

THE COMMONS AT FALCON FIELD

EAST HALF OF SECTION 7, TOWNSHIP 13 SOUTH, RANGE 64 WEST OF THE 6TH P.M. PEYTON, EL PASO COUNTY, COLORADO

COMMERCIAL CONSTRUCTION PLANS

LEGAL DESCRIPTION:

PARCEL ONE:

A PARCEL OF LAND SITUATED IN SECTION 7, TOWNSHIP 13 SOUTH, RANGE 64 WEST OF THE SIXTH P.M., EL PASO COUNTY, COLORADO, BEING A PORTION OF THAT TRACT OF LAND DESCRIBED UNDER RECEPTION NO. 202131510 OF THE RECORDS OF SAID COUNTY, SAID PARCEL OF LAND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHWEST CORNER OF "ARROWHEAD ESTATES FILING NO. 1" AS DESCRIBED IN PLAT BOOK Y3 AT PAGE 39 OF THE RECORDS OF SAID COUNTY, SAID CORNER ALSO BEING THE NORTHWEST CORNER OF LOT 13, OF SAID "ARROWHEAD ESTATES FILING NO. 1"; (THE FOLLOWING FOUR (4) COURSES ARE ALONG THE WESTERLY AND NORTHERLY BOUNDARY LINES OF SAID "ARROWHEAD ESTATES FILING NO. 1".)

- 1) THENCE S 00 DEGREES 46 MINUTES 12 SECONDS W, 197.28 FEET TO THE POINT OF BEGINNING OF THE PARCEL OF LAND HEREIN DESCRIBED;
- 2) THENCE CONTINUE S 00 DEGREES 46 MINUTES 12 SECONDS W, 988.14 FEET;
- 3) THENCE S 86 DEGREES 00 MINUTES 46 SECONDS W, 327.52 FEET (327.53 FEET RECORDED);
- 4) THENCE S 00 DEGREES 25 MINUTES 05 SECONDS W, 68.17 FEET (67.53 FEET RECORDED) TO THE NORTHERLY LINE OF THAT TRACT OF LAND DESCRIBED UNDER RECEPTION NO. 202090702; THENCE N 89 DEGREES 59 MINUTES 43 SECONDS W, 430.45 FEET (430.08 FEET RECORDED) ALONG SAID NORTHERLY LINE TO THE SOUTHWESTERLY CORNER OF VERBURG SUBDIVISION WAIVER, A TRACT OF LAND AS DESCRIBED UNDER RECEPTION NO. 20100639 OF SAID RECORDS; THENCE N 00 DEGREES 14 MINUTES 15 SECONDS E, 1475.39 FEET (1475.79 FEET RECORDED) ALONG THE EASTERLY LINE OF SAID "VERBURG SUBDIVISION WAIVER" TO THE SOUTHEASTERLY RIGHT OF WAY LINE OF U.S. HIGHWAY NO. 24 AS DESCRIBED IN BOOK 840 AT PAGE 258 OF SAID RECORDS; (THE FOLLOWING TWO (2) COURSES ARE ALONG SAID SOUTHWESTERLY RIGHT OF WAY LINE;
- 1) THENCE N 50 DEGREES 05 MINUTES 41 SECONDS E, 125.34 FEET (125.38 FEET RECORDED);
- 2) THENCE ALONG THE ARC OF A CURVE TO THE LEFT, SAID CURVE BEING CONCAVE TO THE NORTHWEST, HAVING A CHORD BEARING AND DISTANCE OF N 49 DEGREES 13 MINUTES 05 SECONDS E, 178.19 FEET, A CENTRAL ANGLE OF 1 DEGREE 45 MINUTES 37 SECONDS, A RADIUS OF 5800.00 FEET, FOR AN ARC DISTANCE OF 178.20 FEET; THENCE S 41 DEGREES 58 MINUTES 50 SECONDS E, 798.01 FEET TO THE POINT OF BEGINNING.

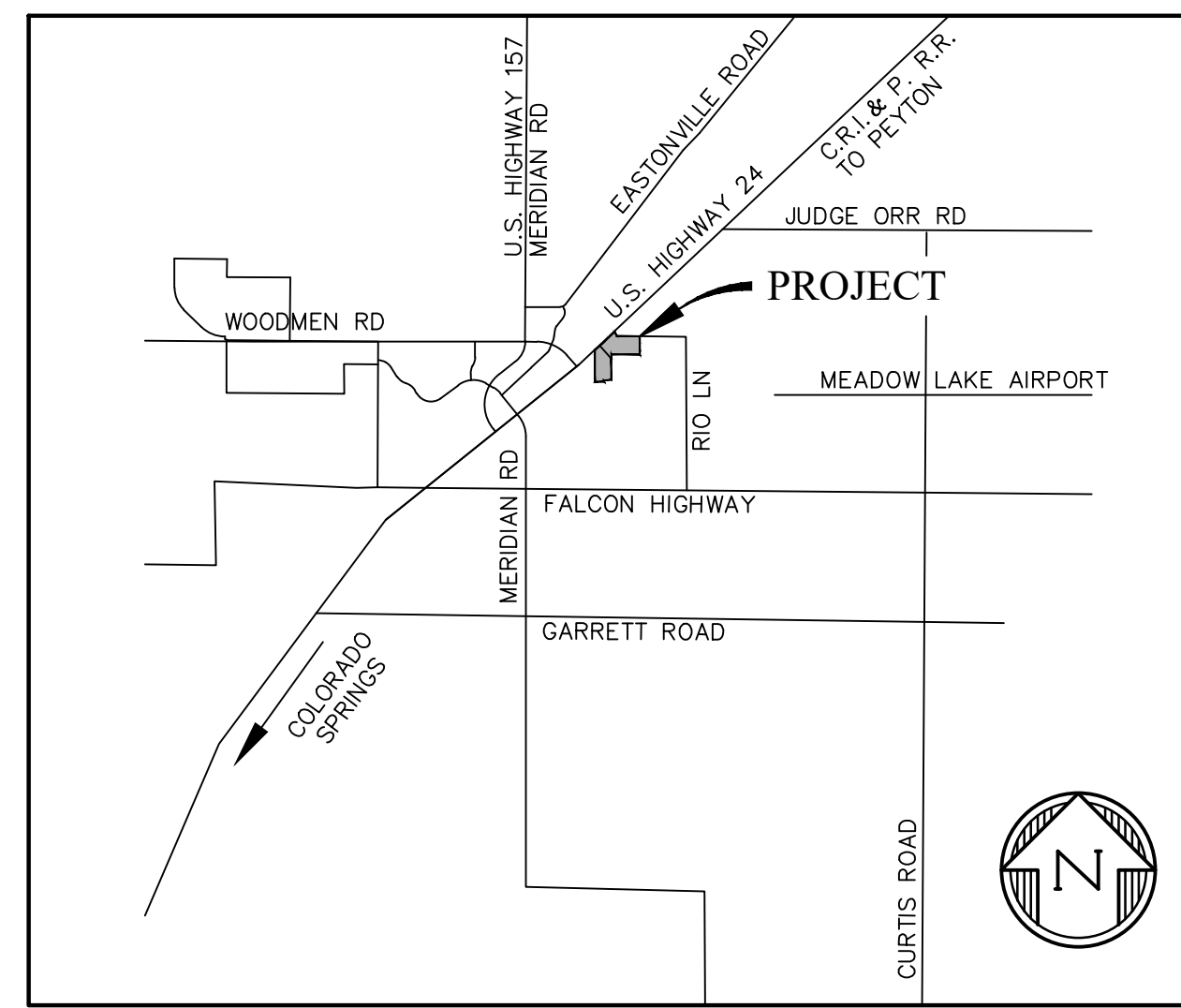
PARCEL TWO:

A PARCEL OF LAND SITUATED IN SECTION 7, TOWNSHIP 13 SOUTH, RANGE 64 WEST OF THE SIXTH P.M., EL PASO COUNTY COLORADO, BEING A PORTION OF THAT TRACT OF LAND DESCRIBED UNDER RECEPTION NO. 202131510 OF THE RECORDS OF SAID COUNTY, SAID PARCEL OF LAND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF "ARROWHEAD ESTATES FILING NO. 1" AS DESCRIBED IN PLAT BOOK Y-3 AT PAGE 39 OF THE RECORDS OF SAID COUNTY, SAID CORNER ALSO BEING THE NORTHWEST CORNER OF LOT 13, OF SAID "ARROWHEAD ESTATES FILING NO. 1"; THENCE S 00 DEGREES 46 MINUTES 12 SECONDS W, 197.28 FEET ALONG THE WESTERLY LINE OF SAID LOT 13 TO A POINT THEREON; THENCE N 41 DEGREES 58 MINUTES 50 SECONDS W, 798.01 FEET TO THE SOUTHEASTERLY RIGHT OF WAY LINE OF U.S. HIGHWAY NO. 24 AS DESCRIBED IN BOOK 840 AT PAGE 258 OF SAID RECORDS; (THE FOLLOWING TWO (2) COURSES ARE ALONG SAID SOUTHEASTERLY RIGHT OF WAY LINE.)

