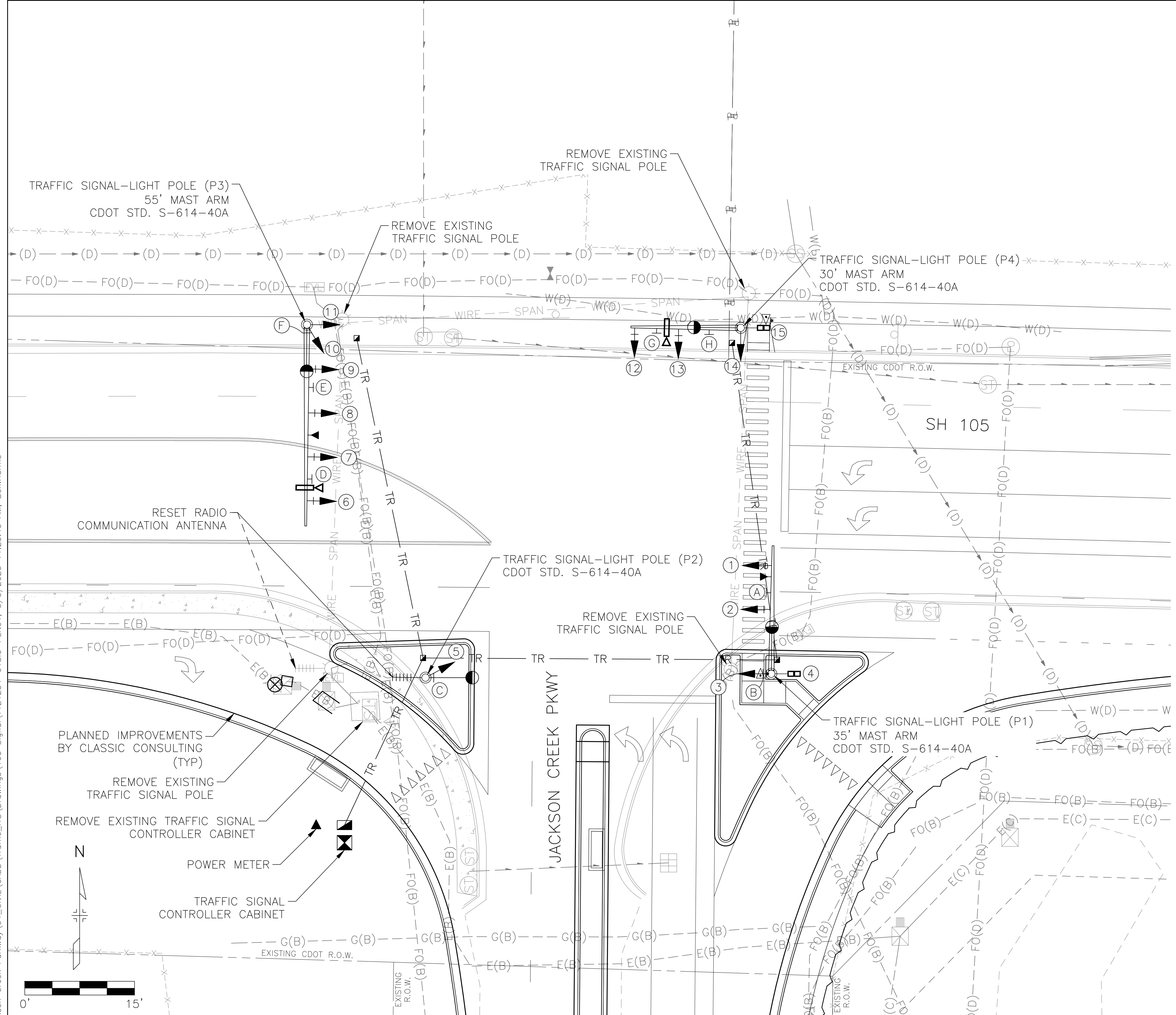
3 South Tejon Street, Suite 300
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| SHEET REVISIONS | | | |
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| DATE | COMMENTS | INITIALS | |
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| AS CONSTRUCTED | JACKSON CREEK PARKWAY TRAFFIC SIGNAL TABULATIONS STATE HIGHWAY 105 | | PROJECT NO./CODE |
| NO. REVISIONS: | | | 121234-01 |
| REVISED: | DESIGNER: BJH | STRUCTURE NUMBERS | |
| | DETAILER: BEN.HARMS | | |
| VOID: | SHEET SUBSET: TRAFFIC | SHEET SUBSET: TT-01 OF 1 | |

