

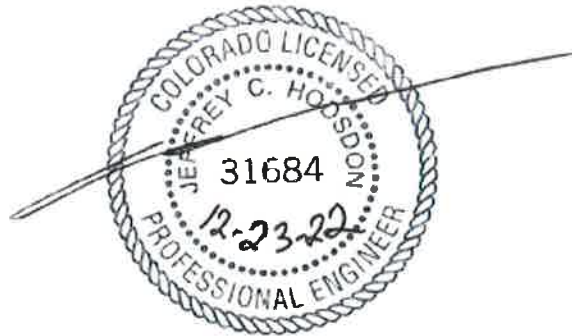


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WireNut Traffic Impact Study (LSC #S224620) December 23, 2022

Traffic Engineer's Statement

This traffic report and supporting information were prepared under my responsible charge and they comport with the standard of care. So far as is consistent with the standard of care, said report was prepared in general conformance with the criteria established by the County for traffic reports.




Developer's Statement

I, the Developer, have read and will comply with all commitments made on my behalf within this report.



HAMMERS CONST.



Date

WireNut

Traffic Impact Study

Prepared for:

Joe Butler
Hammers Construction
1411 Woolsey Heights
Colorado Springs, CO 80915

DECEMBER 23, 2022

LSC Transportation Consultants, Inc.
Jeffrey C. Hodsdon, P.E.

LSC #S224620



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December 23, 2022

Joe Butler
Hammers Construction
1411 Woolsey Heights
Colorado Springs, CO 80915

RE: WireNut
Transportation Memorandum
El Paso County, CO
LSC # S224620

Dear Mr. Butler,

LSC Transportation Consultants, Inc. has prepared this traffic impact study for the proposed WireNut development. The site is located to be located at 6365 East Platte Avenue in Colorado Springs, Colorado. Located at El Paso County parcel ID 5418001005, the 2.97-acre parcel would consist of approximately 17,255 square feet of warehouse and office space.

Access is proposed from the south terminus of the north/south access street located just west of the Hathaway/Platte Avenue (US Highway 24) Frontage Road three-quarter-movement intersection. This access street serves multiple businesses and connects to the US Highway 24 frontage road 185 feet west of the connection to the mainline of US Highway 24.

This report has been prepared for submittal to El Paso County and CDOT.

REPORT CONTENTS

The preparation of this report included the following:

- Inventory of existing adjacent and nearby area street system. This included surface conditions, functional classifications, roadway widths, lane configurations, traffic control, posted speed limits, pavement markings, intersection and access spacing, roadway and intersection alignments, auxiliary left- and right-turn lanes, intersection sight distances, etc.;
- Summary of morning and late-afternoon peak-hour turning-movement traffic counts at the following intersections:
 - Platte Avenue/Motel Road (right-in/right out (RIRO) access)
 - Motel Road/Platte Avenue frontage road
 - Platte Avenue/Hathaway Drive
 - Hathaway Drive/Platte Avenue frontage road

- Estimates of short-term baseline traffic volumes, which have been revised to reflect the required closure of the Motel Road RIRO connection to the US Hwy 24 “mainline;”
- Review of relevant traffic studies for pertinent information and improvements adjacent to this development. Other recent studies completed in the area and any applicable data/transferrable information/analysis etc. from previous LSC studies adjacent to the site were also utilized;
- Estimates of average weekday and peak-hour trip generation for the proposed development;
- Estimation of directional distribution of site-generated vehicle trips on the area street system, the study-area intersections, and the proposed site-access points on Motel Road;
- Projections of site-generated turning-movement traffic volumes at the following “study-area” intersections:
 - Platte Avenue/Hathaway Drive
 - Hathaway Drive/Platte Avenue frontage road
 - Motel Road/North-South access drive
- Estimates of long-term background-traffic volumes at the study-area intersections and access points;
- Total traffic (site traffic-plus-baseline/background traffic) projections at the study-area intersections for the short and long term;
- Level of service (LOS) analysis at the study-area intersections;
- Evaluation of existing, short-term, and long-term projected intersection volumes to determine the potential need for any new auxiliary right-/left-turn lanes, based on the criteria in CDOT’s *State Highway Access Code*.
- Other recommended improvements/modifications to the study-area streets and intersections, including street system/intersection improvements, intersection traffic control, and/or signage and pavement-marking modifications as required; and
- Summary of compiled data, analysis, findings, and recommendations.

PRIOR AREA TRAFFIC REPORTS

LSC utilized the following previous traffic reports to assist in the production of this report:

- *HCD Drilling* – April 20, 2022 (by LSC)
- *Freedom Springs* – July 2018 (by LSC)

LSC has referred to traffic count data and traffic projections contained in the HCD Drilling traffic study as part of preparation of this report. New December 2022 traffic counts have also been conducted at the two frontage road intersections just north of the site.

LAND USE AND ACCESS

Site Land Use

Figure 1 shows the site location relative to the adjacent and nearby streets. The proposed WireNut development is planned to be located at 6365 East Platte Avenue in Colorado Springs, Colorado. Located at El Paso County parcel ID 5418001005, the 2.97-acre parcel would be developed with a 17,255 square-foot building for use by WireNut. The space allocation would be approximately 11,160 square feet of warehouse space and 6,055 square feet of office space. A copy of the site plan is shown in Figure 2. WireNut Home Services employs electricians, professional plumbers, and HVAC technicians.

Site Access

Access is proposed from the south terminus of the north/south access street located just west of the Hathaway/Platte Avenue (US Highway 24) Frontage Road three-quarter-movement intersection. This access street serves multiple businesses and connects to the US Highway 24 frontage road 185 feet west of the connection to the mainline of US Highway 24.

The most recent TIS report for the HCD Drilling application reflects the CDOT directive to HCD Drilling to close the existing right-in/right-out (RIRO) vehicular connection to the US Hwy 24 "mainline." This RIRO connection is located 725 feet west of the frontage road/north-south access drive intersection (which will provide access to the WireNut development). The existing three-quarter-movement connection to the US Highway 24 mainline will remain open to provide access to this area. This report has been prepared assuming the closure of that RIRO.

SITE ACCESS SIGHT DISTANCE

CDOT Requirements

The proposed site-access points must meet *Colorado State Highway Access Code* standards for sight distance. The existing access drive intersection with the frontage road is proposed to remain full-movement and stop-sign controlled. All sight-distance field measurements would utilize a height of 3.5 feet for driver's eye height and for vehicles approaching from the east or west.

Entering Sight Distance

With an assumed 25-mph posted speed limit (unposted in the vicinity of the site), the minimum required entering/intersection sight distance for both approaches at the proposed site-access locations is 250 feet for passenger vehicles and 325 feet for single-unit trucks (per Table 4-2 of the *State Highway Access Code*). Sight-distance field measurements for both approaches at both proposed site-access locations to the frontage road meet the required 250-foot requirement:

- Site access intersection with US Hwy 24 frontage road
 - Looking east – unobstructed to stop sign at three-quarter access (185 feet)
 - Looking west – greater than 250 feet

Sight Distance Along Highway

The minimum required “sight distance along a highway” for both approaches at the proposed site-access locations is 150 feet for passenger vehicles (per Table 4-1 of the *State Highway Access Code*). Sight-distance field measurements for both approaches at the proposed site-access location to the frontage road meets the required 150-foot requirement.

ROAD AND TRAFFIC CONDITIONS

Figure 1 shows the streets adjacent to and in the vicinity of the site. Adjacent roads serving the site are identified below, followed by a brief description of each:

Platte Avenue (US Hwy 24) is a four-lane east/west state highway that locally extends from Colorado Springs to Falcon. US Hwy 24 is classified as an Expressway by the Colorado Department of Transportation and is shown as an Expressway on the *County Major Transportation Corridors Plan (MTCP)*. The US Hwy 24/Peterson Road intersection is grade-separated. There is an existing at-grade three-quarter-movement intersection in the vicinity of the site at Hathaway Drive. Figure 28 of CDOT’s *US 24 Planning and Environmental Linkage (PEL) Study* shows that the three-quarter access intersection at US Hwy 24 just south of Hathaway Drive will remain unchanged through 2040. Please refer to the “Site Access” section above regarding the anticipated/assumed closure of the existing RIRO connection to the US Highway 24 mainline.

Motel Road is a two-lane non-arterial street without a posted speed limit in the vicinity of the site. Eastbound right-turn auxiliary turn lanes currently exist at both stop-sign-controlled frontage road connections to Platte Avenue.

Hathaway Drive is the north leg/connection to the three-quarter-movement intersection with US Hwy 24. Approximately 50 feet north of US 24, there is a yield-sign-controlled T-intersection.

Existing Traffic Volumes

Vehicular turning-movement counts were conducted at the following intersections and dates/times:

- Platte Avenue/Hathaway Drive (three-quarter-movement access)
 - Thursday, December 15, 2022 from 6:30 – 8:30 a.m.
 - Thursday, December 15, 2022 from 4:00 - 6:00 p.m.
- Motel Road/Hathaway Drive (south of Platte Avenue)
 - Thursday, December 15, 2022 from 6:30 – 8:30 a.m.
 - Thursday, December 15, 2022 from 4:00 - 6:00 p.m.
- Platte Avenue/RIRO access to Motel Road
 - Tuesday, July 13, 2021 from 6:30 – 8:30 a.m.
 - Tuesday, July 13, 2021 from 4:00 - 6:00 p.m.
- Motel Road/frontage road RIRO access (south of Platte Avenue)
 - Wednesday, July 14, 2021 from 6:30 – 8:30 a.m.
 - Wednesday, July 14, 2021 from 4:00 - 6:00 p.m.

Existing morning and evening weekday peak-hour traffic volumes at these intersections, as well as the estimated existing annual average daily traffic (AADT) volumes adjacent to the site, are shown in Figure 3. Raw count reports are attached.

Short-Term Baseline Traffic Volumes

Figure 4 shows estimated “short-term baseline” traffic volumes on the study-area streets and at the study-area intersections (short-term peak-hour turning-movement volumes). Previous LSC traffic counts from *HCD Drilling* were also referenced to establish short-term baseline traffic volumes, as those estimates include the completion of the HCD Drilling development in the vicinity that were assumed to have been completed during the short term.

The short-term baseline volumes also reflect LSC adjustments accounting for the required closure of the existing RIRO connection the US Hwy 24 mainline, located approximately 725 feet west of the access drive that will serve the WireNut site. All existing traffic using this RIRO has been reassigned to the existing three-quarter access to reflect adjusted traffic patterns.

Field Observations at US Hwy 24/Three-Quarter Access

LSC conducted field observations of operations at the three-quarter access to US Hwy 24 during both the morning and afternoon peak hours. The eastbound-left/U-turn-lane queue was consistently 6-12 vehicles, with multiple occurrences of additional vehicles arriving at the back of queue as the queue began to clear. In general, drivers making an eastbound-U-turn contributed more towards control delay for this turning movement, often waiting 3-5 minutes before enough of a gap formed upstream on Platte Avenue for them to safely conduct a U-turn.

Several vehicles (mostly heavy vehicles from businesses located on the Motel Road/Platte Avenue frontage road) were observed to turn eastbound-left onto Hathaway Drive rather than waiting in the eastbound-U-turn queue. After turning eastbound-left onto Hathaway Drive, these drivers would immediately make a northbound-U-turn in the wider intersection of Hathaway Drive/Ford Road before

turning 180 degrees back towards Platte Avenue to turn southbound-right onto Platte Avenue. This turning-movement combination was observed to be noticeably quicker for vehicles wishing to travel back towards downtown Colorado Springs compared to those drivers completing a standard eastbound-U-turn movement at Platte Avenue/Hathaway Drive.

Northbound- and southbound right-turning vehicles were seen using their respective right-turn acceleration lane to merge into through traffic on Platte Avenue. The eastbound right-turn acceleration lane is continuous to Peterson Avenue (approximately one-quarter mile to the east), while the westbound right-turn acceleration lane is continuous to Valley Drive (about one-quarter mile to the west).

TRIP GENERATION

Estimates of the projected vehicle trips to be generated by the WireNut development have been made using nationally-published average trip-generation rates for land use code “180 – Specialty Trade Contractor” in *Trip Generation, 11th Edition, 2021* by the Institute of Transportation Engineers (ITE). Estimates are based on ITE rates with “building square footage” as the predictor variable.

Table 1 below presents a summary of the estimated site trip generation. A detailed trip-generation estimate for the site, including ITE rates for the proposed land use, is presented in Table 3 (attached).

Table 1: Estimated External Site Vehicle-Trip Generation

| Analysis Period | Weekday | | |
|-------------------|---------|-----|-------|
| | In | Out | Total |
| Morning Peak Hour | 21 | 7 | 28 |
| Evening Peak Hour | 11 | 23 | 34 |
| Daily/24-hour | 85 | 85 | 169 |

Based on the ITE estimate, the proposed WireNut development is projected to generate about 169 total vehicle trips on the average weekday. During the weekday morning peak hour, approximately 21 vehicles would enter and 7 vehicles would exit the site. Approximately 11 entering vehicles and 23 exiting vehicles are projected for the weekday evening peak hour.

TRIP DISTRIBUTION AND ASSIGNMENT

Trip Directional Distribution

Estimating the directional distribution of site-generated vehicle trips to the study-area roads and intersections is a necessary component in determining the site’s traffic impacts. Figure 5 shows the percentages of the site-generated vehicle trips projected to be oriented to and from the site’s major

approaches. This report utilizes the same directional distribution as the HCD Drilling report, with localized trip routing adjustments specific to the location of and access to/from this site.

Site-Generated Traffic

Figure 6 shows the projected site-generated traffic volumes for the weekday morning and evening peak hours. Site-generated traffic volumes at the study-area intersections have been calculated by applying the directional distribution percentages estimated by LSC (from Figure 5) to the trip-generation estimates (from Table 3).

Short-Term Baseline-Plus-Site-Generated Traffic Volumes

Figure 7 shows the sum of the short-term baseline traffic volumes (from Figure 4) and site-generated peak-hour traffic volumes (shown in Figure 6). These volumes represent the projected short-term total traffic, assuming completion of the site development.

Estimated Future 2042 Background Traffic Volumes

Figure 8 shows the projected 20-year background traffic volumes for the year 2042. Estimated 2042 background through traffic volumes on Platte Avenue, Hathaway Drive, and Motel Road are based on projected background growth of undeveloped parcels in the vicinity of the site. Projected short-term and long-term volumes from previous LSC traffic studies in the vicinity of the site were used to estimate future background traffic growth on roadways adjacent to the site. CDOT's 20-year growth factor for Platte Avenue is 1.42, representing a 2.1 percent annual growth rate. Projected 20-year background traffic volumes do **not** include projected traffic to be generated by the proposed WireNut site.

Adjustments were also made to account for the anticipated closure of the existing RIRO connection between the frontage road and the US Hwy 24 (mainline). All projected traffic at the RIRO has been reassigned to the three-quarter access located to the east.

Future 2042 Total Traffic Volumes

Figure 9 shows the projected 2042 total traffic volumes, which are the sum of 2042 background traffic volumes (from Figure 8) plus the site-generated traffic volumes (from Figure 6).

LEVEL OF SERVICE ANALYSIS

The following intersections have been analyzed to determine the projected intersection levels of service for short- and long-term traffic scenarios for the morning and evening peak-hour time periods:

- Motel Road/Platte Avenue frontage road
- Platte Avenue/Hathaway Drive
- Motel Road/proposed site accesses

Level of service (LOS) is a quantitative measure of the level of congestion or delay at an intersection and is indicated on a scale from “A” to “F.” LOS A is indicative of little congestion or delay. LOS F indicates a high level of congestion or delay. Table 2 shows the level of service delay ranges for signalized and unsignalized intersections.

Table 2: Intersection Levels of Service Delay Ranges

| Level of Service | Signalized Intersections | Unsignalized Intersections |
|------------------|---|--|
| | Average Control Delay (seconds per vehicle) | Average Control Delay (seconds per vehicle) ⁽¹⁾ |
| A | 10.0 sec or less | 10.0 sec or less |
| B | 10.1-20.0 sec | 10.1-15.0 sec |
| C | 20.1-35.0 sec | 15.1-25.0 sec |
| D | 35.1-55.0 sec | 25.1-35.0 sec |
| E | 55.1-80.0 sec | 35.1-50.0 sec |
| F | 80.1 sec or more | 50.1 sec or more |

(1) For unsignalized intersections, if V/C ratio is greater than 1.0 the level of service is LOS F, regardless of the projected average control delay per vehicle.

Detailed Synchro reports are attached. A summary of LOS during the weekday morning and evening peak hours for the following unsignalized intersections is shown in the following figures:

- Figure 3: Existing Traffic, Lane Geometry, Traffic Control, and LOS
- Figure 4: Short-Term Baseline Traffic, Lane Geometry, Traffic Control, and LOS
- Figure 7: Short-Term Total Traffic, Lane Geometry, Traffic Control, and LOS
- Figure 8: 2042 Background Traffic, Lane Geometry, Traffic Control, and LOS
- Figure 9: 2042 Background + Site Traffic, Lane Geometry, Traffic Control, and LOS

Platte Avenue/Hathaway Drive (Three-Quarter Intersection)

Short Term

The eastbound left-turning movement at this three-quarter-movement intersection currently operates at LOS F and is projected to remain at LOS F during at least one short-term peak-hour traffic scenarios. Note: no site-generated traffic would be added to this turning movement. All other individual turning movements would operate at LOS D or better during the short term, with or without the addition of site-generated traffic.

Although the *Highway Capacity Manual (HCM)* LOS analysis indicates LOS E or worse, field observations indicate that upstream signals at US Hwy 94 and Marksheffel Road produce gaps in the westbound traffic stream that allow left-turning movements to occur. Please refer to the “Field Observations at US Hwy 24/Hathaway Drive” section above for more detail.

Long Term

HCM analysis indicates that the following individual turning movements at this three-quarter-movement intersection would operate at LOS F during all long-term scenarios, with or without the addition of site-generated traffic:

- Eastbound-left/U-turn, westbound-left/U-turn, northbound-right, and southbound-right

Synchro *HCM*-calculated control delays are in the LOS F range for this intersection. However, the formula-calculated values likely exceed realistic levels. For additional detail, please refer to the attached *HCM* calculation sheets.

Northbound-to-eastbound right-turn acceleration lanes exist at this access point to Platte Avenue. However, these are not accounted for in the *HCM* LOS results, even though the acceleration lanes would likely reduce delay, assuming drivers used them properly.

Field observations indicate that upstream signals at the US Hwy 94, Marksheffel Road, and Powers Boulevard cross-street intersections produce gaps in the westbound traffic stream that allow left-turning and right-turning movements to occur. Until grade-separated interchanges replace the upstream signalized intersections in the future, these traffic gaps will continue to be generated.

Motel Road (Frontage Road)/North/South Access Drive

All approaches and individual turning movements at the north/south access drive/Motel Road (frontage road) intersection are projected to operate at LOS A through 2042 during both peak hours.

Frontage Road/Hathaway Drive (Adjacent to the Three-Quarter Intersection with US Hwy 24)

All approaches and individual turning movements at the Hathaway Drive/frontage road intersection south of Platte Avenue currently operate at and are projected to remain at LOS A through 2042 during both peak hours, with or without the addition of site-generated traffic.

AUXILIARY TURN-LANE ANALYSIS

The *State Highway Access Code* contain turning-volume thresholds which require auxiliary left- or right-turn lanes by roadway classifications. Roadway classifications for key thoroughfares in the vicinity of the site are based on the *State Highway Access Category Assignment Schedule*.

- Platte Avenue (US Hwy 24) – Expressway
- Motel Road – frontage road

Platte Avenue/Hathaway Drive (Three-Quarter-Movement Intersection)

The addition of site-generated traffic would increase the total entering/exiting traffic volume at this intersection by less than 20 percent. The WireNut development is projected to increase the existing total traffic volume at Platte Avenue/Hathaway Drive (three-quarter access) by 14 percent compared to short-term baseline background-only traffic scenario.

Eastbound Left-Turn Deceleration Lane

NOTE: With the closure of the RIRO connection to US Hwy 24, this turn lane will not be utilized by site traffic. Also, CDOT is not requiring this project to improve this lane.

Westbound Left-Turn Deceleration Lane

The existing westbound left-turn lane length is 600 feet, consisting of approximately 300 feet of full-width lane plus stacking distance and a 300-foot taper.

CDOT “EX – Expressway” left-turn deceleration lane requirements for a 55-mph posted limit at this approach are 862 feet:

- 600 feet of full-width deceleration length
- 40 feet of stacking for turning vehicles (based on projected 2042 volumes)
- 222-foot lane transition taper (18.5:1 ratio)

The existing turn lane does not currently meet CDOT Access Code standards. As such, the existing westbound left-turn lane would need to be lengthened 262 feet (from its existing 600-foot length) in order to meet CDOT turn-lane design requirements. There may be constraints to lengthening this lane, such as the grade difference between eastbound and westbound lanes and/or large sign structures in the center median just to the east of this access location. **Per the meeting with CDOT staff on March 16, 2022 for the HCD Drilling development application, CDOT is not requiring the HCD Drilling project to improve this lane. CDOT indicated that the HCD Drilling project will have the obligation to close the RIRO connection to US Hwy 24 and restripe/resign the eastbound right-turn deceleration lanes. LSC anticipates that CDOT will require fair-share participation/cooperation with HCD Drilling in the completion of these improvements. If for some reason WireNut develops before HCD, it is likely that WireNut will need to complete these improvements, in which case there would likely be an opportunity for cost recovery.**

Eastbound Right-Turn Deceleration Lane

Currently, the eastbound right-turn lane is a continuous deceleration lane extending for approximately 815 feet between the RIRO access (to the west/**to be closed**) and the three-quarter movement intersection at Hathaway Drive. Following the closure of the RIRO access to the west, this lane will need to be restriped/resigned and combined with the current

right-turn deceleration lane for the RIRO intersection. This will result in a longer right-turn deceleration lane for the three-quarter access. **CDOT indicated that the HCD Drilling project will have the obligation to close the RIRO connection to US Hwy 24 and restripe/resign the eastbound right-turn deceleration lanes. LSC anticipates that CDOT will require fair-share participation/cooperation with HCD Drilling in the completion of these improvements. If for some reason WireNut develops before HCD, it is likely that WireNut will need to complete these improvements, in which case there would likely be an opportunity for cost recovery.**

Northbound-to-Eastbound Right-Turn Acceleration Lane

Currently, the northbound-to-eastbound right-turn acceleration lane is a continuous lane extending for approximately one-quarter mile between this three-quarter access at Hathaway Drive and the eastbound off-ramp to Peterson Boulevard (to the east). No modifications would be required to the existing eastbound right-turn acceleration lane design at this intersection.

Platte Avenue/RIRO Movement Intersection

Per CDOT, this RIRO “access” connection to the US Hwy 24 “mainline” will need to be permanently closed by HCD Drilling. Following the closure of this RIRO access, the eastbound right-turn deceleration lane for this access will be added to the current continuous eastbound right turn extending to the three-quarter access to the east (through restriping/resigning). The result will be a longer right-turn deceleration lane for the three-quarter access. The HCD Drilling applicant will submit design plans for access closure, along with a signing/striping modification plan (Notice-to-Proceed (NTP) stage of the permit process). **CDOT indicated that the HCD Drilling project will have the obligation to close the RIRO connection to US Hwy 24 and restripe/resign the eastbound right-turn deceleration lanes. LSC anticipates that CDOT will require fair-share participation/cooperation with HCD Drilling in the completion of these improvements. If for some reason WireNut develops before HCD, it is likely that WireNut will need to complete these improvements, in which case there would likely be an opportunity for cost recovery.**

CDOT “EX – Expressway” right-turn deceleration lane requirements for a 55-mph posted limit at this approach are 822 feet:

- 600 feet of full-width deceleration length
- 222-foot lane transition taper (18.5:1 ratio)

The length of the longer right-turn deceleration lane for the three-quarter access would be 1,275 feet (lane plus taper), which would exceed CDOT’s 822-foot minimum length.

Motel Road Study-Area Intersections

Motel Road is controlled by CDOT and classified as a frontage road. As such, no auxiliary turn lanes would be required at either of the study-area intersections along the frontage road. Through traffic volumes are relatively light, the speed limit is unposted (assumed to be 25 mph),

and the roadway has acceptable sight distance in both directions at all study-area intersections on Motel Road. Additionally, traffic control already exists on the northbound approach at each of the un-named site-access intersection with Motel Road, so the widening for additional turn lanes would not be required at the site access.

Per Section 3.5(5) of CDOT's *State Highway Access Code*:

The auxiliary lanes required in the category design standards may be waived when the 20th year predicted roadway volumes conflicting with the turning vehicle are below the following minimum volume thresholds. The right turn deceleration lane may be dropped if the volume in the travel lane is predicted to be below 150 DHV. The left turn deceleration lane may be dropped if the opposing traffic is predicted to be below 100 DHV. The right turn acceleration lane may be dropped if the adjacent traveled lane is predicted to be below 120 DHV. The left turn acceleration lane may be dropped if the volume in the inside lane in the direction of travel is predicted to be below 120 DHV.

Although the westbound-left turning movement at the frontage road/access drive would exceed the threshold for requiring a left-turn deceleration lane, projected opposing eastbound-through traffic would be less than 100 vph. Per Section 3.5(5) of CDOT's *State Highway Access Code*, the left-turn deceleration lane requirement can be waived at the site-access drive.

EL PASO COUNTY ROAD IMPACT FEE PROGRAM

This project will be required to participate in the El Paso County Road Improvement Fee Program.

The applicant will be able to join one of the two special districts or opt out of the district options. The applicant will select an option. If the opt-out option is selected by the applicant, the building permit fee rate for an "Industrial" land use in the Road Impact Fee Schedule would be \$3,651 per thousand square feet (KSF). The project with 17.225-KSF would translate to a building permit fee of \$62,888.

Note: This is based on the current rate, which is subject to change. El Paso County updates this rate periodically.

This does not match the Letter of Intent which has Office and Warehouse.

CONCLUSIONS

- The site is projected to generate about 169 new driveway vehicle trips on the average weekday.
- During the weekday morning peak hour of adjacent street traffic, 21 vehicles would enter the site while 7 vehicles would exit.
- During the weekday evening peak hour of adjacent street traffic, 11 vehicles would enter the site while 23 vehicles would exit.

- Please refer to the “Level of Service” section above for detailed LOS analysis results for individual turning movements and approaches at all studied intersections, during both peak hours through the 2042 horizon year.
- CDOT indicated that the HCD Drilling project will have the obligation to close the RIRO connection to US Hwy 24 and restripe/resign the eastbound right-turn deceleration lanes. LSC anticipates that CDOT will require fair-share participation/cooperation with HCD Drilling in the completion of these improvements. If for some reason WireNut develops before HCD, it is likely that WireNut will need to complete these improvements, in which case there would likely be an opportunity for cost recovery. Please refer to the “Auxiliary Turn-Lane Analysis” section for details.
- A CDOT access permit will be likely required for this project.

* * * * *

Please contact me if you have any questions regarding this report.

Respectfully Submitted,

LSC TRANSPORTATION CONSULTANTS, INC.

By: Jeffrey C. Hodsdon, P.E.
Principal

JCH/JCH:JAB

Enclosures: Table 3
Figure 1 - Figure 9
Site Plan
Synchro LOS Reports
Traffic Counts

Table 3



Table 3: Detailed Trip Generation Estimate

| ITE | | Value | Units ¹ | Trip Generation Rates ² | | | | | Driveway Trips Generated | | | | |
|---|----------------------------|--------|--------------------|------------------------------------|------|------|------|------|--------------------------|------|----|------|----|
| Code | Description | | | Average Weekday | A.M. | | P.M. | | Average Weekday | A.M. | | P.M. | |
| | | | | In | Out | In | Out | | In | Out | In | Out | |
| 180 | Specialty Trade Contractor | 17.225 | KSF | 9.82 | 1.23 | 0.43 | 0.62 | 1.31 | 169 | 21 | 7 | 11 | 23 |
| ¹ KSF = 1,000 square feet | | | | | | | | | | | | | |
| ² Source: <i>Trip Generation, 11th Edition (2021)</i> by the Institute of Transportation Engineers (ITE) | | | | | | | | | | | | | |
| Updated: December 13, 2022 | | | | | | | | | | | | | |

Figures 1-9





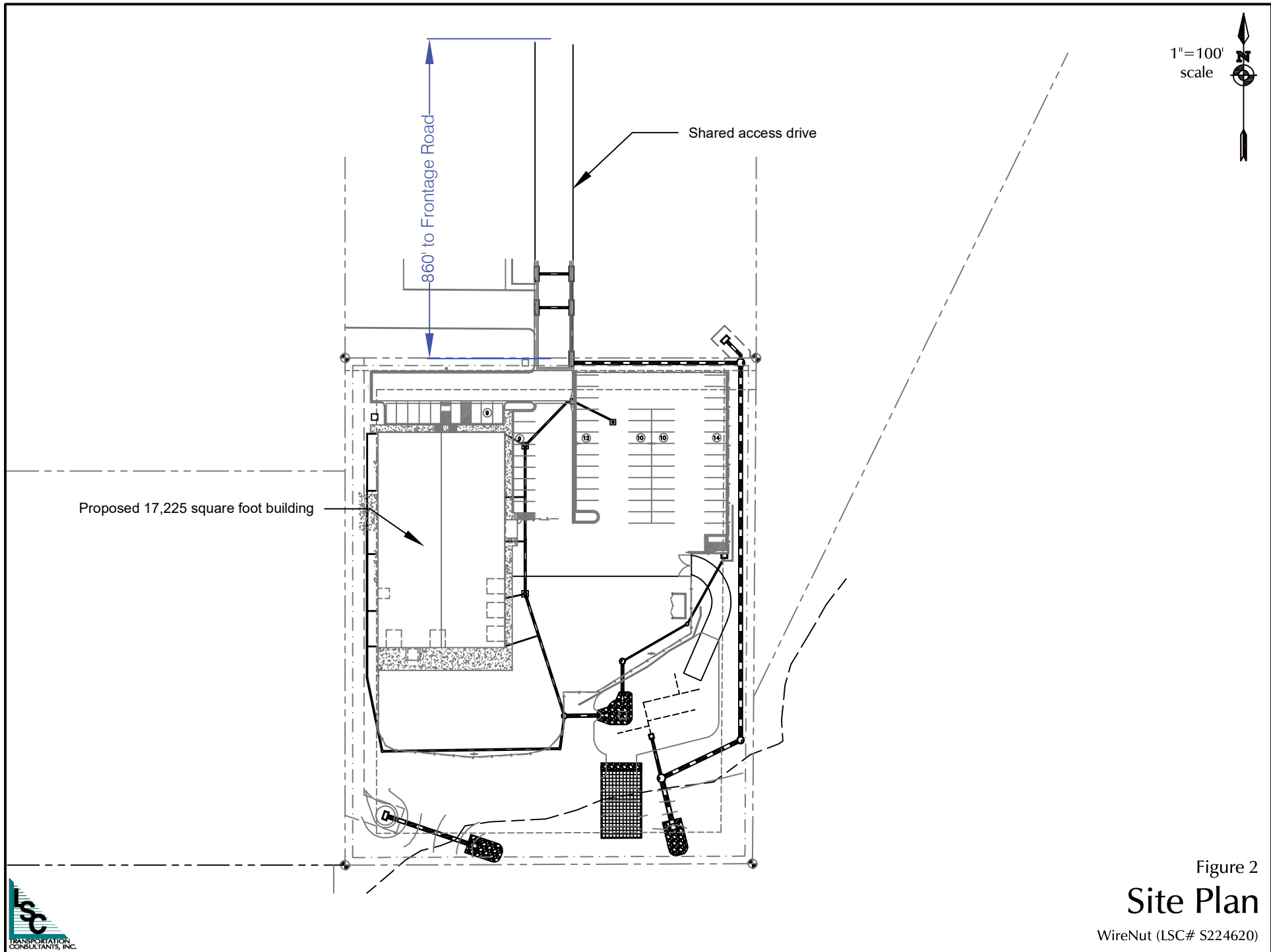
Not to scale



Figure 1

Vicinity Map

WireNut (LSC# S224620)



1"=100'
scale

Proposed 17,225 square foot building

Shared access drive

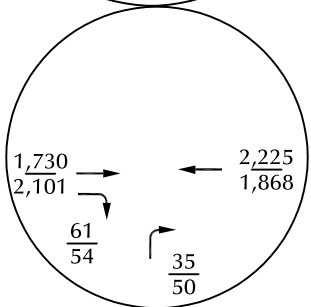
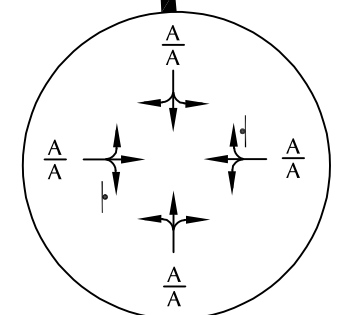
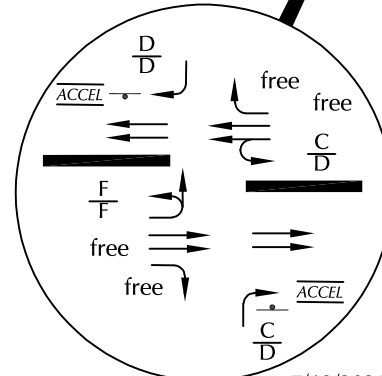
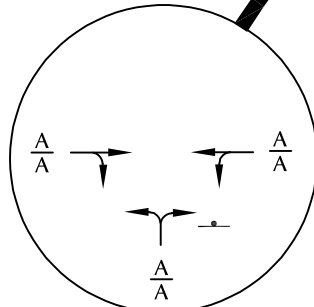
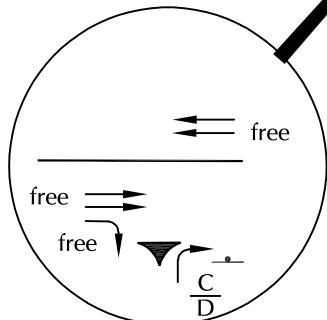
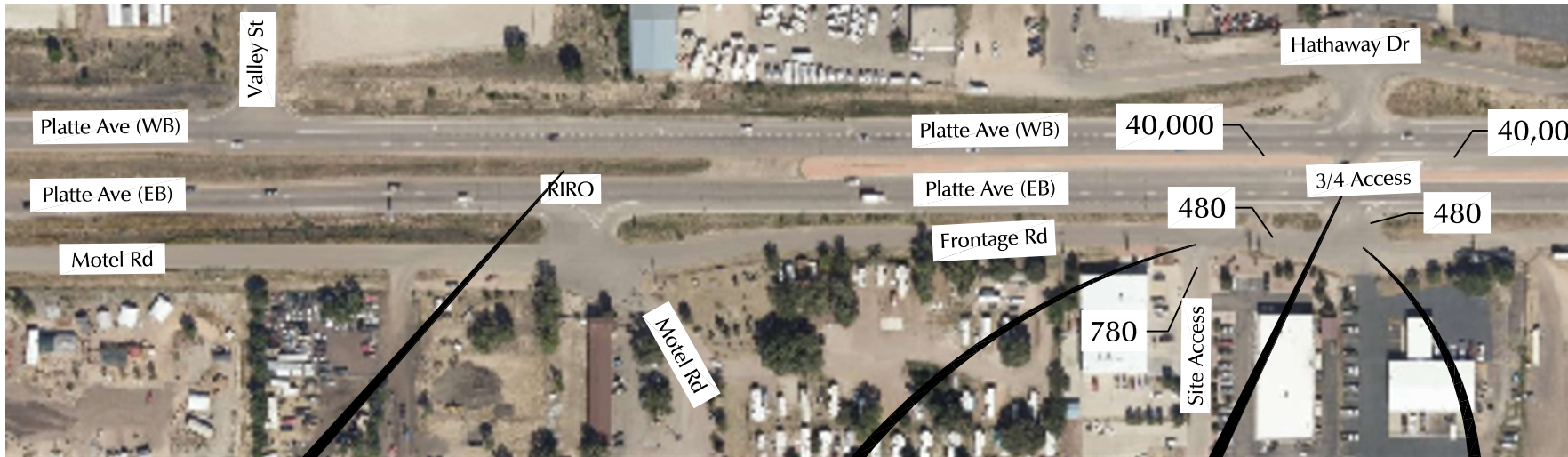
860' to Frontage Road

Figure 2

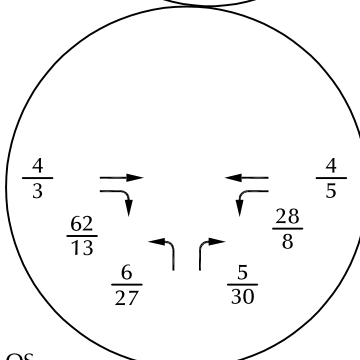
Site Plan

WireNut (LSC# S224620)

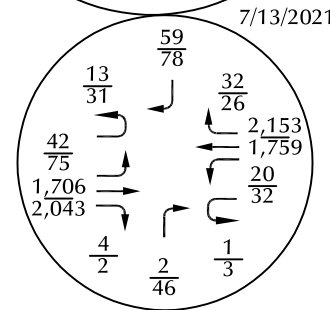




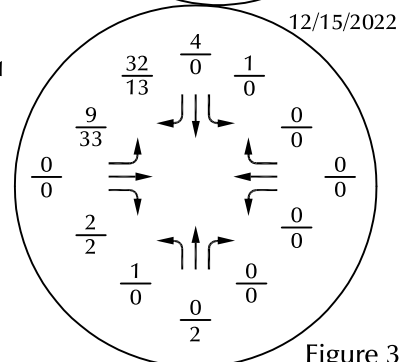
7/14/2021



12/15/2022



7/13/2021



12/15/2022

Figure 3

Existing Traffic, Lane Geometry, Traffic Control, and LOS



$\frac{X}{X}$ = AM Individual Movement Peak-Hour LOS
 $\frac{X}{X}$ = PM Individual Movement Peak-Hour LOS
 $\frac{XX}{XX}$ = AM Weekday Peak-Hour Traffic (Veh/Hour)
 $\frac{XX}{XX}$ = PM Weekday Peak-Hour Traffic (Veh/Hour)
 X,XXX = Average Daily Traffic (Vehicles/Day)

Counts by LSC (dates noted)

\downarrow = Stop Sign \triangleleft = Yield Sign

WireNut (LSC# S224620)