- 1) THENCE ALONG THE ARC OF A CURVE TO THE LEFT, SAID CURVE BEING CONCAVE TO THE NORTHWEST, HAVING A CHORD BEARING AND DISTANCE OF N 47 DEGREES 22 MINUTES 56 SECONDS E, 193.52 FEET, A CENTRAL ANGLE OF 1 DEGREE 54 MINUTES 42 SECONDS, A RADIUS OF 5800.00 FEET, FOR AN ARC DISTANCE OF 193.53 FEET;
- 2) THENCE NORTH 46 DEGREES 25 MINUTES 11 SECONDS E, 760.04 FEET (760.22 FEET RECORDED) TO THE INTERSECTION OF SAID SOUTHEASTERLY RIGHT OF WAY LINE WITH THE WESTERLY RIGHT OF WAY LINE OF RIO ROAD, A 60 FOOT RIGHT OF WAY SHOWN ON "FALCON RANCH ESTATES SUBDIVISION" AS RECORDED IN PLAT BOOK T-2 AT PAGE 47 OF SAID RECORDS; (THE FOLLOWING TWO (2) COURSES ARE ALONG SAID WESTERLY RIGHT OF WAY LINE AND THE SOUTHERLY RIGHT OF WAY LINE OF RIO ROAD.)
- 1.) THENCE S 22 DEGREES 22 MINUTES 28 SECONDS E, 219.81 FEET (219.73 FEET RECORDED);
- 2.) THENCE S 89 DEGREES 10 MINUTES 21 SECONDS E, 1071.23 FEET (1071.46 FEET RECORDED) TO THE NORTHWEST CORNER OF LOT 14 OF SAID "FALCON RANCH ESTATES SUBDIVISION"; THENCE S 00 DEGREES 10 MINUTES 51 SECONDS E, 705.04 FEET ALONG THE WESTERLY LINES OF SAID LOT 14 AND 13 OF SAID "FALCON RANCH ESTATES SUBDIVISION" TO THE MOST NORTHERLY NORTHWEST CORNER OF SAID "ARROWHEAD ESTATES FILING NO. 1"; SAID CORNER ALSO BEING THE MOST NORTHERLY NORTHWEST CORNER OF LOT 10 OF SAID "ARROWHEAD ESTATES FILING NO. 1"; (THE FOLLOWING TWO (2) COURSES ARE ALONG THE WESTERLY AND NORTHERLY LINES OF SAID "ARROWHEAD ESTATE FILING NO. 1".)
- 1.) THENCE CONTINUE S 00 DEGREES 10 MINUTES 51 SECONDS E, 151.74 FEET (152.43 FEET RECORDED);
- 2.) THENCE N 88 DEGREES 55 MINUTES 44 SECONDS W, 1314.29 FEET (1314.59 FEET RECORDED) TO THE POINT OF BEGINNING.

THE ABOVE PARCELS OF LAND CONTAIN 2,511,970 SQUARE FEET OR 57.667 ACRES, MORE OR LESS.



VICINITY MAP
NTS

DESIGN ENGINEER'S STATEMENT:

THESE DETAILED PLANS AND SPECIFICATIONS WERE PREPARED UNDER MY DIRECTION AND SUPERVISION. SAID PLANS AND SPECIFICATIONS HAVE BEEN PREPARED ACCORDING TO CRITERIA ESTABLISHED BY THE COUNTY FOR THE DETAILED ROADWAY, DRAINAGE, GRADING AND EROSION CONTROL PLANS AND SPECIFICATIONS, AND SAID PLANS AND SPECIFICATIONS ARE IN CONFORMITY WITH APPLICABLE MASTER DRAINAGE PLANS AND MASTER TRANSPORTATION PLANS. SAID PLANS AND SPECIFICATIONS MEET THE PURPOSES FOR WHICH THE PARTICULAR ROADWAY AND DRAINAGE FACILITIES ARE DESIGNED AND ARE CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF. I ACCEPT RESPONSIBILITY FOR ANY LIABILITY CAUSED BY ANY NEGLIGENT ACTS, ERRORS OR OMISSIONS ON MY PART IN PREPARATION OF THESE DETAILED PLANS AND SPECIFICATIONS.

TIM D. MCCONNELL _____ DATE _____
P.E. # 33797

OWNER'S STATEMENT:

I, THE OWNER/DEVELOPER HAVE READ AND WILL COMPLY WITH THE REQUIREMENTS OF THE GRADING AND EROSION CONTROL PLAN AND ALL OF THE REQUIREMENTS SPECIFIED IN THESE DETAILED PLANS AND SPECIFICATIONS.

STEVE ROSSOLL _____ DATE _____

EL PASO COUNTY:

COUNTY PLAN REVIEW IS PROVIDED ONLY FOR GENERAL CONFORMANCE WITH COUNTY DESIGN CRITERIA. THE COUNTY IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND/OR ELEVATIONS WHICH SHALL BE CONFIRMED AT THE JOB SITE. THE COUNTY THROUGH THE APPROVAL OF THIS DOCUMENT ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT.

FILED IN ACCORDANCE WITH THE REQUIREMENTS OF THE EL PASO COUNTY LAND DEVELOPMENT CODE, DRAINAGE CRITERIA MANUAL, AND ENGINEERING CRITERIA MANUAL AS AMENDED.

IN ACCORDANCE WITH ECM SECTION 1.12, THESE CONSTRUCTION DOCUMENTS WILL BE VALID FOR CONSTRUCTION FOR A PERIOD OF 2 YEARS FROM THE DATE SIGNED BY THE EL PASO COUNTY ENGINEER. IF CONSTRUCTION HAS NOT STARTED WITHIN THOSE 2 YEARS, THE PLANS WILL NEED TO BE RESUBMITTED FOR APPROVAL, INCLUDING PAYMENT OF REVIEW FEES AT THE PLANNING AND COMMUNITY DEVELOPMENT DIRECTOR'S DISCRETION.

JOSHUA PALMER, P.E. _____ DATE _____
COUNTY ENGINEER

AGENCY CONTACTS:

COUNTY	EL PASO COUNTY PLANNING & COMMUNITY DEVELOPMENT RYAN HOWSER, PLANNER 2880 INTERNATIONAL CIRCLE, SUITE 110 COLORADO SPRINGS, CO 80910 (719) 520-6300
FIRE	FALCON FIRE DEPARTMENT TRENT HARWIG, FIRE CHIEF 7030 OLD MERIDIAN ROAD FALCON, CO 80831 (719)495-4050
WATER	WOODMEN HILLS METROPOLITAN DISTRICT 8046 EASTONVILLE ROAD FALCON, CO 80831 (719) 495-2500
WASTEWATER	WOODMEN HILLS METROPOLITAN DISTRICT 8046 EASTONVILLE ROAD FALCON, CO 80831 (719) 495-2500
ELECTRIC	MOUNTAIN VIEW ELECTRIC ASSOCIATION LES ULFERS 11140 E. WOODMEN ROAD FALCON, CO 80831 (719) 495-2283
GAS	COLORADO SPRINGS UTILITIES TODD STURTEVANT 1521 HANCOCK EXPRESSWAY COLORADO SPRINGS, CO 80947 (719) 688-3556
TELEPHONE	CENTURY LINK SALLY KLEIN (719) 636-4329 (LOCATORS) (719) 597-8418 AT&T (LOCATORS) (719) 635-3674
CABLE	COMCAST DALE STEWART 213 N. UNION BLVD COLORADO SPRINGS, CO 80909 (719) 442-4733

SHEET INDEX

SHT1	C1	COVER SHEET
SHT2	C2	GENERAL NOTES
SHT3	C3	SURVEY CONTROL PLAN
SHT4	C4	DEMOLITION PLAN
SHT5	C5	OVERALL UTILITY
SHT6	C6	SW RETAIL ROW ST. & KNUCKLE PLAN & PROFILES
SHT7	C7	NE RETAIL ROW ST. & KNUCKLE PLAN & PROFILES
SHT8	C8	RIO LANE & NORTH KNUCKLE PLAN & PROFILES
SHT9	C9	DUNLIN HEIGHTS & JACKDAW POINT PLAN & PROFILES
SHT10	C10	PERULA POINT & CUL-DE-SAC PLAN & PROFILES
SHT11	C11	WOODMEN ROAD PLAN & PROFILES
SHT12	C12	W ROUNDABOUT C&G FLOWLINE PLAN & PROFILES
SHT13	C13	N ROUNDABOUT C&G FLOWLINE PLAN & PROFILES
SHT14	C14	SE ROUNDABOUT C&G FLOWLINE PLAN & PROFILES
SHT15	C15	SPLITTER ISLAND ROUNDABOUT PLAN
SHT16	C16	SITE PAVING & CONCRETE PLAN
SHT17	C17	SIGNAGE & STRIPING PLAN
SHT18	C18	HIGHWAY 24 WIDENING & SIGNAL PLAN
SHT19	C19	ROAD SECTIONS
SHT20	C20	ROAD DETAILS
SHT21	STM1	STORM SEWER CHANNEL P&P
SHT22	STM2	STORM SEWER CHANNEL P&P
SHT23	STM3	STORM SEWER CHANNEL P&P
SHT24	STM4	STORM SEWER CHANNEL P&P
SHT25	STM5	STORM SEWER CHANNEL P&P
SHT26	STM6	STORM SEWER CHANNEL P&P
SHT27	EX01	BOX CULVERT P&P
SHT28	DTL	BOX CULVERT DETAILS
SHT29	DTL2	DETAILS

PREPARED BY:



CLIENT:

PROTERRA PROPERTIES

1864 WOODMOOR DRIVE
MONUMENT, CO 80132
(719) 476-0800
CONTACT: STEVE ROSSOLL
rossoll@proterra.com

CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM

FILE NAME: CV&NOTES

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF
DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: N/A
VERTICAL: N/A

COVER SHEET

PROJECT NO. 21604-00CSCV
DRAWING NO.