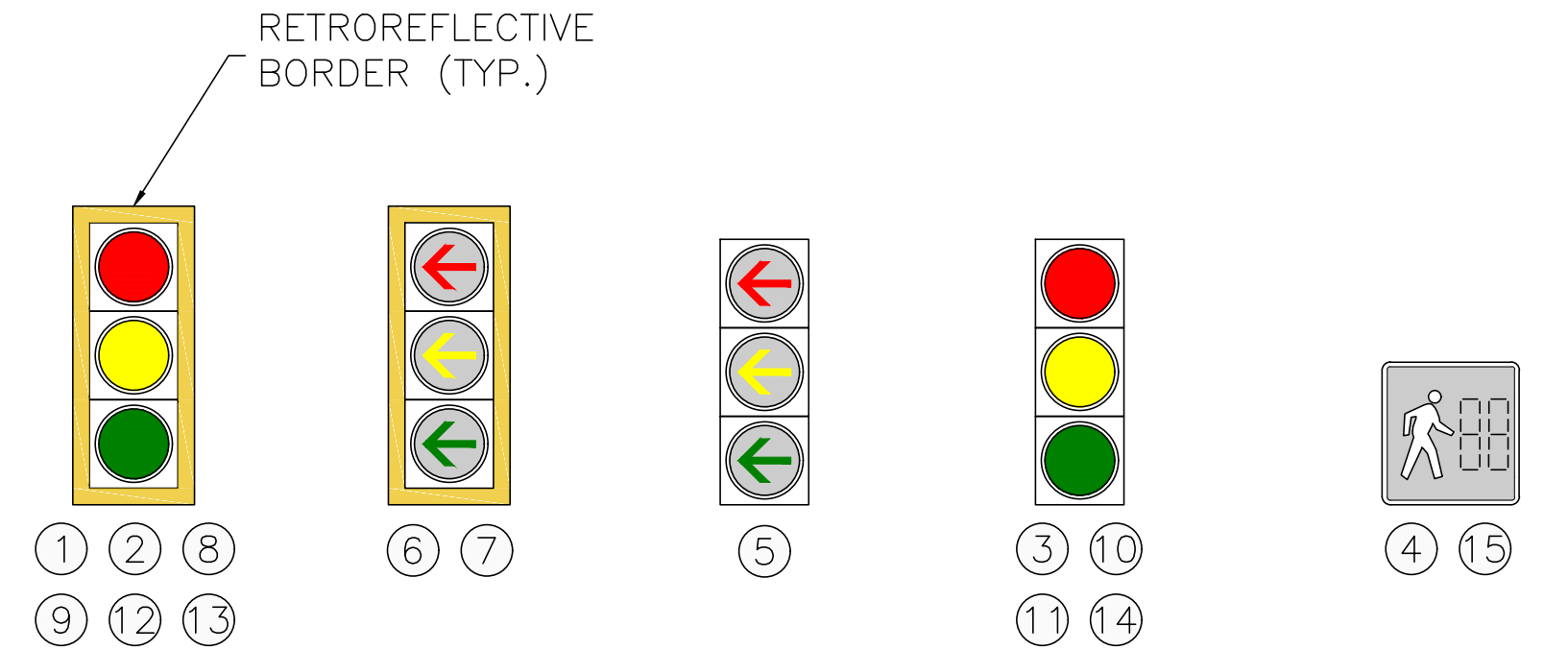
S:\121234-01 - Jackson Creek Parkway\04_CIVIL\CADD\Traffic\ITS\Drawings\105_Signal\T121234-01SIG-PLN01_5/5/2023 11:25:45 AM - Ben.Harms