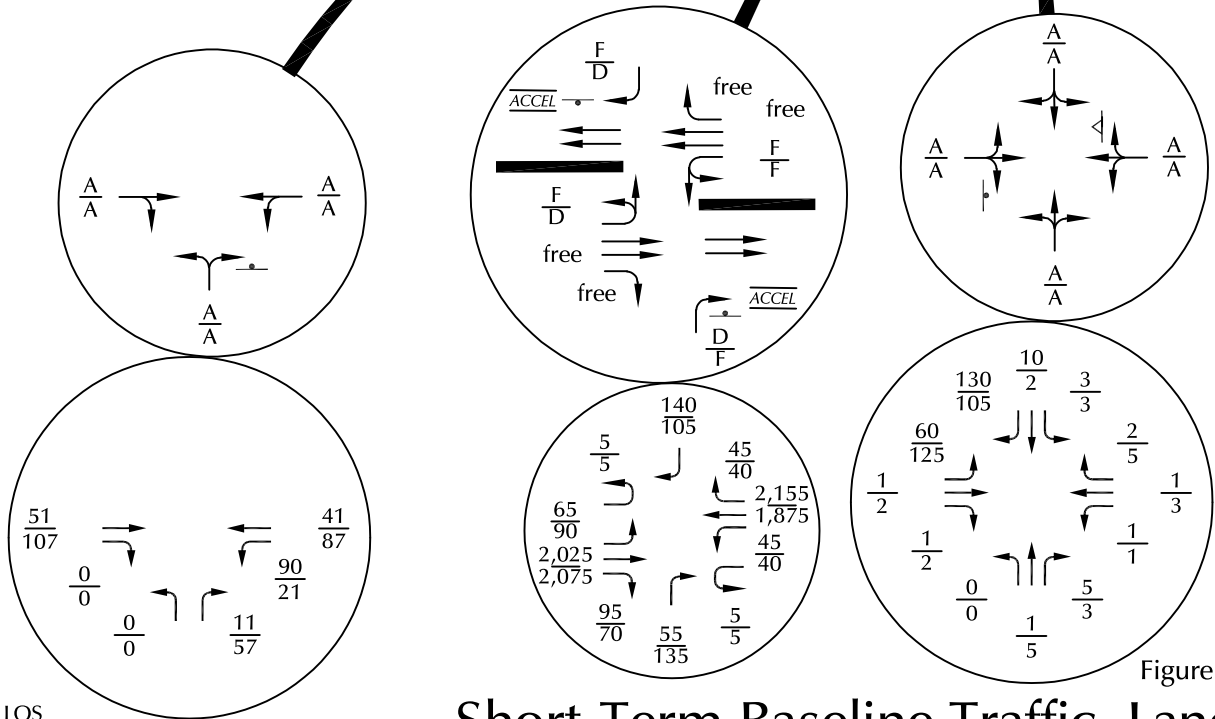
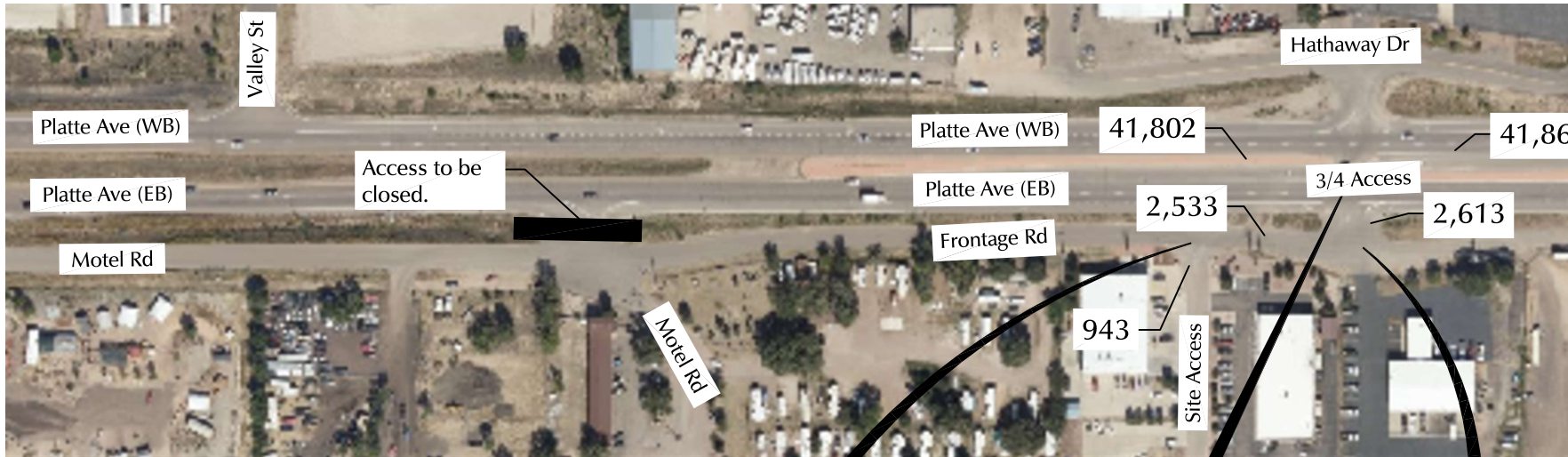
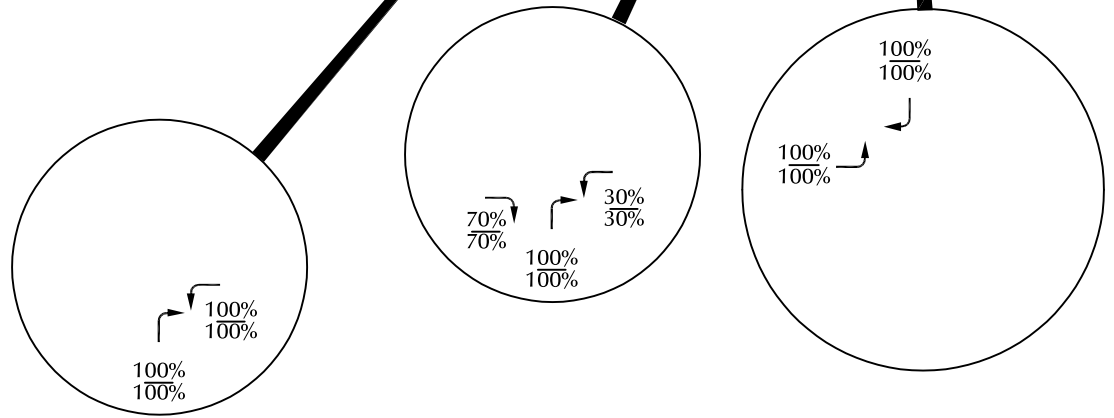
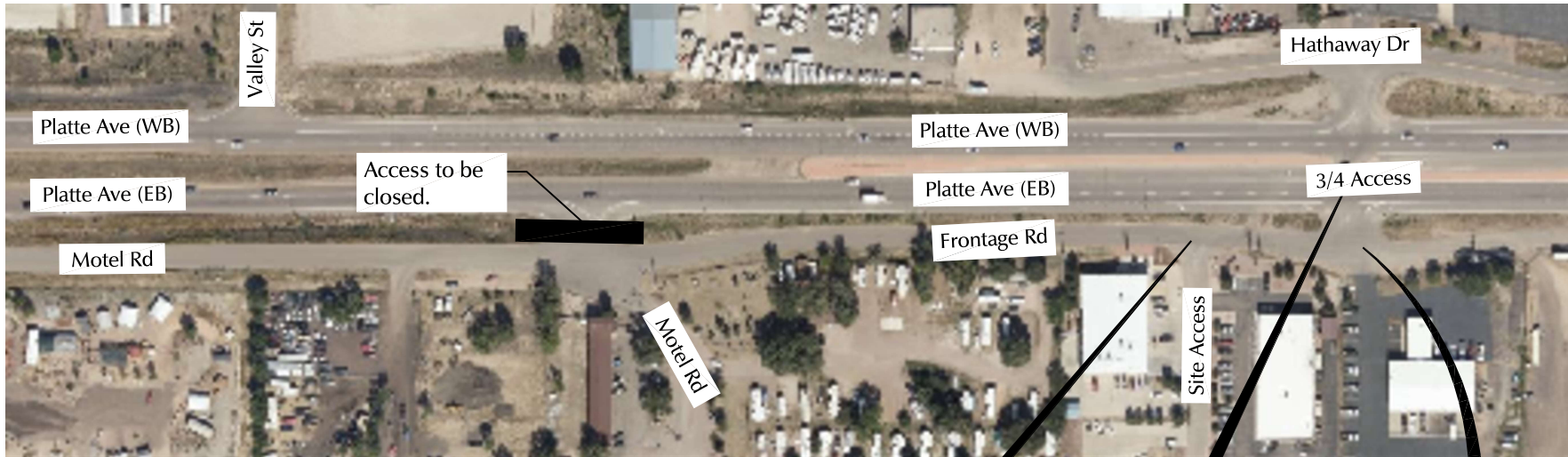


Figure 4

Short-Term Baseline Traffic, Lane Geometry, Traffic Control, and LOS

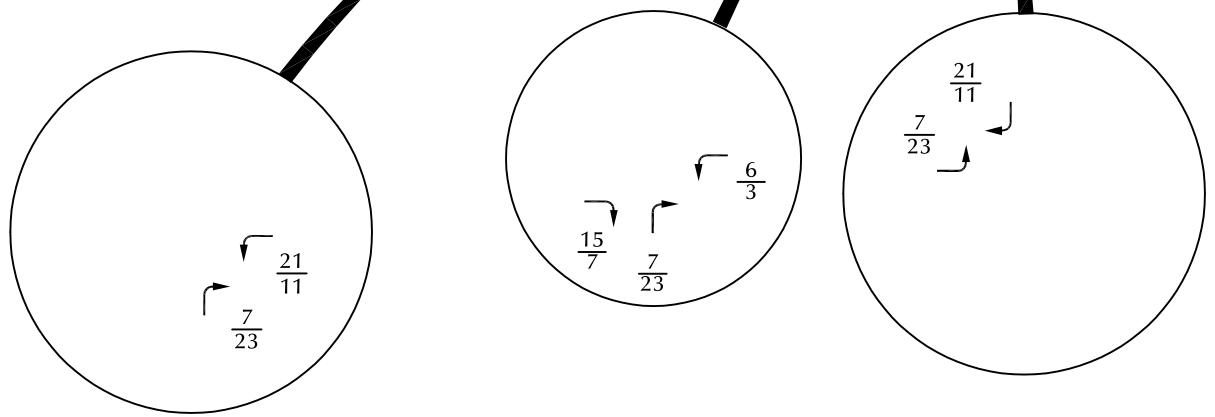
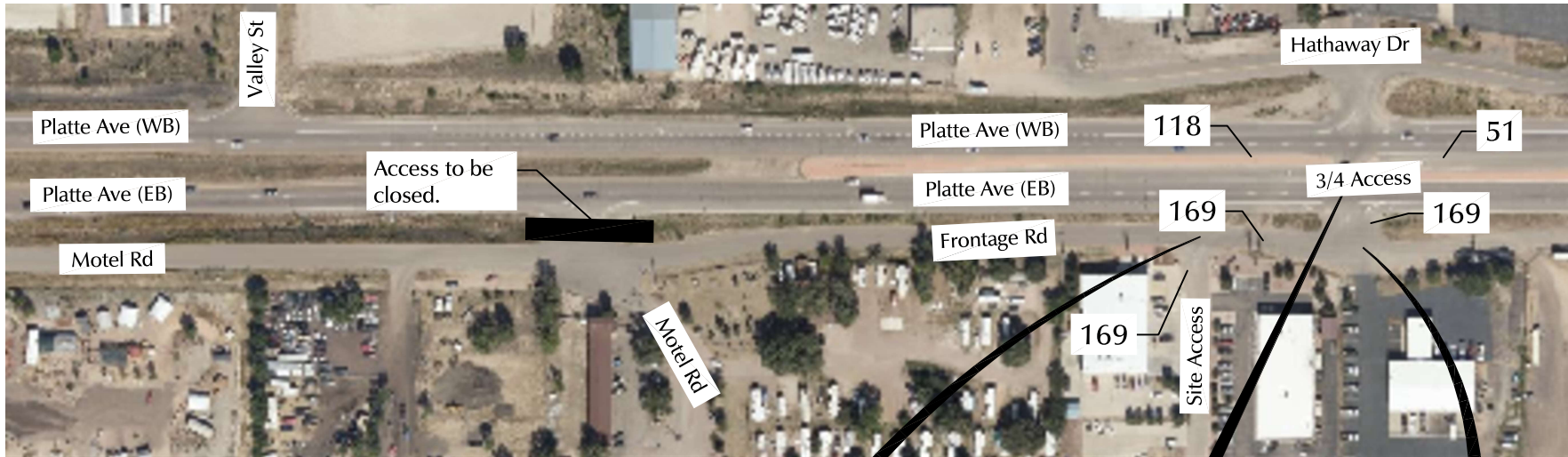
- $\frac{X}{X}$ = AM Individual Movement Peak-Hour LOS
- $\frac{X}{X}$ = PM Individual Movement Peak-Hour LOS
- $\frac{XX}{XX}$ = AM Weekday Peak-Hour Traffic (Veh/Hour)
- $\frac{XX}{XX}$ = PM Weekday Peak-Hour Traffic (Veh/Hour)
- X,XXX = Average Daily Traffic (Vehicles/Day)
- \triangleleft = Yield Sign
- \blacktriangleright = Stop Sign





$\frac{XX\%}{XX\%}$ = A.M. Peak Hour % Distribution
 P.M. Peak Hour % Distribution

Figure 5
Directional Distribution
 WireNut (LSC# S224620)



$\frac{XX}{XX}$ = AM Weekday Peak-Hour Traffic (Veh/Hour)
 $\frac{XX}{XX}$ = PM Weekday Peak-Hour Traffic (Veh/Hour)
 X,XXX = Average Daily Traffic (Vehicles/Day)

Figure 6
Site-Generated Traffic

WireNut (LSC# S224620)

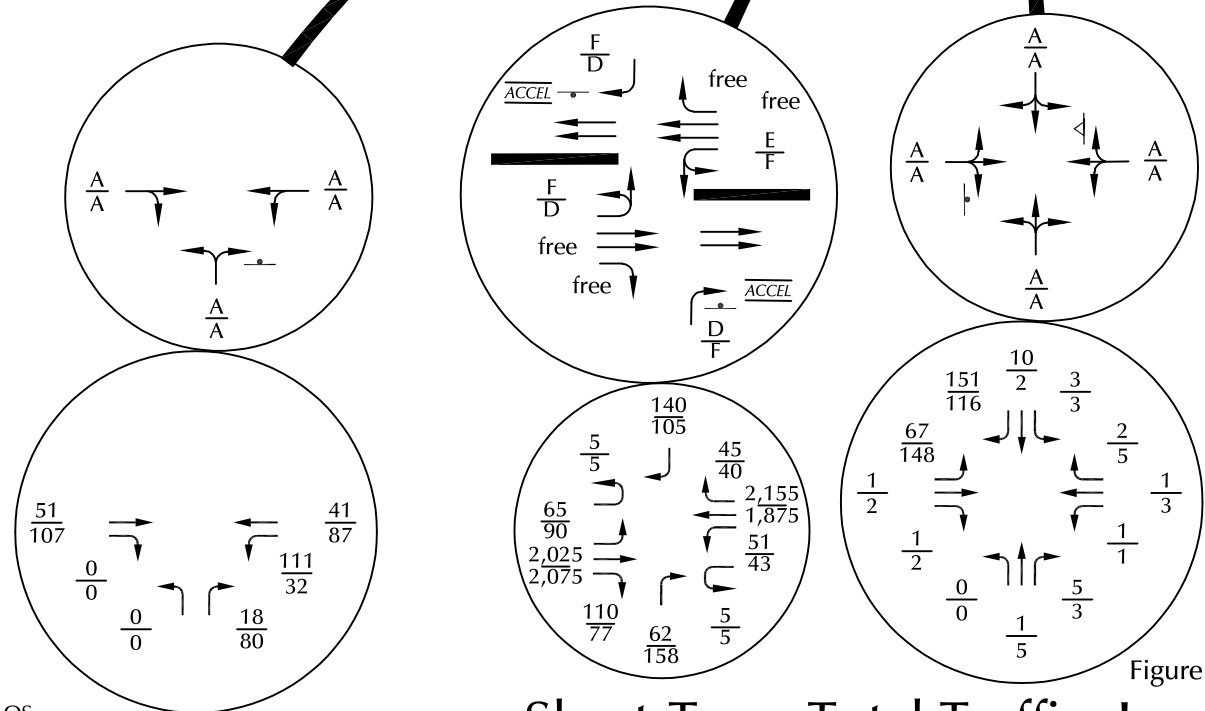
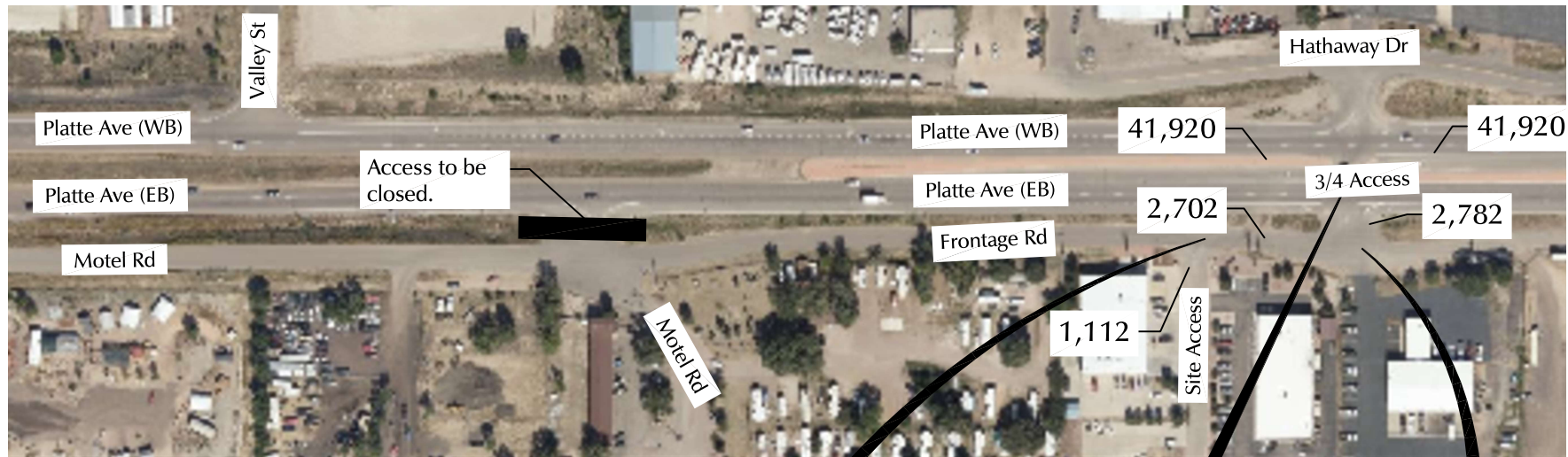


Figure 7

Short-Term Total Traffic, Lane Geometry, Traffic Control, and LOS

- $\frac{X}{X}$ = AM Individual Movement Peak-Hour LOS
- $\frac{XX}{XX}$ = AM Weekday Peak-Hour Traffic (Veh/Hour)
- $\frac{XX}{XX}$ = PM Weekday Peak-Hour Traffic (Veh/Hour)
- X,XXX = Average Daily Traffic (Vehicles/Day)
- \triangleleft = Yield Sign
- \blacktriangleright = Stop Sign



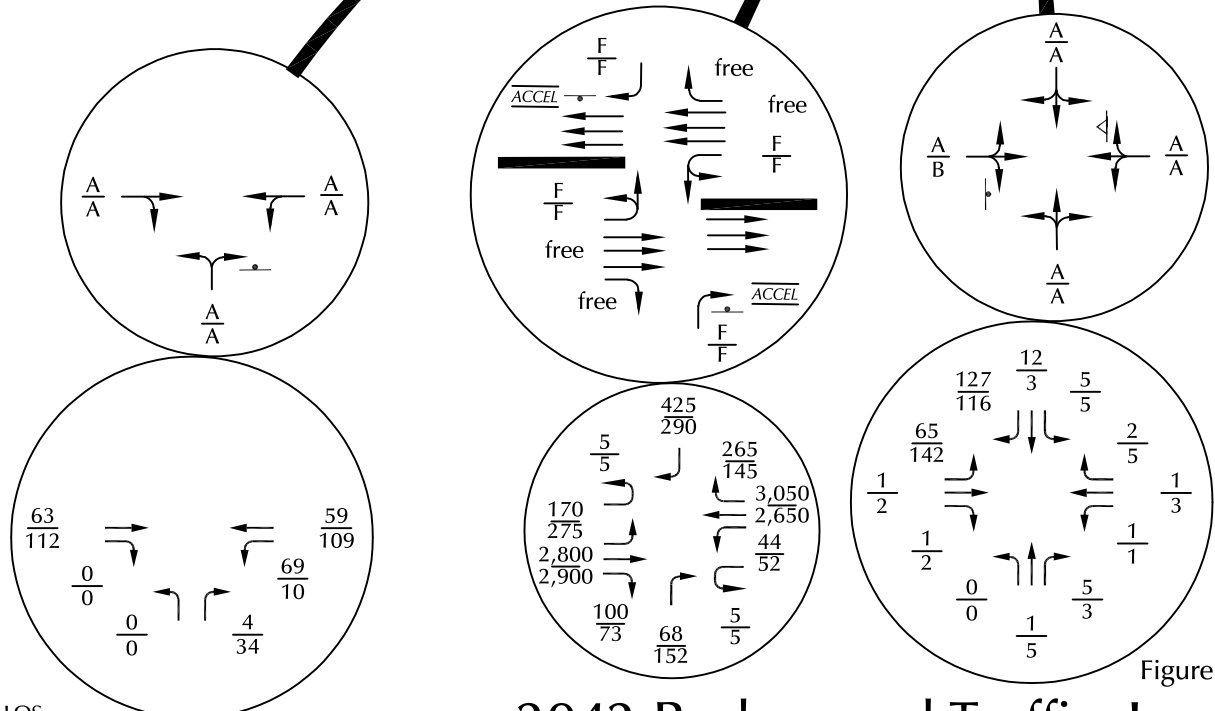
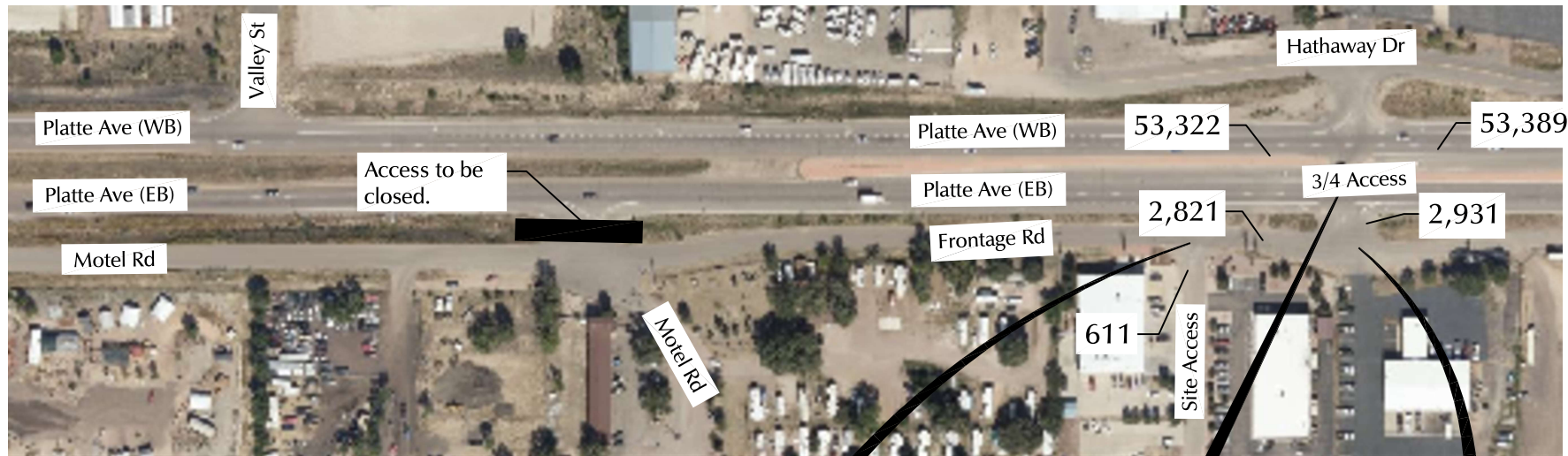


Figure 8

2042 Background Traffic, Lane Geometry, Traffic Control, and LOS

- $\frac{X}{X}$ = AM Individual Movement Peak-Hour LOS
- $\frac{X}{X}$ = PM Individual Movement Peak-Hour LOS
- $\frac{XX}{XX}$ = AM Weekday Peak-Hour Traffic (Veh/Hour)
- $\frac{XX}{XX}$ = PM Weekday Peak-Hour Traffic (Veh/Hour)
- X,XXX = Average Daily Traffic (Vehicles/Day)
- \triangleleft = Yield Sign
- \perp = Stop Sign



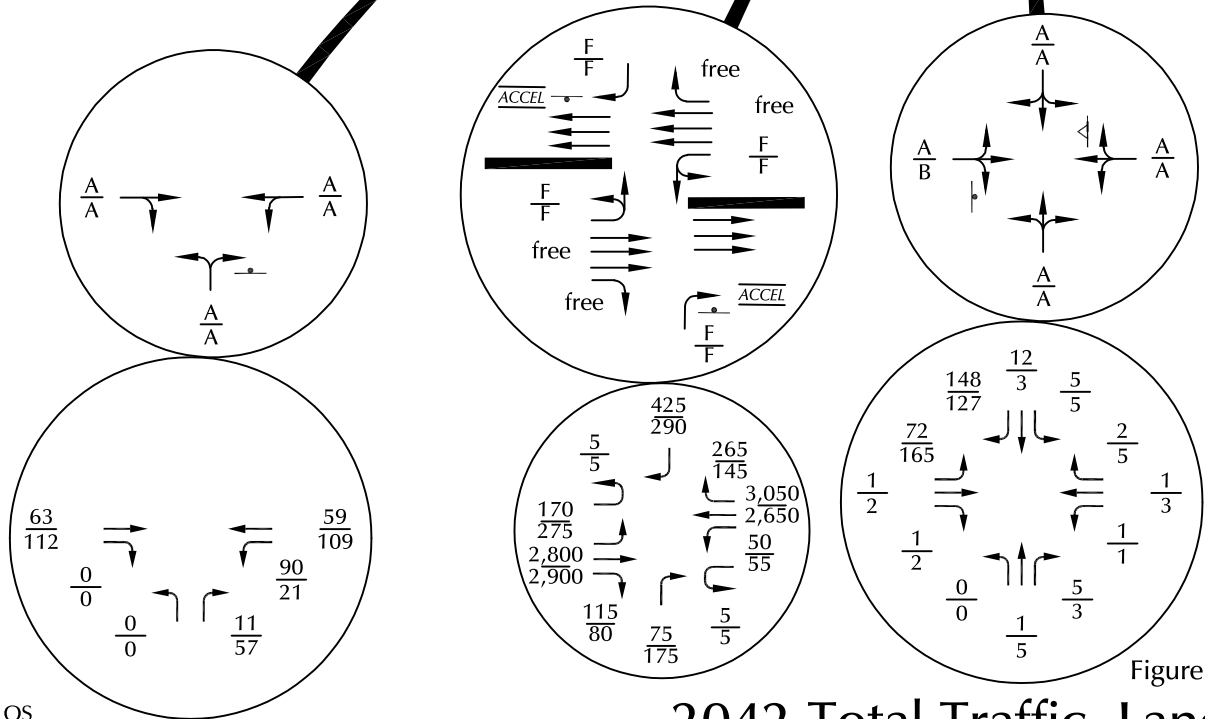
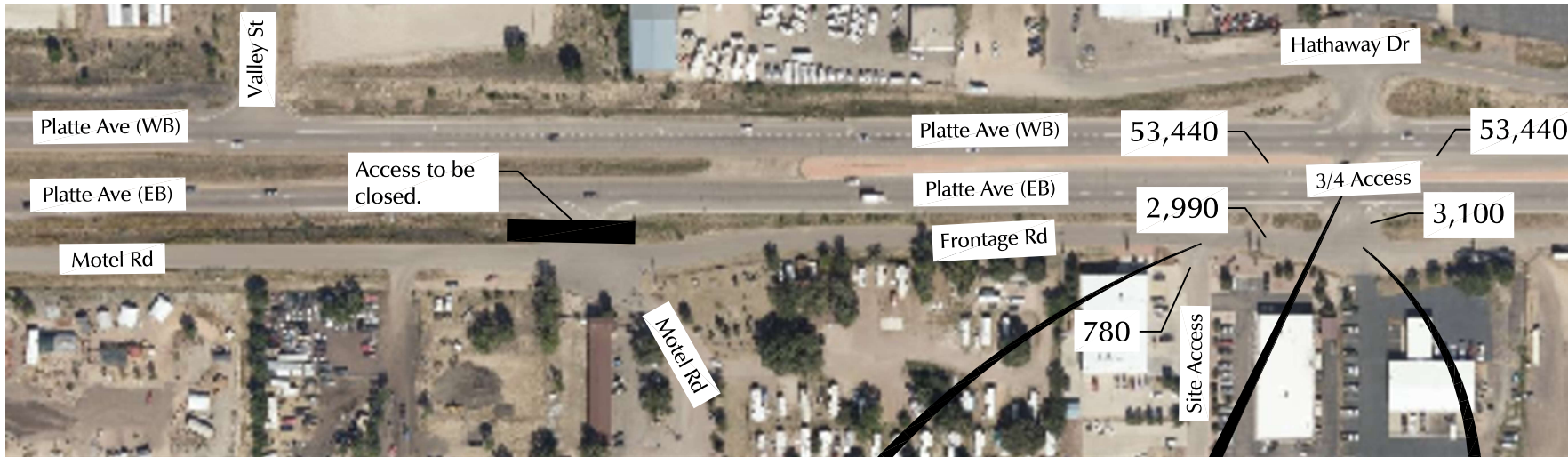


Figure 9

2042 Total Traffic, Lane Geometry, Traffic Control, and LOS

- $\frac{X}{X}$ = AM Individual Movement Peak-Hour LOS
- $\frac{XX}{XX}$ = AM Weekday Peak-Hour Traffic (Veh/Hour)
- $\frac{XX}{XX}$ = PM Weekday Peak-Hour Traffic (Veh/Hour)
- X,XXX = Average Daily Traffic (Vehicles/Day)
- = Yield Sign
- = Stop Sign



Site Plan



DRAWING INDEX

| | |
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| 1 OF 38 - SITE PLAN, PROJECT INFORMATION & DRAWING INDEX | SI01 - 20 OF 38 - DEMOLITION PLAN |
| 2 OF 38 - SITE PLAN DETAILS | SI02 - 21 OF 38 - STREET IMPROVEMENT PLAN |
| 3 OF 38 - WATER PLAN & USEP COVER & INDEX | SI03 - 22 OF 38 - STRIPPIING & SIGNAGE PLAN |
| 4 OF 38 - WATER PLAN & PROFILES | ST01 - 23 OF 38 - STORM SEWER PLAN & PROFILE |
| 5 OF 38 - SEWER PLAN TITLE SHEET | ST02 - 24 OF 38 - STORM SEWER PLAN & PROFILE |
| 6 OF 38 - SANITARY SEWER FORCE MAIN PLAN & PROFILES | ST03 - 25 OF 38 - STORM SEWER PLAN & PROFILE |
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| 9 OF 38 - DETAILED GRADING | ST06 - 28 OF 38 - STORM SEWER DETAILS |
| 10 OF 38 - INITIAL GEC PLAN | ST07 - 29 OF 38 - STORM SEWER DETAILS |
| 11 OF 38 - FINAL GEC PLAN | ST08 - 30 OF 38 - STORM SEWER DETAILS |
| 12 OF 38 - GEC PLAN DETAILS | BMP01 - 31 OF 38 - OUTLET STRUCTURE POND I DETAILS |
| 13 OF 38 - GEC PLAN DETAILS | BMP02 - 32 OF 38 - OUTLET STRUCTURE POND I DETAILS |
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| 15 OF 38 - GEC PLAN DETAILS | LI1 - 34 OF 38 - LANDSCAPE PLAN & SCHEDULE |
| 16 OF 38 - GEC PLAN DETAILS | LI2 - 35 OF 38 - LANDSCAPE PLAN & DETAILS |
| 17 OF 38 - GEC PLAN DETAILS | ES01 - 36 OF 38 - PHOTOMETRIC & SCHEDULES |
| 18 OF 38 - CONSTRUCTION PLAN TITLE SHEET | ES02 - 37 OF 38 - PHOTOMETRIC LIGHTING DETAILS |
| 19 OF 38 - CONSTRUCTION PLAN NOTES & DETAILS | 38 OF 38 - BUILDING ELEVATIONS |

VICINITY MAP



PROJECT INFORMATION

| | |
|--|---|
| PROPERTY INFORMATION | |
| OWNER NAME: | TRENTON B URBAN 6365 E PLATTE AVE COLORADO SPRINGS, CO 80915 |
| LEGAL DESCRIPTION: LOT 5 CLEARWAY | |
| PARCEL NUMBER: | 54180-01-005 |
| ZONING: | CS CAD-0 |
| LOT SIZE: | 129,304 SF (2.91 ACRES) |
| CURRENT USE: | VACANT COMMERCIAL LOTS |
| FLOODPLAIN STATEMENT: | ZONE X (MAP NO. 0804107546, DATED DECEMBER 01, 2018) |
| BUILDING INFORMATION | |
| NEW BUILDING AREA: | 17,255 SF |
| BUILDING OCCUPANCY: | B/S-2 |
| TYPE OF CONSTRUCTION: | II-B |
| FIRE SYSTEMS: | SPRINKLED |
| AREA SEPARATION WALLS: | NONE |
| ZONING CODE STUDY | |
| EXISTING PRINCIPAL USE: | VACANT |
| PROPOSED USE: | OFFICE & WAREHOUSE |
| STRUCTURAL COVERAGE OF LOT: | |
| PAVEMENT COVERAGE: | 15% |
| NEW BUILDING STRUCTURAL HEIGHT: | 5-48' |
| FRONT YARD SETBACK: | 20'-8" |
| SIDE YARD SETBACK: | 25'-0" |
| REAR YARD SETBACK: | 25'-0" |
| REQUIRED PARKING SPACES | |
| WAREHOUSE - 1/1000 SF | 6 |
| 5,840 / 1,000 SF | 6 |
| OFFICE - 1/200 SF | 58 |
| 11,415 / 200 SF | 58 |
| H.C. 1 SPACE/25 REQ'D | 3 |
| TOTAL PARKING SPACES REQUIRED: | 64 |
| TOTAL PARKING PROVIDED: | 64 |
| STANDARD SPACES PROVIDED: | 61 |
| BICYCLE SPACES PROVIDED: | 4 |
| H.C. SPACES PROVIDED: | 3 |
| LOADING SPACE PROVIDED (SEE DETAIL 2/2 FOR DIMENSIONS) | 1(14'x18') |
| DEVELOPMENT SCHEDULE | |
| CONSTRUCTION: | SUMMER 2022 |
| LANDSCAPING: | FALL 2022 |
| DEVELOPMENT APPLICANT | |
| COMPANY: | HAMMERS CONSTRUCTION, INC. 1411 WOOLSEY HEIGHTS COLO. SPGS., CO 80915 (719)-570-1549 |
| PHONE NUMBER: | (719)-570-1008 |
| FAX NUMBER: | (719)-570-1008 |
| APPLICANT NAME: | LISA PETERSON |
| APPLICANT E-MAIL: | lpeterson@hammersconstruction.com |

SITE LEGEND

| | |
|-----|------------------------------|
| --- | PROPERTY LINE |
| --- | RIGHT OF WAY |
| --- | BUILDING SETBACK |
| --- | LANDSCAPE SETBACK |
| --- | UTILITY/DRAINAGE EASEMENT |
| --- | ELECTRICAL EASEMENT |
| --- | PHONE EASEMENT |
| --- | ACCESS EASEMENT |
| --- | NEW CHAINLINK FENCE |
| --- | STORM SEWER LINE |
| --- | RETAINING WALL |
| --- | NEW SIDEWALK LOCATIONS |
| --- | W CONTROL JOINTS @ 5'-0" OC. |
| ● | PROPERTY CORNER |
| → | TRAFFIC FLOW |
| □ | WALL FACK LIGHTING |
| + | SIGN |
| □ | PROPOSED LIGHT POLE |
| □ | ELECTRICAL TRANSFORMER |
| ○ | EXISTING FIRE HYDRANT |
| ● | PROPOSED FIRE HYDRANT |

PCD DIRECTOR APPROVAL

| |
|-------------------------|
| DATE: SEPT. 21, 2022 |
| DRAWN BY: J.CANTERBERRY |
| PROJ. MGR: J. BUTLER |
| SCALE: SEE PLAN |
| APPROVED BY: |
| JOB NO: 1280 |

HAMMERS CONSTRUCTION INC.
COMMERCIAL GENERAL CONTRACTORS SPECIALIZING IN DESIGN/BUILD
PRESIDENT: STEVE R. HAMMERS
VICE PRES: DAVID J. HAMMERS
1411 WOOLSEY HEIGHTS
COLORADO SPRINGS, CO 80915
(719) 570-1599 FAX (719) 570-7008
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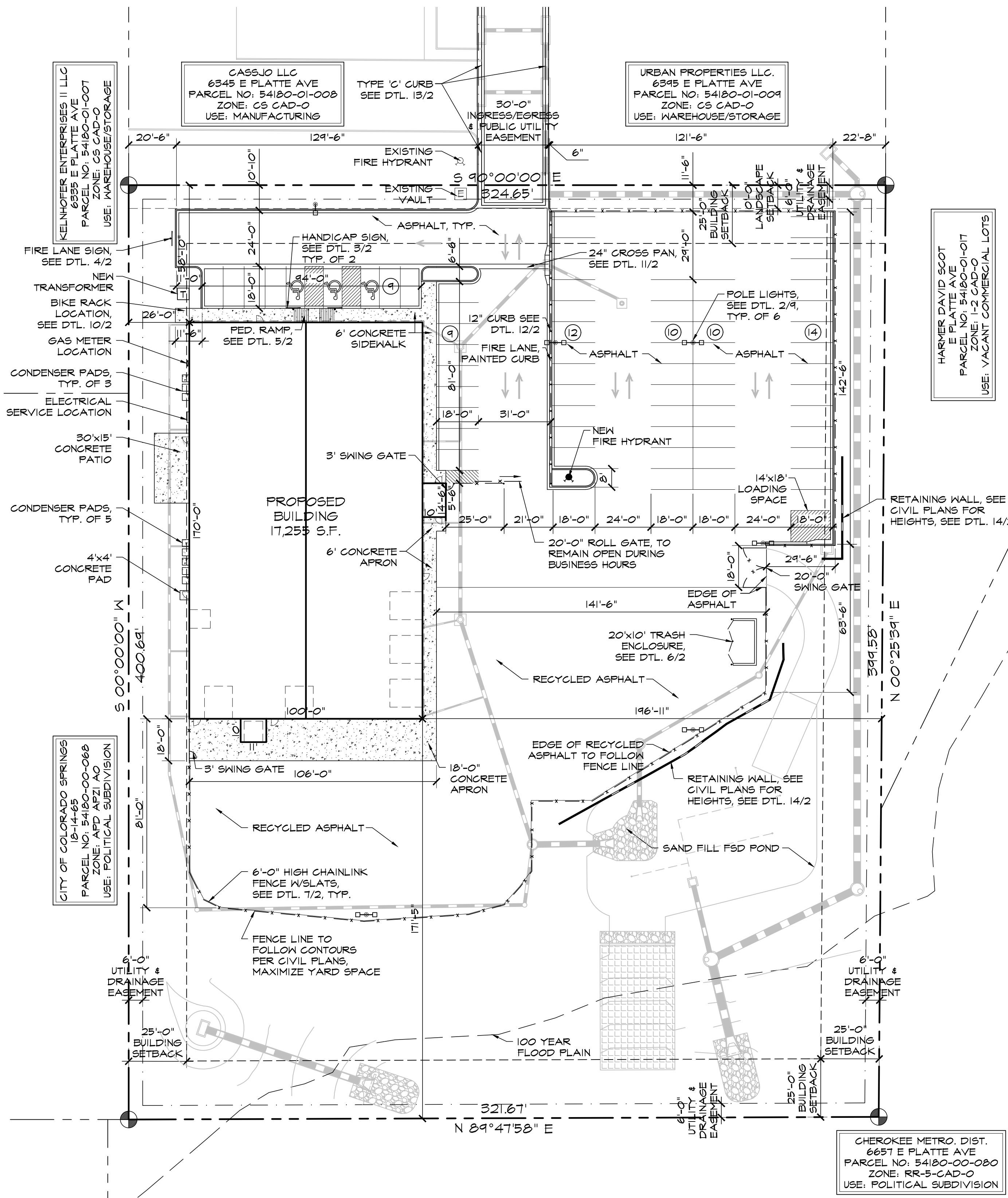
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WIRENUT HOME SERVICES
6365 E PLATTE AVE.
COLORADO SPRINGS, CO 80915
EL PASO, COLORADO