C1

SHEET: 1 OF 29



STANDARD NOTES FOR EL PASO COUNTY GRADING AND EROSION CONTROL PLANS

- STORMWATER DISCHARGES FROM CONSTRUCTION SITES SHALL NOT CAUSE OR THREATEN TO CAUSE POLLUTION, CONTAMINATION, OR DEGRADATION OF STATE WATERS. ALL WORK AND EARTH DISTURBANCE SHALL BE DONE IN A MANNER THAT MINIMIZES POLLUTION OF ANY ON-SITE OR OFF-SITE WATERS, INCLUDING WETLANDS.
- NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS FROM REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING.
- A SEPARATE STORMWATER MANAGEMENT PLAN (SWMP) FOR THIS PROJECT SHALL BE COMPLETED AND AN EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQCP) ISSUED PRIOR TO COMMENCING CONSTRUCTION. MANAGEMENT OF THE SWMP DURING CONSTRUCTION IS THE RESPONSIBILITY OF THE DESIGNATED QUALIFIED STORMWATER MANAGER OR CERTIFIED EROSION CONTROL INSPECTOR. THE SWMP SHALL BE LOCATED ON SITE AT ALL TIMES DURING CONSTRUCTION AND SHALL BE KEPT UP TO DATE WITH WORK PROGRESS AND CHANGES IN THE FIELD.
- ONCE THE ESQCP IS APPROVED AND A 'NOTICE TO PROCEED' HAS BEEN ISSUED, THE CONTRACTOR MAY INSTALL THE INITIAL STAGE EROSION AND SEDIMENT CONTROL MEASURES AS INDICATED ON THE APPROVED GEC. A PRECONSTRUCTION MEETING BETWEEN THE CONTRACTOR, ENGINEER, AND EL PASO COUNTY WILL BE HELD PRIOR TO ANY CONSTRUCTION. IT IS THE RESPONSIBILITY OF THE APPLICANT TO COORDINATE THE MEETING TIME AND PLACE WITH COUNTY STAFF.
- CONTROL MEASURES MUST BE INSTALLED PRIOR TO COMMENCEMENT OF ACTIVITIES THAT COULD CONTRIBUTE POLLUTANTS TO STORMWATER. CONTROL MEASURES FOR ALL SLOPES, CHANNELS, DITCHES, AND DISTURBED LAND AREAS SHALL BE INSTALLED IMMEDIATELY UPON COMPLETION OF THE DISTURBANCE.
- ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE MAINTAINED AND REMAIN IN EFFECTIVE OPERATING CONDITION UNTIL PERMANENT SOIL EROSION CONTROL MEASURES ARE IMPLEMENTED AND FINAL STABILIZATION IS ESTABLISHED. ALL PERSONS ENGAGED IN LAND DISTURBANCE ACTIVITIES SHALL ASSESS THE ADEQUACY OF CONTROL MEASURES AT THE SITE AND IDENTIFY IF CHANGES TO THOSE CONTROL MEASURES ARE NEEDED TO ENSURE THE CONTINUED EFFECTIVE PERFORMANCE OF THE CONTROL MEASURES. ALL CHANGES TO TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES MUST BE INCORPORATED INTO THE STORMWATER MANAGEMENT PLAN.
- TEMPORARY STABILIZATION SHALL BE IMPLEMENTED ON DISTURBED AREAS AND STOCKPILES WHERE GROUND DISTURBING CONSTRUCTION ACTIVITY HAS PERMANENTLY CEASED OR TEMPORARILY CEASED FOR LONGER THAN 14 DAYS.
- FINAL STABILIZATION MUST BE IMPLEMENTED AT ALL APPLICABLE CONSTRUCTION SITES. FINAL STABILIZATION IS ACHIEVED WHEN ALL GROUND DISTURBING ACTIVITIES ARE COMPLETE AND ALL DISTURBED AREAS EITHER HAVE A UNIFORM VEGETATIVE COVER WITH INDIVIDUAL PLANT DENSITY OF 70 PERCENT OF PRE-DISTURBANCE LEVELS ESTABLISHED OR EQUIVALENT PERMANENT ALTERNATIVE STABILIZATION METHOD IS IMPLEMENTED. ALL TEMPORARY SEDIMENT AND EROSION CONTROL MEASURES SHALL BE REMOVED UPON FINAL STABILIZATION AND BEFORE PERMIT CLOSURE.
- ALL PERMANENT STORMWATER MANAGEMENT FACILITIES SHALL BE INSTALLED AS DESIGNED IN THE APPROVED PLANS. ANY PROPOSED CHANGES THAT AFFECT THE DESIGN OR FUNCTION OF PERMANENT STORMWATER MANAGEMENT STRUCTURES MUST BE APPROVED BY THE EGM ADMINISTRATOR PRIOR TO IMPLEMENTATION.
- EARTH DISTURBANCES SHALL BE CONDUCTED IN SUCH A MANNER SO AS TO EFFECTIVELY MINIMIZE ACCELERATED SOIL EROSION AND RESULTING SEDIMENTATION. ALL DISTURBANCES SHALL BE DESIGNED, CONSTRUCTED, AND COMPLETED SO THAT THE EXPOSED AREA OF ANY DISTURBED LAND SHALL BE LIMITED TO THE SHORTEST PRACTICAL PERIOD OF TIME. PRE-EXISTING VEGETATION SHALL BE PROTECTED AND MAINTAINED WITHIN 50 HORIZONTAL FEET OF A WATERS OF THE STATE UNLESS SHOWN TO BE INFEASIBLE AND SPECIFICALLY REQUESTED AND APPROVED.
- COMPACTION OF SOIL MUST BE PREVENTED IN AREAS DESIGNATED FOR INFILTRATION CONTROL MEASURES OR WHERE FINAL STABILIZATION WILL BE ACHIEVED BY VEGETATIVE COVER. AREAS DESIGNATED FOR INFILTRATION CONTROL MEASURES SHALL ALSO BE PROTECTED FROM SEDIMENTATION DURING CONSTRUCTION UNTIL FINAL STABILIZATION IS ACHIEVED. IF COMPACTION PREVENTION IS NOT FEASIBLE DUE TO SITE CONSTRAINTS, ALL AREAS DESIGNATED FOR INFILTRATION AND VEGETATION CONTROL MEASURES MUST BE LOOSENEED PRIOR TO INSTALLATION OF THE CONTROL MEASURE(S).
- ANY TEMPORARY OR PERMANENT FACILITY DESIGNED AND CONSTRUCTED FOR THE CONVEYANCE OF STORMWATER AROUND, THROUGH, OR FROM THE EARTH DISTURBANCE AREA SHALL BE A STABILIZED CONVEYANCE DESIGNED TO MINIMIZE EROSION AND THE DISCHARGE OF SEDIMENT OFF SITE.
- CONCRETE WASH WATER SHALL BE CONTAINED AND DISPOSED OF IN ACCORDANCE WITH THE SWMP. NO WASH WATER SHALL BE DISCHARGED TO OR ALLOWED TO ENTER STATE WATERS, INCLUDING ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR FACILITIES. CONCRETE WASHOUTS SHALL NOT BE LOCATED IN AN AREA WHERE SHALLOW GROUNDWATER MAY BE PRESENT, OR WITHIN 50 FEET OF A SURFACE WATER BODY, CREEK OR STREAM.
- DURING DEWATERING OPERATIONS OF UNCONTAMINATED GROUND WATER MAY BE DISCHARGED ON SITE, BUT SHALL NOT LEAVE THE SITE IN THE FORM OF SURFACE RUNOFF UNLESS AN APPROVED STATE DEWATERING PERMIT IS IN PLACE.
- EROSION CONTROL BLANKETING OR OTHER PROTECTIVE COVERING SHALL BE USED ON SLOPES STEEPER THAN 3:1.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL WASTES FROM THE CONSTRUCTION SITE FOR DISPOSAL IN ACCORDANCE WITH LOCAL AND STATE REGULATORY REQUIREMENTS. NO CONSTRUCTION DEBRIS, TREE SLASH, BUILDING MATERIAL WASTES OR UNUSED BUILDING MATERIALS SHALL BE BURIED, DUMPED, OR DISCHARGED AT THE SITE.
- WASTE MATERIALS SHALL NOT BE TEMPORARILY PLACED OR STORED IN THE STREET, ALLEY, OR OTHER PUBLIC WAY, UNLESS IN ACCORDANCE WITH AN APPROVED TRAFFIC CONTROL PLAN. CONTROL MEASURES MAY BE REQUIRED BY EL PASO COUNTY ENGINEERING IF DEEMED NECESSARY, BASED ON SPECIFIC CONDITIONS AND CIRCUMSTANCES.
- TRACKING OF SOILS AND CONSTRUCTION DEBRIS OFF-SITE SHALL BE MINIMIZED. MATERIALS TRACKED OFF-SITE SHALL BE CLEANED UP AND PROPERLY DISPOSED OF IMMEDIATELY.
- THE OWNER/DEVELOPER SHALL BE RESPONSIBLE FOR THE REMOVAL OF ALL CONSTRUCTION DEBRIS, DIRT, TRASH, ROCK, SEDIMENT, SOIL, AND SAND THAT MAY ACCUMULATE IN ROADS, STORM DRAINS AND OTHER DRAINAGE CONVEYANCE SYSTEMS AND STORMWATER APPURTENANCES AS A RESULT OF SITE DEVELOPMENT.
- THE QUANTITY OF MATERIALS STORED ON THE PROJECT SITE SHALL BE LIMITED, AS MUCH AS PRACTICAL, TO THAT QUANTITY REQUIRED TO PERFORM THE WORK IN AN ORDERLY SEQUENCE. ALL MATERIALS STORED ON-SITE SHALL BE STORED IN A NEAT, ORDERLY MANNER, IN THEIR ORIGINAL CONTAINERS, WITH ORIGINAL MANUFACTURER'S LABELS.
- NO CHEMICAL(S) HAVING THE POTENTIAL TO BE RELEASED IN STORMWATER ARE TO BE STORED OR USED ONSITE UNLESS PERMISSION FOR THE USE OF SUCH CHEMICAL(S) IS GRANTED IN WRITING BY THE EGM ADMINISTRATOR. IN GRANTING APPROVAL FOR THE USE OF SUCH CHEMICAL(S), SPECIAL CONDITIONS AND MONITORING MAY BE REQUIRED.
- BULK STORAGE OF ALLOWED PETROLEUM PRODUCTS OR OTHER ALLOWED LIQUID CHEMICALS IN EXCESS OF 55 GALLONS SHALL REQUIRE ADEQUATE SECONDARY CONTAINMENT PROTECTION TO CONTAIN ALL SPILLS ONSITE AND TO PREVENT ANY SPILLED MATERIALS FROM ENTERING STATE WATERS, ANY SURFACE OR SUBSURFACE STORM DRAINAGE SYSTEM OR OTHER FACILITIES.
- NO PERSON SHALL CAUSE THE IMPEDIMENT OF STORMWATER FLOW IN THE CURB AND GUTTER OR DITCH EXCEPT WITH APPROVED SEDIMENT CONTROL MEASURES.
- OWNER/DEVELOPER AND THEIR AGENTS SHALL COMPLY WITH THE 'COLORADO WATER QUALITY CONTROL ACT' (TITLE 25, ARTICLE 8, CRS), AND THE 'CLEAN WATER ACT' (33 USC 1344), IN ADDITION TO THE REQUIREMENTS OF THE LAND DEVELOPMENT CODE, DCM VOLUME II AND THE EGM APPENDIX I. ALL APPROPRIATE PERMITS MUST BE OBTAINED BY THE CONTRACTOR PRIOR TO CONSTRUCTION (0491, NPDES, FLOODPLAIN, 404, FUGITIVE DUST, ETC.). IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND OTHER LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, STATE, LOCAL, OR COUNTY AGENCIES, THE MOST RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE ONLY AT APPROVED CONSTRUCTION ACCESS POINTS.
- PRIOR TO CONSTRUCTION THE PERMITTEE SHALL VERIFY THE LOCATION OF EXISTING UTILITIES.
- A WATER SOURCE SHALL BE AVAILABLE ON SITE DURING EARTHWORK OPERATIONS AND SHALL BE UTILIZED AS REQUIRED TO MINIMIZE DUST FROM EARTHWORK EQUIPMENT AND WIND.
- THE SOILS REPORT FOR THIS SITE HAS BEEN PREPARED BY GROUND ENGINEERING, DATED AUGUST 25, 2015, WITH ADDENDA #1, DATED MARCH 17, 2017, AND SHALL BE CONSIDERED A PART OF THESE PLANS.
- AT LEAST TEN (10) DAYS PRIOR TO THE ANTICIPATED START OF CONSTRUCTION, FOR PROJECTS THAT WILL DISTURB ONE (1) ACRE OR MORE, THE OWNER OR OPERATOR OF CONSTRUCTION ACTIVITY SHALL SUBMIT A PERMIT APPLICATION FOR STORMWATER DISCHARGE TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT, WATER QUALITY DIVISION. THE APPLICATION CONTAINS CERTIFICATION OF COMPLETION OF A STORMWATER MANAGEMENT PLAN (SWMP), OF WHICH THIS GRADING AND EROSION CONTROL PLAN MAY BE A PART. FOR INFORMATION OR APPLICATION MATERIALS CONTACT:

COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT
 WATER QUALITY CONTROL DIVISION
 WQCD - PERMITS
 4300 CHERRY CREEK DRIVE SOUTH
 DENVER, CO 80246-1530
 ATTN: PERMITS UNIT

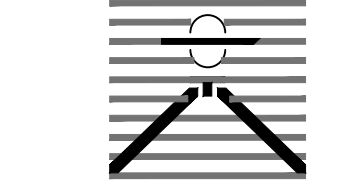
STANDARD NOTES FOR EL PASO COUNTY CONSTRUCTION PLANS

- ALL DRAINAGE AND ROADWAY CONSTRUCTION SHALL MEET THE STANDARDS AND SPECIFICATIONS OF THE CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2, AND THE EL PASO COUNTY ENGINEERING CRITERIA MANUAL.
- CONTRACTOR SHALL BE RESPONSIBLE FOR THE NOTIFICATION AND FIELD NOTIFICATION OF ALL EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, BEFORE BEGINNING CONSTRUCTION. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CALL 811 TO CONTACT THE UTILITY NOTIFICATION CENTER OF COLORADO (UNCC).
- CONTRACTOR SHALL KEEP A COPY OF THESE APPROVED PLANS, THE GRADING AND EROSION CONTROL PLAN, THE STORMWATER MANAGEMENT PLAN (SWMP), THE SOILS AND GEOTECHNICAL REPORT, AND THE APPROPRIATE DESIGN AND CONSTRUCTION STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES, INCLUDING THE FOLLOWING:
 a. EL PASO COUNTY ENGINEERING CRITERIA MANUAL (ECM)
 b. CITY OF COLORADO SPRINGS/EL PASO COUNTY DRAINAGE CRITERIA MANUAL, VOLUMES 1 AND 2
 c. COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
 d. CDOT M & S STANDARDS
- NOTWITHSTANDING ANYTHING DEPICTED IN THESE PLANS IN WORDS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE, THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL, AND THE DRAINAGE CRITERIA MANUAL VOLUME 2. ANY DEVIATIONS FROM REGULATIONS AND STANDARDS MUST BE REQUESTED, AND APPROVED, IN WRITING. ANY MODIFICATIONS NECESSARY TO MEET CRITERIA AFTER-THE-FACT WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- IT IS THE DESIGN ENGINEER'S RESPONSIBILITY TO ACCURATELY SHOW EXISTING CONDITIONS, BOTH ONSITE AND OFFSITE, ON THE CONSTRUCTION PLANS. ANY MODIFICATIONS NECESSARY DUE TO CONFLICTS, OMISSIONS, OR CHANGED CONDITIONS WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY TO RECTIFY.
- CONTRACTOR SHALL SCHEDULE A PRE-CONSTRUCTION MEETING WITH EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (PCD) - INSPECTIONS, PRIOR TO STARTING CONSTRUCTION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO UNDERSTAND THE REQUIREMENTS OF ALL JURISDICTIONAL AGENCIES AND TO OBTAIN ALL REQUIRED PERMITS, INCLUDING BUT NOT LIMITED TO EL PASO COUNTY EROSION AND STORMWATER QUALITY CONTROL PERMIT (ESQCP), REGIONAL BUILDING FLOODPLAIN DEVELOPMENT PERMIT, U.S. ARMY CORPS OF ENGINEERS-ISSUED 401 AND/OR 404 PERMITS, AND COUNTY AND STATE FUGITIVE DUST PERMITS.
- CONTRACTOR SHALL NOT DEVIATE FROM THE PLANS WITHOUT FIRST OBTAINING WRITTEN APPROVAL FROM THE DESIGN ENGINEER AND PCD. CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER IMMEDIATELY UPON DISCOVERY OF ANY ERRORS OR INCONSISTENCIES.
- ALL STORM DRAIN PIPE SHALL BE CLASS III RCP UNLESS OTHERWISE NOTED AND APPROVED BY PCD. WATERTIGHT JOINTS SHALL BE PROVIDED ON THE SPECIFIED PIPES.
- CONTRACTOR SHALL COORDINATE GEOTECHNICAL TESTING PER ECM STANDARDS. PAVEMENT DESIGN SHALL BE APPROVED BY EL PASO COUNTY PCD PRIOR TO PLACEMENT OF CURB AND GUTTER AND PAVEMENT.
- ALL CONSTRUCTION TRAFFIC MUST ENTER/EXIT THE SITE AT APPROVED CONSTRUCTION ACCESS POINTS.
- SIGHT VISIBILITY TRIANGLES AS IDENTIFIED IN THE PLANS SHALL BE PROVIDED AT ALL INTERSECTIONS. OBSTRUCTIONS GREATER THAN 18 INCHES ABOVE FLOWLINE ARE NOT ALLOWED WITHIN SIGHT TRIANGLES.
- SIGNING AND STRIPING SHALL COMPLY WITH EL PASO COUNTY DOT AND MUTCD CRITERIA. [IF APPLICABLE, ADDITIONAL SIGNING AND STRIPING NOTES WILL BE PROVIDED.]
- CONTRACTOR SHALL OBTAIN ANY PERMITS REQUIRED BY EL PASO COUNTY DOT, INCLUDING WORK WITHIN THE RIGHT-OF-WAY AND SPECIAL TRANSPORT PERMITS.
- THE LIMITS OF CONSTRUCTION SHALL REMAIN WITHIN THE PROPERTY LINE UNLESS OTHERWISE NOTED. THE OWNER/DEVELOPER SHALL OBTAIN WRITTEN PERMISSION AND EASEMENTS, WHERE REQUIRED, FROM ADJOINING PROPERTY OWNER(S) PRIOR TO ANY OFF-SITE DISTURBANCE, GRADING, OR CONSTRUCTION.

SIGNAGE AND STRIPING NOTES

- SIGNING AND STRIPING NOTES:
- ALL SIGNS AND PAVEMENT MARKINGS SHALL BE IN COMPLIANCE WITH THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
 - REMOVAL OF EXISTING PAVEMENT MARKINGS SHALL BE ACCOMPLISHED BY A METHOD THAT DOES NOT MATERIALLY DAMAGE THE PAVEMENT. THE PAVEMENT MARKINGS SHALL BE REMOVED TO THE EXTENT THAT THEY WILL NOT BE VISIBLE UNDER DAY OR NIGHT CONDITIONS. AT NO TIME WILL IT BE ACCEPTABLE TO PAINT OVER EXISTING PAVEMENT MARKINGS.
 - ANY DEVIATION FROM THE STRIPING AND SIGNING PLAN SHALL BE APPROVED BY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT.
 - ALL SIGNS SHOWN ON THE SIGNING AND STRIPING PLAN SHALL BE NEW SIGNS. EXISTING SIGNS MAY REMAIN OR BE REUSED IF THEY MEET CURRENT EL PASO COUNTY AND MUTCD STANDARDS.
 - STREET NAME AND REGULATORY STOP SIGNS SHALL BE ON THE SAME POST AT INTERSECTIONS.
 - ALL REMOVED SIGNS SHALL BE DISPOSED OF IN A PROPER MANNER BY THE CONTRACTOR.
 - ALL STREET NAME SIGNS SHALL HAVE 'D' SERIES LETTERS, WITH LOCAL ROADWAY SIGNS BEING 4" UPPER-LOWER CASE LETTERING ON 8" BLANK AND NON-LOCAL ROADWAY SIGNS BEING 6" LETTERING, UPPER-LOWER CASE ON 12" BLANK, WITH A WHITE BORDER THAT IS NOT RECESSED. MULTI-LANE ROADWAYS WITH SPEED LIMITS OF 35 MPH OR HIGHER SHALL HAVE 8" UPPER-LOWER CASE LETTERING ON 12" BLANK WITH A WHITE BORDER THAT IS NOT RECESSED. THE WIDTH OF THE NON-RECESSED WHITE BORDERS SHALL MATCH PAGE 255 OF THE 2012 MUTCD "STANDARD HIGHWAY SIGNS". SIGNAL POLE MOUNTED AND OVERHEAD STREET NAME SIGNS SHALL BE SIZED TO "MUTCD" SIZE STANDARDS.
 - ALL TRAFFIC SIGNS SHALL HAVE A MINIMUM HIGH INTENSITY PRISMATIC GRADE SHEETING.
 - ALL LOCAL RESIDENTIAL STREET SIGNS SHALL BE MOUNTED ON A 1.75" X 1.75" SQUARE TUBE SIGN POST AND STUB POST BASE. FOR OTHER APPLICATIONS, REFER TO THE CDOT STANDARD S-614-8 REGARDING USE OF THE P2 TUBULAR STEEL POST SLIPBASE DESIGN.
 - ALL SIGNS SHALL BE SINGLE SHEET ALUMINUM WITH 0.100" MINIMUM THICKNESS.
 - ALL LIMIT LINES/STOP LINES, CROSSWALK LINES, PAVEMENT LEGENDS, AND ARROWS SHALL BE A MINIMUM 125 MIL THICKNESS PREFORMED THERMOPLASTIC PAVEMENT MARKINGS WITH TAPERED LEADING EDGES PER CDOT STANDARD S-627-1. STOP BARS SHALL BE 24" IN WIDTH. CROSSWALKS LINES SHALL BE 24" WIDE AND 9' LONG.
 - WORD AND SYMBOL MARKINGS SHALL BE THE NARROW TYPE.
 - ALL LONGITUDINAL LINES SHALL BE A MINIMUM 15MIL THICKNESS EPOXY PAINT. ALL NON-LOCAL RESIDENTIAL ROADWAYS SHALL INCLUDE BOTH RIGHT AND LEFT EDGE LINE STRIPING AND ANY ADDITIONAL STRIPING AS REQUIRED BY CDOT S-627-1.
 - THE CONTRACTOR SHALL NOTIFY EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT (719) 520-6819 PRIOR TO AND UPON COMPLETION OF SIGNING AND STRIPING.
 - THE CONTRACTOR SHALL OBTAIN A WORK IN THE RIGHT OF WAY PERMIT FROM THE EL PASO COUNTY DEPARTMENT OF PUBLIC WORKS (DPW) PRIOR TO ANY SIGNAGE OR STRIPING WORK WITHIN AN EXISTING EL PASO COUNTY ROADWAY.