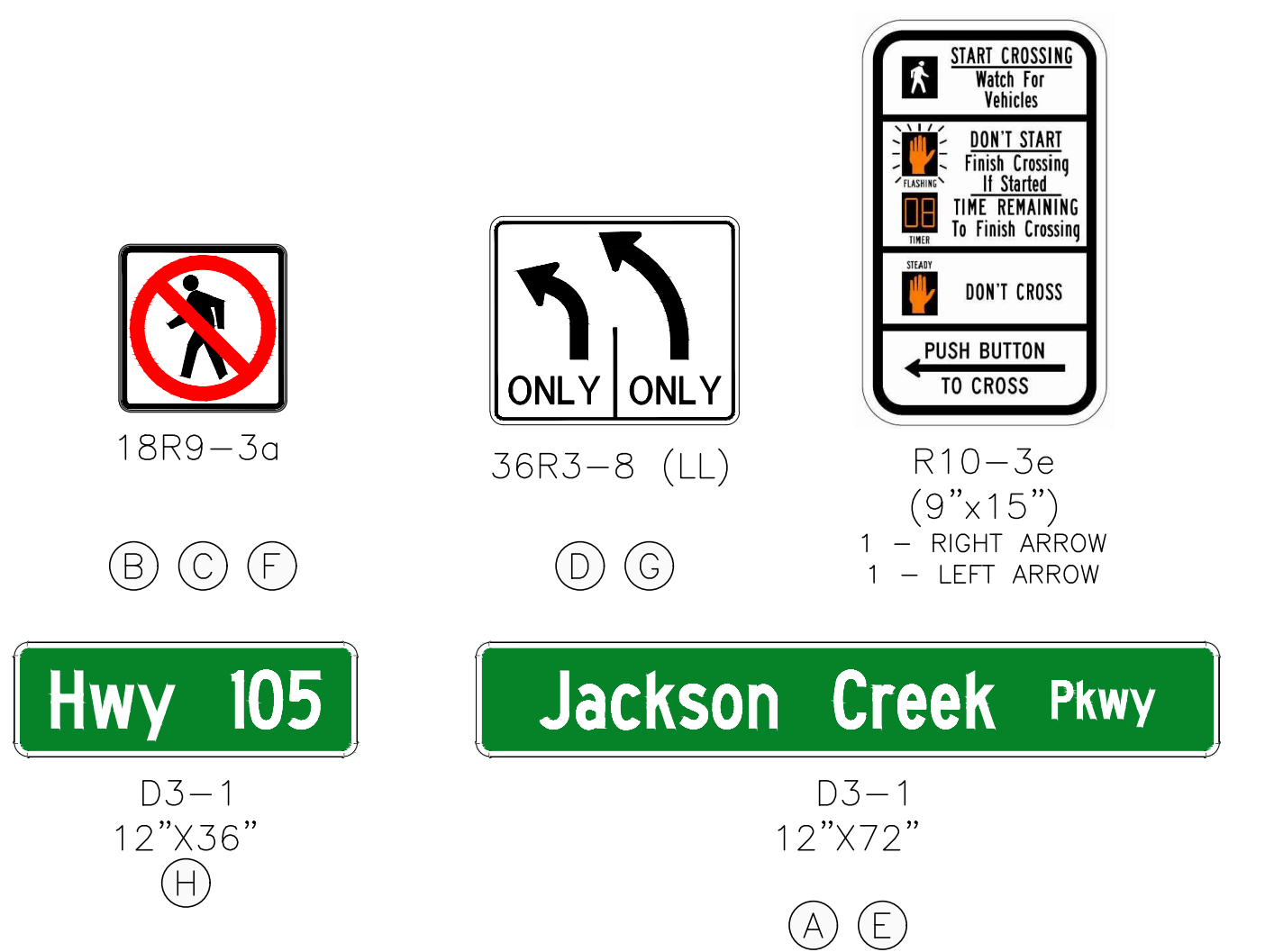
TRAFFIC SIGNAL SYMBOLS

- TRAFFIC SIGNAL POLE AND MAST ARM
- PEDESTRIAN PUSH BUTTON POLE
- LUMINAIRE
- TRAFFIC SIGNAL HEAD
- TRAFFIC SIGNAL HEAD & BACKPLATE
- PEDESTRIAN SIGNAL HEAD
- SIGNAL CONTROLLER CABINET
- (TYPE 3) (TYPE 4) PULL BOX
- SIGNAL CONDUITS
- VEHICLE DETECTOR CAMERA
- PED PUSH BUTTON & SIGN
- FIRE PREEMPTION UNIT
- RADIO COMMUNICATION ANTENNA