DATE: SEPT. 21, 2022
DRAWN BY: J.CANTERBERRY
PROJ. MGR: J. BUTLER
SCALE: SEE PLAN
APPROVED BY:
JOB NO: 1280

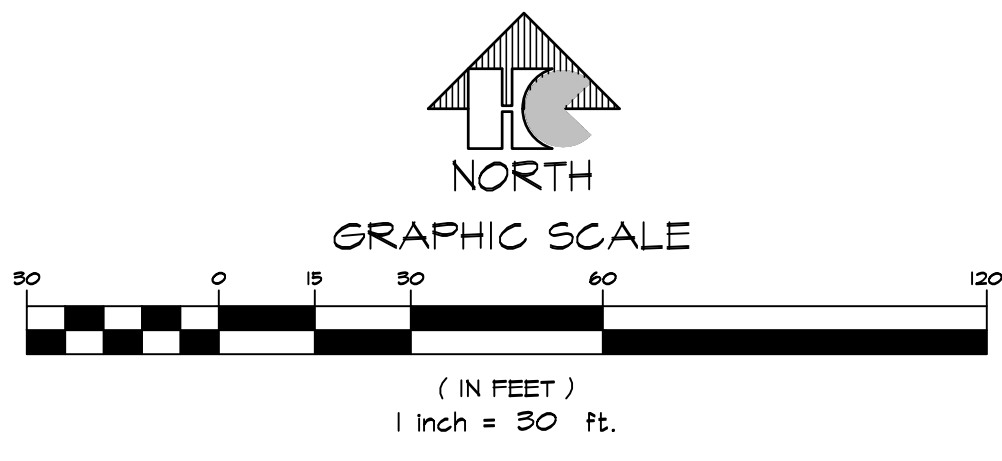
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9-23-22/DP COMMENTS

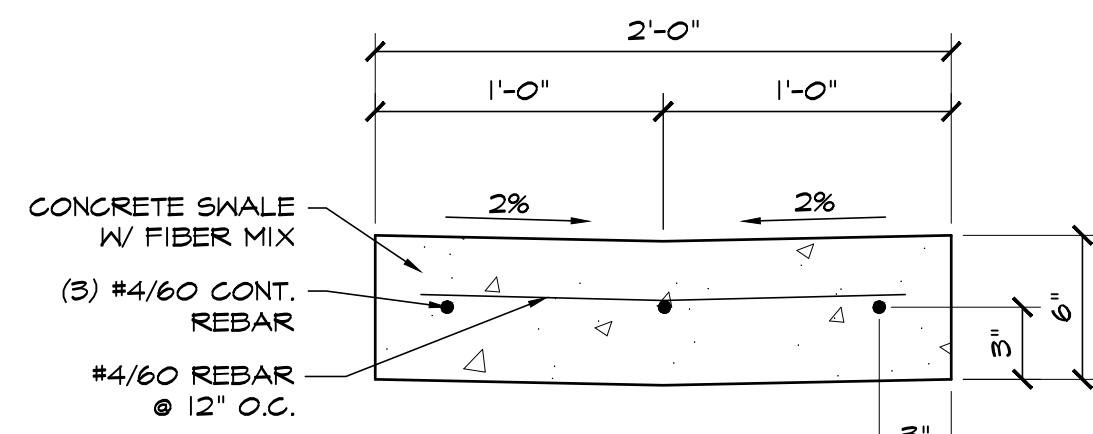
1 of 38
SITE PLAN



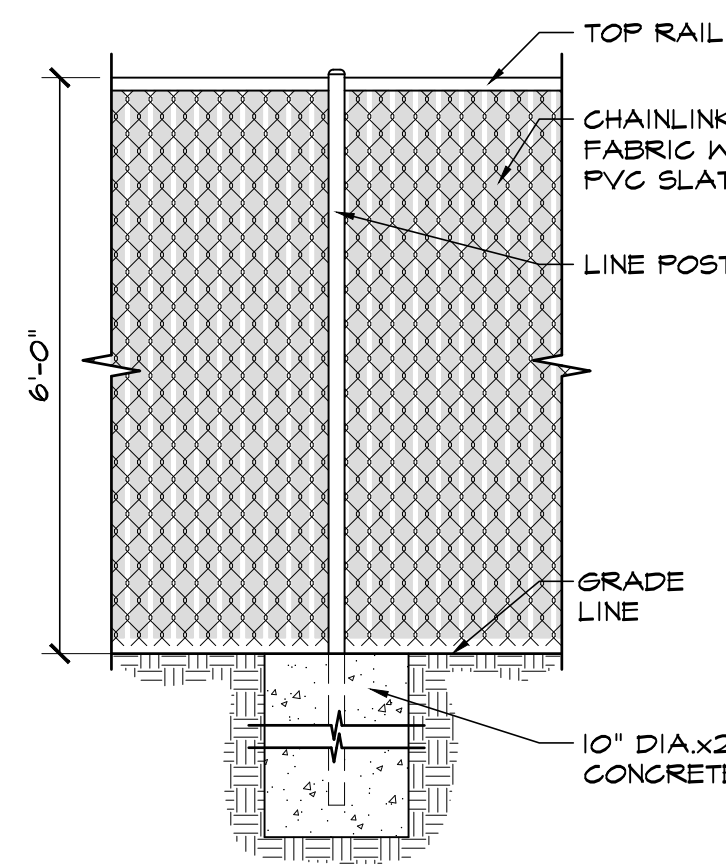
SITE PLAN
SCALE: 1"=30'-0"

COUNTY FILE NO: PPR-2234



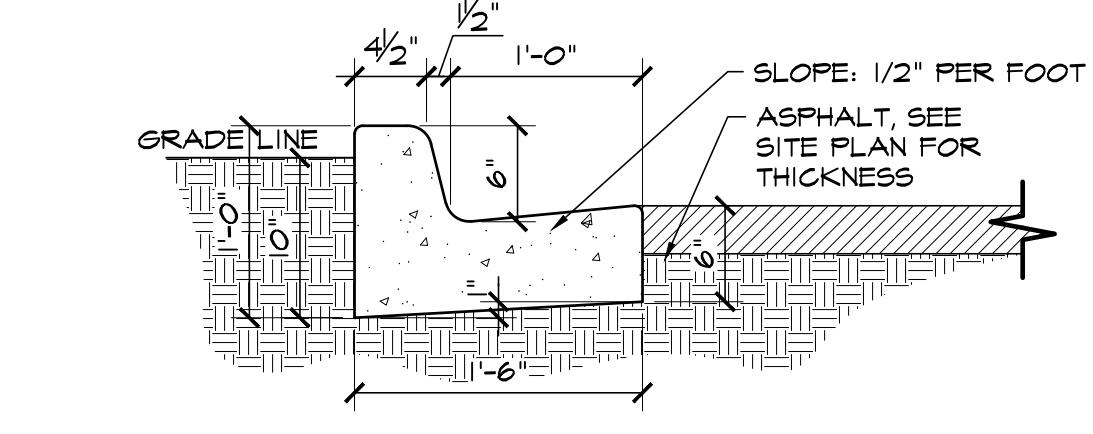
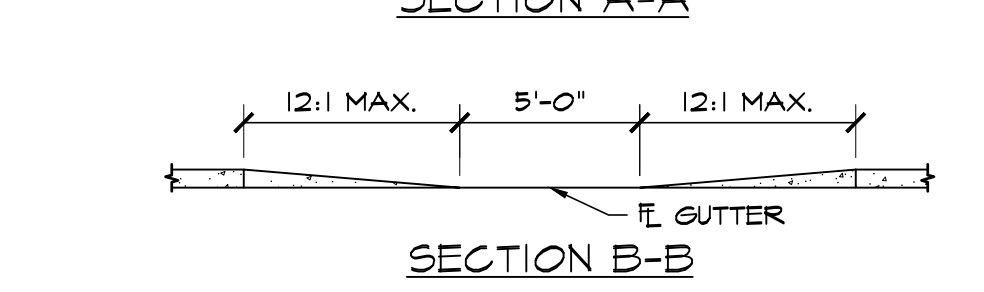
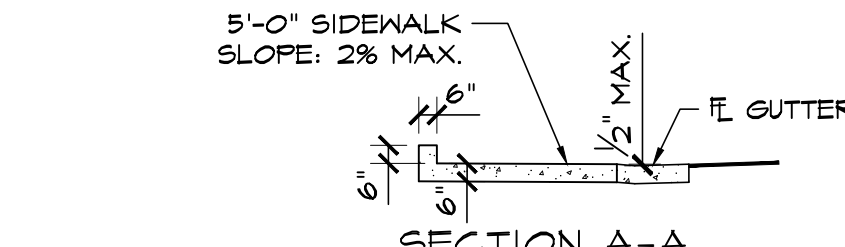
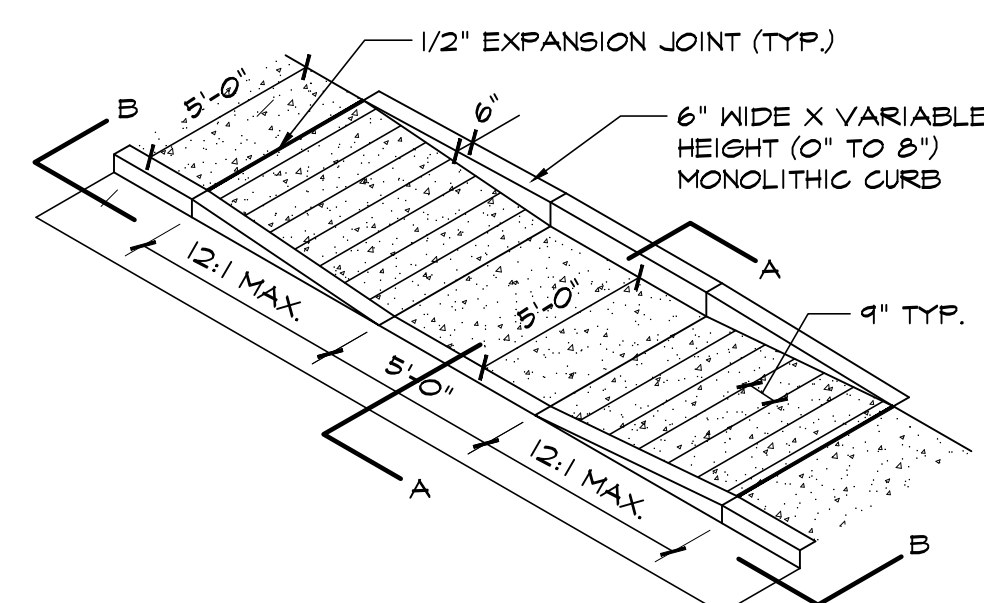


11 2'-0" CROSSSPAN DETAIL
SCALE: 1/2"=1'-0"



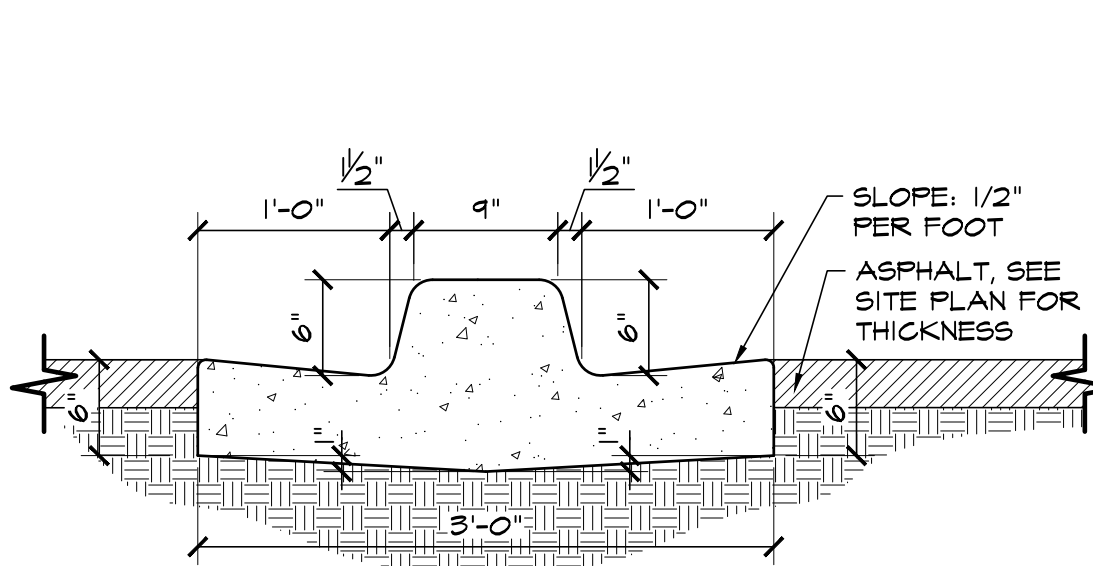
7 CHAINLINK FENCE DETAIL
SCALE: 1/2"=1'-0"

FENCE SPECS:
POSTS:
 - ALL POSTS WILL BE SET IN CONC.
 - END & CORNER - 2 3/8" T-40
 - LINE POSTS - 2" T-40 @ 10'-0" OC
TOP RAIL:
 - 1 5/8" T-20
CHAINLINK FABRIC:
 - 2" MESH x 11 GA - GALVANIZED AFTER WEAVING
 - ATTACH W/ STL TIES
 - START 1" A.F.F.
SLATS:
 - DOUBLE WALL PVC PRIVACY FILLER STRIPS
 - LOCKING TRACK ON BOTTOM
 - START 2" A.F.F.
GATES:
 - ROLLING GATES TO INCLUDE SAFETY POST INSIDE GATE ROLL AREAS

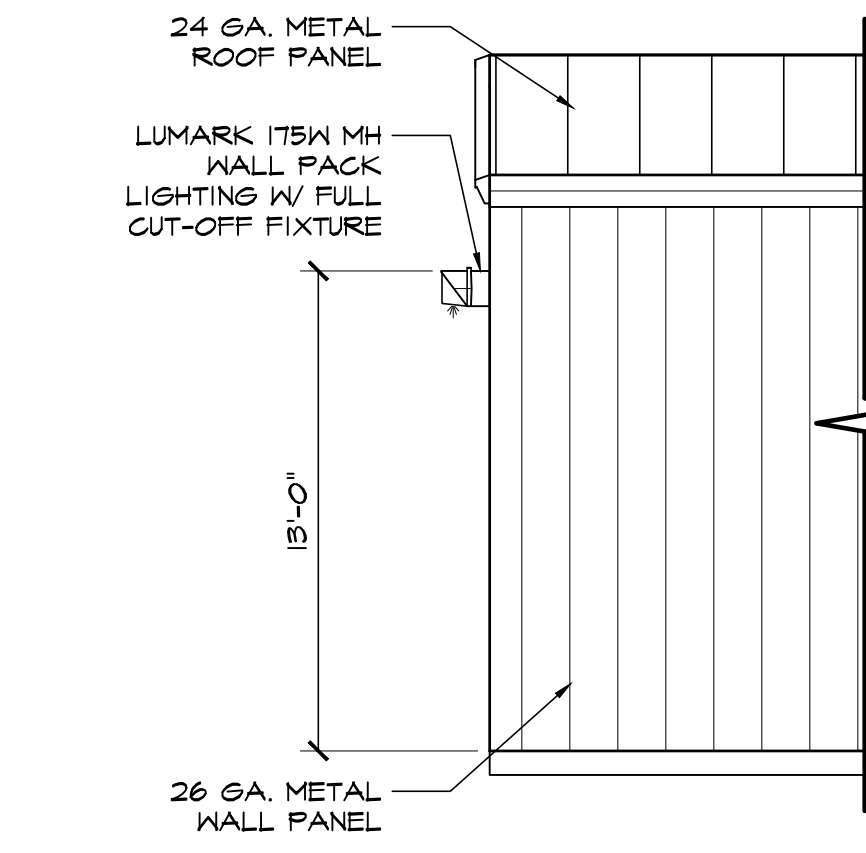


1 EPC TYPE B CURB
SCALE: 1"=1'-0"

NOTE:
 -ALL INTERNAL CURB THIS TYPE UNLESS NOTED OTHERWISE.
 -SEE CIVIL PLANS FOR CARRY OR SPILL CURB LOCATIONS



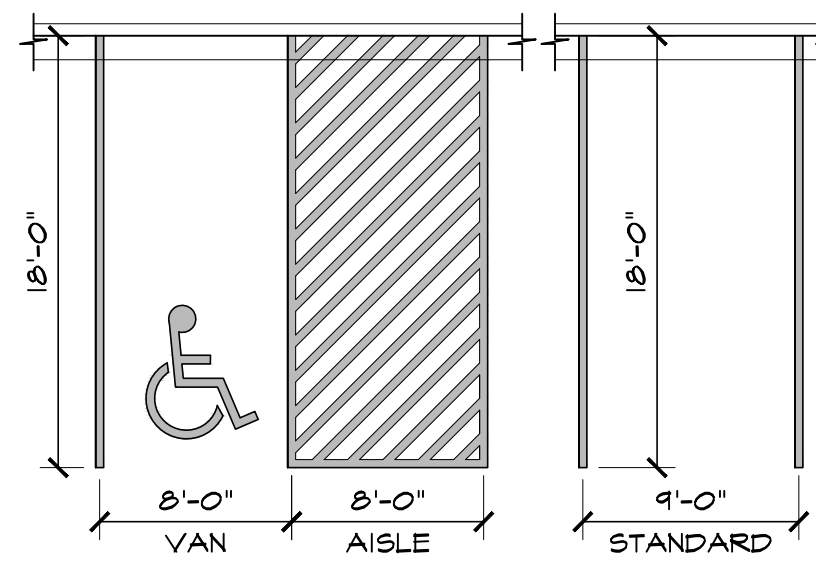
12 12" CURB FOR FENCE
SCALE: 1"=1'-0"



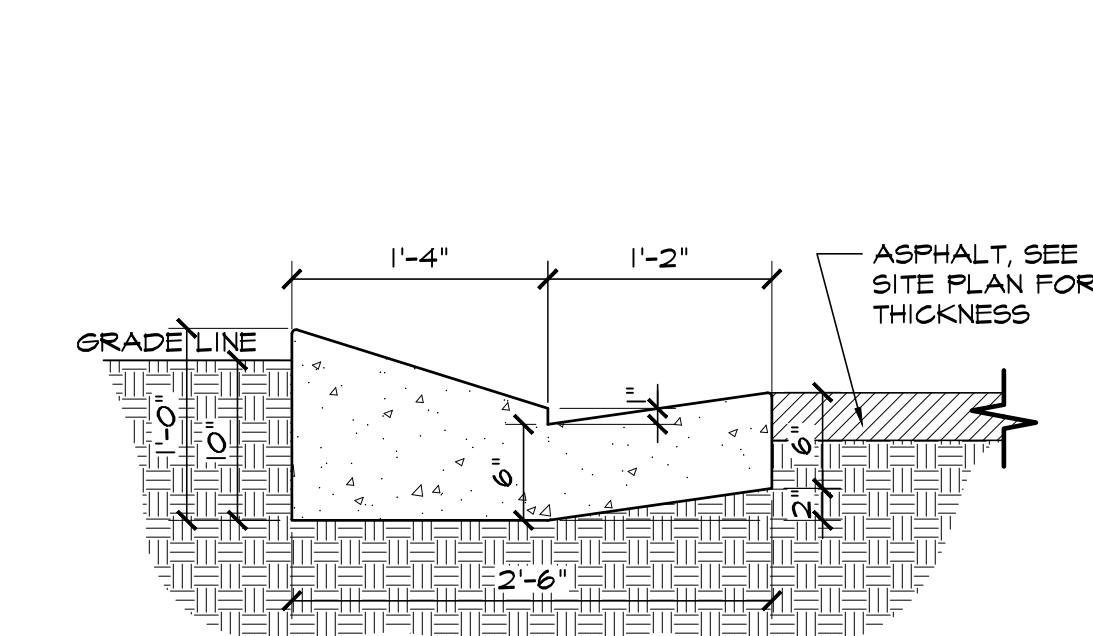
8 WALL PACK LIGHT DETAIL
SCALE: 1/4"=1'-0"

GENERAL NOTES:
 1. ALL WORK TO BE DONE IN ACCORDANCE WITH CURRENT EL PASO COUNTY ENGINEERING DIVISION STANDARD SPECIFICATIONS.
 2. SIDEWALK CROSS-SLOPE: 1/4"/FT.
 3. CONTRACTOR TO OBTAIN REQUIRED CONCRETE PERMIT AND RAMP PERMIT AT ENGINEERING DIVISION INSPECTION OFFICE PRIOR TO CONSTRUCTION.
 4. CONTRACTOR TO NOTIFY ENGINEERING DIVISION INSPECTION OFFICE AT LEAST 24 HOURS PRIOR TO PLACEMENT OF ANY CONCRETE.
 5. THE RAMP SURFACES SHALL BE FINISHED WITH A COARSE BROOM FINISH, PARALLEL WITH THE SCORING, IN ACCORDANCE WITH SECTION 509.02E OF THE STANDARD SPECIFICATIONS.
 6. CONTRACTOR SHALL STAMP THE COMPANY NAME AND CONSTRUCTION DATE WITHIN THE PEDESTRIAN RAMP AREA.
 7. PEDESTRIAN TRAVELWAY AND/OR LOCATION OF EXISTING OR FUTURE PEDESTRIAN RAMPS ON OPPOSITE CORNERS SHALL BE REVIEWED BEFORE CONSTRUCTING NEW RAMPS. NEW RAMPS SHALL ALIGN WITH EXISTING RAMPS AND PEDESTRIAN TRAVELWAY.
 8. SHADED AREA: 6" THICK CONCRETE

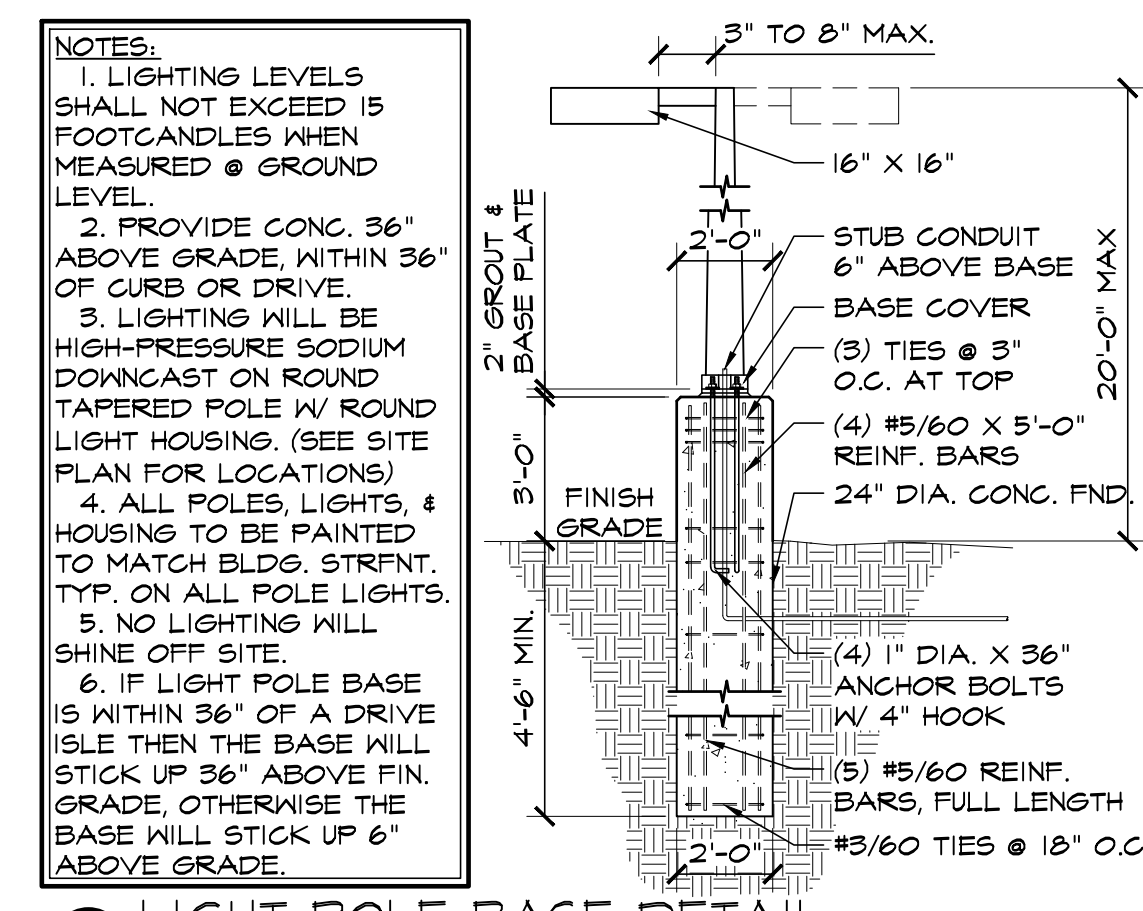
5 PEDESTRIAN RAMP DETAIL D-8J
SCALE: 3/16"=1'-0"



2 ADA / STANDARD PARKING
SCALE: 1/8"=1'-0"

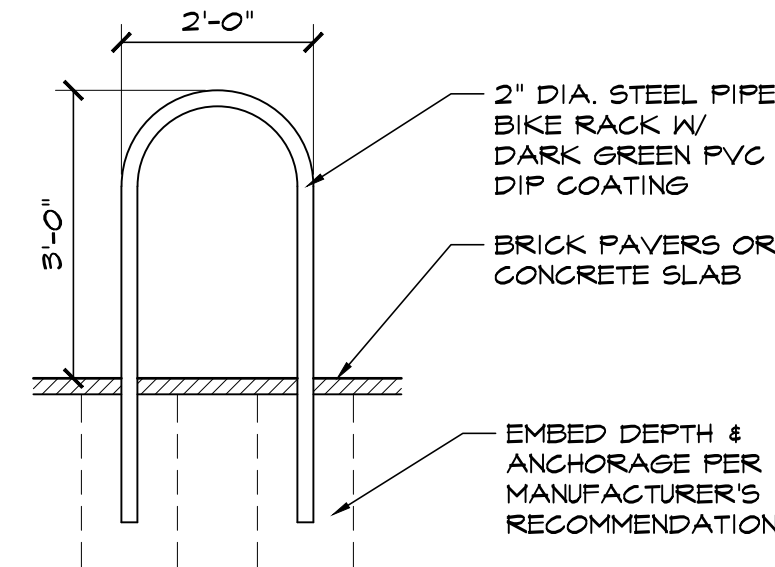


13 EPC TYPE C CURB
SCALE: 1"=1'-0"

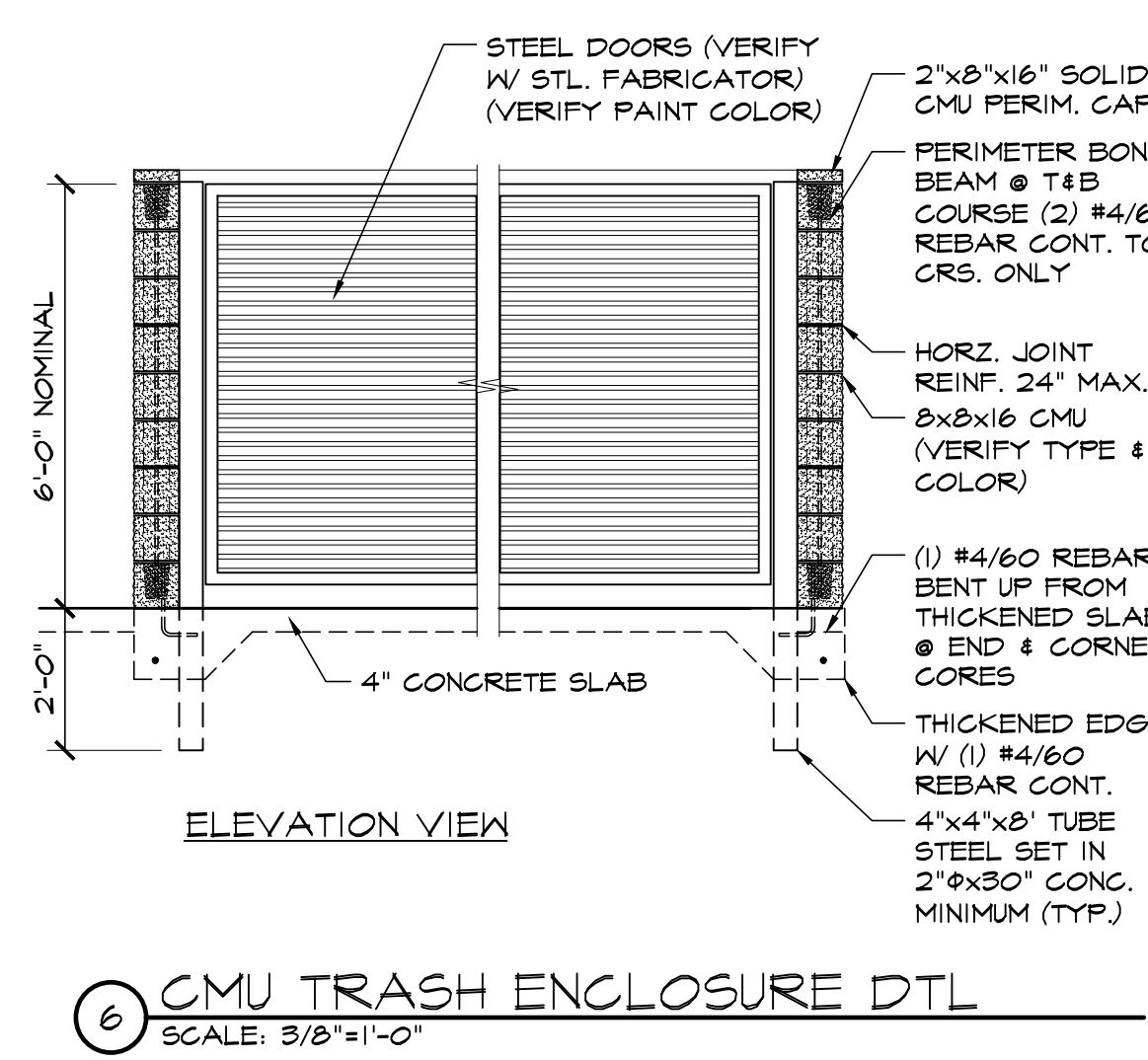
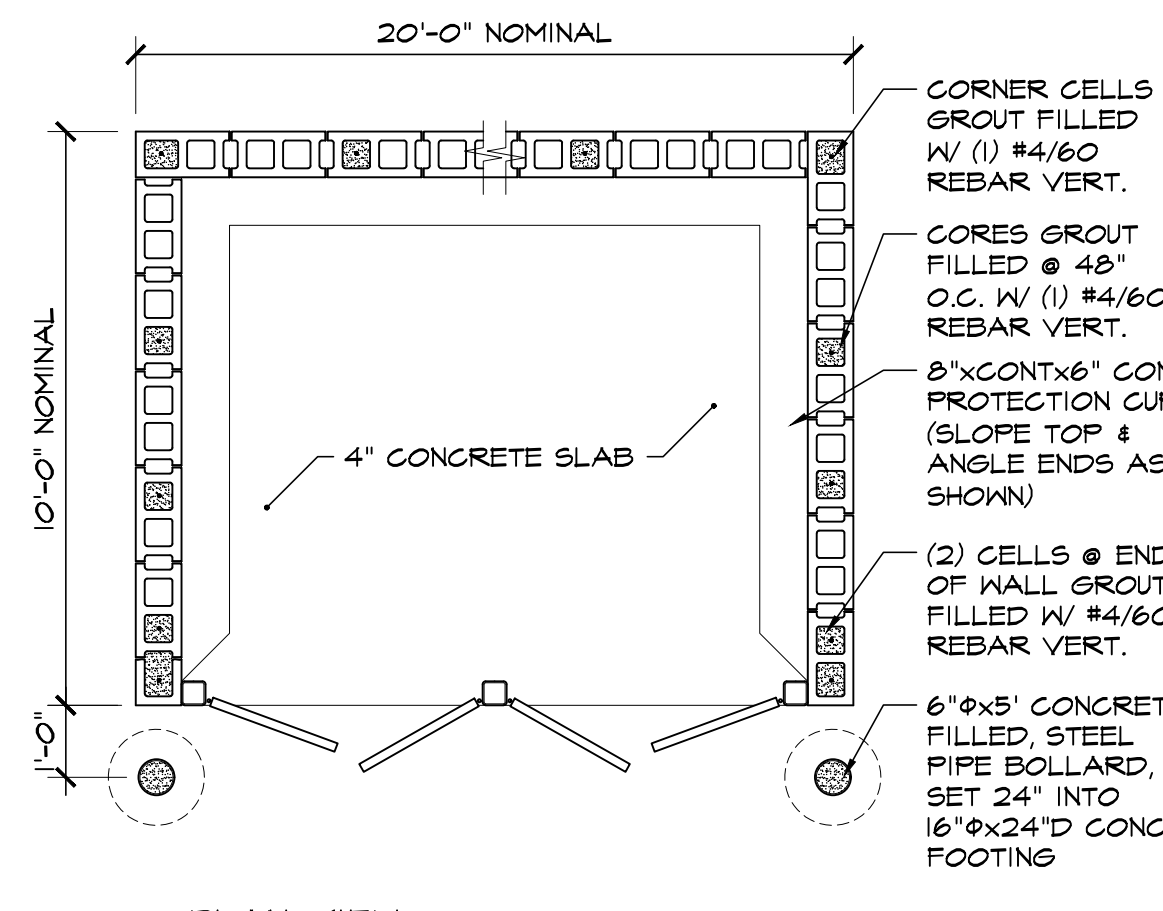


9 LIGHT POLE BASE DETAIL
SCALE: 1/4"=1'-0"

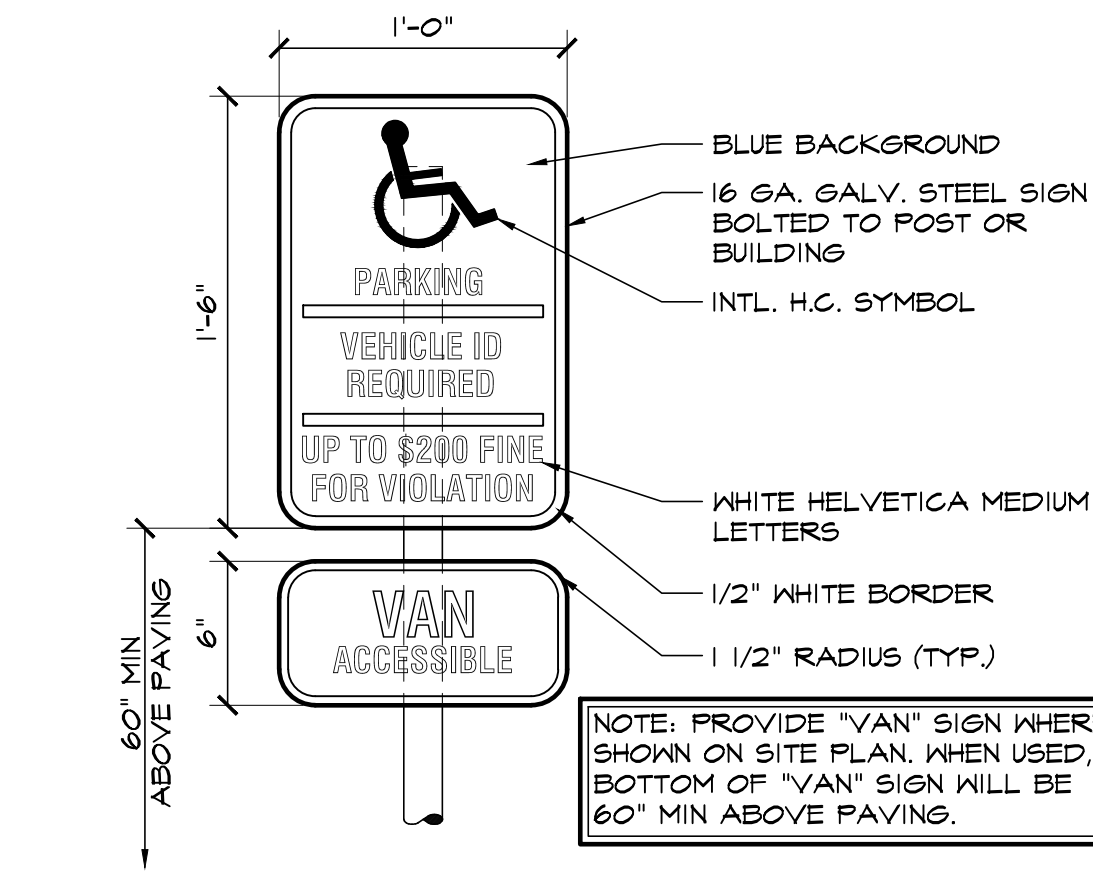
NOTE: PROVIDE RACKS FOR 4 BIKE SPACES, PER EPC REQUIREMENTS



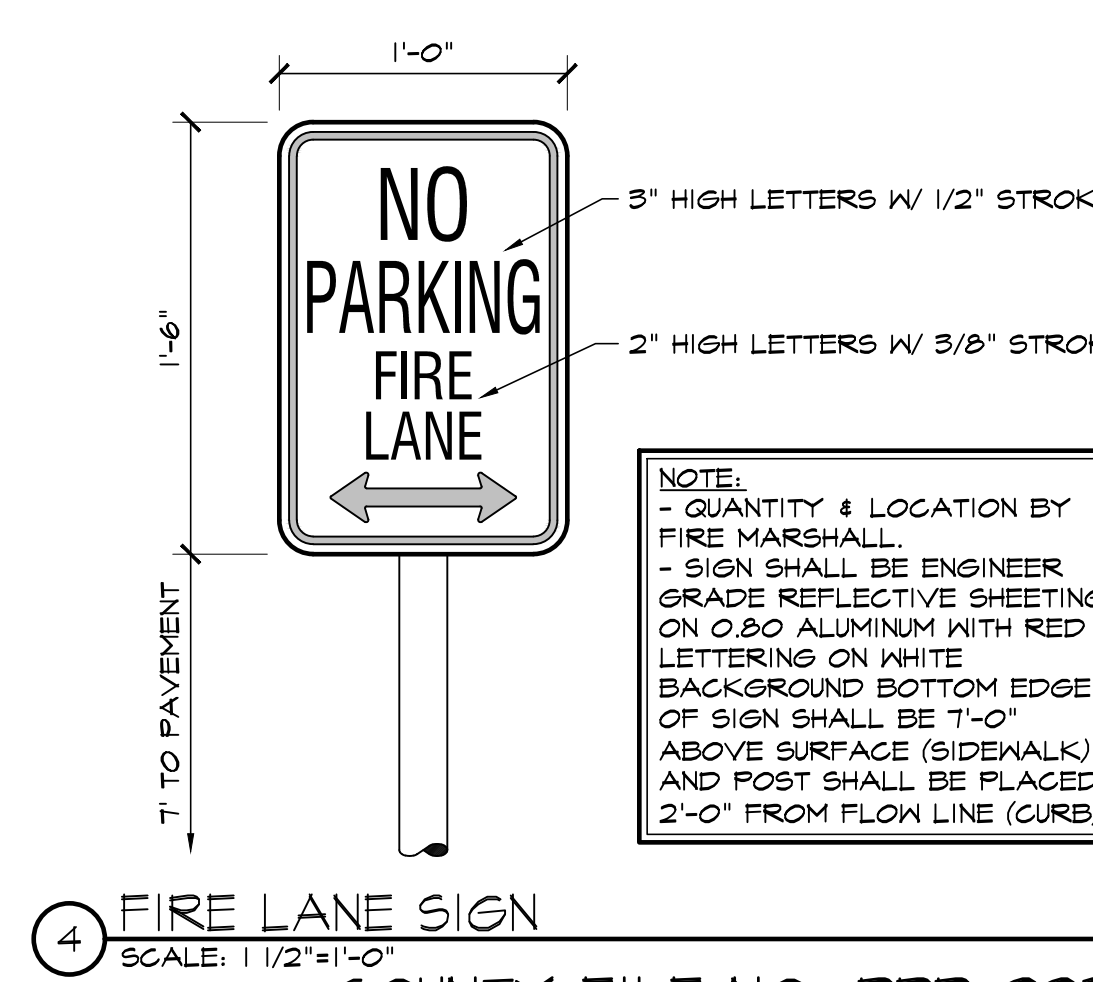
10 BIKE RACK DETAIL
SCALE: 1/2"=1'-0"



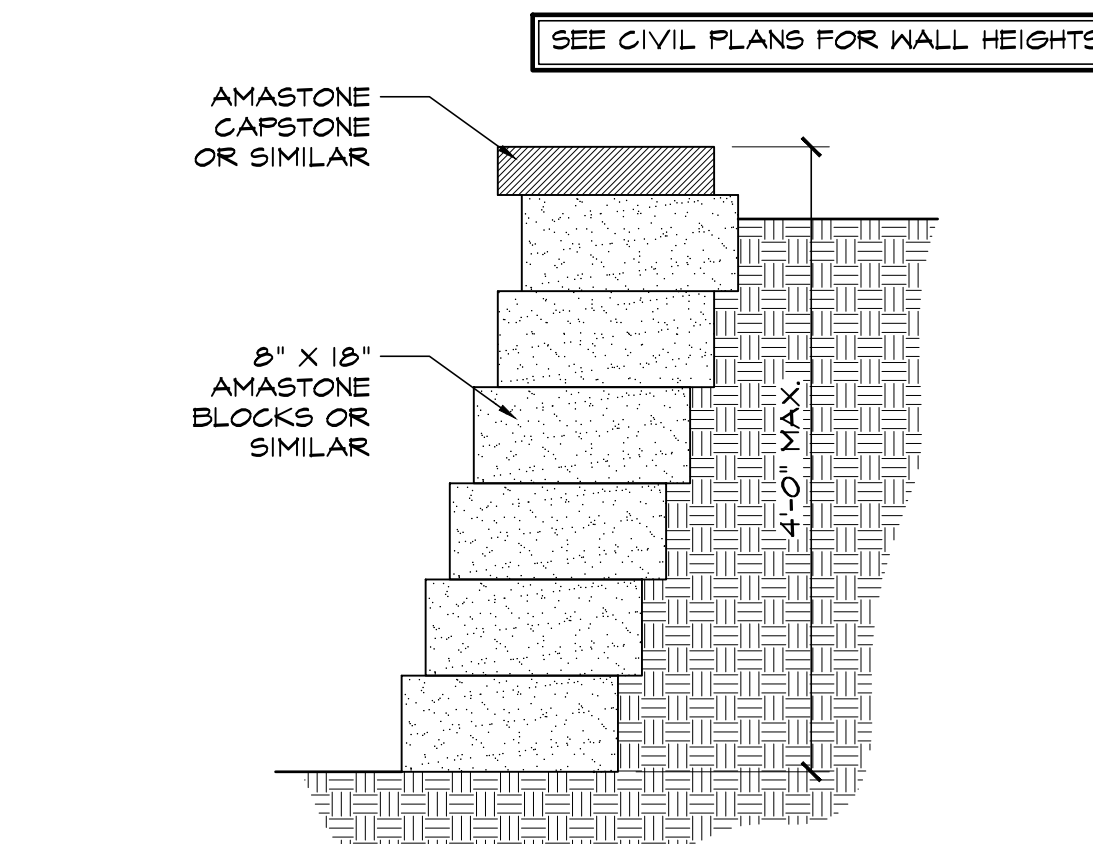
6 CMU TRASH ENCLOSURE DTL
SCALE: 3/8"=1'-0"



3 HANDICAPPED PARKING SIGN
SCALE: 1/2"=1'-0"



4 FIRE LANE SIGN
SCALE: 1/2"=1'-0"



12 RETAINING WALL DETAIL
SCALE: 3/4"=1'-0"

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WIRENUT HOME SERVICES
 6365 E PLATTE AVE.
 COLORADO SPRINGS, CO 80915
 EL PASO, COLORADO

DATE: SEPT. 21, 2022
 DRAWN BY: J.CANTERBERRY
 PROJ. MGR: J. BUTLER
 SCALE: SEE PLAN
 APPROVED BY:
 JOB NO: 1280

RESUBMITTALS:
 9-23-22/DP COMMENTS

NOTE:
 - QUANTITY & LOCATION BY FIRE MARSHALL
 - SIGN SHALL BE ENGINEER GRADE REFLECTIVE SHEETING ON 080 ALUMINUM WITH RED LETTERING ON WHITE BACKGROUND BOTTOM EDGE OF SIGN SHALL BE 1'-0" ABOVE SURFACE (SIDEWALK) AND POST SHALL BE PLACED 2'-0" FROM FLOW LINE (CURB).