PREPARED BY:



DREXEL, BARRELL & CO.
 Engineers & Surveyors
 101 SAWATCH ST., SUITE 100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
 (719)260-0887
 COLORADO SPRINGS • LAFAYETTE

CLIENT:

PROTERRA PROPERTIES

1864 WOODMOOR DRIVE
 MONUMENT, CO 80132
 (719) 476-0800
 CONTACT: STEVE ROSSOLL
 srossoll@proterraco.com

CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
 12445 RIO LANE, AND VACANT LAND
 PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24
DESIGNED BY:	KGV
DRAWN BY:	SDM
CHECKED BY:	TDM
FILE NAME:	CV&NOTES

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF
DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: N/A
 VERTICAL: N/A

GENERAL NOTES

PROJECT NO. 21604-00CSV
 DRAWING NO.

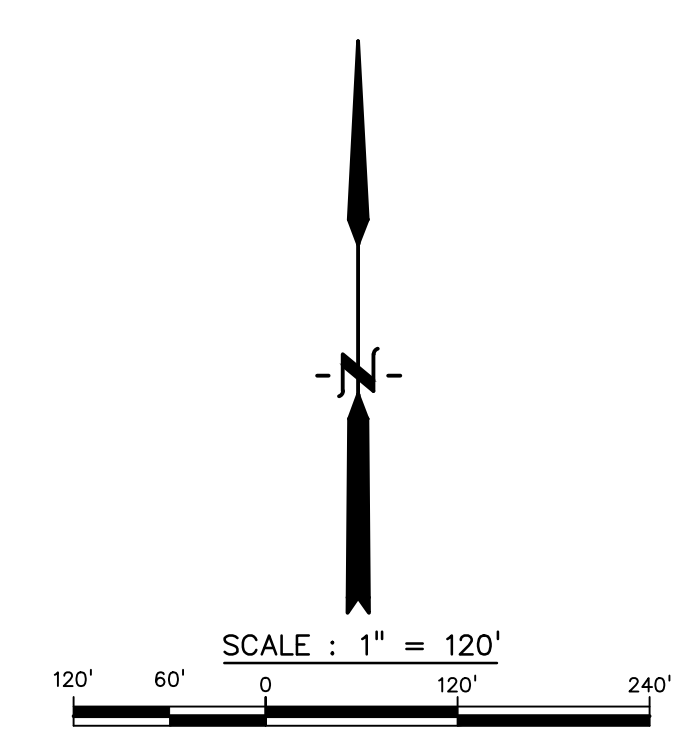
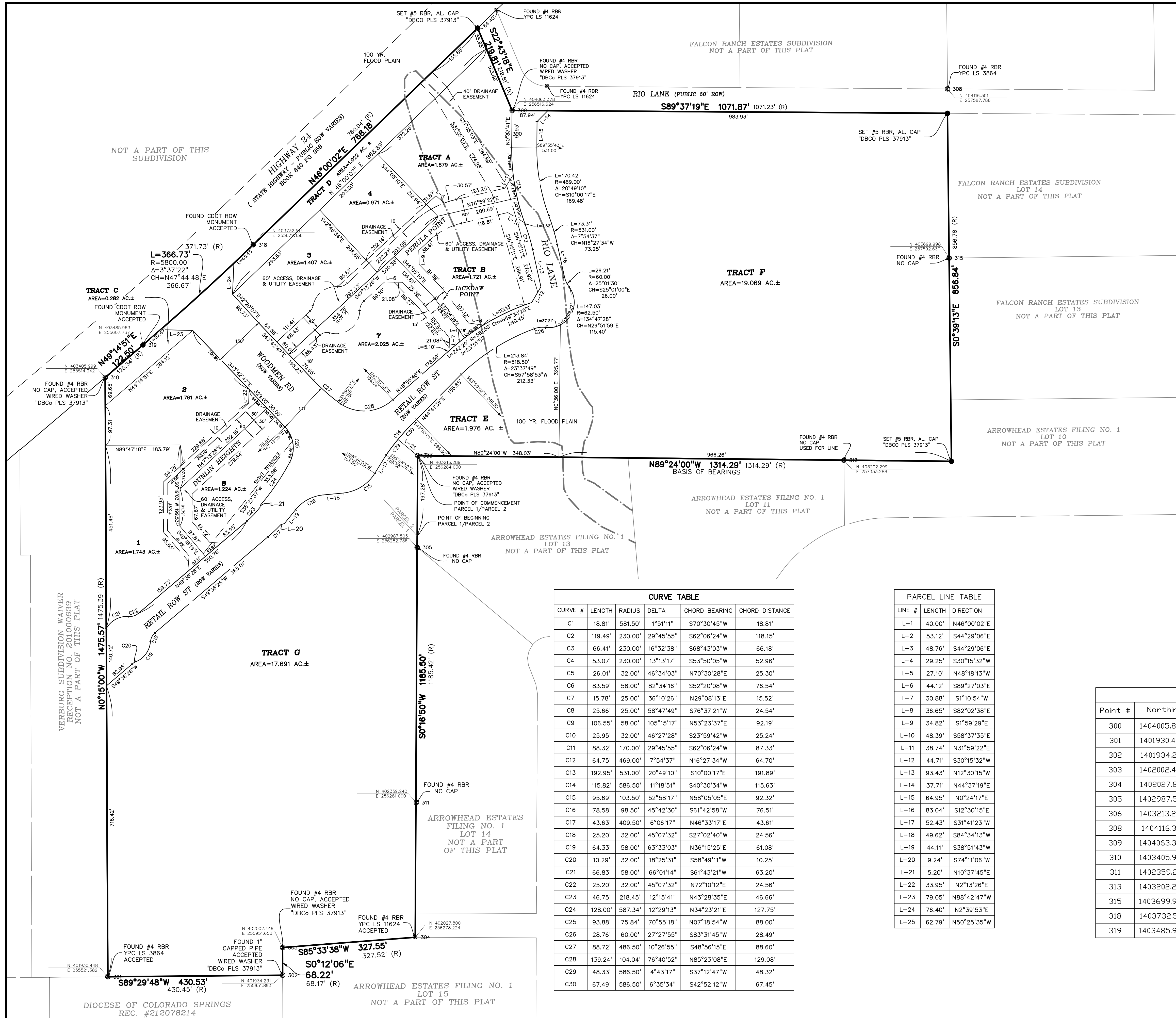
C2

SHEET: 2 OF 29



Know what's below.
 Call before you dig.

CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.



LEGEND

- FOUND #4 REBAR WITH 1" YELLOW PLASTIC CAP PLS 3854
- FOUND #4 REBAR-NO CAP
- FOUND #4 REBAR WITH 1" YELLOW PLASTIC CAP PLS 11624
- FOUND 3.25" CDDT ROW MONUMENT
- SET #5 REBAR & 1.5" ALUMINUM CAP "DBCO PLS 37913"
- FOUND 1" DIAMETER CAPPED PIPE
- RECORD DIMENSION (R)
- RECEPTION NUMBER Rec. No.
- PLUS OR MINUS (MORE OR LESS) +/-
- RIGHT OF WAY ROW
- ACRE AC
- PROPOSED UTILITY, ACCESS, AND DRAINAGE EASEMENT
- PROPOSED DRAINAGE EASEMENT
- PROPOSED DRAINAGE & UTILITY EASEMENT
- PROPOSED RIGHT-OF-WAY LINE
- SUBDIVISION BOUNDARY
- SITE TRIANGLE EASEMENT LINES
- EXISTING R.O.W.
- R.O.W. PER ROAD BOOK A, PAGE 78