PROPOSED SIGNAL HEADS



PROPOSED SIGNS



PRINT DATE: 05/5/2023 11:25 AM
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 HORIZ. SCALE: 1" = 15' VERT. SCALE: N/A

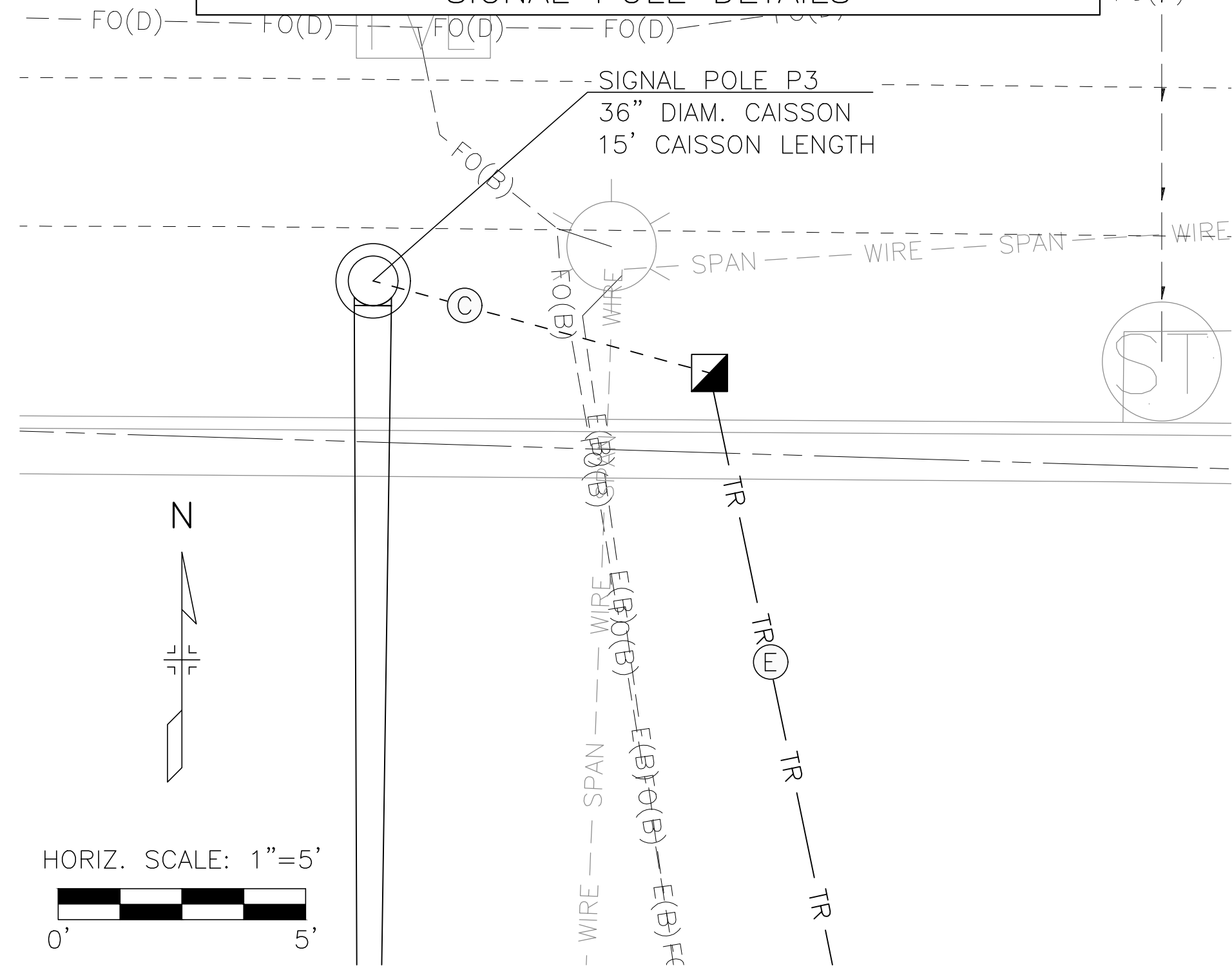