NOTE:
 - QUANTITY & LOCATION BY FIRE MARSHALL
 - SIGN SHALL BE ENGINEER GRADE REFLECTIVE SHEETING ON 080 ALUMINUM WITH RED LETTERING ON WHITE BACKGROUND BOTTOM EDGE OF SIGN SHALL BE 1'-0" ABOVE SURFACE (SIDEWALK) AND POST SHALL BE PLACED 2'-0" FROM FLOW LINE (CURB).

Levels of Service



| Intersection | | | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 1.9 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ | ↕ | ↕ | | ↔ | ↕ | ↕ | | | ↕ | | | ↕ |
| Traffic Vol, veh/h | 13 | 42 | 1706 | 4 | 1 | 20 | 2153 | 32 | 0 | 0 | 2 | 0 | 0 | 59 |
| Future Vol, veh/h | 13 | 42 | 1706 | 4 | 1 | 20 | 2153 | 32 | 0 | 0 | 2 | 0 | 0 | 59 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 78 | 78 | 78 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 14 | 44 | 1796 | 4 | 1 | 21 | 2266 | 34 | 0 | 0 | 3 | 0 | 0 | 71 |

| Major/Minor | Major1 | | Major2 | | Minor1 | | Minor2 | | | | | | | |
|----------------------|--------|------|--------|---|--------|------|--------|---|---|---|------|---|---|------|
| Conflicting Flow All | 2266 | 2300 | 0 | 0 | 1796 | 1800 | 0 | 0 | - | - | 898 | - | - | 1133 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 6.44 | 4.14 | - | - | 6.44 | 4.14 | - | - | - | - | 6.94 | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.52 | 2.22 | - | - | 2.52 | 2.22 | - | - | - | - | 3.32 | - | - | 3.32 |
| Pot Cap-1 Maneuver | 49 | 215 | - | - | 101 | 339 | - | - | 0 | 0 | 282 | 0 | 0 | 197 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 95 | 95 | - | - | 304 | 304 | - | - | - | - | 282 | - | - | 197 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-----|-----|------|------|
| HCM Control Delay, s | 2.8 | 0.2 | 17.9 | 33.2 |
| HCM LOS | | | C | D |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 282 | 95 | - | - | 304 | - | - | 197 |
| HCM Lane V/C Ratio | 0.009 | 0.609 | - | - | 0.073 | - | - | 0.361 |
| HCM Control Delay (s) | 17.9 | 89.3 | - | - | 17.8 | - | - | 33.2 |
| HCM Lane LOS | C | F | - | - | C | - | - | D |
| HCM 95th %tile Q(veh) | 0 | 2.9 | - | - | 0.2 | - | - | 1.5 |

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.1 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 9 | 0 | 2 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 4 | 32 |
| Future Vol, veh/h | 9 | 0 | 2 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 4 | 32 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 78 | 78 | 78 | 78 | 78 | 78 | 78 | 78 | 78 | 78 | 78 | 78 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 12 | 0 | 3 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 5 | 41 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | Major2 | | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 30 | 30 | 26 | 31 | 50 | 0 | 46 | 0 | 0 | 0 | 0 | 0 |
| Stage 1 | 28 | 28 | - | 2 | 2 | - | - | - | - | - | - | - |
| Stage 2 | 2 | 2 | - | 29 | 48 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 979 | 863 | 1050 | 977 | 841 | - | 1562 | - | - | - | - | - |
| Stage 1 | 989 | 872 | - | 1021 | 894 | - | - | - | - | - | - | - |
| Stage 2 | 1021 | 894 | - | 988 | 855 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | 862 | 1050 | 974 | 840 | - | 1562 | - | - | - | - | - |
| Mov Cap-2 Maneuver | - | 862 | - | 974 | 840 | - | - | - | - | - | - | - |
| Stage 1 | 988 | 872 | - | 1020 | 893 | - | - | - | - | - | - | - |
| Stage 2 | 1020 | 893 | - | 986 | 855 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | | | |
|----------------------|----|--|----|--|-----|--|----|--|--|--|
| HCM Control Delay, s | | | 0 | | 7.3 | | | | | |
| HCM LOS | - | | A | | | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|-------|-----|-----|-------|-------|-----|-----|-----|
| Capacity (veh/h) | 1562 | - | - | - | - | - | - | - |
| HCM Lane V/C Ratio | 0.001 | - | - | - | - | - | - | - |
| HCM Control Delay (s) | 7.3 | 0 | - | - | 0 | - | - | - |
| HCM Lane LOS | A | A | - | - | A | - | - | - |
| HCM 95th %tile Q(veh) | 0 | - | - | - | - | - | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 2.9 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 4 | 62 | 28 | 4 | 6 | 5 |
| Future Vol, veh/h | 4 | 62 | 28 | 4 | 6 | 5 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 78 | 78 | 78 | 78 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 75 | 36 | 5 | 8 | 6 |

| Major/Minor | Major1 | Major2 | Minor1 | | |
|----------------------|--------|--------|--------|---|-------------|
| Conflicting Flow All | 0 | 0 | 80 | 0 | 120 43 |
| Stage 1 | - | - | - | - | 43 - |
| Stage 2 | - | - | - | - | 77 - |
| Critical Hdwy | - | - | 4.12 | - | 6.42 6.22 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 - |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 - |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 3.318 |
| Pot Cap-1 Maneuver | - | - | 1518 | - | 876 1027 |
| Stage 1 | - | - | - | - | 979 - |
| Stage 2 | - | - | - | - | 946 - |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1518 | - | 855 1027 |
| Mov Cap-2 Maneuver | - | - | - | - | 855 - |
| Stage 1 | - | - | - | - | 979 - |
| Stage 2 | - | - | - | - | 923 - |

| Approach | EB | WB | NB |
|----------------------|----|-----|----|
| HCM Control Delay, s | 0 | 6.5 | 9 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 925 | - | - | 1518 | - |
| HCM Lane V/C Ratio | 0.015 | - | - | 0.024 | - |
| HCM Control Delay (s) | 9 | - | - | 7.4 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.1 | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.2 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑ | ↑ | | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 1730 | 61 | 0 | 2225 | 0 | 35 |
| Future Vol, veh/h | 1730 | 61 | 0 | 2225 | 0 | 35 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | Stop |
| Storage Length | - | 0 | - | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 92 | 92 | 92 | 92 | 92 | 92 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 1880 | 66 | 0 | 2418 | 0 | 38 |

| Major/Minor | Major1 | Major2 | Minor1 | | | |
|----------------------|--------|--------|--------|---|---|------|
| Conflicting Flow All | 0 | 0 | - | - | - | 940 |
| Stage 1 | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - |
| Critical Hdwy | - | - | - | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - |
| Follow-up Hdwy | - | - | - | - | - | 3.32 |
| Pot Cap-1 Maneuver | - | - | 0 | - | 0 | 265 |
| Stage 1 | - | - | 0 | - | 0 | - |
| Stage 2 | - | - | 0 | - | 0 | - |
| Platoon blocked, % | - | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | - | - | - | 265 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - |

| Approach | EB | WB | NB |
|----------------------|----|----|------|
| HCM Control Delay, s | 0 | 0 | 20.8 |
| HCM LOS | | | C |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBT |
|-----------------------|-------|-----|-----|-----|
| Capacity (veh/h) | 265 | - | - | - |
| HCM Lane V/C Ratio | 0.144 | - | - | - |
| HCM Control Delay (s) | 20.8 | - | - | - |
| HCM Lane LOS | C | - | - | - |
| HCM 95th %tile Q(veh) | 0.5 | - | - | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 4.5 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ | ↕ | ↗ | | ↔ | ↕ | ↗ | | | ↗ | | | ↗ |
| Traffic Vol, veh/h | 5 | 90 | 2075 | 70 | 5 | 40 | 1875 | 40 | 0 | 0 | 135 | 0 | 0 | 105 |
| Future Vol, veh/h | 5 | 90 | 2075 | 70 | 5 | 40 | 1875 | 40 | 0 | 0 | 135 | 0 | 0 | 105 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 83 | 83 | 83 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 95 | 2184 | 74 | 5 | 42 | 1974 | 42 | 0 | 0 | 163 | 0 | 0 | 127 |

| Major/Minor | Major1 | | Major2 | | Minor1 | | Minor2 | | | | | | | |
|----------------------|--------|------|--------|---|--------|------|--------|---|---|---|------|---|---|------|
| Conflicting Flow All | 1974 | 2016 | 0 | 0 | 2184 | 2258 | 0 | 0 | - | - | 1092 | - | - | 987 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 6.44 | 4.14 | - | - | 6.44 | 4.14 | - | - | - | - | 6.94 | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.52 | 2.22 | - | - | 2.52 | 2.22 | - | - | - | - | 3.32 | - | - | 3.32 |
| Pot Cap-1 Maneuver | 77 | 279 | - | - | 56 | 224 | - | - | 0 | 0 | 210 | 0 | 0 | 246 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 218 | 218 | - | - | 105 | 105 | - | - | - | - | 210 | - | - | 246 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-----|-----|------|------|
| HCM Control Delay, s | 1.5 | 1.5 | 63.9 | 34.2 |
| HCM LOS | | | F | D |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 210 | 218 | - | - | 105 | - | - | 246 |
| HCM Lane V/C Ratio | 0.775 | 0.459 | - | - | 0.451 | - | - | 0.514 |
| HCM Control Delay (s) | 63.9 | 34.8 | - | - | 64.8 | - | - | 34.2 |
| HCM Lane LOS | F | D | - | - | F | - | - | D |
| HCM 95th %tile Q(veh) | 5.4 | 2.2 | - | - | 1.9 | - | - | 2.7 |

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 3 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 60 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 3 | 10 | 130 |
| Future Vol, veh/h | 60 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 3 | 10 | 130 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 78 | 78 | 78 | 78 | 78 | 78 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 72 | 1 | 1 | 1 | 1 | 3 | 0 | 1 | 6 | 4 | 12 | 157 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | Major2 | | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 105 | 106 | 91 | 104 | 181 | 4 | 169 | 0 | 0 | 7 | 0 | 0 |
| Stage 1 | 99 | 99 | - | 4 | 4 | - | - | - | - | - | - | - |
| Stage 2 | 6 | 7 | - | 100 | 177 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 875 | 784 | 967 | 876 | 713 | 1080 | 1409 | - | - | 1614 | - | - |
| Stage 1 | 907 | 813 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1016 | 890 | - | 906 | 753 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 870 | 782 | 967 | 872 | 711 | 1080 | 1409 | - | - | 1614 | - | - |
| Mov Cap-2 Maneuver | 870 | 782 | - | 872 | 711 | - | - | - | - | - | - | - |
| Stage 1 | 907 | 811 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1012 | 890 | - | 901 | 751 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|-----|--|----|--|----|--|-----|--|
| HCM Control Delay, s | 9.5 | | 9 | | 0 | | 0.2 | |
| HCM LOS | A | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1409 | - | - | 870 | 908 | 1614 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.086 | 0.006 | 0.002 | - | - |
| HCM Control Delay (s) | 0 | - | - | 9.5 | 9 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | A | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.3 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 4 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 51 | 0 | 90 | 41 | 0 | 11 |
| Future Vol, veh/h | 51 | 0 | 90 | 41 | 0 | 11 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 78 | 78 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 61 | 0 | 108 | 49 | 0 | 14 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 61 | 0 | 326 |
| Stage 1 | - | - | - | - | 61 |
| Stage 2 | - | - | - | - | 265 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1542 | - | 668 |
| Stage 1 | - | - | - | - | 962 |
| Stage 2 | - | - | - | - | 779 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1542 | - | 620 |
| Mov Cap-2 Maneuver | - | - | - | - | 620 |
| Stage 1 | - | - | - | - | 962 |
| Stage 2 | - | - | - | - | 723 |

| Approach | EB | WB | NB |
|----------------------|----|-----|-----|
| HCM Control Delay, s | 0 | 5.2 | 8.6 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|------|-----|
| Capacity (veh/h) | 1004 | - | - | 1542 | - |
| HCM Lane V/C Ratio | 0.014 | - | - | 0.07 | - |
| HCM Control Delay (s) | 8.6 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.2 | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 0.4 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑↑ | ↑ | | ↑↑↑ | | ↑ |
| Traffic Vol, veh/h | 2101 | 54 | 0 | 1868 | 0 | 50 |
| Future Vol, veh/h | 2101 | 54 | 0 | 1868 | 0 | 50 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | Stop |
| Storage Length | - | 0 | - | - | - | 0 |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 2212 | 57 | 0 | 1966 | 0 | 60 |

| Major/Minor | Major1 | Major2 | Minor1 | | | |
|----------------------|--------|--------|--------|---|---|------|
| Conflicting Flow All | 0 | 0 | - | - | - | 1106 |
| Stage 1 | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - |
| Critical Hdwy | - | - | - | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - |
| Follow-up Hdwy | - | - | - | - | - | 3.32 |
| Pot Cap-1 Maneuver | - | - | 0 | - | 0 | 205 |
| Stage 1 | - | - | 0 | - | 0 | - |
| Stage 2 | - | - | 0 | - | 0 | - |
| Platoon blocked, % | - | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | - | - | - | 205 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - |

| Approach | EB | WB | NB |
|----------------------|----|----|------|
| HCM Control Delay, s | 0 | 0 | 29.7 |
| HCM LOS | | | D |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBT |
|-----------------------|-------|-----|-----|-----|
| Capacity (veh/h) | 205 | - | - | - |
| HCM Lane V/C Ratio | 0.294 | - | - | - |
| HCM Control Delay (s) | 29.7 | - | - | - |
| HCM Lane LOS | D | - | - | - |
| HCM 95th %tile Q(veh) | 1.2 | - | - | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 5 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ | ↕ | ↕ | | ↔ | ↕ | ↕ | | | ↕ | | | ↕ |
| Traffic Vol, veh/h | 5 | 65 | 2025 | 95 | 5 | 45 | 2155 | 45 | 0 | 0 | 55 | 0 | 0 | 140 |
| Future Vol, veh/h | 5 | 65 | 2025 | 95 | 5 | 45 | 2155 | 45 | 0 | 0 | 55 | 0 | 0 | 140 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 83 | 83 | 83 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 68 | 2132 | 100 | 5 | 47 | 2268 | 47 | 0 | 0 | 66 | 0 | 0 | 169 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | | | |
|----------------------|--------|------|---|--------|------|------|--------|---|---|--------|------|---|---|------|
| Conflicting Flow All | 2268 | 2315 | 0 | 0 | 2132 | 2232 | 0 | 0 | - | - | 1066 | - | - | 1134 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 6.44 | 4.14 | - | - | 6.44 | 4.14 | - | - | - | - | 6.94 | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.52 | 2.22 | - | - | 2.52 | 2.22 | - | - | - | - | 3.32 | - | - | 3.32 |
| Pot Cap-1 Maneuver | 49 | 213 | - | - | 61 | 229 | - | - | 0 | 0 | 218 | 0 | 0 | 196 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 106 | 106 | - | - | 164 | 164 | - | - | - | - | 218 | - | - | 196 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|----|-----|------|----|
| HCM Control Delay, s | 3 | 0.8 | 28.6 | 82 |
| HCM LOS | | | D | F |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 218 | 106 | - | - | 164 | - | - | 196 |
| HCM Lane V/C Ratio | 0.304 | 0.695 | - | - | 0.321 | - | - | 0.861 |
| HCM Control Delay (s) | 28.6 | 94.7 | - | - | 37 | - | - | 82 |
| HCM Lane LOS | D | F | - | - | E | - | - | F |
| HCM 95th %tile Q(veh) | 1.2 | 3.6 | - | - | 1.3 | - | - | 6.4 |

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 3.1 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 60 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 3 | 10 | 130 |
| Future Vol, veh/h | 60 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 3 | 10 | 130 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 78 | 78 | 78 | 78 | 78 | 78 | 87 | 87 | 87 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 72 | 1 | 1 | 1 | 1 | 3 | 0 | 1 | 6 | 3 | 11 | 149 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | Major2 | | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 98 | 99 | 86 | 97 | 170 | 4 | 160 | 0 | 0 | 7 | 0 | 0 |
| Stage 1 | 92 | 92 | - | 4 | 4 | - | - | - | - | - | - | - |
| Stage 2 | 6 | 7 | - | 93 | 166 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 884 | 791 | 973 | 885 | 723 | 1080 | 1419 | - | - | 1614 | - | - |
| Stage 1 | 915 | 819 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1016 | 890 | - | 914 | 761 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 880 | 789 | 973 | 881 | 722 | 1080 | 1419 | - | - | 1614 | - | - |
| Mov Cap-2 Maneuver | 880 | 789 | - | 881 | 722 | - | - | - | - | - | - | - |
| Stage 1 | 915 | 817 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1012 | 890 | - | 910 | 759 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|-----|--|----|--|----|--|-----|--|
| HCM Control Delay, s | 9.5 | | 9 | | 0 | | 0.2 | |
| HCM LOS | A | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1419 | - | - | 880 | 915 | 1614 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.085 | 0.006 | 0.002 | - | - |
| HCM Control Delay (s) | 0 | - | - | 9.5 | 9 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | A | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.3 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 4 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 51 | 0 | 90 | 41 | 0 | 11 |
| Future Vol, veh/h | 51 | 0 | 90 | 41 | 0 | 11 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 78 | 78 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 61 | 0 | 108 | 49 | 0 | 14 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 61 | 0 | 326 |
| Stage 1 | - | - | - | - | 61 |
| Stage 2 | - | - | - | - | 265 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1542 | - | 668 |
| Stage 1 | - | - | - | - | 962 |
| Stage 2 | - | - | - | - | 779 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1542 | - | 620 |
| Mov Cap-2 Maneuver | - | - | - | - | 620 |
| Stage 1 | - | - | - | - | 962 |
| Stage 2 | - | - | - | - | 723 |

| Approach | EB | WB | NB |
|----------------------|----|-----|-----|
| HCM Control Delay, s | 0 | 5.2 | 8.6 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|------|-----|
| Capacity (veh/h) | 1004 | - | - | 1542 | - |
| HCM Lane V/C Ratio | 0.014 | - | - | 0.07 | - |
| HCM Control Delay (s) | 8.6 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.2 | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 4.5 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ | ↕ | ↗ | | ↔ | ↕ | ↗ | | | ↗ | | | ↗ |
| Traffic Vol, veh/h | 5 | 90 | 2075 | 70 | 5 | 40 | 1875 | 40 | 0 | 0 | 135 | 0 | 0 | 105 |
| Future Vol, veh/h | 5 | 90 | 2075 | 70 | 5 | 40 | 1875 | 40 | 0 | 0 | 135 | 0 | 0 | 105 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 83 | 83 | 83 | 87 | 87 | 87 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 95 | 2184 | 74 | 5 | 42 | 1974 | 42 | 0 | 0 | 163 | 0 | 0 | 121 |

| Major/Minor | Major1 | | Major2 | | Minor1 | | Minor2 | | | | | | | |
|----------------------|--------|------|--------|---|--------|------|--------|---|---|---|------|---|---|------|
| Conflicting Flow All | 1974 | 2016 | 0 | 0 | 2184 | 2258 | 0 | 0 | - | - | 1092 | - | - | 987 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 6.44 | 4.14 | - | - | 6.44 | 4.14 | - | - | - | - | 6.94 | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.52 | 2.22 | - | - | 2.52 | 2.22 | - | - | - | - | 3.32 | - | - | 3.32 |
| Pot Cap-1 Maneuver | 77 | 279 | - | - | 56 | 224 | - | - | 0 | 0 | 210 | 0 | 0 | 246 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 218 | 218 | - | - | 105 | 105 | - | - | - | - | 210 | - | - | 246 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-----|-----|------|----|
| HCM Control Delay, s | 1.5 | 1.5 | 63.9 | 33 |
| HCM LOS | | | F | D |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 210 | 218 | - | - | 105 | - | - | 246 |
| HCM Lane V/C Ratio | 0.775 | 0.459 | - | - | 0.451 | - | - | 0.491 |
| HCM Control Delay (s) | 63.9 | 34.8 | - | - | 64.8 | - | - | 33 |
| HCM Lane LOS | F | D | - | - | F | - | - | D |
| HCM 95th %tile Q(veh) | 5.4 | 2.2 | - | - | 1.9 | - | - | 2.5 |

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 5.9 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 160 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 3 | 2 | 105 |
| Future Vol, veh/h | 160 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 3 | 2 | 105 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 87 | 87 | 87 | 78 | 78 | 78 | 78 | 78 | 78 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 184 | 2 | 2 | 1 | 4 | 6 | 0 | 6 | 4 | 4 | 2 | 127 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | Major2 | | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 87 | 84 | 66 | 84 | 145 | 8 | 129 | 0 | 0 | 10 | 0 | 0 |
| Stage 1 | 74 | 74 | - | 8 | 8 | - | - | - | - | - | - | - |
| Stage 2 | 13 | 10 | - | 76 | 137 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 899 | 806 | 998 | 903 | 746 | 1074 | 1457 | - | - | 1610 | - | - |
| Stage 1 | 935 | 833 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 1007 | 887 | - | 933 | 783 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 888 | 804 | 998 | 897 | 744 | 1074 | 1457 | - | - | 1610 | - | - |
| Mov Cap-2 Maneuver | 888 | 804 | - | 897 | 744 | - | - | - | - | - | - | - |
| Stage 1 | 935 | 831 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 997 | 887 | - | 925 | 781 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|------|--|----|--|----|--|-----|--|
| HCM Control Delay, s | 10.1 | | 9 | | 0 | | 0.2 | |
| HCM LOS | B | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1457 | - | - | 888 | 918 | 1610 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.212 | 0.013 | 0.002 | - | - |
| HCM Control Delay (s) | 0 | - | - | 10.1 | 9 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | B | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.8 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 2.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 107 | 0 | 21 | 87 | 0 | 57 |
| Future Vol, veh/h | 107 | 0 | 21 | 87 | 0 | 57 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 129 | 0 | 25 | 105 | 0 | 69 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 129 | 0 | 284 |
| Stage 1 | - | - | - | - | 129 |
| Stage 2 | - | - | - | - | 155 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1457 | - | 706 |
| Stage 1 | - | - | - | - | 897 |
| Stage 2 | - | - | - | - | 873 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1457 | - | 693 |
| Mov Cap-2 Maneuver | - | - | - | - | 693 |
| Stage 1 | - | - | - | - | 897 |
| Stage 2 | - | - | - | - | 857 |

| Approach | EB | WB | NB |
|----------------------|----|-----|-----|
| HCM Control Delay, s | 0 | 1.5 | 9.2 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 921 | - | - | 1457 | - |
| HCM Lane V/C Ratio | 0.075 | - | - | 0.017 | - |
| HCM Control Delay (s) | 9.2 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0.2 | - | - | 0.1 | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 4.8 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ | ↕ | ↕ | | ↔ | ↕ | ↕ | | | ↕ | | | ↕ |
| Traffic Vol, veh/h | 5 | 65 | 2025 | 110 | 5 | 51 | 2155 | 45 | 0 | 0 | 62 | 0 | 0 | 140 |
| Future Vol, veh/h | 5 | 65 | 2025 | 110 | 5 | 51 | 2155 | 45 | 0 | 0 | 62 | 0 | 0 | 140 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 83 | 83 | 83 | 87 | 87 | 87 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 68 | 2132 | 116 | 5 | 54 | 2268 | 47 | 0 | 0 | 75 | 0 | 0 | 161 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | | | |
|----------------------|--------|------|---|--------|------|------|--------|---|---|--------|------|---|---|------|
| Conflicting Flow All | 2268 | 2315 | 0 | 0 | 2132 | 2248 | 0 | 0 | - | - | 1066 | - | - | 1134 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 6.44 | 4.14 | - | - | 6.44 | 4.14 | - | - | - | - | 6.94 | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.52 | 2.22 | - | - | 2.52 | 2.22 | - | - | - | - | 3.32 | - | - | 3.32 |
| Pot Cap-1 Maneuver | 49 | 213 | - | - | 61 | 226 | - | - | 0 | 0 | 218 | 0 | 0 | 196 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 106 | 106 | - | - | 165 | 165 | - | - | - | - | 218 | - | - | 196 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|----|----|------|------|
| HCM Control Delay, s | 3 | 1 | 29.9 | 74.8 |
| HCM LOS | | | D | F |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 218 | 106 | - | - | 165 | - | - | 196 |
| HCM Lane V/C Ratio | 0.343 | 0.695 | - | - | 0.357 | - | - | 0.821 |
| HCM Control Delay (s) | 29.9 | 94.7 | - | - | 38.5 | - | - | 74.8 |
| HCM Lane LOS | D | F | - | - | E | - | - | F |
| HCM 95th %tile Q(veh) | 1.4 | 3.6 | - | - | 1.5 | - | - | 5.9 |

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 3 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 67 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 3 | 10 | 151 |
| Future Vol, veh/h | 67 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 3 | 10 | 151 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 78 | 78 | 78 | 78 | 78 | 78 | 87 | 87 | 87 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 81 | 1 | 1 | 1 | 1 | 3 | 0 | 1 | 6 | 3 | 11 | 174 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | Major2 | | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 110 | 111 | 98 | 109 | 195 | 4 | 185 | 0 | 0 | 7 | 0 | 0 |
| Stage 1 | 104 | 104 | - | 4 | 4 | - | - | - | - | - | - | - |
| Stage 2 | 6 | 7 | - | 105 | 191 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 868 | 779 | 958 | 870 | 700 | 1080 | 1390 | - | - | 1614 | - | - |
| Stage 1 | 902 | 809 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1016 | 890 | - | 901 | 742 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 864 | 777 | 958 | 867 | 699 | 1080 | 1390 | - | - | 1614 | - | - |
| Mov Cap-2 Maneuver | 864 | 777 | - | 867 | 699 | - | - | - | - | - | - | - |
| Stage 1 | 902 | 807 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1012 | 890 | - | 897 | 741 | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-----|----|----|-----|
| HCM Control Delay, s | 9.6 | 9 | 0 | 0.1 |
| HCM LOS | A | A | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|------------|-------|-------|-----|
| Capacity (veh/h) | 1390 | - | - | 864 | 902 | 1614 | - |
| HCM Lane V/C Ratio | - | - | - | 0.096 | 0.006 | 0.002 | - |
| HCM Control Delay (s) | 0 | - | - | 9.6 | 9 | 7.2 | 0 |
| HCM Lane LOS | A | - | - | A | A | A | A |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.3 | 0 | 0 | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 4.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 51 | 0 | 111 | 41 | 0 | 18 |
| Future Vol, veh/h | 51 | 0 | 111 | 41 | 0 | 18 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 78 | 78 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 61 | 0 | 134 | 49 | 0 | 23 |

| Major/Minor | Major1 | Major2 | Minor1 | | |
|----------------------|--------|--------|--------|---|-------------|
| Conflicting Flow All | 0 | 0 | 61 | 0 | 378 61 |
| Stage 1 | - | - | - | - | 61 - |
| Stage 2 | - | - | - | - | 317 - |
| Critical Hdwy | - | - | 4.12 | - | 6.42 6.22 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 - |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 - |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 3.318 |
| Pot Cap-1 Maneuver | - | - | 1542 | - | 624 1004 |
| Stage 1 | - | - | - | - | 962 - |
| Stage 2 | - | - | - | - | 738 - |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1542 | - | 568 1004 |
| Mov Cap-2 Maneuver | - | - | - | - | 568 - |
| Stage 1 | - | - | - | - | 962 - |
| Stage 2 | - | - | - | - | 672 - |

| Approach | EB | WB | NB |
|----------------------|----|-----|-----|
| HCM Control Delay, s | 0 | 5.5 | 8.7 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 1004 | - | - | 1542 | - |
| HCM Lane V/C Ratio | 0.023 | - | - | 0.087 | - |
| HCM Control Delay (s) | 8.7 | - | - | 7.6 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0.1 | - | - | 0.3 | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 5.7 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ | ↕ | ↕ | | ↔ | ↕ | ↕ | | | ↕ | | | ↕ |
| Traffic Vol, veh/h | 5 | 90 | 2075 | 77 | 5 | 43 | 1875 | 40 | 0 | 0 | 158 | 0 | 0 | 105 |
| Future Vol, veh/h | 5 | 90 | 2075 | 77 | 5 | 43 | 1875 | 40 | 0 | 0 | 158 | 0 | 0 | 105 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 87 | 87 | 87 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 95 | 2184 | 81 | 5 | 45 | 1974 | 42 | 0 | 0 | 182 | 0 | 0 | 127 |

| Major/Minor | Major1 | | Major2 | | Minor1 | | Minor2 | | | | | | | |
|----------------------|--------|------|--------|---|--------|------|--------|---|---|---|------|---|---|------|
| Conflicting Flow All | 1974 | 2016 | 0 | 0 | 2184 | 2265 | 0 | 0 | - | - | 1092 | - | - | 987 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 6.44 | 4.14 | - | - | 6.44 | 4.14 | - | - | - | - | 6.94 | - | - | 6.94 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.52 | 2.22 | - | - | 2.52 | 2.22 | - | - | - | - | 3.32 | - | - | 3.32 |
| Pot Cap-1 Maneuver | 77 | 279 | - | - | 56 | 222 | - | - | 0 | 0 | 210 | 0 | 0 | 246 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 218 | 218 | - | - | 87 | 87 | - | - | - | - | 210 | - | - | 246 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-----|-----|------|------|
| HCM Control Delay, s | 1.5 | 2.3 | 78.9 | 34.2 |
| HCM LOS | | | F | D |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-------|-----|-----|-------|-----|-----|-------|
| Capacity (veh/h) | 210 | 218 | - | - | 87 | - | - | 246 |
| HCM Lane V/C Ratio | 0.865 | 0.459 | - | - | 0.581 | - | - | 0.514 |
| HCM Control Delay (s) | 78.9 | 34.8 | - | - | 92.2 | - | - | 34.2 |
| HCM Lane LOS | F | D | - | - | F | - | - | D |
| HCM 95th %tile Q(veh) | 6.7 | 2.2 | - | - | 2.6 | - | - | 2.7 |

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 4.7 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 103 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 3 | 2 | 116 |
| Future Vol, veh/h | 103 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 3 | 2 | 116 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 78 | 78 | 78 | 78 | 78 | 78 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 124 | 2 | 2 | 1 | 4 | 6 | 0 | 6 | 4 | 4 | 2 | 140 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | | Major2 | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|-------|--------|---|-------|---|---|
| Conflicting Flow All | 93 | 90 | 72 | 90 | 158 | 8 | 142 | 0 | 0 | 10 | 0 | 0 |
| Stage 1 | 80 | 80 | - | 8 | 8 | - | - | - | - | - | - | - |
| Stage 2 | 13 | 10 | - | 82 | 150 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 891 | 800 | 990 | 895 | 734 | 1074 | 1441 | - | - | 1610 | - | - |
| Stage 1 | 929 | 828 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 1007 | 887 | - | 926 | 773 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 880 | 798 | 990 | 889 | 732 | 1074 | 1441 | - | - | 1610 | - | - |
| Mov Cap-2 Maneuver | 880 | 798 | - | 889 | 732 | - | - | - | - | - | - | - |
| Stage 1 | 929 | 826 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 997 | 887 | - | 918 | 771 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|-----|--|----|--|----|--|-----|--|
| HCM Control Delay, s | 9.8 | | 9 | | 0 | | 0.2 | |
| HCM LOS | A | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1441 | - | - | 880 | 911 | 1610 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.146 | 0.013 | 0.002 | - | - |
| HCM Control Delay (s) | 0 | - | - | 9.8 | 9 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | A | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.5 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 3.2 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 107 | 0 | 32 | 87 | 0 | 80 |
| Future Vol, veh/h | 107 | 0 | 32 | 87 | 0 | 80 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 129 | 0 | 39 | 105 | 0 | 96 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 129 | 0 | 312 |
| Stage 1 | - | - | - | - | 129 |
| Stage 2 | - | - | - | - | 183 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1457 | - | 681 |
| Stage 1 | - | - | - | - | 897 |
| Stage 2 | - | - | - | - | 848 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1457 | - | 662 |
| Mov Cap-2 Maneuver | - | - | - | - | 662 |
| Stage 1 | - | - | - | - | 897 |
| Stage 2 | - | - | - | - | 824 |

| Approach | EB | WB | NB |
|----------------------|----|----|-----|
| HCM Control Delay, s | 0 | 2 | 9.4 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 921 | - | - | 1457 | - |
| HCM Lane V/C Ratio | 0.105 | - | - | 0.026 | - |
| HCM Control Delay (s) | 9.4 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 0.1 | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|-------|-------|-------|------|------|-------|-------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 213.2 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | | ↔ ↑ | | | ↔ ↑ |
| Traffic Vol, veh/h | 5 | 170 | 2800 | 100 | 5 | 44 | 3050 | 285 | 0 | 0 | 68 | 0 | 0 | 425 |
| Future Vol, veh/h | 5 | 170 | 2800 | 100 | 5 | 44 | 3050 | 285 | 0 | 0 | 68 | 0 | 0 | 425 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 83 | 83 | 83 | 92 | 92 | 92 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 179 | 2947 | 105 | 5 | 46 | 3211 | 300 | 0 | 0 | 82 | 0 | 0 | 462 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | | | |
|----------------------|--------|------|---|--------|------|------|--------|---|---|--------|------|---|---|------|
| Conflicting Flow All | 2344 | 3511 | 0 | 0 | 2152 | 3052 | 0 | 0 | - | - | 1474 | - | - | 1606 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 5.64 | 5.34 | - | - | 5.64 | 5.34 | - | - | - | - | 7.14 | - | - | 7.14 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.32 | 3.12 | - | - | 2.32 | 3.12 | - | - | - | - | 3.92 | - | - | 3.92 |
| Pot Cap-1 Maneuver | 76 | ~ 20 | - | - | 99 | ~ 36 | - | - | 0 | 0 | 99 | 0 | 0 | ~ 80 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 29 | ~ 29 | - | - | 35 | ~ 35 | - | - | - | - | 99 | - | - | ~ 80 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-------|-----|-------|-----------|
| HCM Control Delay, s | 151.3 | 7.1 | 125.2 | \$ 2251.6 |
| HCM LOS | | | F | F |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----------|-----|-----|----------|-----|-----|-----------|
| Capacity (veh/h) | 99 | ~ 29 | - | - | ~ 35 | - | - | 80 |
| HCM Lane V/C Ratio | 0.828 | 6.352 | - | - | 1.474 | - | - | 5.774 |
| HCM Control Delay (s) | 125.3 | \$ 2659.1 | - | - | \$ 487.1 | - | - | \$ 2251.6 |
| HCM Lane LOS | F | F | - | - | F | - | - | F |
| HCM 95th %tile Q(veh) | 4.6 | 22.5 | - | - | 5.6 | - | - | 51.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 4.7 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 103 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 3 | 2 | 116 |
| Future Vol, veh/h | 103 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 3 | 2 | 116 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 78 | 78 | 78 | 78 | 78 | 78 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 124 | 2 | 2 | 1 | 4 | 6 | 0 | 6 | 4 | 4 | 2 | 140 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | | Major2 | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|-------|--------|---|-------|---|---|
| Conflicting Flow All | 93 | 90 | 72 | 90 | 158 | 8 | 142 | 0 | 0 | 10 | 0 | 0 |
| Stage 1 | 80 | 80 | - | 8 | 8 | - | - | - | - | - | - | - |
| Stage 2 | 13 | 10 | - | 82 | 150 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 891 | 800 | 990 | 895 | 734 | 1074 | 1441 | - | - | 1610 | - | - |
| Stage 1 | 929 | 828 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 1007 | 887 | - | 926 | 773 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 880 | 798 | 990 | 889 | 732 | 1074 | 1441 | - | - | 1610 | - | - |
| Mov Cap-2 Maneuver | 880 | 798 | - | 889 | 732 | - | - | - | - | - | - | - |
| Stage 1 | 929 | 826 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 997 | 887 | - | 918 | 771 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|-----|--|----|--|----|--|-----|--|
| HCM Control Delay, s | 9.8 | | 9 | | 0 | | 0.2 | |
| HCM LOS | A | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1441 | - | - | 880 | 911 | 1610 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.146 | 0.013 | 0.002 | - | - |
| HCM Control Delay (s) | 0 | - | - | 9.8 | 9 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | A | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.5 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 3.2 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 107 | 0 | 32 | 87 | 0 | 80 |
| Future Vol, veh/h | 107 | 0 | 32 | 87 | 0 | 80 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 129 | 0 | 39 | 105 | 0 | 96 |

| Major/Minor | Major1 | Major2 | Minor1 | | |
|----------------------|--------|--------|--------|---|-------|
| Conflicting Flow All | 0 | 0 | 129 | 0 | 312 |
| Stage 1 | - | - | - | - | 129 |
| Stage 2 | - | - | - | - | 183 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1457 | - | 681 |
| Stage 1 | - | - | - | - | 897 |
| Stage 2 | - | - | - | - | 848 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1457 | - | 662 |
| Mov Cap-2 Maneuver | - | - | - | - | 662 |
| Stage 1 | - | - | - | - | 897 |
| Stage 2 | - | - | - | - | 824 |

| Approach | EB | WB | NB |
|----------------------|----|----|-----|
| HCM Control Delay, s | 0 | 2 | 9.4 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 921 | - | - | 1457 | - |
| HCM Lane V/C Ratio | 0.105 | - | - | 0.026 | - |
| HCM Control Delay (s) | 9.4 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0.3 | - | - | 0.1 | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|-------|-------|-------|------|------|-------|-------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 125.6 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | | ↔ ↑ | | | ↔ ↑ |
| Traffic Vol, veh/h | 5 | 275 | 2900 | 73 | 5 | 52 | 2650 | 145 | 0 | 0 | 152 | 0 | 0 | 290 |
| Future Vol, veh/h | 5 | 275 | 2900 | 73 | 5 | 52 | 2650 | 145 | 0 | 0 | 152 | 0 | 0 | 290 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 87 | 87 | 87 | 92 | 92 | 92 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 289 | 3053 | 77 | 5 | 55 | 2789 | 153 | 0 | 0 | 175 | 0 | 0 | 315 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | | | |
|----------------------|--------|------|---|--------|------|------|--------|---|---|--------|------|---|---|------|
| Conflicting Flow All | 2036 | 2942 | 0 | 0 | 2228 | 3130 | 0 | 0 | - | - | 1527 | - | - | 1395 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 5.64 | 5.34 | - | - | 5.64 | 5.34 | - | - | - | - | 7.14 | - | - | 7.14 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.32 | 3.12 | - | - | 2.32 | 3.12 | - | - | - | - | 3.92 | - | - | 3.92 |
| Pot Cap-1 Maneuver | 115 | ~41 | - | - | 89 | ~32 | - | - | 0 | 0 | ~91 | 0 | 0 | ~112 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 67 | ~67 | - | - | ~65 | ~65 | - | - | - | - | ~91 | - | - | ~112 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-------|----|----------|----------|
| HCM Control Delay, s | 143.7 | | \$ 529.1 | \$ 900.8 |
| HCM LOS | | | F | F |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|----------|---------|-----|-----|-----|-----|-----|----------|
| Capacity (veh/h) | 91 | ~67 | - | - | + | - | - | 112 |
| HCM Lane V/C Ratio | 1.92 | 4.399 | - | - | - | - | - | 2.814 |
| HCM Control Delay (s) | \$ 529.1 | \$ 1669 | - | - | - | - | - | \$ 900.8 |
| HCM Lane LOS | F | F | - | - | - | - | - | F |
| HCM 95th %tile Q(veh) | 14.9 | 31.9 | - | - | - | - | - | 29.4 |