CURVE TABLE

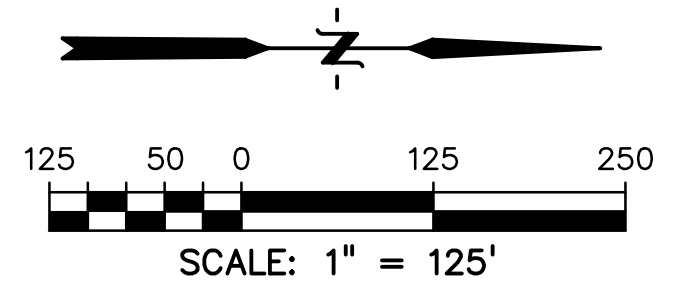
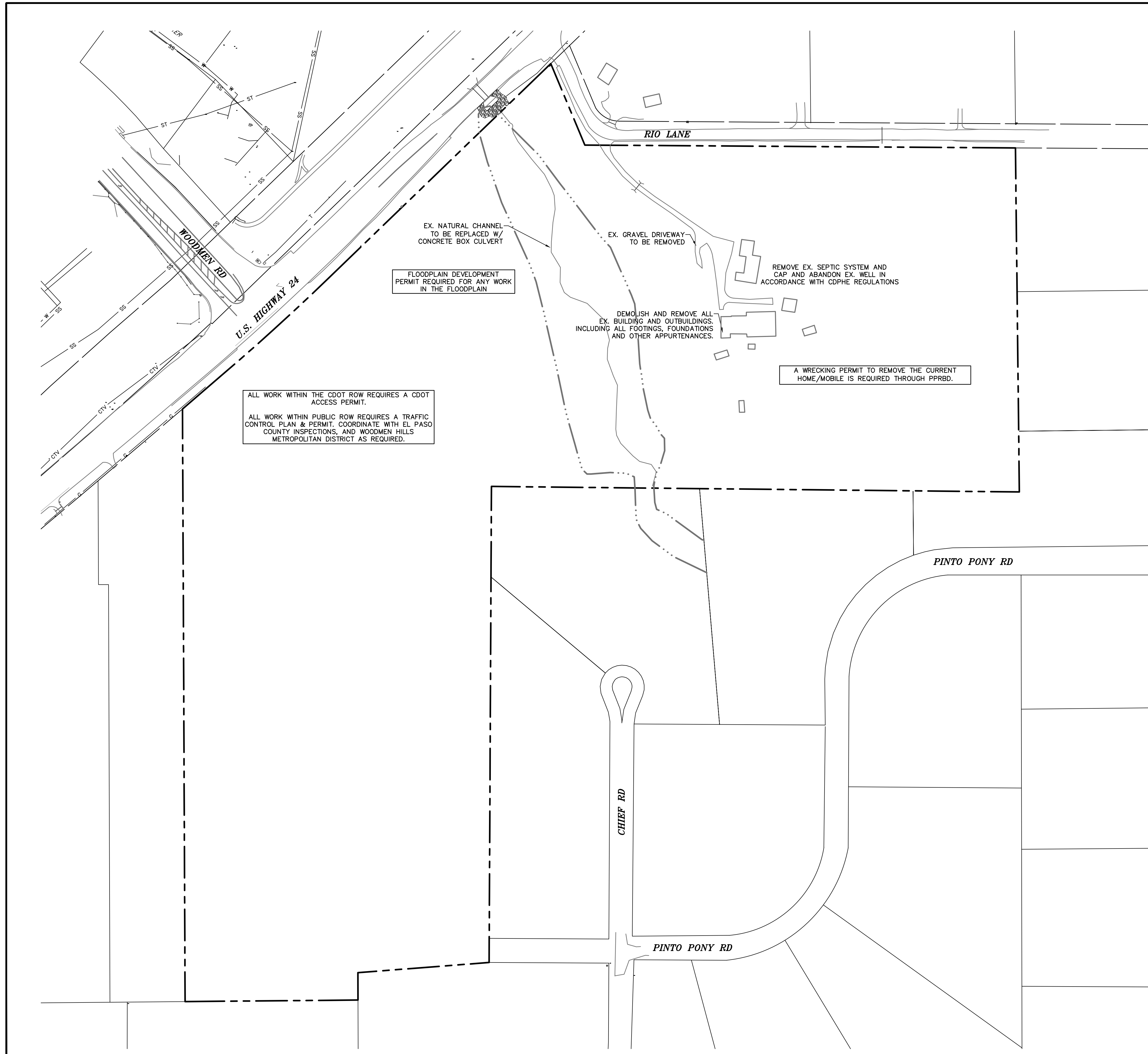
CURVE #	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD DISTANCE
C1	18.81'	581.50'	1°51'11"	S70°30'45"W	18.81'
C2	119.49'	230.00'	29°45'55"	S62°06'24"W	118.15'
C3	66.41'	230.00'	16°32'38"	S68°43'03"W	66.18'
C4	53.07'	230.00'	13°13'17"	S53°50'05"W	52.96'
C5	26.01'	32.00'	46°34'03"	N70°30'28"E	25.30'
C6	83.59'	58.00'	82°34'16"	S52°20'08"W	76.54'
C7	15.78'	25.00'	36°10'26"	N29°08'13"E	15.52'
C8	25.66'	25.00'	58°47'49"	S76°37'21"W	24.54'
C9	106.55'	58.00'	105°15'17"	N53°23'37"E	92.19'
C10	25.95'	32.00'	46°27'28"	S23°59'42"W	25.24'
C11	88.32'	170.00'	29°45'55"	S62°06'24"W	87.33'
C12	64.75'	469.00'	7°54'37"	N16°27'34"W	64.70'
C13	192.95'	531.00'	20°49'10"	S10°00'17"E	191.89'
C14	115.82'	586.50'	11°18'51"	S40°30'34"W	115.63'
C15	95.69'	103.50'	52°58'17"	N58°05'05"E	92.32'
C16	78.58'	98.50'	45°42'30"	S61°42'58"W	76.51'
C17	43.63'	409.50'	6°06'17"	N46°33'17"E	43.61'
C18	25.20'	32.00'	45°07'32"	S27°02'40"W	24.56'
C19	64.33'	58.00'	63°33'03"	N36°15'25"E	61.08'
C20	10.29'	32.00'	18°25'31"	S58°49'11"W	10.25'
C21	66.83'	58.00'	66°01'14"	S61°43'21"W	63.20'
C22	25.20'	32.00'	45°07'32"	N72°10'12"E	24.56'
C23	46.75'	218.45'	12°15'41"	N43°28'35"E	46.66'
C24	128.00'	587.34'	12°29'13"	N34°23'21"E	127.75'
C25	93.88'	75.84'	70°55'18"	N07°18'54"W	88.00'
C26	28.76'	60.00'	27°27'55"	S83°31'45"W	28.49'
C27	88.72'	486.50'	10°26'55"	S48°56'15"E	88.60'
C28	139.24'	104.04'	76°40'52"	N85°23'08"E	129.08'
C29	48.33'	586.50'	4°43'17"	S37°12'47"W	48.32'
C30	67.49'	586.50'	6°35'34"	S42°52'12"W	67.45'

PARCEL LINE TABLE

LINE #	LENGTH	DIRECTION
L-1	40.00'	N46°00'02"E
L-2	53.12'	S44°29'06"E
L-3	48.76'	S44°29'06"E
L-4	29.25'	S30°15'32"W
L-5	27.10'	N48°18'13"W
L-6	44.12'	S89°27'03"E
L-7	30.88'	S1°10'54"W
L-8	36.65'	S82°02'38"E
L-9	34.82'	S1°59'29"E
L-10	48.39'	S58°37'35"E
L-11	38.74'	N31°59'22"E
L-12	44.71'	S30°15'32"W
L-13	93.43'	N12°30'15"W
L-14	37.71'	N44°37'19"E
L-15	64.95'	N0°24'17"E
L-16	83.04'	S12°30'15"E
L-17	52.43'	S31°41'23"W
L-18	49.62'	S84°34'13"W
L-19	44.11'	S38°51'43"W
L-20	9.24'	S74°11'06"W
L-21	5.20'	N10°37'45"E
L-22	33.95'	N2°13'26"E
L-23	79.05'	N88°42'47"W
L-24	76.40'	N2°39'53"E
L-25	62.79'	N50°25'35"W

Point Table

Point #	Northing	Easting	Elevation	Row Description
300	1404005.8770	3256504.2010	6849.064	REBAR W CAP
301	1401930.4480	3255521.3820	6810.361	#4RBR YPC 3864
302	1401934.2310	3255951.8930	6799.934	1" CAPPED PIPE
303	1402002.4460	3255951.6530	6802.954	#4RBR ND CAP
304	1402027.8000	3256278.2240	6804.822	#4RBR YPC 11624
305	1402987.5050	3256282.7360	6832.730	#4RBR ND CAP
306	1403213.2890	3256284.0300	6837.434	#4RBR ND CAP
308	1404116.3010	3257587.7880	6836.615	#4RBR YPC 3864
309	1404063.3780	3256516.6240	6850.430	#4RBR ND CAP
310	1403405.9990	3255514.9420	6858.376	#4RBR ND CAP
311	1402359.2400	3256281.0000	6822.561	#4RBR ND CAP
313	1403202.2990	3257333.2880	6827.884	#4RBR ND CAP
315	1403699.9980	3257592.6300	6828.655	#4RBR ND CAP
318	1403732.5140	3255879.1380	6855.034	CDDT ROW MONUMENT
319	1403485.9630	3255607.7370	6857.813	CDDT ROW MONUMENT



LEGEND

- PROPERTY LINE..... - - - - -
- EX. OVERHEAD ELECTRIC..... - - - - - OE
- EX. SANITARY SEWER..... - - - - - SS
- EX. GAS..... - - - - - G
- EX. WATER..... - - - - - W
- EX. STORM SEWER..... - - - - - ST

PREPARED BY:



DREXEL, BARRELL & CO.
 Engineers & Surveyors
 101 SAWATCH ST., SUITE 100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
 (719) 476-0800
 (719) 260-0887
 COLORADO SPRINGS • LAFAYETTE

CLIENT:

PROTERRA PROPERTIES
 1864 WOODMOOR DRIVE
 MONUMENT, CO 80132
 (719) 476-0800
 CONTACT: STEVE ROSSOLL
 srossoll@proterraco.com

CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
 12445 RIO LANE, AND VACANT LAND
 PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
 DRAWN BY: KGV
 CHECKED BY: TDM
 FILE NAME: 21604-DMO

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF
DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: 1" = 40'
 VERTICAL: N/A