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| DATE | COMMENTS | INITIALS | |
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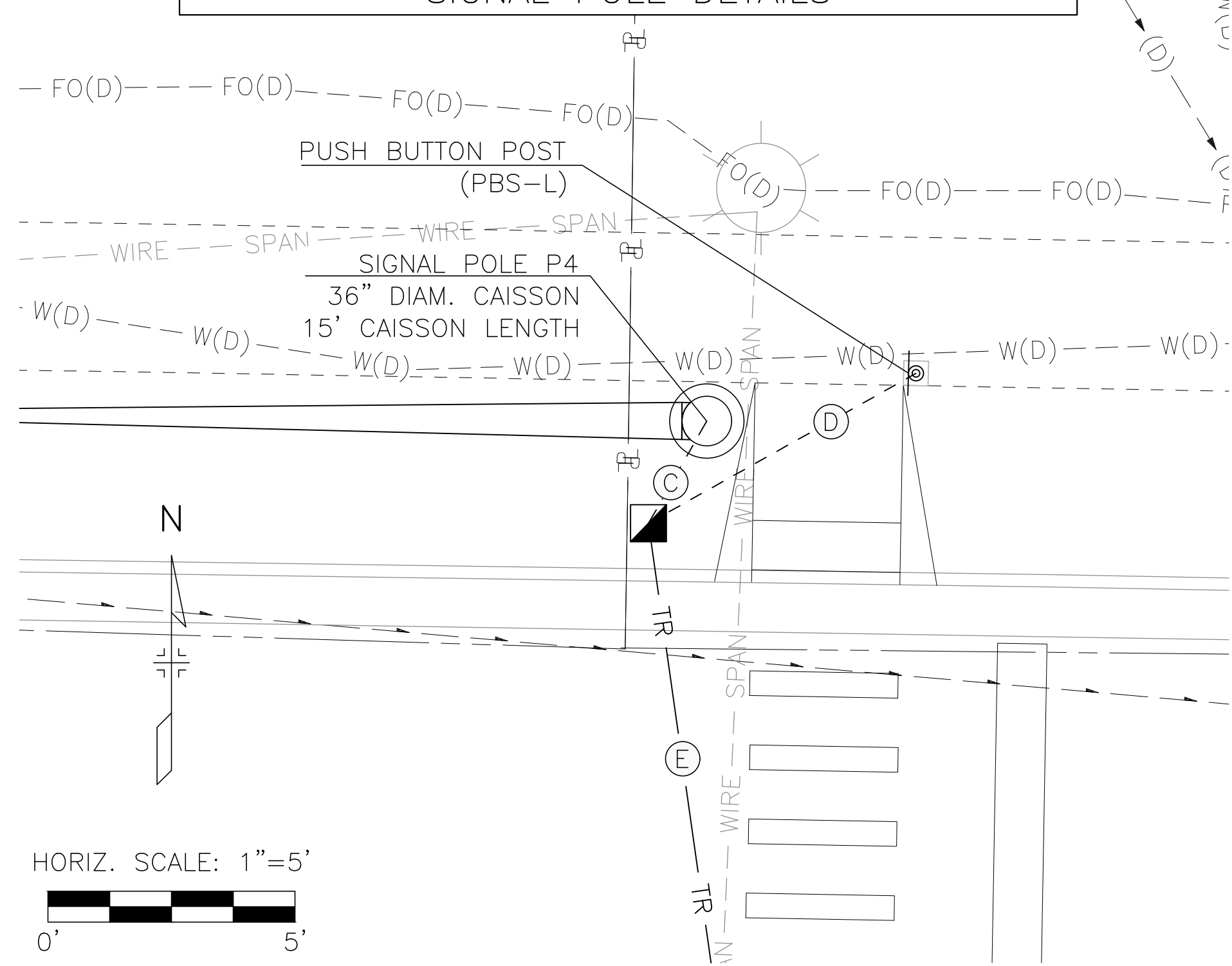