Notes
~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 5.6 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 142 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 5 | 3 | 116 |
| Future Vol, veh/h | 142 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 5 | 3 | 116 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 78 | 78 | 78 | 78 | 78 | 78 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 171 | 2 | 2 | 1 | 4 | 6 | 0 | 6 | 4 | 6 | 4 | 140 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | Major2 | | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 99 | 96 | 74 | 96 | 164 | 8 | 144 | 0 | 0 | 10 | 0 | 0 |
| Stage 1 | 86 | 86 | - | 8 | 8 | - | - | - | - | - | - | - |
| Stage 2 | 13 | 10 | - | 88 | 156 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 883 | 794 | 988 | 887 | 729 | 1074 | 1438 | - | - | 1610 | - | - |
| Stage 1 | 922 | 824 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 1007 | 887 | - | 920 | 769 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 872 | 791 | 988 | 880 | 726 | 1074 | 1438 | - | - | 1610 | - | - |
| Mov Cap-2 Maneuver | 872 | 791 | - | 880 | 726 | - | - | - | - | - | - | - |
| Stage 1 | 922 | 821 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 997 | 887 | - | 911 | 766 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|------|--|----|--|----|--|-----|--|
| HCM Control Delay, s | 10.2 | | 9 | | 0 | | 0.3 | |
| HCM LOS | B | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1438 | - | - | 872 | 907 | 1610 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.202 | 0.013 | 0.004 | - | - |
| HCM Control Delay (s) | 0 | - | - | 10.2 | 9 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | B | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.8 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 1.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 112 | 0 | 10 | 109 | 0 | 34 |
| Future Vol, veh/h | 112 | 0 | 10 | 109 | 0 | 34 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 78 | 78 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 135 | 0 | 12 | 131 | 0 | 44 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 135 | 0 | 290 |
| Stage 1 | - | - | - | - | 135 |
| Stage 2 | - | - | - | - | 155 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1449 | - | 701 |
| Stage 1 | - | - | - | - | 891 |
| Stage 2 | - | - | - | - | 873 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1449 | - | 695 |
| Mov Cap-2 Maneuver | - | - | - | - | 695 |
| Stage 1 | - | - | - | - | 891 |
| Stage 2 | - | - | - | - | 865 |

| Approach | EB | WB | NB |
|----------------------|----|-----|-----|
| HCM Control Delay, s | 0 | 0.6 | 9.1 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 914 | - | - | 1449 | - |
| HCM Lane V/C Ratio | 0.048 | - | - | 0.008 | - |
| HCM Control Delay (s) | 9.1 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0.1 | - | - | 0 | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|-------|-------|-------|------|------|-------|-------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 209.4 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | | ↔ ↑ | | | ↔ ↑ |
| Traffic Vol, veh/h | 5 | 170 | 2800 | 115 | 5 | 50 | 3050 | 265 | 0 | 0 | 75 | 0 | 0 | 425 |
| Future Vol, veh/h | 5 | 170 | 2800 | 115 | 5 | 50 | 3050 | 265 | 0 | 0 | 75 | 0 | 0 | 425 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 83 | 83 | 83 | 92 | 92 | 92 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 179 | 2947 | 121 | 5 | 53 | 3211 | 279 | 0 | 0 | 90 | 0 | 0 | 462 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | | | |
|----------------------|--------|------|---|--------|------|------|--------|---|---|--------|------|---|---|------|
| Conflicting Flow All | 2344 | 3490 | 0 | 0 | 2152 | 3068 | 0 | 0 | - | - | 1474 | - | - | 1606 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 5.64 | 5.34 | - | - | 5.64 | 5.34 | - | - | - | - | 7.14 | - | - | 7.14 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.32 | 3.12 | - | - | 2.32 | 3.12 | - | - | - | - | 3.92 | - | - | 3.92 |
| Pot Cap-1 Maneuver | 76 | ~21 | - | - | 99 | ~35 | - | - | 0 | 0 | 99 | 0 | 0 | ~80 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 31 | ~31 | - | - | 34 | ~34 | - | - | - | - | 99 | - | - | ~80 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | | | WB | | | NB | | | SB | | |
|----------------------|-------|--|--|-----|--|--|-------|--|--|-----------|--|--|
| HCM Control Delay, s | 138.8 | | | 9.9 | | | 145.5 | | | \$ 2251.6 | | |
| HCM LOS | | | | | | | F | | | F | | |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|-------|-----------|-----|-----|----------|-----|-----|-----------|
| Capacity (veh/h) | 99 | ~31 | - | - | ~34 | - | - | 80 |
| HCM Lane V/C Ratio | 0.913 | 5.942 | - | - | 1.703 | - | - | 5.774 |
| HCM Control Delay (s) | 145.5 | \$ 2451.3 | - | - | \$ 605.8 | - | - | \$ 2251.6 |
| HCM Lane LOS | F | F | - | - | F | - | - | F |
| HCM 95th %tile Q(veh) | 5.3 | 22.3 | - | - | 6.4 | - | - | 51.1 |

Notes
 ~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 3.3 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 72 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 5 | 12 | 148 |
| Future Vol, veh/h | 72 | 1 | 1 | 1 | 1 | 2 | 0 | 1 | 5 | 5 | 12 | 148 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 78 | 78 | 78 | 78 | 78 | 78 | 87 | 87 | 87 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 87 | 1 | 1 | 1 | 1 | 3 | 0 | 1 | 6 | 6 | 14 | 170 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | Major2 | | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|--------|---|---|-------|---|---|
| Conflicting Flow All | 117 | 118 | 99 | 116 | 200 | 4 | 184 | 0 | 0 | 7 | 0 | 0 |
| Stage 1 | 111 | 111 | - | 4 | 4 | - | - | - | - | - | - | - |
| Stage 2 | 6 | 7 | - | 112 | 196 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 859 | 772 | 957 | 861 | 696 | 1080 | 1391 | - | - | 1614 | - | - |
| Stage 1 | 894 | 804 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1016 | 890 | - | 893 | 739 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 853 | 769 | 957 | 857 | 693 | 1080 | 1391 | - | - | 1614 | - | - |
| Mov Cap-2 Maneuver | 853 | 769 | - | 857 | 693 | - | - | - | - | - | - | - |
| Stage 1 | 894 | 801 | - | 1018 | 892 | - | - | - | - | - | - | - |
| Stage 2 | 1012 | 890 | - | 887 | 736 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|-----|--|----|--|----|--|-----|--|
| HCM Control Delay, s | 9.7 | | 9 | | 0 | | 0.2 | |
| HCM LOS | A | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1391 | - | - | 853 | 897 | 1614 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.105 | 0.006 | 0.004 | - | - |
| HCM Control Delay (s) | 0 | - | - | 9.7 | 9 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | A | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.3 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 3.5 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 63 | 0 | 90 | 59 | 0 | 11 |
| Future Vol, veh/h | 63 | 0 | 90 | 59 | 0 | 11 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 78 | 78 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 76 | 0 | 108 | 71 | 0 | 14 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 76 | 0 | 363 |
| Stage 1 | - | - | - | - | 76 |
| Stage 2 | - | - | - | - | 287 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1523 | - | 636 |
| Stage 1 | - | - | - | - | 947 |
| Stage 2 | - | - | - | - | 762 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1523 | - | 589 |
| Mov Cap-2 Maneuver | - | - | - | - | 589 |
| Stage 1 | - | - | - | - | 947 |
| Stage 2 | - | - | - | - | 706 |

| Approach | EB | WB | NB |
|----------------------|----|-----|-----|
| HCM Control Delay, s | 0 | 4.6 | 8.7 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 985 | - | - | 1523 | - |
| HCM Lane V/C Ratio | 0.014 | - | - | 0.071 | - |
| HCM Control Delay (s) | 8.7 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.2 | - |

| Intersection | | | | | | | | | | | | | | |
|--------------------------|-------|-------|-------|------|------|-------|-------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 130.5 | | | | | | | | | | | | | |
| Movement | EBU | EBL | EBT | EBR | WBU | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | ↔ ↑↑↑ | ↔ ↑↑↑ | ↔ ↑ | | | ↔ ↑ | | | ↔ ↑ |
| Traffic Vol, veh/h | 5 | 275 | 2900 | 80 | 5 | 55 | 2650 | 145 | 0 | 0 | 175 | 0 | 0 | 290 |
| Future Vol, veh/h | 5 | 275 | 2900 | 80 | 5 | 55 | 2650 | 145 | 0 | 0 | 175 | 0 | 0 | 290 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Free | Free | Free | Free | Stop | Stop | Stop | Stop | Stop | Stop |
| RT Channelized | - | - | - | None | - | - | - | None | - | - | Stop | - | - | Stop |
| Storage Length | - | 325 | - | 0 | - | 300 | - | 0 | - | - | 0 | - | - | 0 |
| Veh in Median Storage, # | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | - | 0 | - | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 95 | 87 | 87 | 87 | 92 | 92 | 92 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 5 | 289 | 3053 | 84 | 5 | 58 | 2789 | 153 | 0 | 0 | 201 | 0 | 0 | 315 |

| Major/Minor | Major1 | | | Major2 | | | Minor1 | | | Minor2 | | | | |
|----------------------|--------|------|---|--------|------|------|--------|---|---|--------|------|---|---|------|
| Conflicting Flow All | 2036 | 2942 | 0 | 0 | 2228 | 3137 | 0 | 0 | - | - | 1527 | - | - | 1395 |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy | 5.64 | 5.34 | - | - | 5.64 | 5.34 | - | - | - | - | 7.14 | - | - | 7.14 |
| Critical Hdwy Stg 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Follow-up Hdwy | 2.32 | 3.12 | - | - | 2.32 | 3.12 | - | - | - | - | 3.92 | - | - | 3.92 |
| Pot Cap-1 Maneuver | 115 | ~41 | - | - | 89 | ~32 | - | - | 0 | 0 | ~91 | 0 | 0 | ~112 |
| Stage 1 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Stage 2 | - | - | - | - | - | - | - | - | 0 | 0 | - | 0 | 0 | - |
| Platoon blocked, % | | | - | - | | | - | - | | | | | | |
| Mov Cap-1 Maneuver | 67 | ~67 | - | - | ~77 | ~77 | - | - | - | - | ~91 | - | - | ~112 |
| Mov Cap-2 Maneuver | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 1 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |
| Stage 2 | - | - | - | - | - | - | - | - | - | - | - | - | - | - |

| Approach | EB | WB | NB | SB |
|----------------------|-------|----|----------|----------|
| HCM Control Delay, s | 143.3 | | \$ 653.8 | \$ 900.8 |
| HCM LOS | | | F | F |

| Minor Lane/Major Mvmt | NBLn1 | EBL | EBT | EBR | WBL | WBT | WBR | SBLn1 |
|-----------------------|----------|---------|-----|-----|-----|-----|-----|----------|
| Capacity (veh/h) | 91 | ~67 | - | - | + | - | - | 112 |
| HCM Lane V/C Ratio | 2.21 | 4.399 | - | - | - | - | - | 2.814 |
| HCM Control Delay (s) | \$ 653.8 | \$ 1669 | - | - | - | - | - | \$ 900.8 |
| HCM Lane LOS | F | F | - | - | - | - | - | F |
| HCM 95th %tile Q(veh) | 18 | 31.9 | - | - | - | - | - | 29.4 |

Notes
~: Volume exceeds capacity \$: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

| Intersection | | | | | | | | | | | | |
|--------------------------|------|------|------|------|------|------|------|------|------|------|------|------|
| Int Delay, s/veh | 5.7 | | | | | | | | | | | |
| Movement | EBL | EBT | EBR | WBL | WBT | WBR | NBL | NBT | NBR | SBL | SBT | SBR |
| Lane Configurations | | ↕ | | | ↕ | | | ↕ | | | ↕ | |
| Traffic Vol, veh/h | 165 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 5 | 3 | 127 |
| Future Vol, veh/h | 165 | 2 | 2 | 1 | 3 | 5 | 0 | 5 | 3 | 5 | 3 | 127 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Stop | Stop | Stop | Stop | Stop | Stop | Free | Free | Free | Free | Free | Free |
| RT Channelized | - | - | None | - | - | None | - | - | None | - | - | None |
| Storage Length | - | - | - | - | - | - | - | - | - | - | - | - |
| Veh in Median Storage, # | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Grade, % | - | 0 | - | - | 0 | - | - | 0 | - | - | 0 | - |
| Peak Hour Factor | 87 | 87 | 87 | 78 | 78 | 78 | 78 | 78 | 78 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 190 | 2 | 2 | 1 | 4 | 6 | 0 | 6 | 4 | 6 | 4 | 153 |

| Major/Minor | Minor2 | | Minor1 | | Major1 | | | Major2 | | | | |
|----------------------|--------|-------|--------|-------|--------|-------|-------|--------|---|-------|---|---|
| Conflicting Flow All | 106 | 103 | 81 | 103 | 177 | 8 | 157 | 0 | 0 | 10 | 0 | 0 |
| Stage 1 | 93 | 93 | - | 8 | 8 | - | - | - | - | - | - | - |
| Stage 2 | 13 | 10 | - | 95 | 169 | - | - | - | - | - | - | - |
| Critical Hdwy | 7.12 | 6.52 | 6.22 | 7.12 | 6.52 | 6.22 | 4.12 | - | - | 4.12 | - | - |
| Critical Hdwy Stg 1 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Critical Hdwy Stg 2 | 6.12 | 5.52 | - | 6.12 | 5.52 | - | - | - | - | - | - | - |
| Follow-up Hdwy | 3.518 | 4.018 | 3.318 | 3.518 | 4.018 | 3.318 | 2.218 | - | - | 2.218 | - | - |
| Pot Cap-1 Maneuver | 873 | 787 | 979 | 877 | 717 | 1074 | 1423 | - | - | 1610 | - | - |
| Stage 1 | 914 | 818 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 1007 | 887 | - | 912 | 759 | - | - | - | - | - | - | - |
| Platoon blocked, % | | | | | | | | - | - | - | - | - |
| Mov Cap-1 Maneuver | 862 | 784 | 979 | 870 | 714 | 1074 | 1423 | - | - | 1610 | - | - |
| Mov Cap-2 Maneuver | 862 | 784 | - | 870 | 714 | - | - | - | - | - | - | - |
| Stage 1 | 914 | 815 | - | 1013 | 889 | - | - | - | - | - | - | - |
| Stage 2 | 997 | 887 | - | 904 | 756 | - | - | - | - | - | - | - |

| Approach | EB | | WB | | NB | | SB | |
|----------------------|------|--|-----|--|----|--|-----|--|
| HCM Control Delay, s | 10.4 | | 9.1 | | 0 | | 0.3 | |
| HCM LOS | B | | A | | | | | |

| Minor Lane/Major Mvmt | NBL | NBT | NBR | EBLn1 | WBLn1 | SBL | SBT | SBR |
|-----------------------|------|-----|-----|-------|-------|-------|-----|-----|
| Capacity (veh/h) | 1423 | - | - | 862 | 899 | 1610 | - | - |
| HCM Lane V/C Ratio | - | - | - | 0.225 | 0.013 | 0.004 | - | - |
| HCM Control Delay (s) | 0 | - | - | 10.4 | 9.1 | 7.2 | 0 | - |
| HCM Lane LOS | A | - | - | B | A | A | A | - |
| HCM 95th %tile Q(veh) | 0 | - | - | 0.9 | 0 | 0 | - | - |

| Intersection | | | | | | |
|--------------------------|------|------|------|------|------|------|
| Int Delay, s/veh | 2.3 | | | | | |
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | | | | | | |
| Traffic Vol, veh/h | 112 | 0 | 21 | 109 | 0 | 57 |
| Future Vol, veh/h | 112 | 0 | 21 | 109 | 0 | 57 |
| Conflicting Peds, #/hr | 0 | 0 | 0 | 0 | 0 | 0 |
| Sign Control | Free | Free | Free | Free | Stop | Stop |
| RT Channelized | - | None | - | None | - | None |
| Storage Length | - | - | - | - | 0 | - |
| Veh in Median Storage, # | 0 | - | - | 0 | 0 | - |
| Grade, % | 0 | - | - | 0 | 0 | - |
| Peak Hour Factor | 83 | 83 | 83 | 83 | 83 | 83 |
| Heavy Vehicles, % | 2 | 2 | 2 | 2 | 2 | 2 |
| Mvmt Flow | 135 | 0 | 25 | 131 | 0 | 69 |

| Major/Minor | Major1 | Major2 | Minor1 | Minor2 | Minor3 |
|----------------------|--------|--------|--------|--------|--------|
| Conflicting Flow All | 0 | 0 | 135 | 0 | 316 |
| Stage 1 | - | - | - | - | 135 |
| Stage 2 | - | - | - | - | 181 |
| Critical Hdwy | - | - | 4.12 | - | 6.42 |
| Critical Hdwy Stg 1 | - | - | - | - | 5.42 |
| Critical Hdwy Stg 2 | - | - | - | - | 5.42 |
| Follow-up Hdwy | - | - | 2.218 | - | 3.518 |
| Pot Cap-1 Maneuver | - | - | 1449 | - | 677 |
| Stage 1 | - | - | - | - | 891 |
| Stage 2 | - | - | - | - | 850 |
| Platoon blocked, % | - | - | - | - | - |
| Mov Cap-1 Maneuver | - | - | 1449 | - | 664 |
| Mov Cap-2 Maneuver | - | - | - | - | 664 |
| Stage 1 | - | - | - | - | 891 |
| Stage 2 | - | - | - | - | 834 |

| Approach | EB | WB | NB |
|----------------------|----|-----|-----|
| HCM Control Delay, s | 0 | 1.2 | 9.3 |
| HCM LOS | | | A |

| Minor Lane/Major Mvmt | NBLn1 | EBT | EBR | WBL | WBT |
|-----------------------|-------|-----|-----|-------|-----|
| Capacity (veh/h) | 914 | - | - | 1449 | - |
| HCM Lane V/C Ratio | 0.075 | - | - | 0.017 | - |
| HCM Control Delay (s) | 9.3 | - | - | 7.5 | 0 |
| HCM Lane LOS | A | - | - | A | A |
| HCM 95th %tile Q(veh) | 0.2 | - | - | 0.1 | - |

Traffic Counts



LSC Transportation Consultants, Inc.

545 E Pikes Peak Ave, Suite 210
 Colorado Springs, CO 80905
 719-633-2868

File Name : Hathaway Dr - Platte Ave AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 1

Groups Printed- Unshifted

| Start Time | Hathaway Dr Southbound | | | | | Platte Ave Westbound | | | | | Hathaway Dr Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|-------------|------------------------|-----|------|---|------------|----------------------|-----|------|---|------------|------------------------|-----|------|---|------------|----------------------|------|-----|-----|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 06:30 AM | 0 | 0 | 16 | 0 | 16 | 6 | 3 | 5 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 10 | 398 | 0 | 2 | 410 | 440 |
| 06:45 AM | 0 | 0 | 15 | 0 | 15 | 11 | 0 | 6 | 0 | 17 | 0 | 0 | 0 | 0 | 0 | 15 | 418 | 0 | 2 | 435 | 467 |
| Total | 0 | 0 | 31 | 0 | 31 | 17 | 3 | 11 | 0 | 31 | 0 | 0 | 0 | 0 | 0 | 25 | 816 | 0 | 4 | 845 | 907 |
| 07:00 AM | 0 | 0 | 18 | 0 | 18 | 5 | 0 | 3 | 1 | 9 | 0 | 0 | 0 | 0 | 0 | 9 | 471 | 2 | 2 | 484 | 511 |
| 07:15 AM | 0 | 0 | 10 | 0 | 10 | 10 | 0 | 6 | 0 | 16 | 0 | 0 | 2 | 0 | 2 | 8 | 419 | 2 | 7 | 436 | 464 |
| 07:30 AM | 0 | 0 | 17 | 0 | 17 | 6 | 0 | 3 | 0 | 9 | 0 | 0 | 4 | 0 | 4 | 18 | 362 | 2 | 5 | 387 | 417 |
| 07:45 AM | 0 | 1 | 14 | 0 | 15 | 9 | 0 | 7 | 0 | 16 | 0 | 1 | 4 | 0 | 5 | 20 | 344 | 1 | 8 | 373 | 409 |
| Total | 0 | 1 | 59 | 0 | 60 | 30 | 0 | 19 | 1 | 50 | 0 | 1 | 10 | 0 | 11 | 55 | 1596 | 7 | 22 | 1680 | 1801 |
| 08:00 AM | 0 | 0 | 13 | 0 | 13 | 6 | 0 | 6 | 0 | 12 | 0 | 0 | 12 | 0 | 12 | 15 | 281 | 0 | 9 | 305 | 342 |
| 08:15 AM | 0 | 0 | 6 | 0 | 6 | 3 | 0 | 1 | 0 | 4 | 0 | 0 | 4 | 0 | 4 | 14 | 259 | 0 | 7 | 280 | 294 |
| Grand Total | 0 | 1 | 109 | 0 | 110 | 56 | 3 | 37 | 1 | 97 | 0 | 1 | 26 | 0 | 27 | 109 | 2952 | 7 | 42 | 3110 | 3344 |
| Apprch % | 0 | 0.9 | 99.1 | 0 | | 57.7 | 3.1 | 38.1 | 1 | | 0 | 3.7 | 96.3 | 0 | | 3.5 | 94.9 | 0.2 | 1.4 | | |
| Total % | 0 | 0 | 3.3 | 0 | 3.3 | 1.7 | 0.1 | 1.1 | 0 | 2.9 | 0 | 0 | 0.8 | 0 | 0.8 | 3.3 | 88.3 | 0.2 | 1.3 | 93 | |

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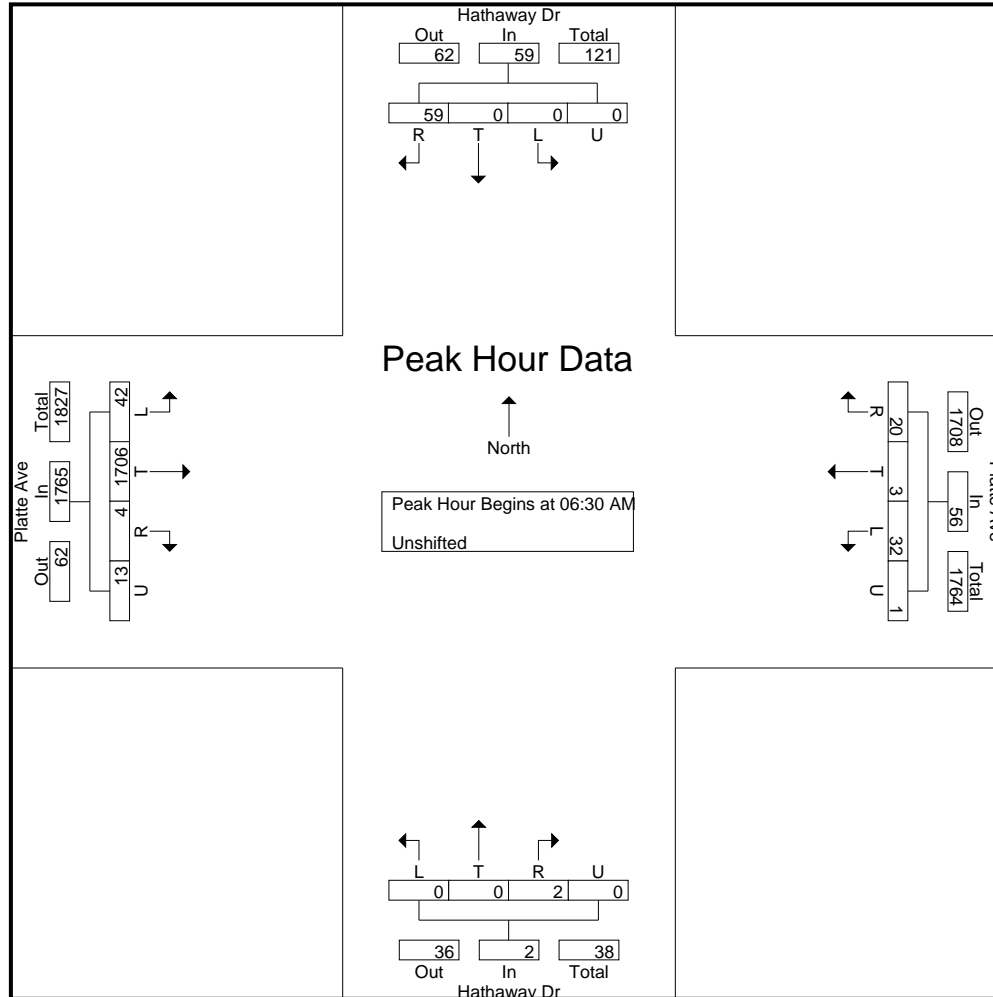
File Name : Hathaway Dr - Platte Ave AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 2

| Start Time | Hathaway Dr Southbound | | | | | Platte Ave Westbound | | | | | Hathaway Dr Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|---|------------------------|------|------|------|------------|----------------------|------|------|------|------------|------------------------|------|------|------|------------|----------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 6:30:00 AM | | | | | | | | | | | | | | | | | | | | | |
| 6:30:00 AM | 0 | 0 | 16 | 0 | 16 | 6 | 3 | 5 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 10 | 398 | 0 | 2 | 410 | 440 |
| 6:45:00 AM | 0 | 0 | 15 | 0 | 15 | 11 | 0 | 6 | 0 | 17 | 0 | 0 | 0 | 0 | 0 | 15 | 418 | 0 | 2 | 435 | 467 |
| 7:00:00 AM | 0 | 0 | 18 | 0 | 18 | 5 | 0 | 3 | 1 | 9 | 0 | 0 | 0 | 0 | 0 | 9 | 471 | 2 | 2 | 484 | 511 |
| 7:15:00 AM | 0 | 0 | 10 | 0 | 10 | 10 | 0 | 6 | 0 | 16 | 0 | 0 | 2 | 0 | 2 | 8 | 419 | 2 | 7 | 436 | 464 |
| Total Volume | 0 | 0 | 59 | 0 | 59 | 32 | 3 | 20 | 1 | 56 | 0 | 0 | 2 | 0 | 2 | 42 | 1706 | 4 | 13 | 1765 | 1882 |
| % App. Total | 0 | 0 | 100 | 0 | | 57.1 | 5.4 | 35.7 | 1.8 | | 0 | 0 | 100 | 0 | | 2.4 | 96.7 | 0.2 | 0.7 | | |
| PHF | .000 | .000 | .819 | .000 | .819 | .727 | .250 | .833 | .250 | .824 | .000 | .000 | .250 | .000 | .250 | .700 | .906 | .500 | .464 | .912 | .921 |

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File Name : Hathaway Dr - Platte Ave AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 3



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File Name : Hathaway Dr - Platte Ave AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 4

| Start Time | Hathaway Dr Southbound | | | | | Platte Ave Westbound | | | | | Hathaway Dr Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|------------|------------------------|---|---|---|------------|----------------------|---|---|---|------------|------------------------|---|---|---|------------|----------------------|---|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |

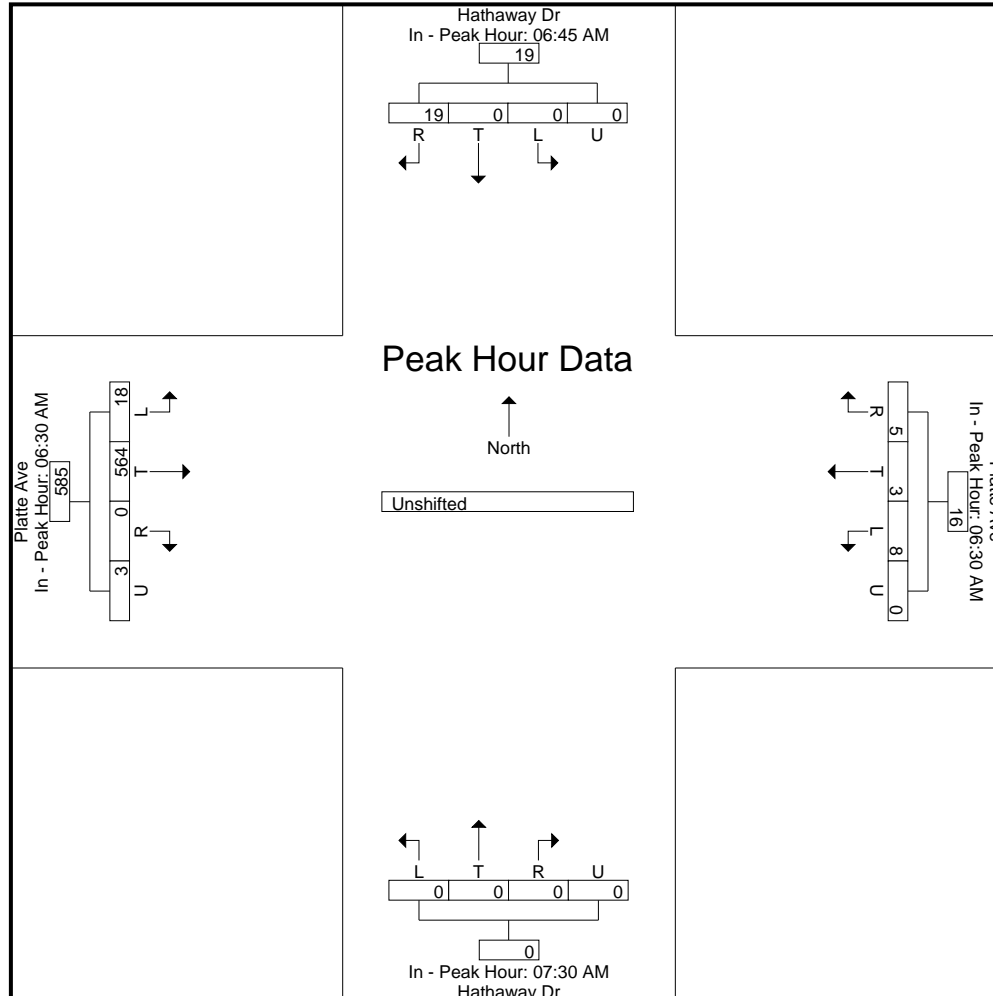
Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

| | 6:45:00 AM | | | | | 6:30:00 AM | | | | | 7:30:00 AM | | | | | 6:30:00 AM | | | | |
|--------------|------------|------|------|------|------|------------|------|------|------|------|------------|------|------|------|------|------------|------|------|------|------|
| +0 mins. | 0 | 0 | 15 | 0 | 15 | 6 | 3 | 5 | 0 | 14 | 0 | 0 | 4 | 0 | 4 | 10 | 398 | 0 | 2 | 410 |
| +5 mins. | 0 | 0 | 18 | 0 | 18 | 11 | 0 | 6 | 0 | 17 | 0 | 1 | 4 | 0 | 5 | 15 | 418 | 0 | 2 | 435 |
| +10 mins. | 0 | 0 | 10 | 0 | 10 | 5 | 0 | 3 | 1 | 9 | 0 | 0 | 12 | 0 | 12 | 9 | 471 | 2 | 2 | 484 |
| +15 mins. | 0 | 0 | 17 | 0 | 17 | 10 | 0 | 6 | 0 | 16 | 0 | 0 | 4 | 0 | 4 | 8 | 419 | 2 | 7 | 436 |
| Total Volume | 0 | 0 | 60 | 0 | 60 | 32 | 3 | 20 | 1 | 56 | 0 | 1 | 24 | 0 | 25 | 42 | 1706 | 4 | 13 | 1765 |
| % App. Total | 0 | 0 | 100 | 0 | | 57.1 | 5.4 | 35.7 | 1.8 | | 0 | 4 | 96 | 0 | | 2.4 | 96.7 | 0.2 | 0.7 | |
| PHF | .000 | .000 | .833 | .000 | .833 | .727 | .250 | .833 | .250 | .824 | .000 | .250 | .500 | .000 | .521 | .700 | .906 | .500 | .464 | .912 |

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File Name : Hathaway Dr - Platte Ave AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 5



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File Name : Hathaway Dr - Platte Ave PM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 1

Groups Printed- Unshifted

| Start Time | HathawayDr Southbound | | | | | Platte Ave Westbound | | | | | Hathaway Dr Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|-------------|-----------------------|---|------|-----|------------|----------------------|-----|------|-----|------------|------------------------|---|-----|---|------------|----------------------|------|-----|-----|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 04:00 PM | 0 | 0 | 21 | 0 | 21 | 7 | 1 | 5 | 1 | 14 | 0 | 0 | 14 | 0 | 14 | 18 | 418 | 3 | 3 | 442 | 491 |
| 04:15 PM | 0 | 0 | 13 | 0 | 13 | 9 | 0 | 4 | 1 | 14 | 0 | 0 | 17 | 0 | 17 | 23 | 478 | 0 | 5 | 506 | 550 |
| 04:30 PM | 0 | 0 | 21 | 0 | 21 | 8 | 0 | 7 | 1 | 16 | 0 | 0 | 7 | 0 | 7 | 14 | 520 | 1 | 4 | 539 | 583 |
| 04:45 PM | 0 | 0 | 24 | 1 | 25 | 11 | 1 | 11 | 1 | 24 | 0 | 0 | 13 | 0 | 13 | 14 | 550 | 0 | 4 | 568 | 630 |
| Total | 0 | 0 | 79 | 1 | 80 | 35 | 2 | 27 | 4 | 68 | 0 | 0 | 51 | 0 | 51 | 69 | 1966 | 4 | 16 | 2055 | 2254 |
| 05:00 PM | 0 | 0 | 20 | 0 | 20 | 4 | 0 | 4 | 2 | 10 | 0 | 0 | 9 | 0 | 9 | 24 | 495 | 1 | 18 | 538 | 577 |
| 05:15 PM | 0 | 0 | 15 | 0 | 15 | 3 | 0 | 2 | 0 | 5 | 0 | 0 | 8 | 0 | 8 | 16 | 471 | 1 | 9 | 497 | 525 |
| 05:30 PM | 0 | 0 | 8 | 0 | 8 | 2 | 0 | 7 | 0 | 9 | 0 | 0 | 5 | 0 | 5 | 20 | 463 | 0 | 4 | 487 | 509 |
| 05:45 PM | 0 | 0 | 6 | 0 | 6 | 3 | 0 | 1 | 0 | 4 | 0 | 0 | 4 | 0 | 4 | 15 | 430 | 0 | 8 | 453 | 467 |
| Total | 0 | 0 | 49 | 0 | 49 | 12 | 0 | 14 | 2 | 28 | 0 | 0 | 26 | 0 | 26 | 75 | 1859 | 2 | 39 | 1975 | 2078 |
| Grand Total | 0 | 0 | 128 | 1 | 129 | 47 | 2 | 41 | 6 | 96 | 0 | 0 | 77 | 0 | 77 | 144 | 3825 | 6 | 55 | 4030 | 4332 |
| Apprch % | 0 | 0 | 99.2 | 0.8 | | 49 | 2.1 | 42.7 | 6.2 | | 0 | 0 | 100 | 0 | | 3.6 | 94.9 | 0.1 | 1.4 | | |
| Total % | 0 | 0 | 3 | 0 | 3 | 1.1 | 0 | 0.9 | 0.1 | 2.2 | 0 | 0 | 1.8 | 0 | 1.8 | 3.3 | 88.3 | 0.1 | 1.3 | 93 | |