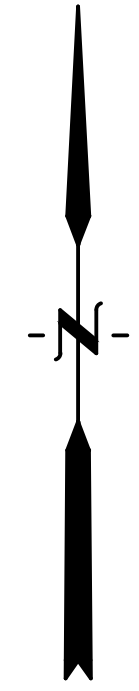
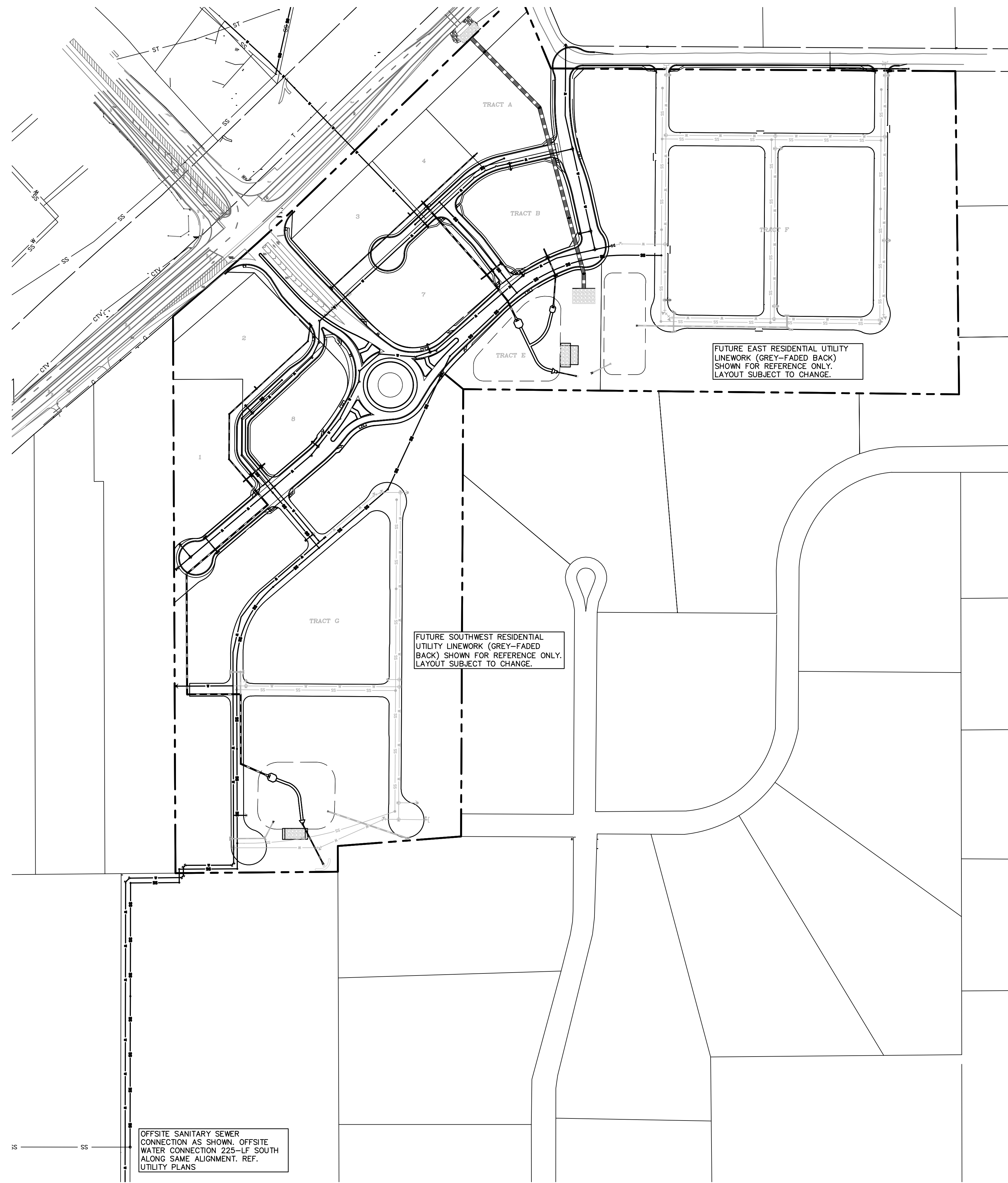
EXISTING CONDITIONS & SITE DEMO

PROJECT NO. 21604-00CSCV
 DRAWING NO.

C3

SHEET: 3 OF 29

811 Know what's below.
 Call before you dig.
 CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.



LEGEND

- PROPERTY LINE - - - - -
- EASEMENT - - - - -
- PP RIGHT OF WAY - - - - -
- PP LOT LINE - - - - -
- PP CURB & GUTTER - - - - -
- PP SIDEWALK - - - - -
- PP STORM SEWER LINE - - - - -
- PP STORM INLET - - - - -
- PP MANHOLE - - - - -
- PP WATER LINE - - - - -
- PP SANITARY SEWER LINE - - - - -
- LOT NUMBER 26
- PP POND - - - - -

PREPARED BY:



DREXEL, BARRELL & CO.
 Engineers & Surveyors
 101 SAWATCH ST., SUITE 100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. McCONNELL, P.E.
 (719) 476-0800
 (719) 260-0887
 COLORADO SPRINGS • LAFAYETTE

CLIENT:

PROTERRA PROPERTIES
 1864 WOODMOOR DRIVE
 MONUMENT, CO 80132
 (719) 476-0800
 CONTACT: STEVE ROSSOLL
 srossoll@proterraco.com

CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
 12445 RIO LANE, AND VACANT LAND
 PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
 DRAWN BY: CGH
 CHECKED BY: KGV
 FILE NAME: 21604-00UTIL-OVR

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: 1" = 150'
 VERTICAL: N/A

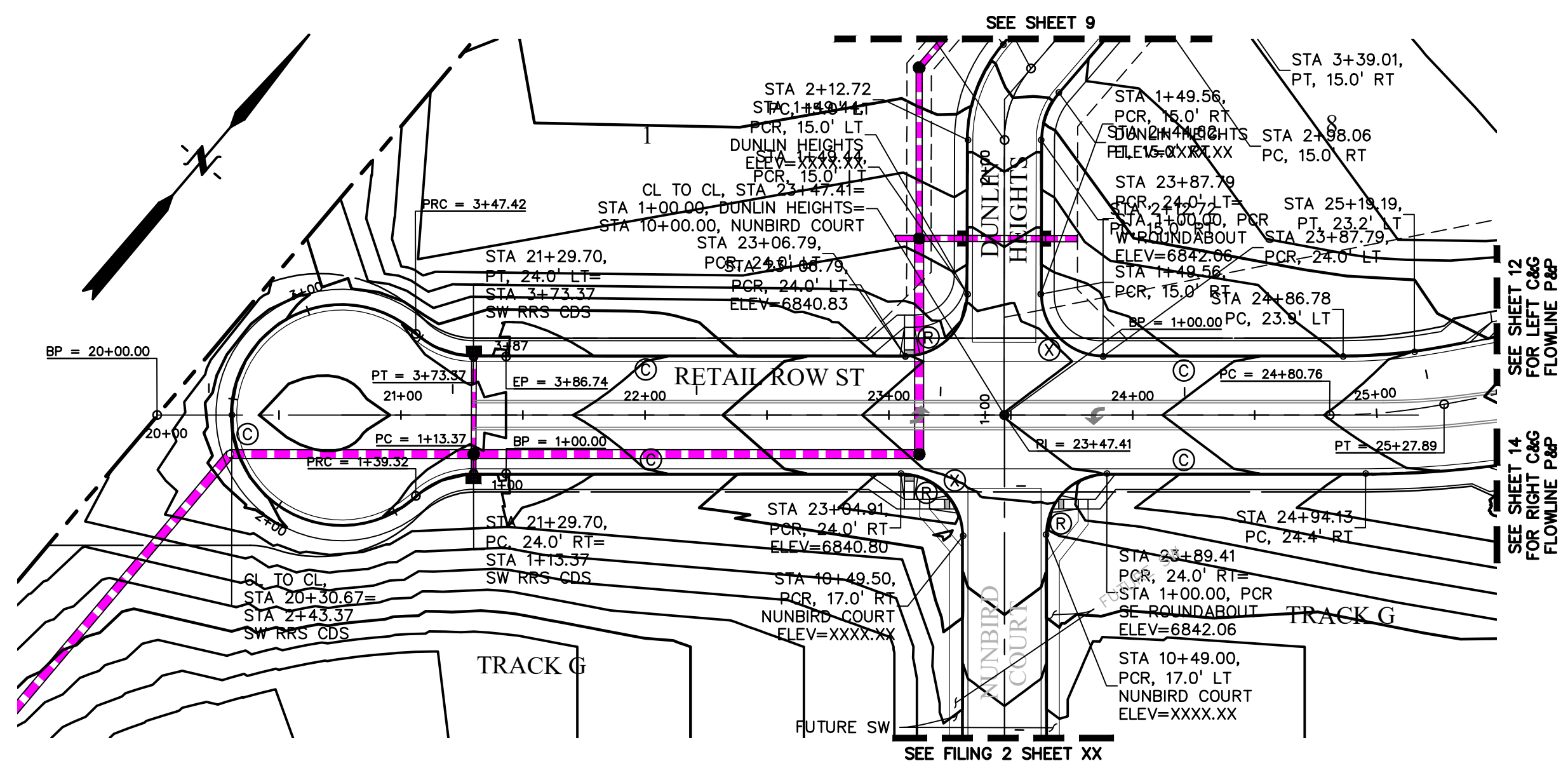
OVERALL UTILITY PLAN

PROJECT NO. 21604-00CSCV
 DRAWING NO.

OVR

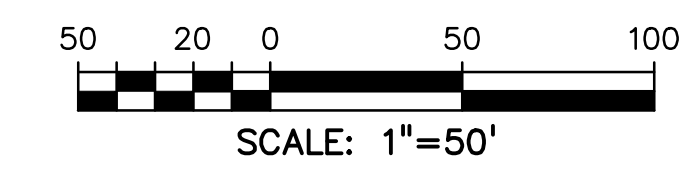
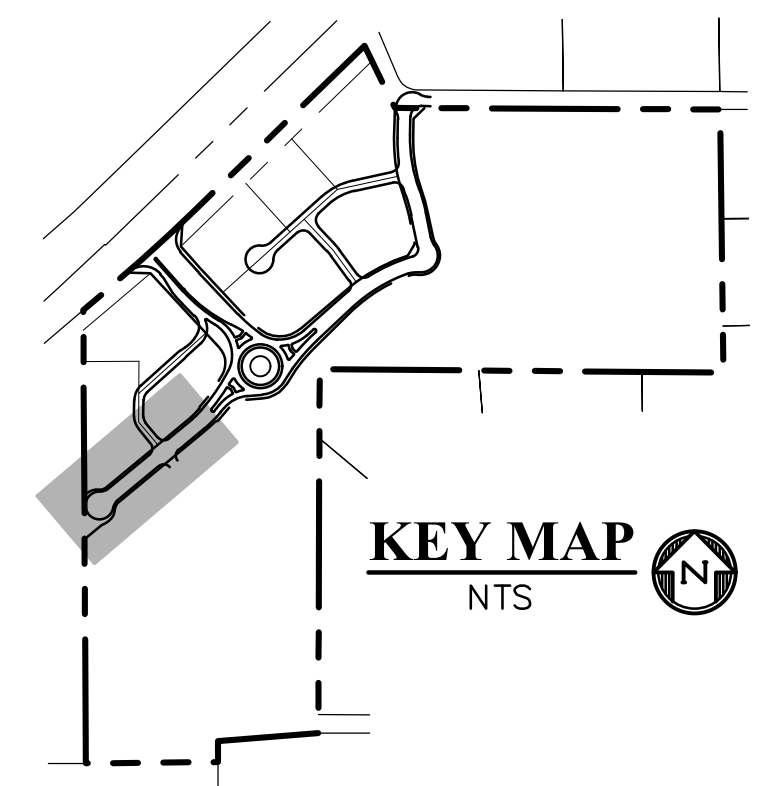
SHEET: 5 OF 29