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| AS CONSTRUCTED | JACKSON CREEK PARKWAY TRAFFIC SIGNAL PLAN STATE HIGHWAY 105 | | PROJECT NO./CODE |
| NO. REVISIONS: | | | 121234-01 |
| REVISED: | DESIGNER: BJH | STRUCTURE NUMBERS | |
| VOID: | DETAILER: BEN.HARMS | SHEET SUBSET: TS-01 OF 2 | |
| | SHEET SUBSET: TRAFFIC | | |

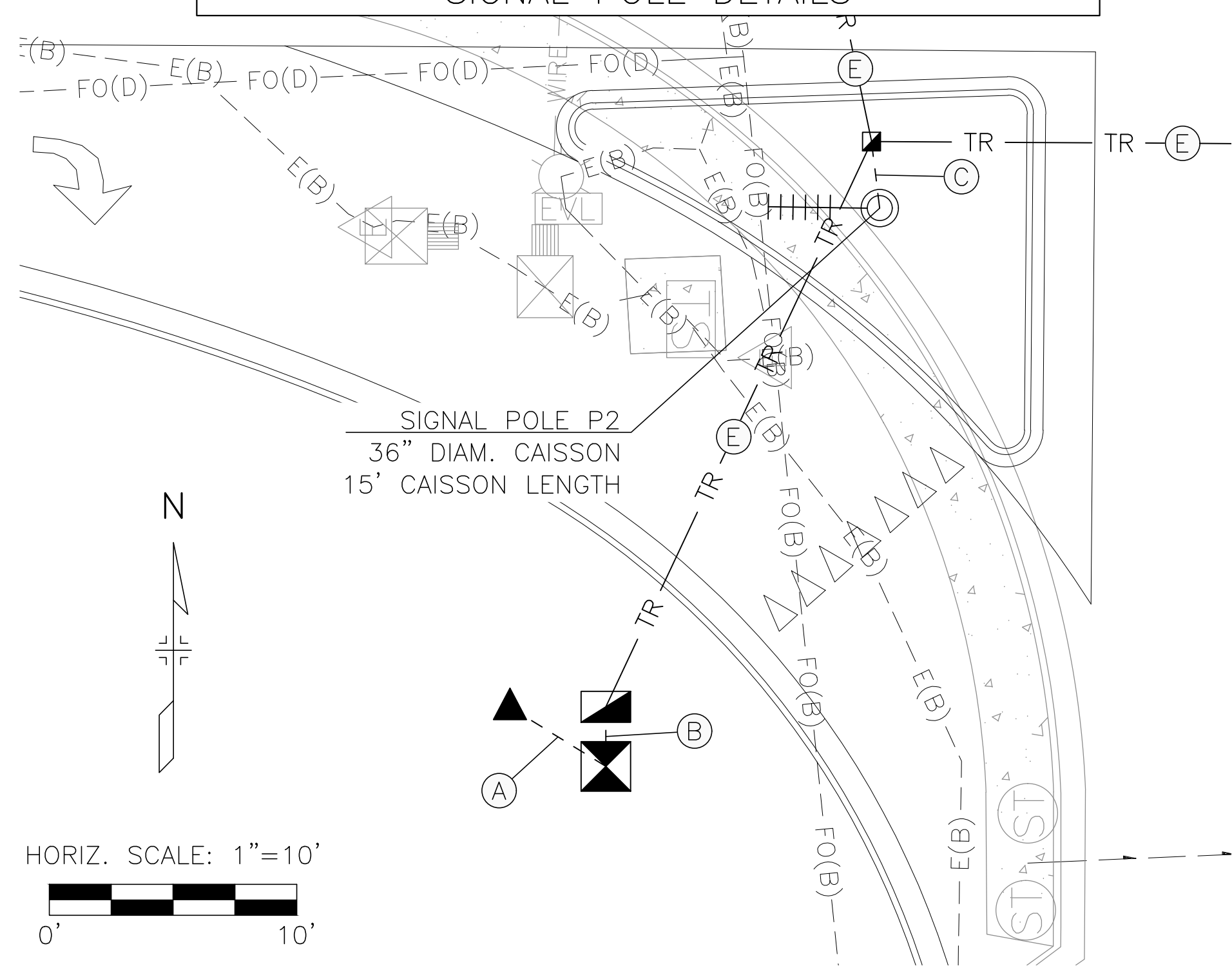
JACKSON CREEK PKWY/S.H. 105—NW CORNER
SIGNAL POLE DETAILS