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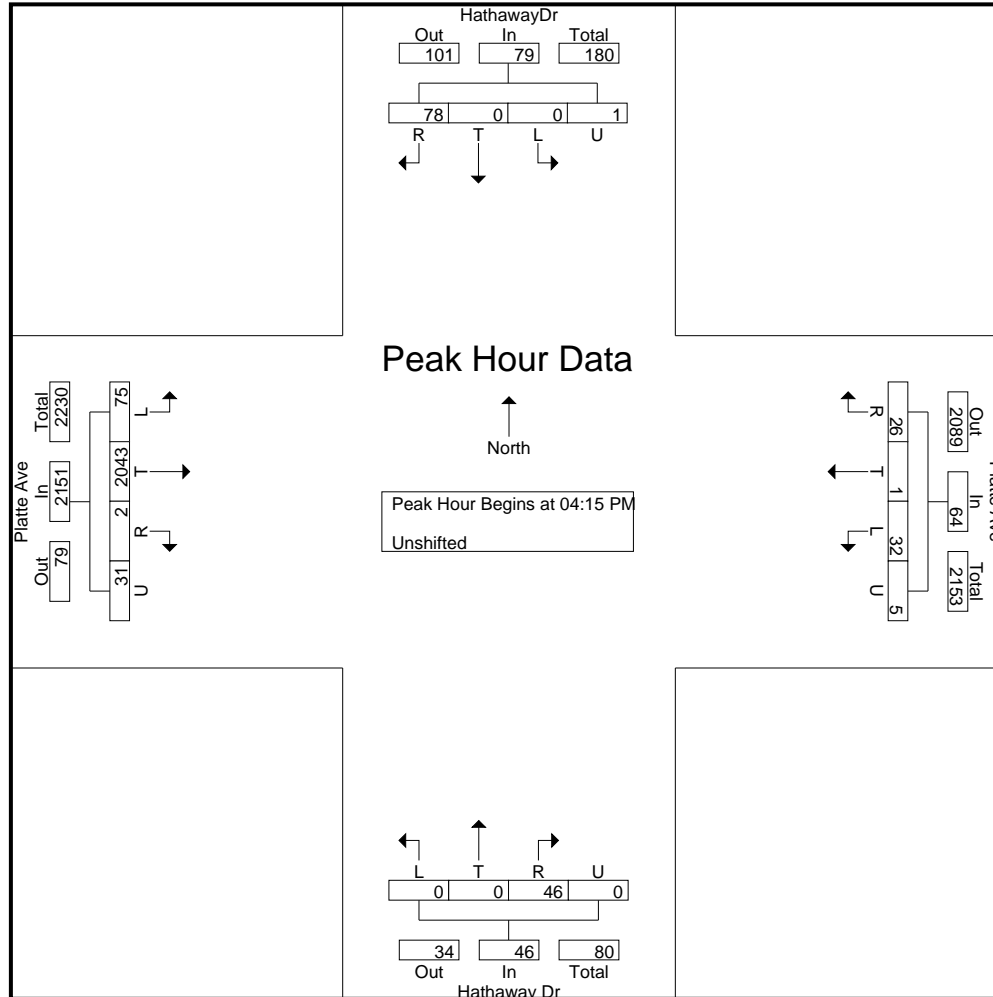
File Name : Hathaway Dr - Platte Ave PM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 2

| Start Time | HathawayDr Southbound | | | | | Platte Ave Westbound | | | | | Hathaway Dr Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|---|-----------------------|------|------|------|------------|----------------------|------|------|------|------------|------------------------|------|------|------|------------|----------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 4:15:00 PM | | | | | | | | | | | | | | | | | | | | | |
| 4:15:00 PM | 0 | 0 | 13 | 0 | 13 | 9 | 0 | 4 | 1 | 14 | 0 | 0 | 17 | 0 | 17 | 23 | 478 | 0 | 5 | 506 | 550 |
| 4:30:00 PM | 0 | 0 | 21 | 0 | 21 | 8 | 0 | 7 | 1 | 16 | 0 | 0 | 7 | 0 | 7 | 14 | 520 | 1 | 4 | 539 | 583 |
| 4:45:00 PM | 0 | 0 | 24 | 1 | 25 | 11 | 1 | 11 | 1 | 24 | 0 | 0 | 13 | 0 | 13 | 14 | 550 | 0 | 4 | 568 | 630 |
| 5:00:00 PM | 0 | 0 | 20 | 0 | 20 | 4 | 0 | 4 | 2 | 10 | 0 | 0 | 9 | 0 | 9 | 24 | 495 | 1 | 18 | 538 | 577 |
| Total Volume | 0 | 0 | 78 | 1 | 79 | 32 | 1 | 26 | 5 | 64 | 0 | 0 | 46 | 0 | 46 | 75 | 2043 | 2 | 31 | 2151 | 2340 |
| % App. Total | 0 | 0 | 98.7 | 1.3 | | 50 | 1.6 | 40.6 | 7.8 | | 0 | 0 | 100 | 0 | | 3.5 | 95 | 0.1 | 1.4 | | |
| PHF | .000 | .000 | .813 | .250 | .790 | .727 | .250 | .591 | .625 | .667 | .000 | .000 | .676 | .000 | .676 | .781 | .929 | .500 | .431 | .947 | .929 |

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File Name : Hathaway Dr - Platte Ave PM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 3



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File Name : Hathaway Dr - Platte Ave PM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 4

| Start Time | HathawayDr Southbound | | | | | Platte Ave Westbound | | | | | Hathaway Dr Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|------------|-----------------------|---|---|---|------------|----------------------|---|---|---|------------|------------------------|---|---|---|------------|----------------------|---|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |

Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1

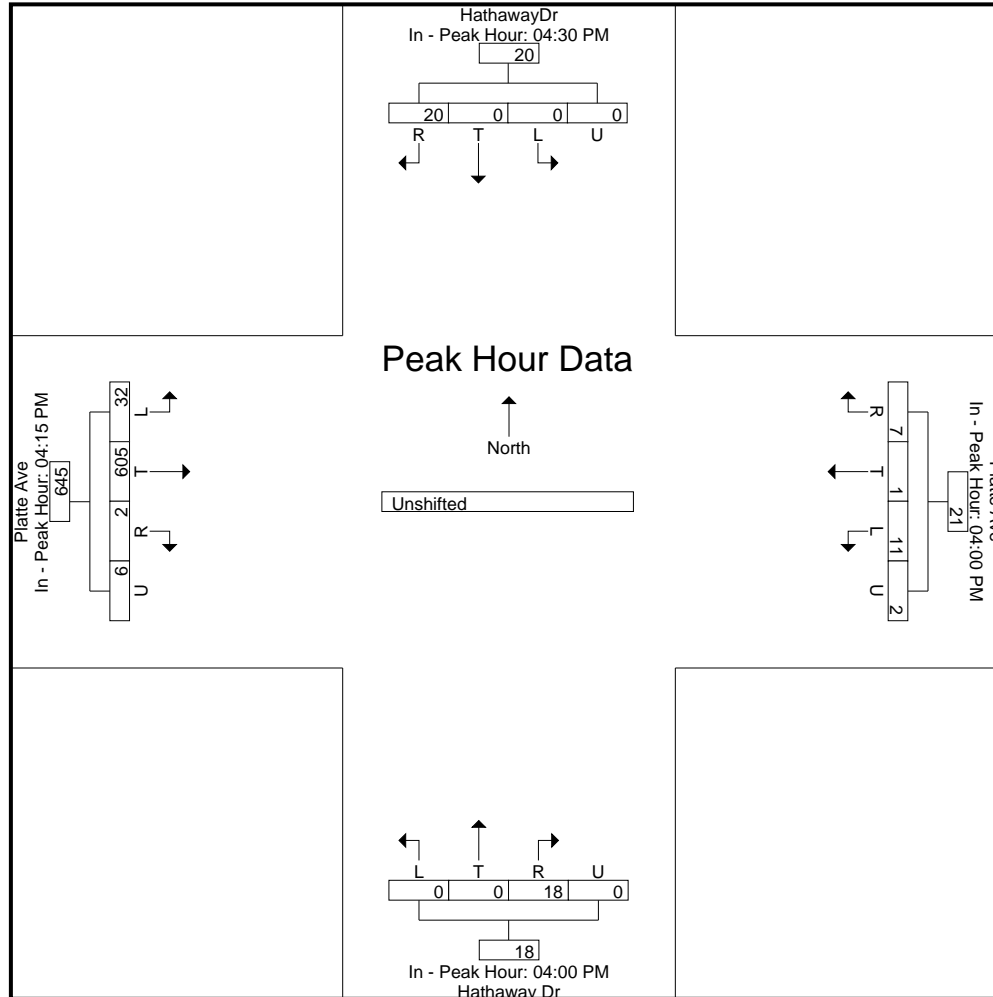
Peak Hour for Each Approach Begins at:

| | 4:30:00 PM | | | | | 4:00:00 PM | | | | | 4:00:00 PM | | | | | 4:15:00 PM | | | | |
|--------------|------------|------|-----------|----------|-----------|------------|----------|-----------|----------|-----------|------------|------|-----------|------|-----------|------------|------------|----------|-----------|------------|
| +0 mins. | 0 | 0 | 21 | 0 | 21 | 7 | 1 | 5 | 1 | 14 | 0 | 0 | 14 | 0 | 14 | 23 | 478 | 0 | 5 | 506 |
| +5 mins. | 0 | 0 | 24 | 1 | 25 | 9 | 0 | 4 | 1 | 14 | 0 | 0 | 17 | 0 | 17 | 14 | 520 | 1 | 4 | 539 |
| +10 mins. | 0 | 0 | 20 | 0 | 20 | 8 | 0 | 7 | 1 | 16 | 0 | 0 | 7 | 0 | 7 | 14 | 550 | 0 | 4 | 568 |
| +15 mins. | 0 | 0 | 15 | 0 | 15 | 11 | 1 | 11 | 1 | 24 | 0 | 0 | 13 | 0 | 13 | 24 | 495 | 1 | 18 | 538 |
| Total Volume | 0 | 0 | 80 | 1 | 81 | 35 | 2 | 27 | 4 | 68 | 0 | 0 | 51 | 0 | 51 | 75 | 2043 | 2 | 31 | 2151 |
| % App. Total | 0 | 0 | 98.8 | 1.2 | | 51.5 | 2.9 | 39.7 | 5.9 | | 0 | 0 | 100 | 0 | | 3.5 | 95 | 0.1 | 1.4 | |
| PHF | .000 | .000 | .833 | .250 | .810 | .795 | .500 | .614 | 1.000 | .708 | .000 | .000 | .750 | .000 | .750 | .781 | .929 | .500 | .431 | .947 |

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File Name : Hathaway Dr - Platte Ave PM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 5



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File Name : Hathaway Dr - Platte Frontage Rd AM1
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 1

Groups Printed- Bank 1

| Start Time | Hathaway Dr Southbound | | | | | East Frontage Rd Westbound | | | | | Diesel Rapair Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|-------------|------------------------|------|------|---|------------|----------------------------|---|-----|---|------------|--------------------------|------|------|---|------------|----------------------------|---|------|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 06:30 AM | 0 | 1 | 4 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 |
| 06:45 AM | 0 | 0 | 9 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 10 |
| Total | 0 | 1 | 13 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 15 |
| 07:00 AM | 1 | 1 | 5 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 1 | 0 | 0 | 0 | 1 | 9 |
| 07:15 AM | 1 | 4 | 7 | 0 | 12 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 2 | 0 | 2 | 3 | 0 | 0 | 0 | 3 | 18 |
| 07:30 AM | 0 | 1 | 7 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 3 | 0 | 0 | 0 | 3 | 12 |
| 07:45 AM | 1 | 0 | 9 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 3 | 0 | 5 | 0 | 8 | 19 |
| Total | 3 | 6 | 28 | 0 | 37 | 0 | 0 | 1 | 0 | 1 | 0 | 2 | 3 | 0 | 5 | 10 | 0 | 5 | 0 | 15 | 58 |
| 08:00 AM | 0 | 0 | 6 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 10 | 0 | 0 | 0 | 10 | 17 |
| 08:15 AM | 0 | 1 | 2 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 4 | 7 |
| Grand Total | 3 | 8 | 49 | 0 | 60 | 0 | 0 | 1 | 0 | 1 | 0 | 3 | 4 | 0 | 7 | 24 | 0 | 5 | 0 | 29 | 97 |
| Apprch % | 5 | 13.3 | 81.7 | 0 | | 0 | 0 | 100 | 0 | | 0 | 42.9 | 57.1 | 0 | | 82.8 | 0 | 17.2 | 0 | | |
| Total % | 3.1 | 8.2 | 50.5 | 0 | 61.9 | 0 | 0 | 1 | 0 | 1 | 0 | 3.1 | 4.1 | 0 | 7.2 | 24.7 | 0 | 5.2 | 0 | 29.9 | |

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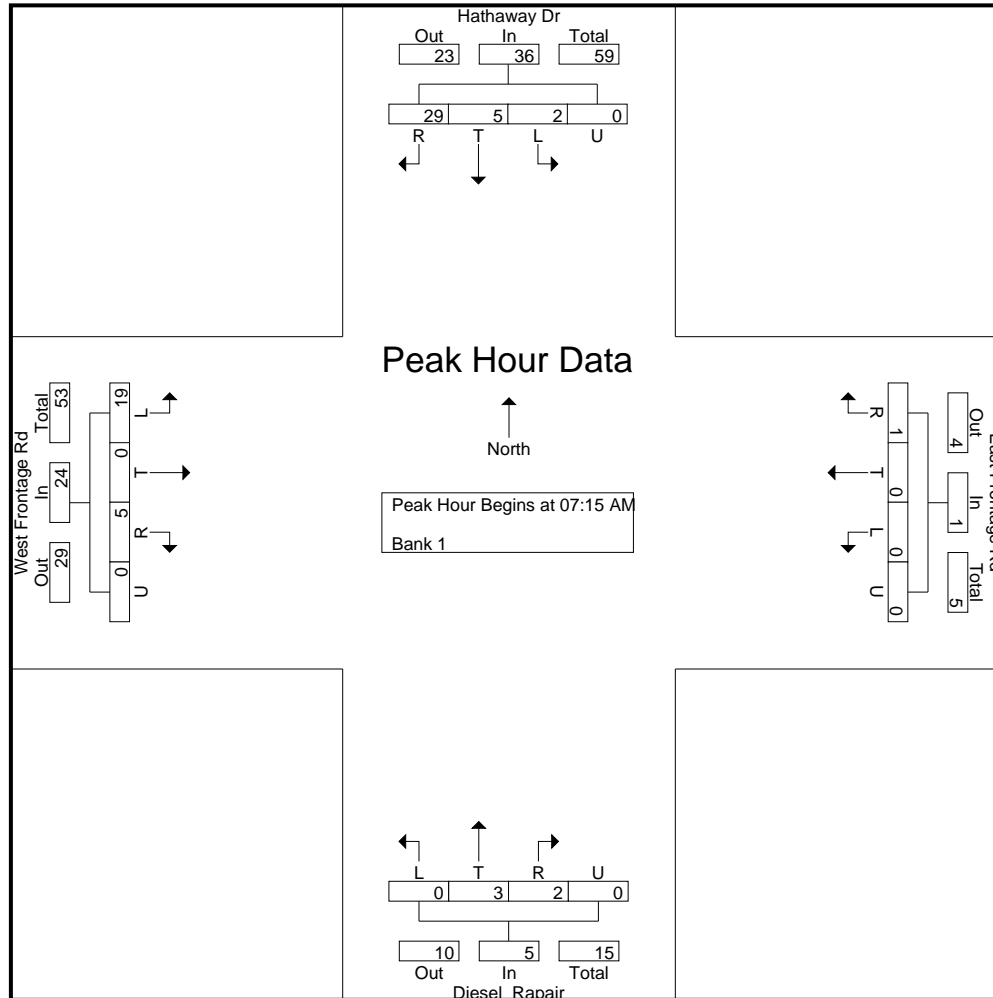
File Name : Hathaway Dr - Platte Frontage Rd AM1
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 2

| Start Time | Hathaway Dr Southbound | | | | | East Frontage Rd Westbound | | | | | Diesel Rapair Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|---|------------------------|------|------|------|------------|----------------------------|------|------|------|------------|--------------------------|------|------|------|------------|----------------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 7:15:00 AM | | | | | | | | | | | | | | | | | | | | | |
| 7:15:00 AM | 1 | 4 | 7 | 0 | 12 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 2 | 0 | 2 | 3 | 0 | 0 | 0 | 3 | 18 |
| 7:30:00 AM | 0 | 1 | 7 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 0 | 0 | 3 | 12 | |
| 7:45:00 AM | 1 | 0 | 9 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 5 | 0 | 8 | 19 | |
| 8:00:00 AM | 0 | 0 | 6 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 10 | 0 | 0 | 0 | 10 | 17 | |
| Total Volume | 2 | 5 | 29 | 0 | 36 | 0 | 0 | 1 | 0 | 1 | 0 | 3 | 2 | 5 | 19 | 0 | 5 | 0 | 24 | 66 | |
| % App. Total | 5.6 | 13.9 | 80.6 | 0 | | 0 | 0 | 100 | 0 | | 0 | 60 | 40 | 0 | 79.2 | 0 | 20.8 | 0 | | | |
| PHF | .500 | .313 | .806 | .000 | .750 | .000 | .000 | .250 | .000 | .250 | .000 | .750 | .250 | .000 | .625 | .475 | .000 | .250 | .000 | .600 | .868 |

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File Name : Hathaway Dr - Platte Frontage Rd AM1
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 3



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File Name : Hathaway Dr - Platte Frontage Rd AM1
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 4

| Start Time | Hathaway Dr Southbound | | | | | East Frontage Rd Westbound | | | | | Diesel Rapair Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|------------|------------------------|---|---|---|------------|----------------------------|---|---|---|------------|--------------------------|---|---|---|------------|----------------------------|---|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |

Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

| | 7:00:00 AM | | | | | 6:30:00 AM | | | | | 6:45:00 AM | | | | | 7:30:00 AM | | | | |
|--------------|------------|----------|----------|------|-----------|------------|------|----------|------|----------|------------|----------|----------|------|----------|------------|------|----------|------|-----------|
| +0 mins. | 1 | 1 | 5 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 0 | 0 | 3 |
| +5 mins. | 1 | 4 | 7 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 5 | 0 | 8 |
| +10 mins. | 0 | 1 | 7 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 10 | 0 | 0 | 0 | 10 |
| +15 mins. | 1 | 0 | 9 | 0 | 10 | 0 | 0 | 1 | 0 | 1 | 0 | 1 | 0 | 0 | 1 | 4 | 0 | 0 | 0 | 4 |
| Total Volume | 3 | 6 | 28 | 0 | 37 | 0 | 0 | 1 | 0 | 1 | 0 | 1 | 4 | 0 | 5 | 20 | 0 | 5 | 0 | 25 |
| % App. Total | 8.1 | 16.2 | 75.7 | 0 | | 0 | 0 | 100 | 0 | | 0 | 20 | 80 | 0 | | 80 | 0 | 20 | 0 | |
| PHF | .750 | .375 | .778 | .000 | .771 | .000 | .000 | .250 | .000 | .250 | .000 | .250 | .500 | .000 | .625 | .500 | .000 | .250 | .000 | .625 |

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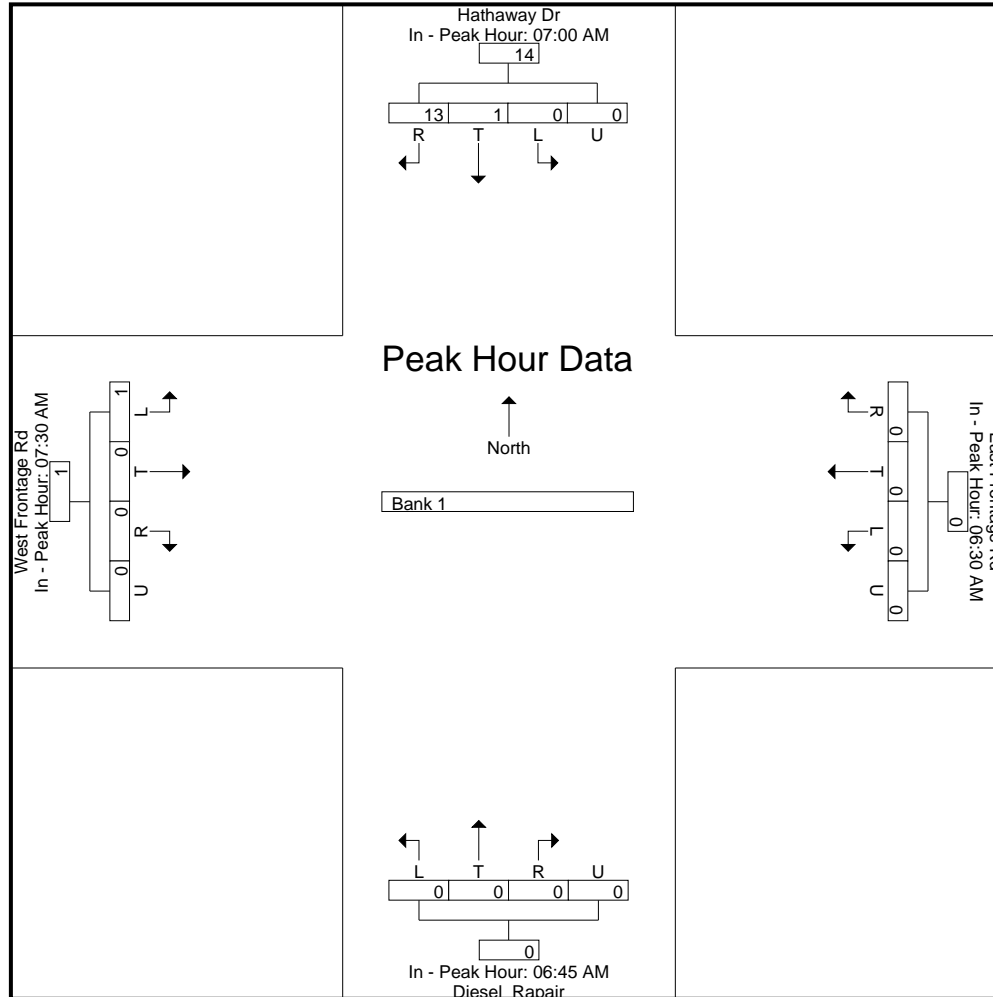
545 E Pikes Peak Ave, Suite 210
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File Name : Hathaway Dr - Platte Frontage Rd AM1

Site Code : S214660

Start Date : 7/13/2021

Page No : 5



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File Name : Hathaway Dr - Platte Frontage Rd AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 1

Groups Printed- Bank 1

| Start Time | HathawayDr Southbound | | | | | East Frontage Rd Westbound | | | | | Diesel Repair Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|-------------|-----------------------|-----|------|---|------------|----------------------------|------|------|---|------------|--------------------------|------|------|---|------------|----------------------------|-----|-----|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 04:00 PM | 2 | 1 | 7 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 14 | 0 | 0 | 0 | 14 | 24 |
| 04:15 PM | 0 | 0 | 9 | 0 | 9 | 0 | 0 | 2 | 0 | 2 | 0 | 3 | 0 | 0 | 3 | 11 | 0 | 0 | 0 | 11 | 25 |
| 04:30 PM | 1 | 0 | 6 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 10 | 1 | 0 | 0 | 11 | 19 |
| 04:45 PM | 1 | 0 | 11 | 0 | 12 | 0 | 1 | 1 | 0 | 2 | 0 | 1 | 1 | 0 | 2 | 10 | 0 | 1 | 0 | 11 | 27 |
| Total | 4 | 1 | 33 | 0 | 38 | 0 | 1 | 3 | 0 | 4 | 0 | 4 | 2 | 0 | 6 | 45 | 1 | 1 | 0 | 47 | 95 |
| 05:00 PM | 0 | 0 | 3 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 0 | 9 | 12 |
| 05:15 PM | 0 | 0 | 4 | 0 | 4 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 7 | 12 |
| 05:30 PM | 0 | 1 | 1 | 0 | 2 | 0 | 1 | 1 | 0 | 2 | 0 | 2 | 0 | 0 | 2 | 1 | 0 | 0 | 0 | 1 | 7 |
| 05:45 PM | 0 | 0 | 3 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 4 | 1 | 0 | 0 | 5 | 9 |
| Total | 0 | 1 | 11 | 0 | 12 | 0 | 1 | 2 | 0 | 3 | 0 | 3 | 0 | 0 | 3 | 21 | 1 | 0 | 0 | 22 | 40 |
| Grand Total | 4 | 2 | 44 | 0 | 50 | 0 | 2 | 5 | 0 | 7 | 0 | 7 | 2 | 0 | 9 | 66 | 2 | 1 | 0 | 69 | 135 |
| Apprch % | 8 | 4 | 88 | 0 | | 0 | 28.6 | 71.4 | 0 | | 0 | 77.8 | 22.2 | 0 | | 95.7 | 2.9 | 1.4 | 0 | | |
| Total % | 3 | 1.5 | 32.6 | 0 | 37 | 0 | 1.5 | 3.7 | 0 | 5.2 | 0 | 5.2 | 1.5 | 0 | 6.7 | 48.9 | 1.5 | 0.7 | 0 | 51.1 | |

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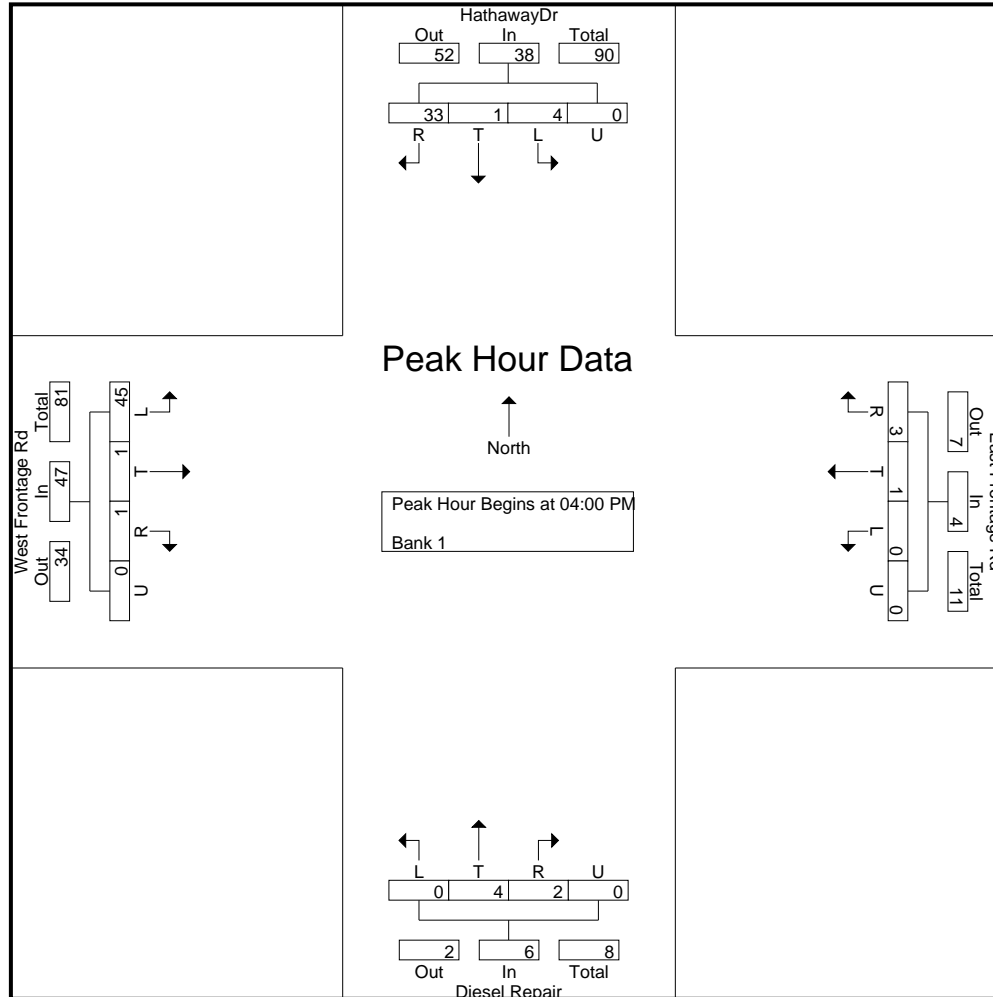
File Name : Hathaway Dr - Platte Frontage Rd AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 2

| Start Time | HathawayDr Southbound | | | | | East Frontage Rd Westbound | | | | | Diesel Repair Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|---|-----------------------|------|------|------|------------|----------------------------|------|------|------|------------|--------------------------|------|------|------|------------|----------------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 4:00:00 PM | | | | | | | | | | | | | | | | | | | | | |
| 4:00:00 PM | 2 | 1 | 7 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 14 | 0 | 0 | 0 | 14 | 24 |
| 4:15:00 PM | 0 | 0 | 9 | 0 | 9 | 0 | 0 | 2 | 0 | 2 | 0 | 3 | 0 | 0 | 3 | 11 | 0 | 0 | 0 | 11 | 25 |
| 4:30:00 PM | 1 | 0 | 6 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 10 | 1 | 0 | 0 | 11 | 19 |
| 4:45:00 PM | 1 | 0 | 11 | 0 | 12 | 0 | 1 | 1 | 0 | 2 | 0 | 1 | 1 | 0 | 2 | 10 | 0 | 1 | 0 | 11 | 27 |
| Total Volume | 4 | 1 | 33 | 0 | 38 | 0 | 1 | 3 | 0 | 4 | 0 | 4 | 2 | 0 | 6 | 45 | 1 | 1 | 0 | 47 | 95 |
| % App. Total | 10.5 | 2.6 | 86.8 | 0 | | 0 | 25 | 75 | 0 | | 0 | 66.7 | 33.3 | 0 | | 95.7 | 2.1 | 2.1 | 0 | | |
| PHF | .500 | .250 | .750 | .000 | .792 | .000 | .250 | .375 | .000 | .500 | .000 | .333 | .500 | .000 | .500 | .804 | .250 | .250 | .000 | .839 | .880 |

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File Name : Hathaway Dr - Platte Frontage Rd AM
 Site Code : S214660
 Start Date : 7/13/2021
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File Name : Hathaway Dr - Platte Frontage Rd AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 4

| Start Time | HathawayDr Southbound | | | | | East Frontage Rd Westbound | | | | | Diesel Repair Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|------------|--------------------------|---|---|---|------------|-------------------------------|---|---|---|------------|-----------------------------|---|---|---|------------|-------------------------------|---|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |

Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1

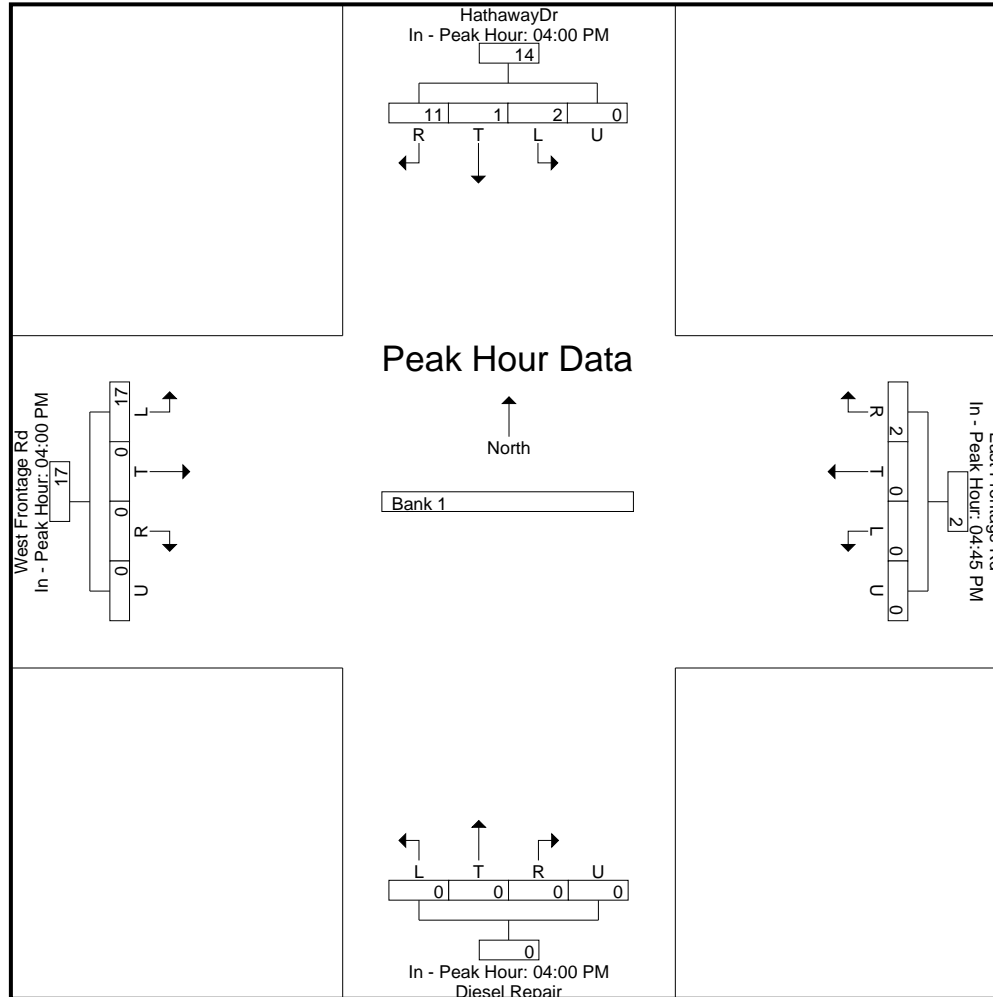
Peak Hour for Each Approach Begins at:

| | 4:00:00 PM | | | | | 4:45:00 PM | | | | | 4:00:00 PM | | | | | 4:00:00 PM | | | | |
|--------------|------------|----------|-----------|------|-----------|------------|----------|----------|------|------|------------|----------|----------|------|----------|------------|----------|----------|------|-----------|
| +0 mins. | 2 | 1 | 7 | 0 | 10 | 0 | 1 | 1 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 14 | 0 | 0 | 0 | 14 |
| +5 mins. | 0 | 0 | 9 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 11 | 0 | 0 | 0 | 11 |
| +10 mins. | 1 | 0 | 6 | 0 | 7 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 1 | 0 | 1 | 10 | 1 | 0 | 0 | 11 |
| +15 mins. | 1 | 0 | 11 | 0 | 12 | 0 | 1 | 1 | 0 | 2 | 0 | 1 | 1 | 0 | 2 | 10 | 0 | 1 | 0 | 11 |
| Total Volume | 4 | 1 | 33 | 0 | 38 | 0 | 2 | 3 | 0 | 5 | 0 | 4 | 2 | 0 | 6 | 45 | 1 | 1 | 0 | 47 |
| % App. Total | 10.5 | 2.6 | 86.8 | 0 | | 0 | 40 | 60 | 0 | | 0 | 66.7 | 33.3 | 0 | | 95.7 | 2.1 | 2.1 | 0 | |
| PHF | .500 | .250 | .750 | .000 | .792 | .000 | .500 | .750 | .000 | .625 | .000 | .333 | .500 | .000 | .500 | .804 | .250 | .250 | .000 | .839 |

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File Name : Hathaway Dr - Platte Frontage Rd AM
 Site Code : S214660
 Start Date : 7/13/2021
 Page No : 5



LSC Transportation Consultants, Inc.

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File Name : Motel Rd - Platte Ave AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 1

Groups Printed- Unshifted

| Start Time | Southbound | | | | | Platte Ave Westbound | | | | | Motel Rd Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|-------------|------------|---|---|---|------------|----------------------|------|---|---|------------|---------------------|---|-----|---|------------|----------------------|---|-----|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 06:30 AM | 0 | 0 | 0 | 0 | 0 | 0 | 531 | 0 | 0 | 531 | 0 | 0 | 6 | 0 | 6 | 0 | 0 | 10 | 0 | 10 | 547 |
| 06:45 AM | 1 | 0 | 0 | 0 | 1 | 0 | 559 | 0 | 0 | 559 | 0 | 0 | 10 | 0 | 10 | 0 | 0 | 16 | 0 | 16 | 586 |
| Total | 1 | 0 | 0 | 0 | 1 | 0 | 1090 | 0 | 0 | 1090 | 0 | 0 | 16 | 0 | 16 | 0 | 0 | 26 | 0 | 26 | 1133 |
| 07:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 537 | 0 | 0 | 537 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 20 | 0 | 20 | 565 |
| 07:15 AM | 0 | 0 | 0 | 0 | 0 | 0 | 598 | 0 | 0 | 598 | 0 | 0 | 11 | 0 | 11 | 0 | 0 | 15 | 0 | 15 | 624 |
| 07:30 AM | 0 | 0 | 0 | 0 | 0 | 0 | 674 | 0 | 0 | 674 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 18 | 0 | 18 | 700 |
| 07:45 AM | 0 | 0 | 0 | 0 | 0 | 0 | 565 | 0 | 0 | 565 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | 12 | 0 | 12 | 584 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 2374 | 0 | 0 | 2374 | 0 | 0 | 34 | 0 | 34 | 0 | 0 | 65 | 0 | 65 | 2473 |
| 08:00 AM | 1 | 0 | 0 | 0 | 1 | 0 | 471 | 0 | 0 | 471 | 0 | 0 | 21 | 0 | 21 | 0 | 0 | 7 | 0 | 7 | 500 |
| 08:15 AM | 0 | 0 | 0 | 0 | 0 | 0 | 476 | 0 | 0 | 476 | 0 | 0 | 11 | 0 | 11 | 0 | 0 | 12 | 0 | 12 | 499 |
| Grand Total | 2 | 0 | 0 | 0 | 2 | 0 | 4411 | 0 | 0 | 4411 | 0 | 0 | 82 | 0 | 82 | 0 | 0 | 110 | 0 | 110 | 4605 |
| Apprch % | 100 | 0 | 0 | 0 | | 0 | 100 | 0 | 0 | | 0 | 0 | 100 | 0 | | 0 | 0 | 100 | 0 | | |
| Total % | 0 | 0 | 0 | 0 | | 0 | 95.8 | 0 | 0 | | 0 | 0 | 1.8 | 0 | | 0 | 0 | 2.4 | 0 | | |

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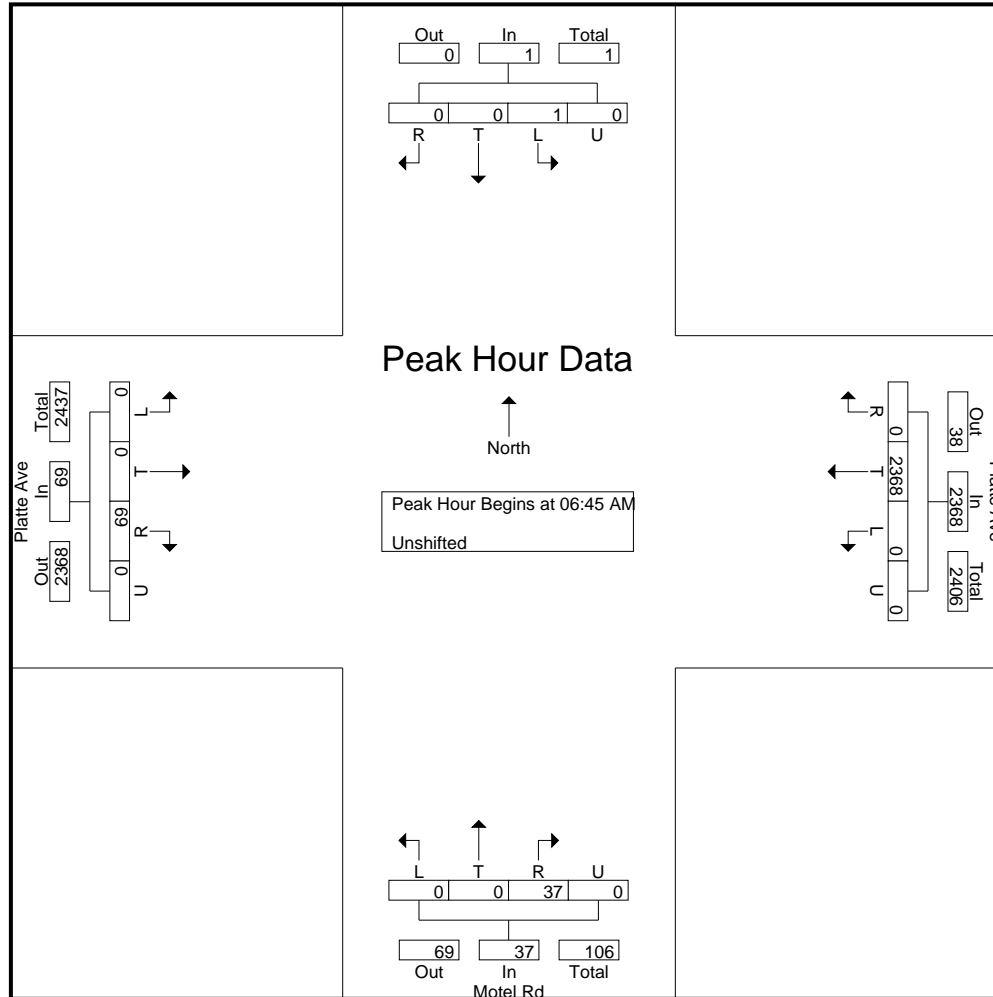
File Name : Motel Rd - Platte Ave AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 2

| Start Time | Southbound | | | | | Platte Ave Westbound | | | | | Motel Rd Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|---|------------|------|------|------|------------|----------------------|------|------|------|------------|---------------------|------|------|------|------------|----------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 6:45:00 AM | | | | | | | | | | | | | | | | | | | | | |
| 6:45:00 AM | 1 | 0 | 0 | 0 | 1 | 0 | 559 | 0 | 0 | 559 | 0 | 0 | 10 | 0 | 10 | 0 | 0 | 16 | 0 | 16 | 586 |
| 7:00:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 537 | 0 | 0 | 537 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 20 | 0 | 20 | 565 |
| 7:15:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 598 | 0 | 0 | 598 | 0 | 0 | 11 | 0 | 11 | 0 | 0 | 15 | 0 | 15 | 624 |
| 7:30:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 674 | 0 | 0 | 674 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 18 | 0 | 18 | 700 |
| Total Volume | 1 | 0 | 0 | 0 | 1 | 0 | 2368 | 0 | 0 | 2368 | 0 | 0 | 37 | 0 | 37 | 0 | 0 | 69 | 0 | 69 | 2475 |
| % App. Total | 100 | 0 | 0 | 0 | | 0 | 100 | 0 | 0 | | 0 | 0 | 100 | 0 | | 0 | 0 | 100 | 0 | | |
| PHF | .250 | .000 | .000 | .000 | .250 | .000 | .878 | .000 | .000 | .878 | .000 | .000 | .841 | .000 | .841 | .000 | .000 | .863 | .000 | .863 | .884 |

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File Name : Motel Rd - Platte Ave AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 3



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File Name : Motel Rd - Platte Ave AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 4

| Start Time | Southbound | | | | | Platte Ave Westbound | | | | | Motel Rd Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|------------|------------|---|---|---|------------|----------------------|---|---|---|------------|---------------------|---|---|---|------------|----------------------|---|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |

Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1

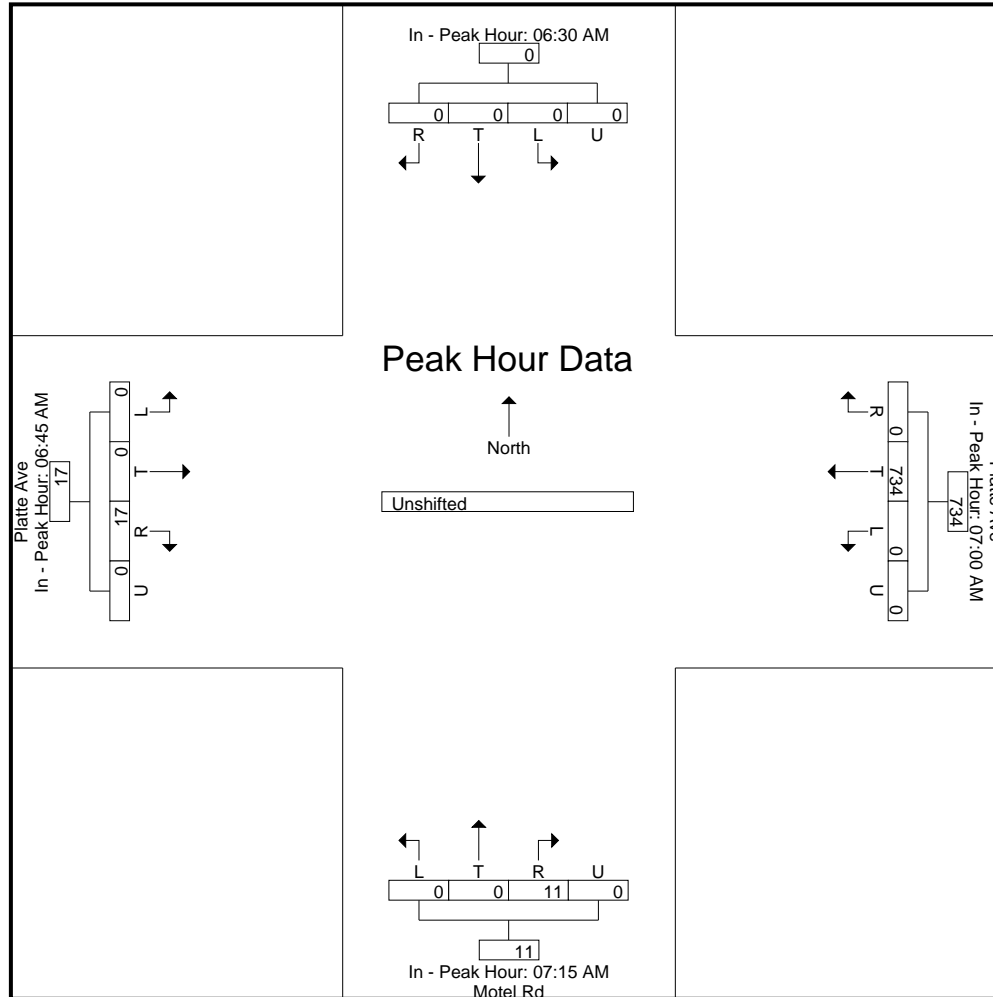
Peak Hour for Each Approach Begins at:

| | 6:30:00 AM | | | | | 7:00:00 AM | | | | | 7:15:00 AM | | | | | 6:45:00 AM | | | | |
|--------------|------------|------|------|------|------|------------|------|------|------|------|------------|------|------|------|------|------------|------|------|------|------|
| +0 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 537 | 0 | 0 | 537 | 0 | 0 | 11 | 0 | 11 | 0 | 0 | 16 | 0 | 16 |
| +5 mins. | 1 | 0 | 0 | 0 | 1 | 0 | 598 | 0 | 0 | 598 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 20 | 0 | 20 |
| +10 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 674 | 0 | 0 | 674 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | 15 | 0 | 15 |
| +15 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 565 | 0 | 0 | 565 | 0 | 0 | 21 | 0 | 21 | 0 | 0 | 18 | 0 | 18 |
| Total Volume | 1 | 0 | 0 | 0 | 1 | 0 | 2374 | 0 | 0 | 2374 | 0 | 0 | 47 | 0 | 47 | 0 | 0 | 69 | 0 | 69 |
| % App. Total | 100 | 0 | 0 | 0 | | 0 | 100 | 0 | 0 | | 0 | 0 | 100 | 0 | | 0 | 0 | 100 | 0 | |
| PHF | .250 | .000 | .000 | .000 | .250 | .000 | .881 | .000 | .000 | .881 | .000 | .000 | .560 | .000 | .560 | .000 | .000 | .863 | .000 | .863 |

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File Name : Motel Rd - Platte Ave AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 5



LSC Transportation Consultants, Inc.

545 E Pikes Peak Ave, Suite 210
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File Name : Motel Rd - Platte Ave PM1
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 1

Groups Printed- Unshifted

| Start Time | Southbound | | | | | Platte Ave Westbound | | | | | Motel Rd Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|-------------|------------|---|---|---|------------|----------------------|------|---|---|------------|---------------------|---|-----|---|------------|----------------------|---|-----|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 04:00 PM | 0 | 0 | 0 | 0 | 0 | 1 | 461 | 0 | 0 | 462 | 0 | 0 | 12 | 0 | 12 | 0 | 0 | 10 | 0 | 10 | 484 |
| 04:15 PM | 0 | 0 | 0 | 0 | 0 | 1 | 451 | 0 | 0 | 452 | 0 | 0 | 17 | 0 | 17 | 0 | 0 | 20 | 0 | 20 | 489 |
| 04:30 PM | 0 | 0 | 0 | 0 | 0 | 0 | 460 | 0 | 0 | 460 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 16 | 0 | 16 | 484 |
| 04:45 PM | 1 | 0 | 0 | 0 | 1 | 0 | 525 | 0 | 0 | 525 | 0 | 0 | 12 | 0 | 12 | 0 | 0 | 12 | 0 | 12 | 550 |
| Total | 1 | 0 | 0 | 0 | 1 | 2 | 1897 | 0 | 0 | 1899 | 0 | 0 | 49 | 0 | 49 | 0 | 0 | 58 | 0 | 58 | 2007 |
| 05:00 PM | 0 | 0 | 0 | 0 | 0 | 0 | 432 | 0 | 0 | 432 | 0 | 0 | 13 | 0 | 13 | 0 | 0 | 6 | 0 | 6 | 451 |
| 05:15 PM | 0 | 0 | 0 | 0 | 0 | 0 | 445 | 0 | 0 | 445 | 0 | 0 | 13 | 0 | 13 | 0 | 0 | 5 | 0 | 5 | 463 |
| 05:30 PM | 0 | 0 | 0 | 0 | 0 | 0 | 450 | 0 | 0 | 450 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 11 | 0 | 11 | 469 |
| 05:45 PM | 0 | 0 | 0 | 0 | 0 | 0 | 407 | 0 | 0 | 407 | 0 | 0 | 7 | 0 | 7 | 0 | 0 | 10 | 0 | 10 | 424 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 1734 | 0 | 0 | 1734 | 0 | 0 | 41 | 0 | 41 | 0 | 0 | 32 | 0 | 32 | 1807 |
| Grand Total | 1 | 0 | 0 | 0 | 1 | 2 | 3631 | 0 | 0 | 3633 | 0 | 0 | 90 | 0 | 90 | 0 | 0 | 90 | 0 | 90 | 3814 |
| Apprch % | 100 | 0 | 0 | 0 | | 0.1 | 99.9 | 0 | 0 | | 0 | 0 | 100 | 0 | | 0 | 0 | 100 | 0 | | |
| Total % | 0 | 0 | 0 | 0 | 0 | 0.1 | 95.2 | 0 | 0 | 95.3 | 0 | 0 | 2.4 | 0 | 2.4 | 0 | 0 | 2.4 | 0 | 2.4 | |

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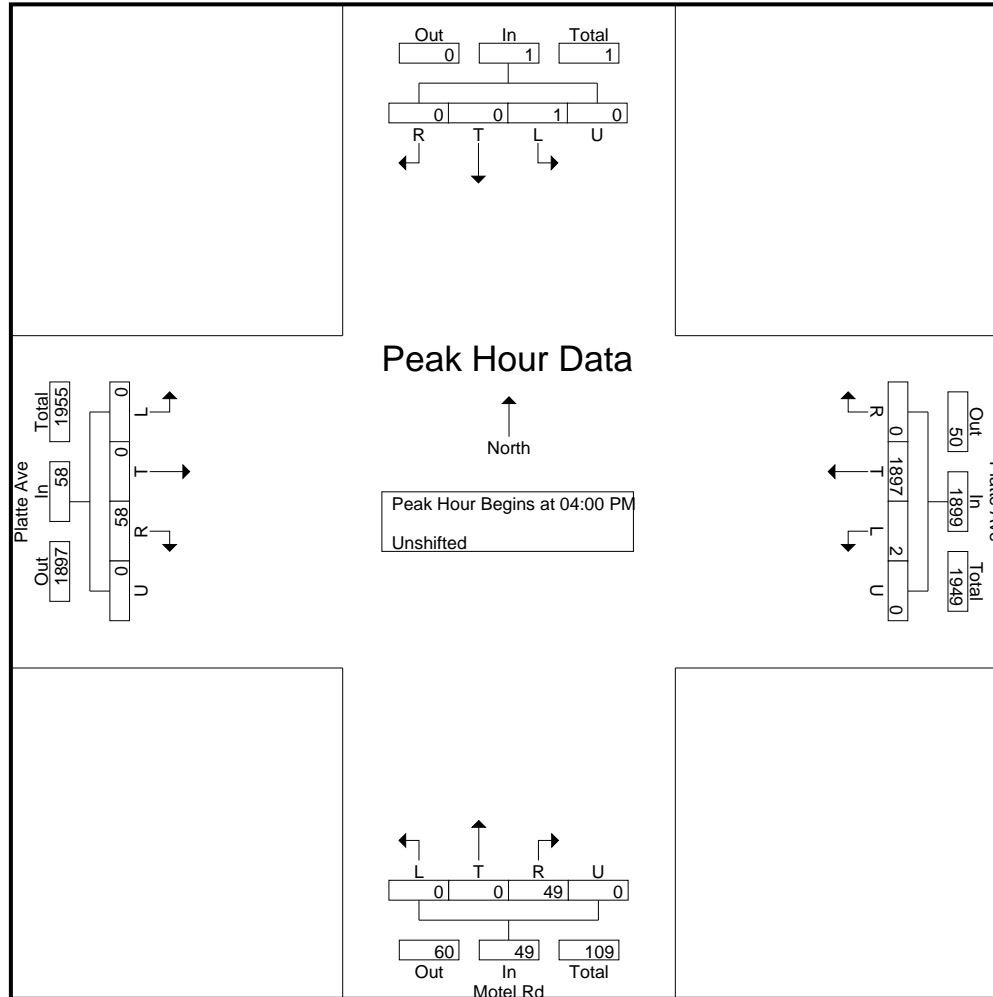
File Name : Motel Rd - Platte Ave PM1
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 2

| Start Time | Southbound | | | | | Platte Ave Westbound | | | | | Motel Rd Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|---|------------|------|------|------|------------|----------------------|------|------|------|------------|---------------------|------|------|------|------------|----------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 4:00:00 PM | | | | | | | | | | | | | | | | | | | | | |
| 4:00:00 PM | 0 | 0 | 0 | 0 | 0 | 1 | 461 | 0 | 0 | 462 | 0 | 0 | 12 | 0 | 12 | 0 | 0 | 10 | 0 | 10 | 484 |
| 4:15:00 PM | 0 | 0 | 0 | 0 | 0 | 1 | 451 | 0 | 0 | 452 | 0 | 0 | 17 | 0 | 17 | 0 | 0 | 20 | 0 | 20 | 489 |
| 4:30:00 PM | 0 | 0 | 0 | 0 | 0 | 0 | 460 | 0 | 0 | 460 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 16 | 0 | 16 | 484 |
| 4:45:00 PM | 1 | 0 | 0 | 0 | 1 | 0 | 525 | 0 | 0 | 525 | 0 | 0 | 12 | 0 | 12 | 0 | 0 | 12 | 0 | 12 | 550 |
| Total Volume | 1 | 0 | 0 | 0 | 1 | 2 | 1897 | 0 | 0 | 1899 | 0 | 0 | 49 | 0 | 49 | 0 | 0 | 58 | 0 | 58 | 2007 |
| % App. Total | 100 | 0 | 0 | 0 | | 0.1 | 99.9 | 0 | 0 | | 0 | 0 | 100 | 0 | | 0 | 0 | 100 | 0 | | |
| PHF | .250 | .000 | .000 | .000 | .250 | .500 | .903 | .000 | .000 | .904 | .000 | .000 | .721 | .000 | .721 | .000 | .000 | .725 | .000 | .725 | .912 |

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File Name : Motel Rd - Platte Ave PM1
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 3



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File Name : Motel Rd - Platte Ave PM1
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 4

| Start Time | Southbound | | | | | Platte Ave Westbound | | | | | Motel Rd Northbound | | | | | Platte Ave Eastbound | | | | | Int. Total |
|------------|------------|---|---|---|------------|----------------------|---|---|---|------------|---------------------|---|---|---|------------|----------------------|---|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |

Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1

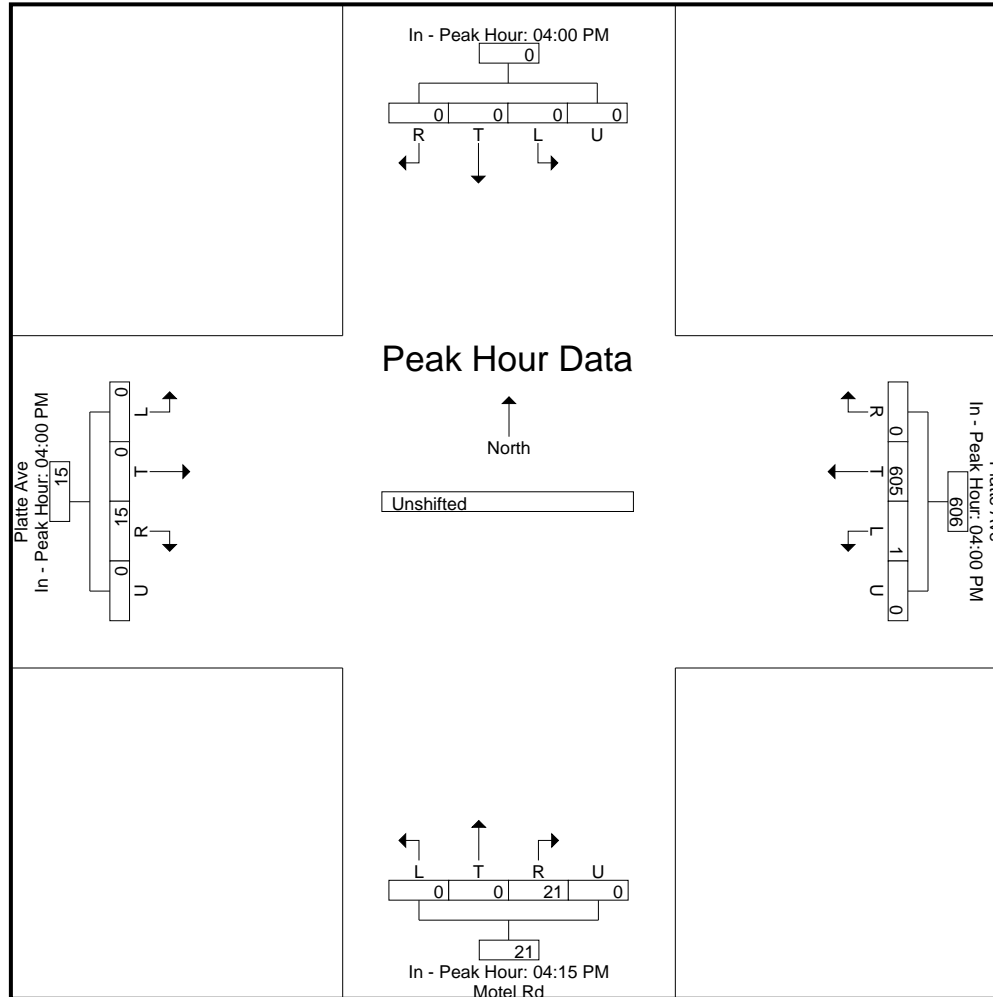
Peak Hour for Each Approach Begins at:

| | 4:00:00 PM | | | | | 4:00:00 PM | | | | | 4:15:00 PM | | | | | 4:00:00 PM | | | | |
|--------------|------------|------|------|------|----------|------------|------------|------|------|------------|------------|------|-----------|------|-----------|------------|------|-----------|------|-----------|
| +0 mins. | 0 | 0 | 0 | 0 | 0 | 1 | 461 | 0 | 0 | 462 | 0 | 0 | 17 | 0 | 17 | 0 | 0 | 10 | 0 | 10 |
| +5 mins. | 0 | 0 | 0 | 0 | 0 | 1 | 451 | 0 | 0 | 452 | 0 | 0 | 8 | 0 | 8 | 0 | 0 | 20 | 0 | 20 |
| +10 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 460 | 0 | 0 | 460 | 0 | 0 | 12 | 0 | 12 | 0 | 0 | 16 | 0 | 16 |
| +15 mins. | 1 | 0 | 0 | 0 | 1 | 0 | 525 | 0 | 0 | 525 | 0 | 0 | 13 | 0 | 13 | 0 | 0 | 12 | 0 | 12 |
| Total Volume | 1 | 0 | 0 | 0 | 1 | 2 | 1897 | 0 | 0 | 1899 | 0 | 0 | 50 | 0 | 50 | 0 | 0 | 58 | 0 | 58 |
| % App. Total | 100 | 0 | 0 | 0 | | 0.1 | 99.9 | 0 | 0 | | 0 | 0 | 100 | 0 | | 0 | 0 | 100 | 0 | |
| PHF | .250 | .000 | .000 | .000 | .250 | .500 | .903 | .000 | .000 | .904 | .000 | .000 | .735 | .000 | .735 | .000 | .000 | .725 | .000 | .725 |

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File Name : Motel Rd - Platte Ave PM1
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 5



LSC Transportation Consultants, Inc.

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File Name : Motel Rd - Platte Frontage Rd AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 1

Groups Printed- Bank 1

| Start Time | Motel Rd Southbound | | | | | East Frontage Rd Westbound | | | | | Motel Rd Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|-------------|---------------------|-----|------|---|------------|----------------------------|------|------|---|------------|---------------------|------|------|---|------------|----------------------------|-----|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 06:30 AM | 8 | 0 | 2 | 0 | 10 | 0 | 1 | 0 | 0 | 1 | 0 | 3 | 0 | 0 | 3 | 3 | 1 | 0 | 0 | 4 | 18 |
| 06:45 AM | 12 | 1 | 2 | 0 | 15 | 0 | 1 | 2 | 0 | 3 | 0 | 5 | 0 | 0 | 5 | 3 | 0 | 0 | 0 | 3 | 26 |
| Total | 20 | 1 | 4 | 0 | 25 | 0 | 2 | 2 | 0 | 4 | 0 | 8 | 0 | 0 | 8 | 6 | 1 | 0 | 0 | 7 | 44 |
| 07:00 AM | 13 | 1 | 3 | 0 | 17 | 0 | 0 | 1 | 0 | 1 | 0 | 5 | 4 | 0 | 9 | 2 | 0 | 0 | 0 | 2 | 29 |
| 07:15 AM | 13 | 2 | 0 | 0 | 15 | 0 | 1 | 1 | 0 | 2 | 0 | 4 | 0 | 0 | 4 | 5 | 0 | 0 | 0 | 5 | 26 |
| 07:30 AM | 11 | 0 | 7 | 0 | 18 | 0 | 1 | 1 | 0 | 2 | 0 | 6 | 1 | 0 | 7 | 1 | 0 | 0 | 0 | 1 | 28 |
| 07:45 AM | 10 | 0 | 2 | 0 | 12 | 1 | 1 | 5 | 0 | 7 | 0 | 0 | 1 | 0 | 1 | 2 | 0 | 0 | 0 | 2 | 22 |
| Total | 47 | 3 | 12 | 0 | 62 | 1 | 3 | 8 | 0 | 12 | 0 | 15 | 6 | 0 | 21 | 10 | 0 | 0 | 0 | 10 | 105 |
| 08:00 AM | 4 | 1 | 2 | 0 | 7 | 0 | 1 | 12 | 0 | 13 | 0 | 6 | 0 | 0 | 6 | 4 | 0 | 0 | 0 | 4 | 30 |
| 08:15 AM | 9 | 1 | 2 | 0 | 12 | 1 | 2 | 4 | 0 | 7 | 0 | 5 | 1 | 0 | 6 | 2 | 0 | 0 | 0 | 2 | 27 |
| Grand Total | 80 | 6 | 20 | 0 | 106 | 2 | 8 | 26 | 0 | 36 | 0 | 34 | 7 | 0 | 41 | 22 | 1 | 0 | 0 | 23 | 206 |
| Apprch % | 75.5 | 5.7 | 18.9 | 0 | | 5.6 | 22.2 | 72.2 | 0 | | 0 | 82.9 | 17.1 | 0 | | 95.7 | 4.3 | 0 | 0 | | |
| Total % | 38.8 | 2.9 | 9.7 | 0 | 51.5 | 1 | 3.9 | 12.6 | 0 | 17.5 | 0 | 16.5 | 3.4 | 0 | 19.9 | 10.7 | 0.5 | 0 | 0 | 11.2 | |

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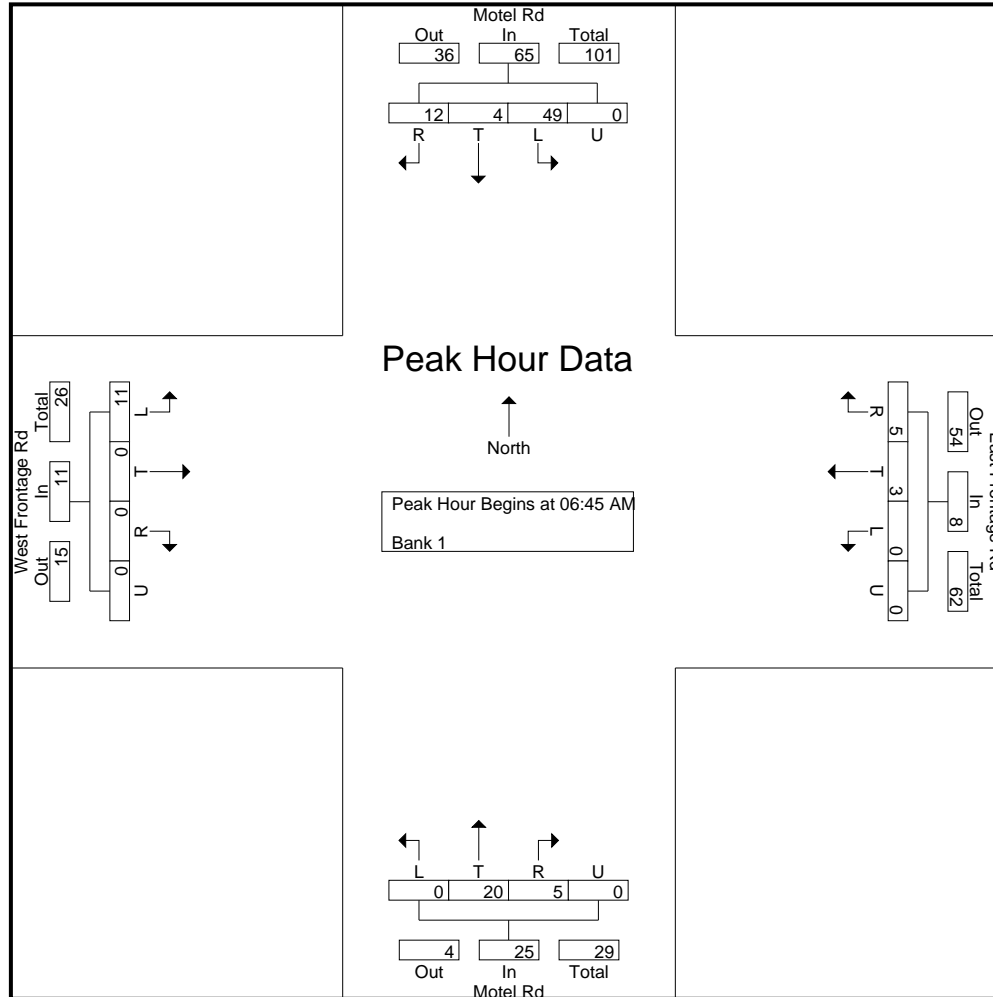
File Name : Motel Rd - Platte Frontage Rd AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 2

| Start Time | Motel Rd Southbound | | | | | East Frontage Rd Westbound | | | | | Motel Rd Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|---|---------------------|------|------|------|------------|----------------------------|------|------|------|------------|---------------------|------|------|------|------------|----------------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 6:45:00 AM | | | | | | | | | | | | | | | | | | | | | |
| 6:45:00 AM | 12 | 1 | 2 | 0 | 15 | 0 | 1 | 2 | 0 | 3 | 0 | 5 | 0 | 0 | 5 | 3 | 0 | 0 | 0 | 3 | 26 |
| 7:00:00 AM | 13 | 1 | 3 | 0 | 17 | 0 | 0 | 1 | 0 | 1 | 0 | 5 | 4 | 0 | 9 | 2 | 0 | 0 | 0 | 2 | 29 |
| 7:15:00 AM | 13 | 2 | 0 | 0 | 15 | 0 | 1 | 1 | 0 | 2 | 0 | 4 | 0 | 0 | 4 | 5 | 0 | 0 | 0 | 5 | 26 |
| 7:30:00 AM | 11 | 0 | 7 | 0 | 18 | 0 | 1 | 1 | 0 | 2 | 0 | 6 | 1 | 0 | 7 | 1 | 0 | 0 | 0 | 1 | 28 |
| Total Volume | 49 | 4 | 12 | 0 | 65 | 0 | 3 | 5 | 0 | 8 | 0 | 20 | 5 | 0 | 25 | 11 | 0 | 0 | 0 | 11 | 109 |
| % App. Total | 75.4 | 6.2 | 18.5 | 0 | | 0 | 37.5 | 62.5 | 0 | | 0 | 80 | 20 | 0 | | 100 | 0 | 0 | 0 | | |
| PHF | .942 | .500 | .429 | .000 | .903 | .000 | .750 | .625 | .000 | .667 | .000 | .833 | .313 | .000 | .694 | .550 | .000 | .000 | .000 | .550 | .940 |

LSC Transportation Consultants, Inc.

545 E Pikes Peak Ave, Suite 210
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File Name : Motel Rd - Platte Frontage Rd AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 3



LSC Transportation Consultants, Inc.

545 E Pikes Peak Ave, Suite 210
 Colorado Springs, CO 80905
 719-633-2868

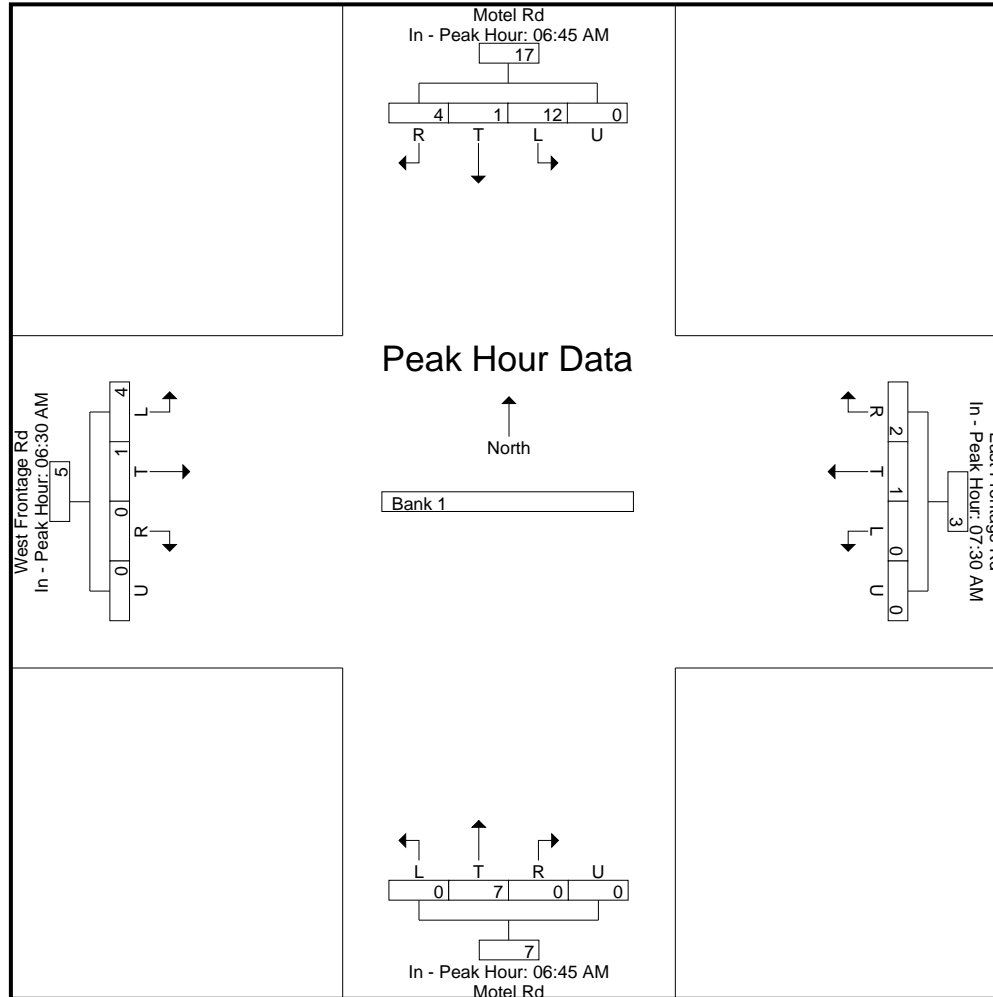
File Name : Motel Rd - Platte Frontage Rd AM
 Site Code : S214610
 Start Date : 7/14/2021
 Page No : 4

| Start Time | Motel Rd Southbound | | | | | East Frontage Rd Westbound | | | | | Motel Rd Northbound | | | | | West Frontage Rd Eastbound | | | | | Int. Total |
|---|---------------------|------|------|------|------------|----------------------------|------|------|------|------------|---------------------|------|------|------|------------|----------------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 6:30:00 AM to 8:15:00 AM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Each Approach Begins at: | | | | | | | | | | | | | | | | | | | | | |
| | 6:45:00 AM | | | | | 7:30:00 AM | | | | | 6:45:00 AM | | | | | 6:30:00 AM | | | | | |
| +0 mins. | 12 | 1 | 2 | 0 | 15 | 0 | 1 | 1 | 0 | 2 | 0 | 5 | 0 | 0 | 5 | 3 | 1 | 0 | 0 | 4 | |
| +5 mins. | 13 | 1 | 3 | 0 | 17 | 1 | 1 | 5 | 0 | 7 | 0 | 5 | 4 | 0 | 9 | 3 | 0 | 0 | 0 | 3 | |
| +10 mins. | 13 | 2 | 0 | 0 | 15 | 0 | 1 | 12 | 0 | 13 | 0 | 4 | 0 | 0 | 4 | 2 | 0 | 0 | 0 | 2 | |
| +15 mins. | 11 | 0 | 7 | 0 | 18 | 1 | 2 | 4 | 0 | 7 | 0 | 6 | 1 | 0 | 7 | 5 | 0 | 0 | 0 | 5 | |
| Total Volume | 49 | 4 | 12 | 0 | 65 | 2 | 5 | 22 | 0 | 29 | 0 | 20 | 5 | 0 | 25 | 13 | 1 | 0 | 0 | 14 | |
| % App. Total | 75.4 | 6.2 | 18.5 | 0 | | 6.9 | 17.2 | 75.9 | 0 | | 0 | 80 | 20 | 0 | | 92.9 | 7.1 | 0 | 0 | | |
| PHF | .942 | .500 | .429 | .000 | .903 | .500 | .625 | .458 | .000 | .558 | .000 | .833 | .313 | .000 | .694 | .650 | .250 | .000 | .000 | .700 | |

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File Name : Motel Rd - Platte Frontage Rd AM
Site Code : S214610
Start Date : 7/14/2021
Page No : 5



LSC Transportation Consultants, Inc.