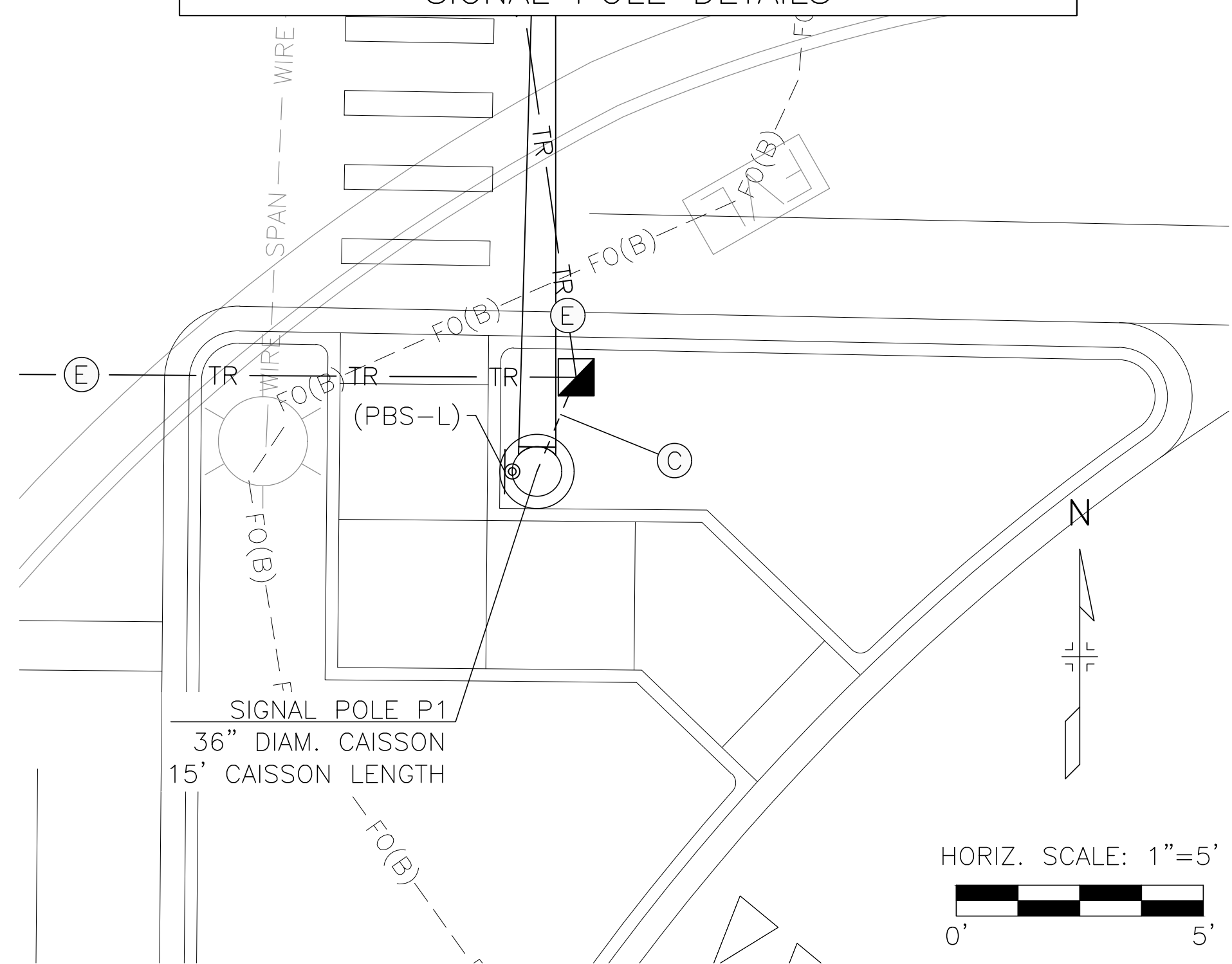
JACKSON CREEK PKWY/S.H. 105—NE CORNER
SIGNAL POLE DETAILS



JACKSON CREEK PKWY/S.H. 105—SW CORNER
SIGNAL POLE DETAILS



JACKSON CREEK PKWY/S.H. 105—SE CORNER
SIGNAL POLE DETAILS



LEGEND

(PBS-R/L = PED PUSH BUTTON STATION WITH
R10-3e RIGHT OR LEFT SIGN

- PULL BOX (TYPE 4) (24"x36"x24")
- PULL BOX (TYPE 3) (17"x30"x12")
- TR — TRAFFIC SIGNAL CONDUIT STREET CROSSINGS
- TRAFFIC SIGNAL CONDUIT PULL BOX CONNECTIONS

SIGNAL CONDUITS

- (A) POWER METER: 1-2" (TRENCHED)
- (B) CONTROLLER 1-2" AND 4-3" (TRENCHED)
- (C) SIGNAL POLES 1-2" AND 2-3" (TRENCHED)
- (D) PUSH BUTTON POST 2-2" (TRENCHED)
- (E) STREET CROSSINGS 1-2" AND 2-3" (BORED)

NOTE:

PULL BOX AND CONDUIT LOCATIONS ARE APPROXIMATE ONLY. TO THE EXTENT POSSIBLE, THE CONTRACTOR SHALL INSTALL PULL BOXES OUTSIDE OF SIDEWALK AREAS; HOWEVER, IF PULL BOXES NEED TO BE PLACED WITHIN SIDEWALKS, THEY SHALL BE INSTALLED FLUSH WITH THE FINISHED SURFACE AND THE LIDS SHALL HAVE AN ANTI-SKID SURFACE TREATMENT. PULL BOXES SHALL NOT BE INSTALLED IN ANY CURB RAMPS.

SCHEDULE OF TRAFFIC SIGNAL POLES

| POLE ID | TRAFFIC SIGNAL POLE | | STAKING LOCATION AND CAISSON DETAILS | | | |
|---------|---------------------|---------------|--------------------------------------|------------|----------|-------|
| | MAST ARM | LUMINAIRE ARM | NORTHING | EASTING | DIAMETER | DEPTH |
| P1 | 35 FT | 15 FT ARM | 1459419.60 | 3183115.91 | 36 IN | 15 FT |
| P2 | - | 15 FT ARM | 1459418.55 | 3183021.45 | 36 IN | 15 FT |
| P3 | 55 FT | 15 FT ARM | 1459515.01 | 3182989.04 | 36 IN | 15 FT |
| P4 | 30 FT | 15 FT ARM | 1459514.13 | 3183107.67 | 36 IN | 15 FT |



Know what's below.
Call before you dig.

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| AS CONSTRUCTED | JACKSON CREEK PARKWAY TRAFFIC SIGNAL POLE DETAILS STATE HIGHWAY 105 | | PROJECT NO./CODE |
| NO. REVISIONS: | | | 121234-01 |
| REVISED: | DESIGNER: BJH | STRUCTURE NUMBERS | |
| VOID: | DETAILER: BEN.HARMS | SHEET SUBSET: TRAFFIC | SHEET SUBSET: TS-02 OF 2 |

S:\121234-01 - Jackson Creek Parkway\04_CIVIL\CADD\Traffic\ITS\Drawings\105 Signal\T121234-01SIG-PLN02_5/5/2023 11:25:29 AM - Ben.Harms