545 E Pikes Peak Ave, Suite 210
 Colorado Springs, CO 80905
 719-633-2868

File Name : Motel Rd - Platte Ave PM
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 1

Groups Printed- Bank 1

| Start Time | Motel Rd Southbound | | | | | N Frontage Rd Westbound | | | | | Motel Rd Northbound | | | | | S Frontage rd Eastbound | | | | | Int. Total |
|-------------|---------------------|------|------|---|------------|-------------------------|------|------|---|------------|---------------------|------|-----|---|------------|-------------------------|------|-----|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| 04:00 PM | 2 | 4 | 5 | 0 | 11 | 0 | 2 | 5 | 0 | 7 | 0 | 3 | 0 | 0 | 3 | 4 | 1 | 1 | 0 | 6 | 27 |
| 04:15 PM | 9 | 0 | 9 | 0 | 18 | 0 | 3 | 5 | 0 | 8 | 0 | 2 | 1 | 0 | 3 | 10 | 0 | 0 | 0 | 10 | 39 |
| 04:30 PM | 3 | 10 | 3 | 0 | 16 | 1 | 0 | 3 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 5 | 2 | 0 | 0 | 7 | 27 |
| 04:45 PM | 1 | 9 | 1 | 0 | 11 | 1 | 2 | 1 | 0 | 4 | 0 | 5 | 0 | 0 | 5 | 6 | 1 | 0 | 0 | 7 | 27 |
| Total | 15 | 23 | 18 | 0 | 56 | 2 | 7 | 14 | 0 | 23 | 0 | 10 | 1 | 0 | 11 | 25 | 4 | 1 | 0 | 30 | 120 |
| 05:00 PM | 3 | 2 | 1 | 0 | 6 | 0 | 3 | 8 | 0 | 11 | 0 | 1 | 0 | 0 | 1 | 4 | 1 | 0 | 0 | 5 | 23 |
| 05:15 PM | 1 | 4 | 0 | 0 | 5 | 0 | 0 | 7 | 0 | 7 | 0 | 2 | 0 | 0 | 2 | 4 | 0 | 0 | 0 | 4 | 18 |
| 05:30 PM | 2 | 6 | 3 | 0 | 11 | 1 | 1 | 4 | 0 | 6 | 0 | 3 | 0 | 0 | 3 | 1 | 0 | 0 | 0 | 1 | 21 |
| 05:45 PM | 2 | 7 | 1 | 0 | 10 | 5 | 1 | 2 | 0 | 8 | 0 | 4 | 1 | 0 | 5 | 1 | 1 | 0 | 0 | 2 | 25 |
| Total | 8 | 19 | 5 | 0 | 32 | 6 | 5 | 21 | 0 | 32 | 0 | 10 | 1 | 0 | 11 | 10 | 2 | 0 | 0 | 12 | 87 |
| Grand Total | 23 | 42 | 23 | 0 | 88 | 8 | 12 | 35 | 0 | 55 | 0 | 20 | 2 | 0 | 22 | 35 | 6 | 1 | 0 | 42 | 207 |
| Apprch % | 26.1 | 47.7 | 26.1 | 0 | | 14.5 | 21.8 | 63.6 | 0 | | 0 | 90.9 | 9.1 | 0 | | 83.3 | 14.3 | 2.4 | 0 | | |
| Total % | 11.1 | 20.3 | 11.1 | 0 | 42.5 | 3.9 | 5.8 | 16.9 | 0 | 26.6 | 0 | 9.7 | 1 | 0 | 10.6 | 16.9 | 2.9 | 0.5 | 0 | 20.3 | |

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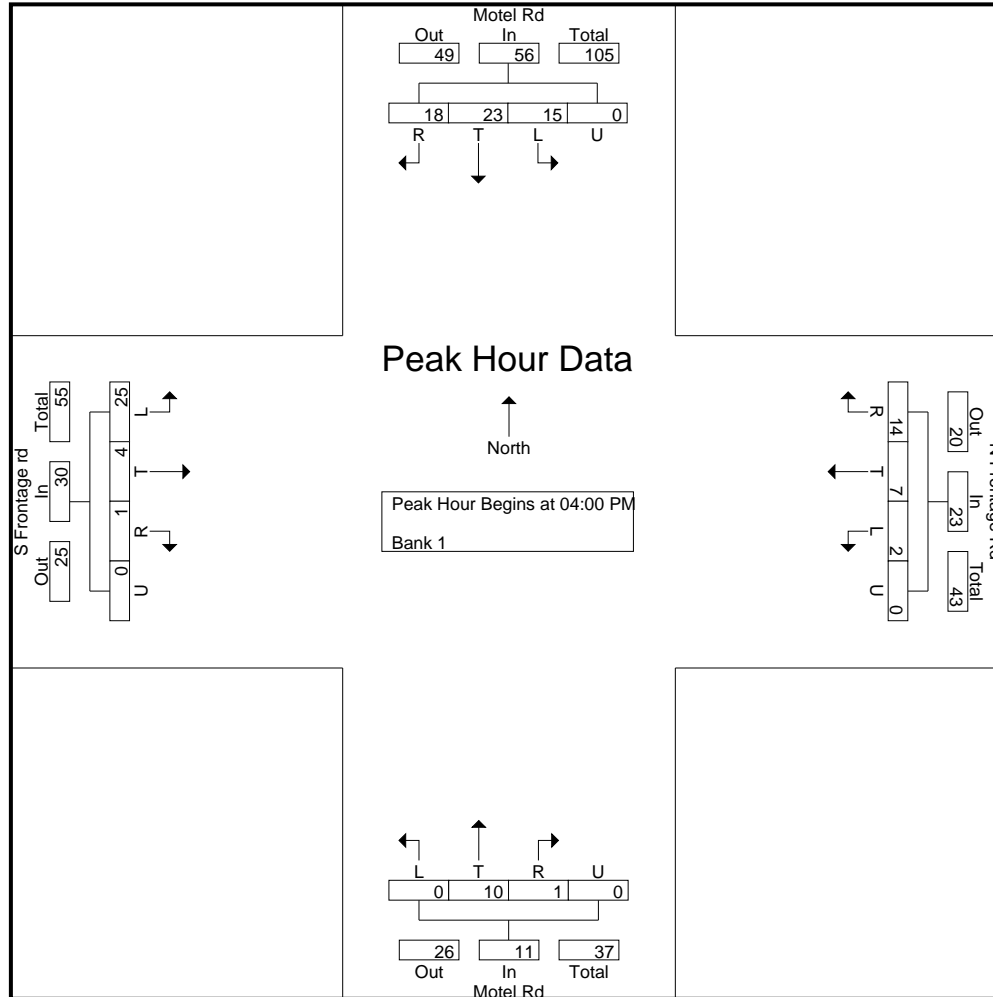
File Name : Motel Rd - Platte Ave PM
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 2

| Start Time | Motel Rd Southbound | | | | | N Frontage Rd Westbound | | | | | Motel Rd Northbound | | | | | S Frontage rd Eastbound | | | | | Int. Total |
|---|---------------------|------|------|------|------------|-------------------------|------|------|------|------------|---------------------|------|------|------|------------|-------------------------|------|------|------|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |
| Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 4:00:00 PM | | | | | | | | | | | | | | | | | | | | | |
| 4:00:00 PM | 2 | 4 | 5 | 0 | 11 | 0 | 2 | 5 | 0 | 7 | 0 | 3 | 0 | 0 | 3 | 4 | 1 | 1 | 0 | 6 | 27 |
| 4:15:00 PM | 9 | 0 | 9 | 0 | 18 | 0 | 3 | 5 | 0 | 8 | 0 | 2 | 1 | 0 | 3 | 10 | 0 | 0 | 0 | 10 | 39 |
| 4:30:00 PM | 3 | 10 | 3 | 0 | 16 | 1 | 0 | 3 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 5 | 2 | 0 | 0 | 7 | 27 |
| 4:45:00 PM | 1 | 9 | 1 | 0 | 11 | 1 | 2 | 1 | 0 | 4 | 0 | 5 | 0 | 0 | 5 | 6 | 1 | 0 | 0 | 7 | 27 |
| Total Volume | 15 | 23 | 18 | 0 | 56 | 2 | 7 | 14 | 0 | 23 | 0 | 10 | 1 | 0 | 11 | 25 | 4 | 1 | 0 | 30 | 120 |
| % App. Total | 26.8 | 41.1 | 32.1 | 0 | | 8.7 | 30.4 | 60.9 | 0 | | 0 | 90.9 | 9.1 | 0 | | 83.3 | 13.3 | 3.3 | 0 | | |
| PHF | .417 | .575 | .500 | .000 | .778 | .500 | .583 | .700 | .000 | .719 | .000 | .500 | .250 | .000 | .550 | .625 | .500 | .250 | .000 | .750 | .769 |

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File Name : Motel Rd - Platte Ave PM
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 3



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File Name : Motel Rd - Platte Ave PM
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 4

| Start Time | Motel Rd Southbound | | | | | N Frontage Rd Westbound | | | | | Motel Rd Northbound | | | | | S Frontage rd Eastbound | | | | | Int. Total |
|------------|---------------------|---|---|---|------------|-------------------------|---|---|---|------------|---------------------|---|---|---|------------|-------------------------|---|---|---|------------|------------|
| | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | L | T | R | U | App. Total | |

Peak Hour Analysis From 4:00:00 PM to 5:45:00 PM - Peak 1 of 1

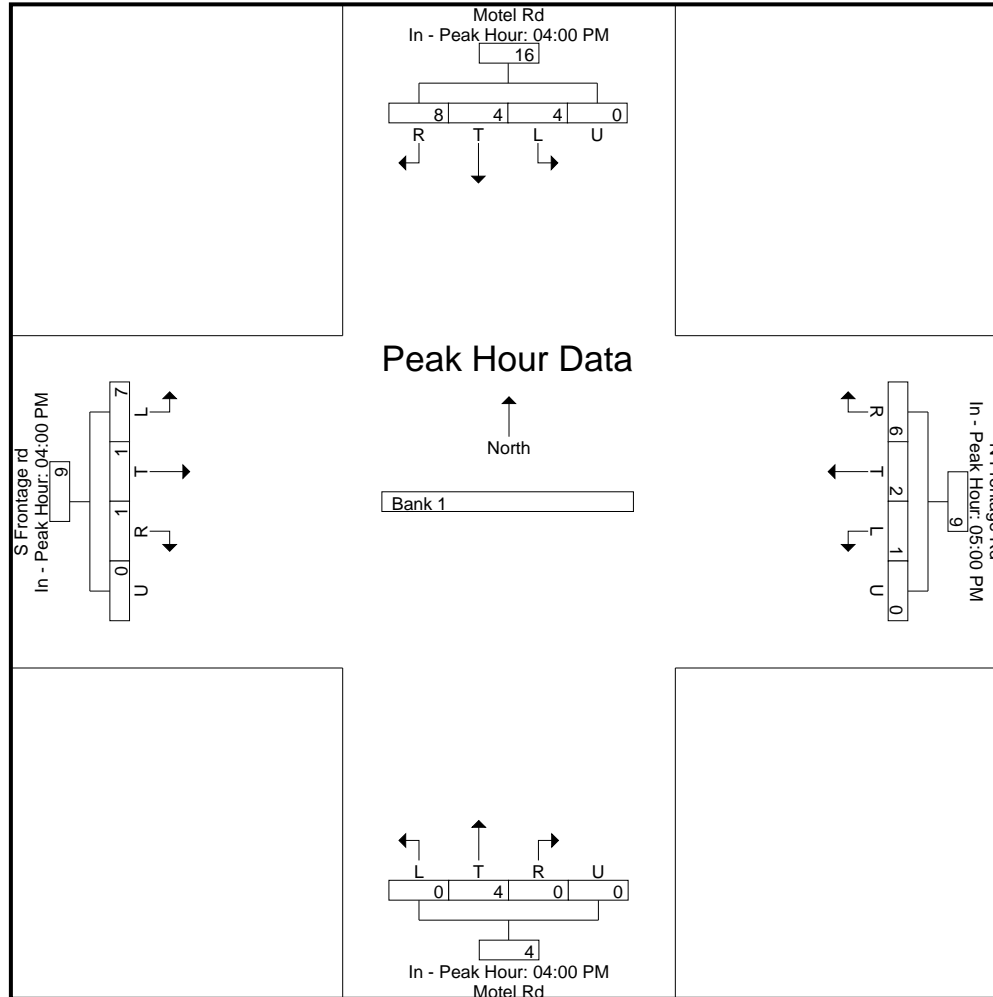
Peak Hour for Each Approach Begins at:

| | 4:00:00 PM | | | | | 5:00:00 PM | | | | | 4:00:00 PM | | | | | 4:00:00 PM | | | | |
|--------------|------------|------|------|------|------|------------|------|------|------|------|------------|------|------|------|------|------------|------|------|------|------|
| +0 mins. | 2 | 4 | 5 | 0 | 11 | 0 | 3 | 8 | 0 | 11 | 0 | 3 | 0 | 0 | 3 | 4 | 1 | 1 | 0 | 6 |
| +5 mins. | 9 | 0 | 9 | 0 | 18 | 0 | 0 | 7 | 0 | 7 | 0 | 2 | 1 | 0 | 3 | 10 | 0 | 0 | 0 | 10 |
| +10 mins. | 3 | 10 | 3 | 0 | 16 | 1 | 1 | 4 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 5 | 2 | 0 | 0 | 7 |
| +15 mins. | 1 | 9 | 1 | 0 | 11 | 5 | 1 | 2 | 0 | 8 | 0 | 5 | 0 | 0 | 5 | 6 | 1 | 0 | 0 | 7 |
| Total Volume | 15 | 23 | 18 | 0 | 56 | 6 | 5 | 21 | 0 | 32 | 0 | 10 | 1 | 0 | 11 | 25 | 4 | 1 | 0 | 30 |
| % App. Total | 26.8 | 41.1 | 32.1 | 0 | | 18.8 | 15.6 | 65.6 | 0 | | 0 | 90.9 | 9.1 | 0 | | 83.3 | 13.3 | 3.3 | 0 | |
| PHF | .417 | .575 | .500 | .000 | .778 | .300 | .417 | .656 | .000 | .727 | .000 | .500 | .250 | .000 | .550 | .625 | .500 | .250 | .000 | .750 |

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File Name : Motel Rd - Platte Ave PM
 Site Code : S214660
 Start Date : 7/14/2021
 Page No : 5



LSC Transportation Consultants, Inc.

2504 E. Pikes Peak Ave, Suite 304
 Colorado Springs, CO 80909
 719-633-2868

File Name : Platte Frontage Rd - 6001-6425 AM PM

Site Code : S224620

Start Date : 12/15/2022

Page No : 1

Groups Printed- Unshifted

| Start Time | 6001-6423 Southbound | | | | | Platte Frontage Rd Westbound | | | | | Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|---------------|----------------------|----------|----------|----------|------------|------------------------------|----------|----------|----------|------------|------------|----------|----------|----------|------------|------------------------------|----------|-----------|----------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |
| 06:30 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 2 |
| 06:35 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| 06:40 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 1 |
| 06:45 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 |
| 06:50 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 |
| 06:55 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| Total | 7 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 2 | 0 | 3 | 10 |
| | | | | | | | | | | | | | | | | | | | | | |
| 07:00 | 4 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| 07:05 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| 07:10 | 4 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 5 |
| 07:15 | 3 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 4 |
| 07:20 | 3 | 1 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| 07:25 | 2 | 2 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 2 | 6 |
| 07:30 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 | 5 |
| 07:35 | 3 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 4 |
| 07:40 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 1 | 3 |
| 07:45 | 5 | 1 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 |
| 07:50 | 5 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 6 |
| 07:55 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 2 |
| Total | 32 | 4 | 1 | 0 | 37 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 2 | 2 | 0 | 9 | 0 | 11 | 50 |
| | | | | | | | | | | | | | | | | | | | | | |
| 08:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 2 |
| 08:05 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 |
| 08:10 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 | 5 |
| 08:15 | 3 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 |
| 08:20 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 |
| 08:25 | 2 | 0 | 1 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 5 | 0 | 5 | 9 |
| *** BREAK *** | | | | | | | | | | | | | | | | | | | | | |
| Total | 8 | 1 | 1 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 14 | 0 | 14 | 25 |
| | | | | | | | | | | | | | | | | | | | | | |
| 16:00 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 4 |
| 16:05 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 4 | 0 | 4 | 7 |
| 16:10 | 3 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 6 | 9 |
| 16:15 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 7 | 8 |
| 16:20 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 |
| 16:25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 | 4 |
| 16:30 | 4 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 2 | 0 | 2 | 7 |
| 16:35 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 4 |
| 16:40 | 1 | 0 | 0 | 1 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 |
| 16:45 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 5 | 7 |
| 16:50 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 | 5 |

LSC Transportation Consultants, Inc.

2504 E. Pikes Peak Ave, Suite 304
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File Name : Platte Frontage Rd - 6001-6425 AM PM

Site Code : S224620

Start Date : 12/15/2022

Page No : 2

Groups Printed- Unshifted

| Start Time | 6001-6423 Southbound | | | | | Platte Frontage Rd Westbound | | | | | Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|--------------------|----------------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |
| 16:55 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 1 | 0 | 2 | 5 |
| Total | 20 | 0 | 0 | 1 | 21 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 1 | 0 | 42 | 0 | 43 | 67 |
| 17:00 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 |
| 17:05 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 4 |
| 17:10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 6 | 6 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 2 |
| 17:20 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 1 |
| 17:25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 1 |
| 17:30 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 0 | 0 | 2 | 0 | 2 | 4 |
| 17:35 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 2 |
| 17:40 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 |
| 17:45 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 |
| 17:50 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 1 |
| 17:55 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 |
| Total | 7 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 1 | 0 | 21 | 0 | 22 | 31 |
| Grand Total | 74 | 5 | 2 | 1 | 82 | 0 | 0 | 0 | 0 | 0 | 1 | 6 | 1 | 0 | 8 | 5 | 0 | 88 | 0 | 93 | 183 |
| Apprch % | 90.2 | 6.1 | 2.4 | 1.2 | | 0 | 0 | 0 | 0 | | 12.5 | 75 | 12.5 | 0 | | 5.4 | 0 | 94.6 | 0 | | |
| Total % | 40.4 | 2.7 | 1.1 | 0.5 | 44.8 | 0 | 0 | 0 | 0 | 0 | 0.5 | 3.3 | 0.5 | 0 | 4.4 | 2.7 | 0 | 48.1 | 0 | 50.8 | |

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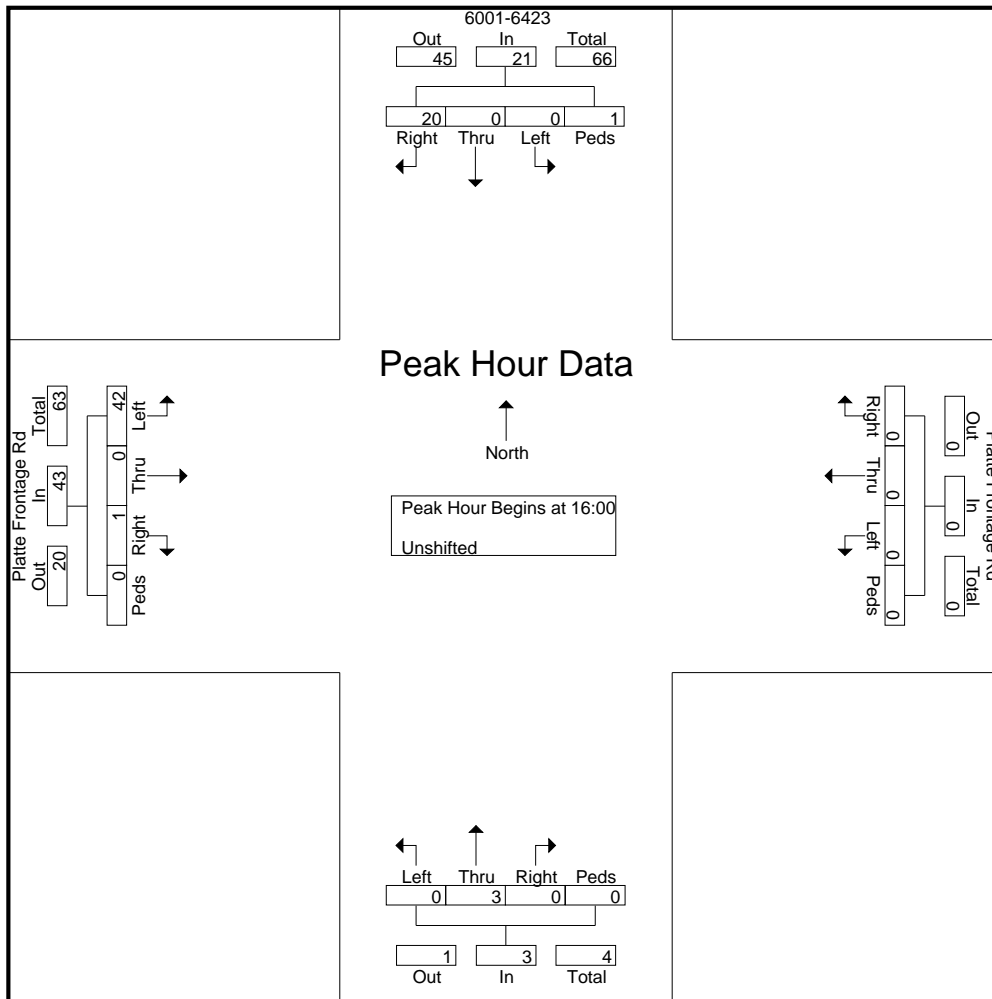
File Name : Platte Frontage Rd - 6001-6425 AM PM

Site Code : S224620

Start Date : 12/15/2022

Page No : 3

| Start Time | 6001-6423 Southbound | | | | | Platte Frontage Rd Westbound | | | | | Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|--|----------------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |
| Peak Hour Analysis From 06:30 to 17:55 - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 16:00 | | | | | | | | | | | | | | | | | | | | | |
| 16:00 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 4 |
| 16:05 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 4 | 0 | 4 | 7 |
| 16:10 | 3 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 6 | 9 |
| 16:15 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 7 | 8 |
| 16:20 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 |
| 16:25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 | 4 |
| 16:30 | 4 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 2 | 0 | 2 | 7 |
| 16:35 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 4 |
| 16:40 | 1 | 0 | 0 | 1 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 |
| 16:45 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 5 | 7 |
| 16:50 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 | 5 |
| 16:55 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 1 | 0 | 2 | 5 |
| Total Volume | 20 | 0 | 0 | 1 | 21 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 1 | 0 | 42 | 0 | 43 | 67 |
| % App. Total | 95.2 | 0 | 0 | 4.8 | | 0 | 0 | 0 | 0 | | 0 | 100 | 0 | 0 | | 2.3 | 0 | 97.7 | 0 | | |
| PHF | .417 | .000 | .000 | .083 | .438 | .000 | .000 | .000 | .000 | .000 | .000 | .250 | .000 | .000 | .250 | .083 | .000 | .500 | .000 | .512 | .620 |



LSC Transportation Consultants, Inc.

2504 E. Pikes Peak Ave, Suite 304
 Colorado Springs, CO 80909
 719-633-2868

File Name : Platte Frontage Rd - 6001-6425 AM PM

Site Code : S224620

Start Date : 12/15/2022

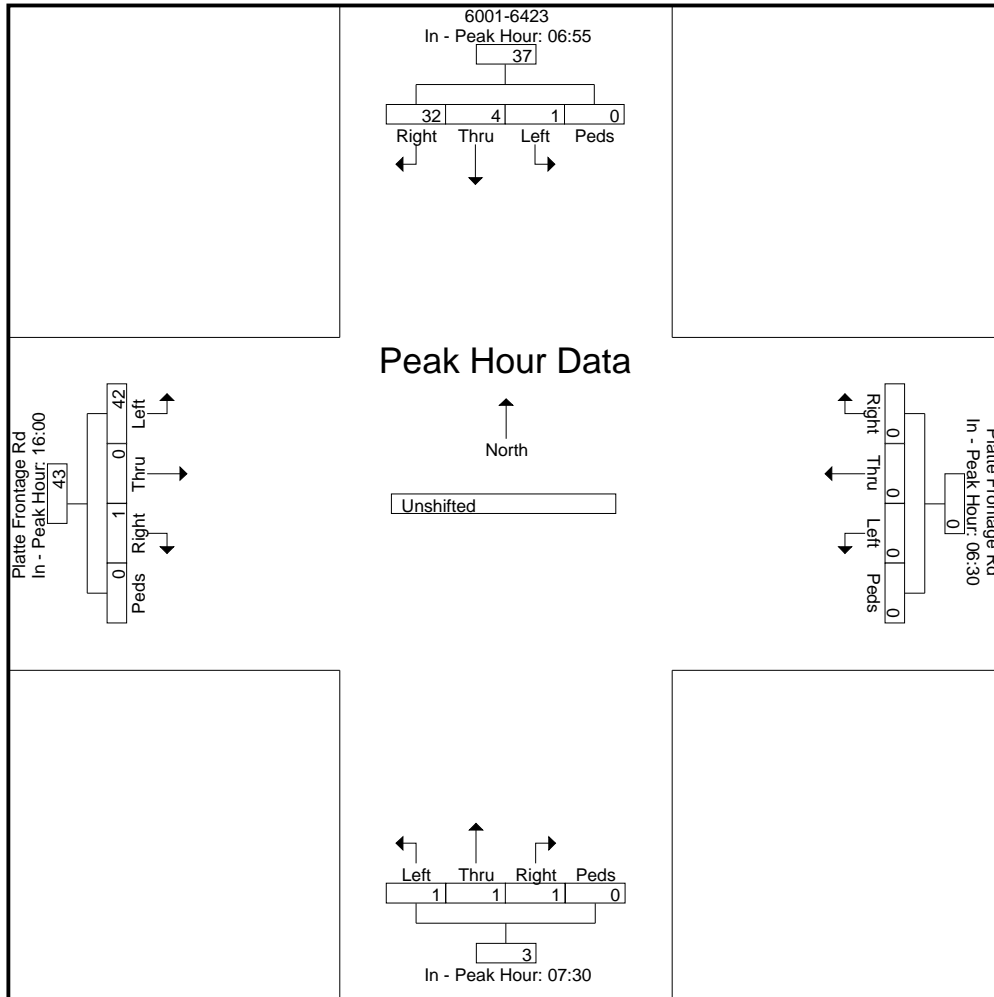
Page No : 4

| Start Time | 6001-6423 Southbound | | | | | Platte Frontage Rd Westbound | | | | | Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|------------|----------------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |

Peak Hour Analysis From 06:30 to 17:55 - Peak 1 of 1

Peak Hour for Each Approach Begins at:

| | 06:55 | | | | | 06:30 | | | | | 07:30 | | | | | 16:00 | | | | |
|--------------|-------|------|------|------|------|-------|------|------|------|------|-------|------|------|------|------|-------|------|------|------|------|
| +0 mins. | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 |
| +5 mins. | 4 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 |
| +10 mins. | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 6 | 0 | 6 |
| +15 mins. | 4 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 7 |
| +20 mins. | 3 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 |
| +25 mins. | 3 | 1 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 4 | 0 | 4 |
| +30 mins. | 2 | 2 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 |
| +35 mins. | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 |
| +40 mins. | 3 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 |
| +45 mins. | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 5 |
| +50 mins. | 5 | 1 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 |
| +55 mins. | 5 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 1 | 0 | 2 |
| Total Volume | 32 | 4 | 1 | 0 | 37 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 1 | 0 | 3 | 1 | 0 | 42 | 0 | 43 |
| % App. Total | 86.5 | 10.8 | 2.7 | 0 | | 0 | 0 | 0 | 0 | | 33.3 | 33.3 | 33.3 | 0 | | 2.3 | 0 | 97.7 | 0 | |
| PHF | .533 | .167 | .083 | .000 | .514 | .000 | .000 | .000 | .000 | .000 | .083 | .083 | .083 | .000 | .250 | .083 | .000 | .500 | .000 | .512 |



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LSC Transportation Consultants, Inc.

2504 E. Pikes Peak Ave, Suite 304
 Colorado Springs, CO 80909
 719-633-2868

File Name : Platte Frontage Rd - 6275-6417

Site Code : S224620

Start Date : 12/15/2022

Page No : 1

Groups Printed- Bank 1

| Start Time | Southbound | | | | | Platte Frontage Rd Westbound | | | | | 6275-6417 Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|---------------|------------|------|------|------|------------|------------------------------|------|------|------|------------|----------------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |
| *** BREAK *** | | | | | | | | | | | | | | | | | | | | | |
| 06:40 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 |
| 06:45 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 0 | 5 | 7 |
| 06:50 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 3 | 5 |
| 06:55 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 10 | 0 | 0 | 0 | 10 | 11 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 17 | 2 | 0 | 0 | 19 | 24 |
| 07:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 0 | 2 | 0 | 2 | 10 | 0 | 0 | 0 | 10 | 14 |
| 07:05 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 8 | 10 |
| 07:10 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 7 | 1 | 0 | 0 | 8 | 13 |
| 07:15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 1 | 0 | 0 | 0 | 1 | 3 | 0 | 0 | 0 | 3 | 7 |
| 07:20 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 5 | 8 |
| 07:25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 0 | 1 | 0 | 1 | 5 | 1 | 0 | 0 | 6 | 9 |
| 07:30 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 3 | 1 | 1 | 0 | 0 | 2 | 5 |
| 07:35 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 1 | 0 | 1 | 0 | 2 | 4 | 0 | 0 | 0 | 4 | 9 |
| 07:40 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 2 | 0 | 2 | 1 | 0 | 0 | 0 | 1 | 4 |
| 07:45 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 3 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 4 | 9 |
| 07:50 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 0 | 5 | 10 |
| 07:55 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 0 | 0 | 1 | 0 | 1 | 4 | 0 | 0 | 0 | 4 | 7 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 28 | 0 | 33 | 5 | 0 | 7 | 0 | 12 | 56 | 4 | 0 | 0 | 60 | 105 |
| 08:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 3 | 0 | 4 | 7 | 1 | 0 | 0 | 8 | 12 |
| 08:05 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 2 | 0 | 2 | 0 | 4 | 4 | 0 | 0 | 0 | 4 | 10 |
| 08:10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 0 | 6 | 0 | 1 | 0 | 0 | 1 | 7 |
| 08:15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 0 | 0 | 4 | 0 | 4 | 3 | 0 | 0 | 0 | 3 | 10 |
| 08:20 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 | 0 | 4 | 0 | 6 | 4 | 0 | 0 | 0 | 4 | 11 |
| 08:25 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 5 | 0 | 2 | 0 | 7 | 3 | 0 | 0 | 0 | 3 | 12 |
| *** BREAK *** | | | | | | | | | | | | | | | | | | | | | |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 6 | 0 | 8 | 13 | 0 | 18 | 0 | 31 | 21 | 2 | 0 | 0 | 23 | 62 |
| *** BREAK *** | | | | | | | | | | | | | | | | | | | | | |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 2 | 0 | 5 | 3 | 0 | 0 | 0 | 3 | 9 |
| 16:05 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 | 0 | 1 | 0 | 5 | 4 | 0 | 0 | 0 | 4 | 11 |
| 16:10 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 4 | 0 | 3 | 0 | 7 | 2 | 2 | 0 | 0 | 4 | 14 |
| 16:15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 7 | 0 | 3 | 0 | 10 | 2 | 0 | 0 | 0 | 2 | 13 |
| 16:20 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 1 | 0 | 0 | 0 | 1 | 2 | 0 | 0 | 0 | 2 | 5 |
| 16:25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 1 | 0 | 5 | 2 | 0 | 0 | 0 | 2 | 7 |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 3 | 0 | 4 | 2 | 0 | 2 | 0 | 4 | 2 | 0 | 0 | 0 | 2 | 10 |
| 16:35 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 | 0 | 4 | 0 | 6 | 3 | 1 | 0 | 0 | 4 | 11 |
| 16:40 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 3 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 5 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 5 | 0 | 3 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 11 |
| 16:50 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 1 | 0 | 4 | 1 | 1 | 0 | 0 | 2 | 7 |
| 16:55 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 2 | 0 | 2 | 0 | 4 | 1 | 0 | 0 | 0 | 1 | 7 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 13 | 0 | 20 | 39 | 0 | 25 | 0 | 64 | 22 | 4 | 0 | 0 | 26 | 110 |

LSC Transportation Consultants, Inc.

2504 E. Pikes Peak Ave, Suite 304
 Colorado Springs, CO 80909
 719-633-2868

File Name : Platte Frontage Rd - 6275-6417

Site Code : S224620

Start Date : 12/15/2022

Page No : 2

Groups Printed- Bank 1

| Start Time | Southbound | | | | | Platte Frontage Rd Westbound | | | | | 6275-6417 Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|--------------------|------------|------|------|------|------------|------------------------------|------|------|------|------------|----------------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 2 | 0 | 1 | 0 | 3 | 1 | 0 | 0 | 0 | 1 | 5 |
| 17:05 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 3 | 0 | 2 | 0 | 5 | 2 | 0 | 0 | 0 | 2 | 8 |
| 17:10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 4 | 0 | 7 | 1 | 0 | 0 | 0 | 1 | 8 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 3 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 5 |
| 17:20 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 1 | 0 | 0 | 1 | 2 |
| 17:25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 |
| 17:30 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 4 |
| 17:35 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 4 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 8 |
| 17:40 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 2 |
| 17:45 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 2 | 0 | 1 | 0 | 3 | 1 | 0 | 0 | 0 | 1 | 6 |
| 17:50 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 1 |
| 17:55 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 1 | 0 | 7 | 19 | 0 | 18 | 0 | 37 | 6 | 2 | 0 | 0 | 8 | 52 |
| Grand Total | 0 | 0 | 0 | 0 | 0 | 0 | 20 | 53 | 0 | 73 | 76 | 0 | 68 | 0 | 144 | 122 | 14 | 0 | 0 | 136 | 353 |
| Apprch % | 0 | 0 | 0 | 0 | 0 | 0 | 27.4 | 72.6 | 0 | | 52.8 | 0 | 47.2 | 0 | | 89.7 | 10.3 | 0 | 0 | | |
| Total % | 0 | 0 | 0 | 0 | 0 | 0 | 5.7 | 15 | 0 | 20.7 | 21.5 | 0 | 19.3 | 0 | 40.8 | 34.6 | 4 | 0 | 0 | 38.5 | |

LSC Transportation Consultants, Inc.

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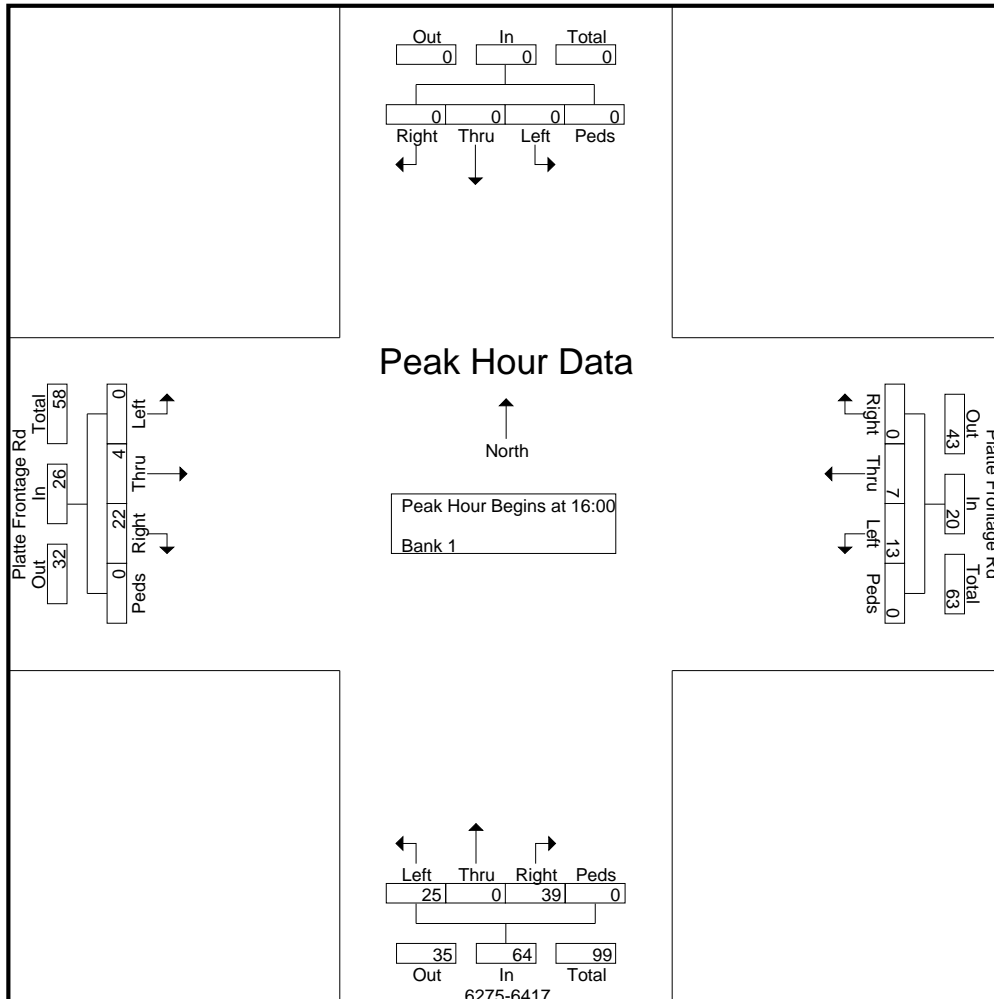
File Name : Platte Frontage Rd - 6275-6417

Site Code : S224620

Start Date : 12/15/2022

Page No : 3

| Start Time | Southbound | | | | | Platte Frontage Rd Westbound | | | | | 6275-6417 Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|--|------------|------|------|------|------------|------------------------------|------|------|------|------------|----------------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |
| Peak Hour Analysis From 06:30 to 17:55 - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Entire Intersection Begins at 16:00 | | | | | | | | | | | | | | | | | | | | | |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 2 | 0 | 5 | 3 | 0 | 0 | 0 | 3 | 9 |
| 16:05 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 | 0 | 1 | 0 | 5 | 4 | 0 | 0 | 0 | 4 | 11 |
| 16:10 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 4 | 0 | 3 | 0 | 7 | 2 | 2 | 0 | 0 | 4 | 14 |
| 16:15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 7 | 0 | 3 | 0 | 10 | 2 | 0 | 0 | 0 | 2 | 13 |
| 16:20 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 1 | 0 | 0 | 0 | 1 | 2 | 0 | 0 | 0 | 2 | 5 |
| 16:25 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 1 | 0 | 5 | 2 | 0 | 0 | 0 | 2 | 7 |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 3 | 0 | 4 | 2 | 0 | 2 | 0 | 4 | 2 | 0 | 0 | 0 | 2 | 10 |
| 16:35 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 | 0 | 4 | 0 | 6 | 3 | 1 | 0 | 0 | 4 | 11 |
| 16:40 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 3 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 5 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 5 | 0 | 3 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 11 |
| 16:50 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 3 | 0 | 1 | 0 | 4 | 1 | 1 | 0 | 0 | 2 | 7 |
| 16:55 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 2 | 0 | 2 | 0 | 4 | 1 | 0 | 0 | 0 | 1 | 7 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 13 | 0 | 20 | 39 | 0 | 25 | 0 | 64 | 22 | 4 | 0 | 0 | 26 | 110 |
| % App. Total | 0 | 0 | 0 | 0 | 0 | 0 | 35 | 65 | 0 | | 60.9 | 0 | 39.1 | 0 | | 84.6 | 15.4 | 0 | 0 | | |
| PHF | .000 | .000 | .000 | .000 | .000 | .000 | .194 | .361 | .000 | .417 | .464 | .000 | .521 | .000 | .533 | .458 | .167 | .000 | .000 | .542 | .655 |

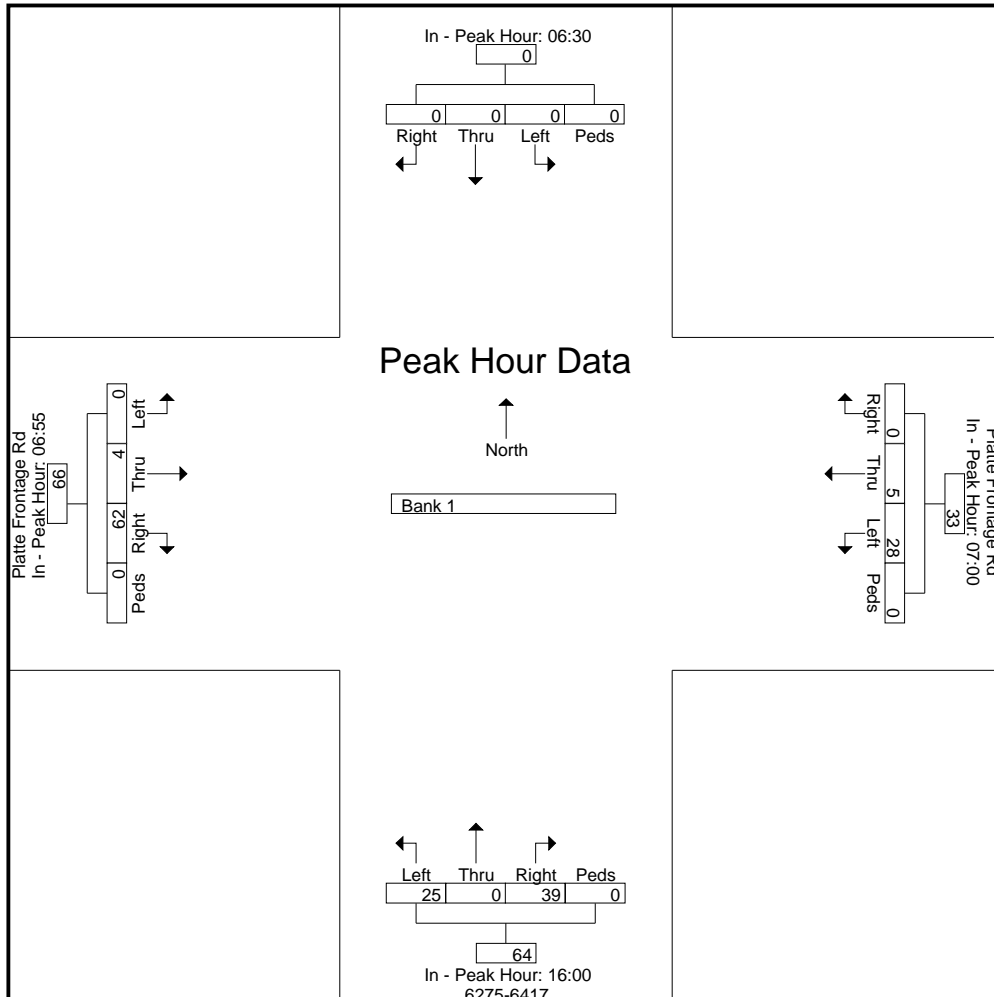


LSC Transportation Consultants, Inc.

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 Colorado Springs, CO 80909
 719-633-2868

File Name : Platte Frontage Rd - 6275-6417
 Site Code : S224620
 Start Date : 12/15/2022
 Page No : 4

| Start Time | Southbound | | | | | Platte Frontage Rd Westbound | | | | | 6275-6417 Northbound | | | | | Platte Frontage Rd Eastbound | | | | | Int. Total |
|--|------------|------|------|------|------------|------------------------------|------|------|------|------------|----------------------|------|------|------|------------|------------------------------|------|------|------|------------|------------|
| | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | Right | Thru | Left | Peds | App. Total | |
| Peak Hour Analysis From 06:30 to 17:55 - Peak 1 of 1 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour for Each Approach Begins at: | | | | | | | | | | | | | | | | | | | | | |
| | 06:30 | | | | | 07:00 | | | | | 16:00 | | | | | 06:55 | | | | | |
| +0 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 3 | 0 | 2 | 0 | 5 | 10 | 0 | 0 | 0 | 10 | |
| +5 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 | 0 | 1 | 0 | 5 | 10 | 0 | 0 | 0 | 10 | |
| +10 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 5 | 4 | 0 | 3 | 0 | 7 | 8 | 0 | 0 | 0 | 8 | |
| +15 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 7 | 0 | 3 | 0 | 10 | 7 | 1 | 0 | 0 | 8 | |
| +20 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 1 | 0 | 0 | 0 | 1 | 3 | 0 | 0 | 0 | 3 | |
| +25 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 4 | 0 | 1 | 0 | 5 | 5 | 0 | 0 | 0 | 5 | |
| +30 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 4 | 5 | 1 | 0 | 0 | 6 | |
| +35 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 2 | 0 | 4 | 0 | 6 | 1 | 1 | 0 | 0 | 2 | |
| +40 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 | 0 | 3 | 0 | 5 | 4 | 0 | 0 | 0 | 4 | |
| +45 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 3 | 0 | 5 | 5 | 0 | 3 | 0 | 8 | 1 | 0 | 0 | 0 | 1 | |
| +50 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 5 | 3 | 0 | 1 | 0 | 4 | 4 | 0 | 0 | 0 | 4 | |
| +55 mins. | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 2 | 0 | 2 | 0 | 4 | 4 | 1 | 0 | 0 | 5 | |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 28 | 0 | 33 | 39 | 0 | 25 | 0 | 64 | 62 | 4 | 0 | 0 | 66 | |
| % App. Total | 0 | 0 | 0 | 0 | 0 | 0 | 15.2 | 84.8 | 0 | 33 | 60.9 | 0 | 39.1 | 0 | 64 | 93.9 | 6.1 | 0 | 0 | 66 | |
| PHF | .000 | .000 | .000 | .000 | .000 | .000 | .208 | .583 | .000 | .550 | .464 | .000 | .521 | .000 | .533 | .517 | .333 | .000 | .000 | .550 | |



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