SOUTHWEST RETAIL ROW ST. LINE TABLE		
LINE #	LENGTH	DIRECTION

SOUTHWEST RETAIL ROW ST. CURVE TABLE			
CURVE #	LENGTH	RADIUS	DELTA



PREPARED BY:



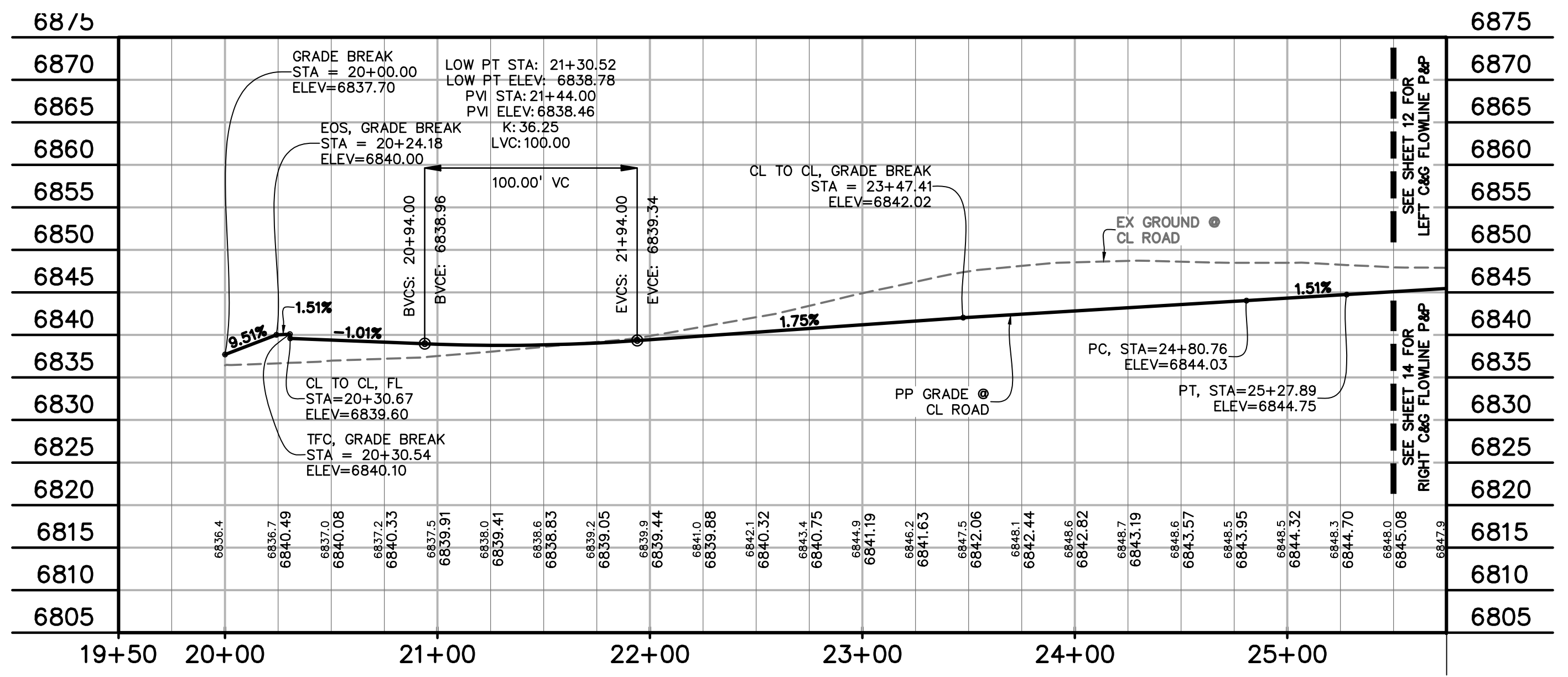
DREXEL, BARRELL & CO.
Engineers-Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719) 260-0887
COLORADO SPRINGS • LAFAYETTE

CLIENT:

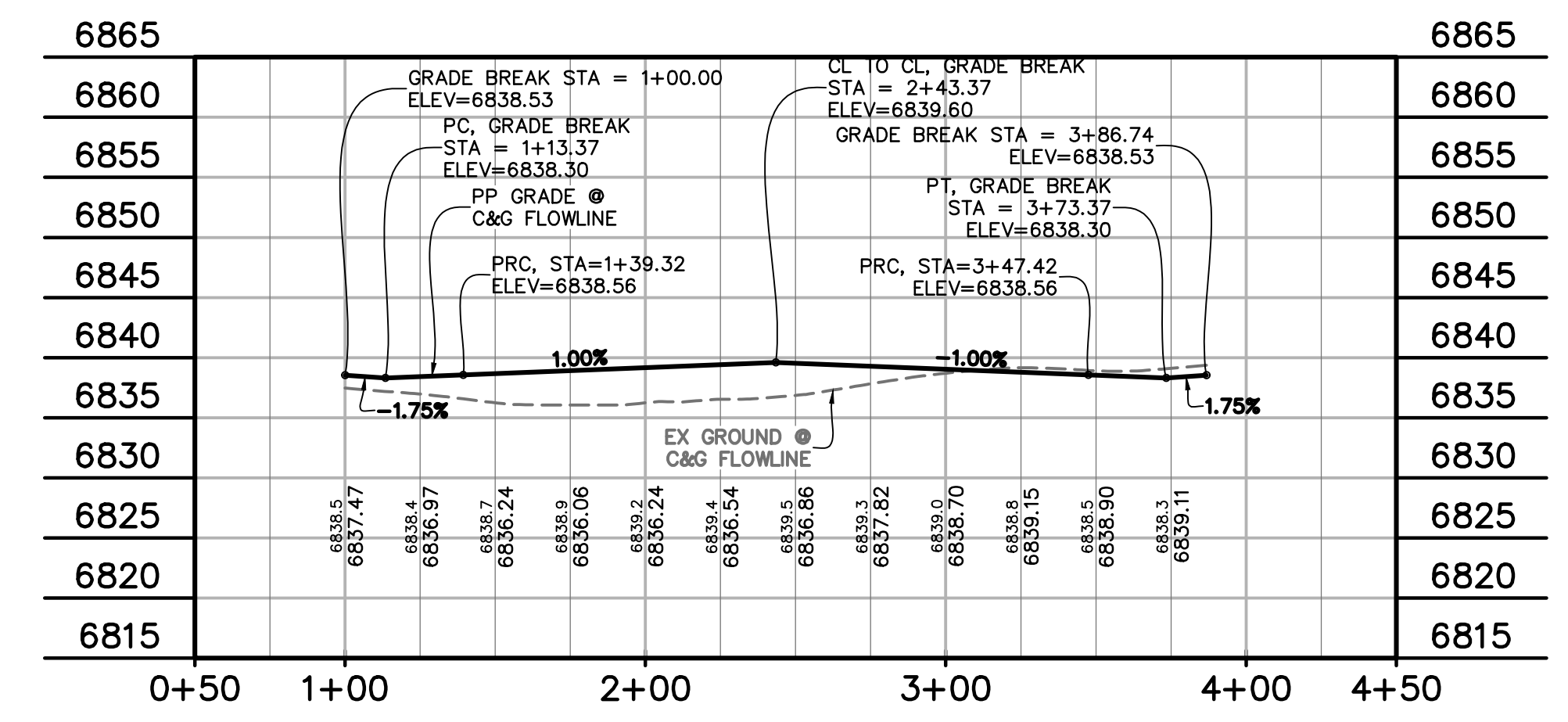
PROTERRA PROPERTIES
1864 WOODMOOR DRIVE
MONUMENT, CO 80132
(719) 476-0800
CONTACT: STEVE ROSSOLL
rossoll@proterraco.com

CONSTRUCTION DOCUMENTS FOR:

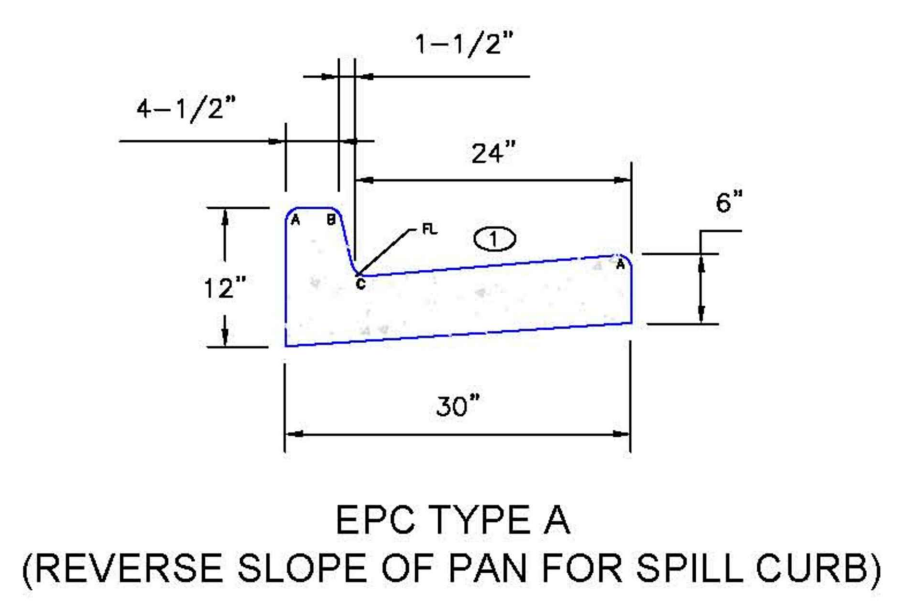
THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO



SOUTHWEST RETAIL ROW ST.
STA 20+00.00 - 25+27.89
SCALE: 1"=50'



SOUTHWEST RETAIL ROW ST. CUL-DE-SAC
STA 1+00.00 - 3+86.74
SCALE: 1"=50'



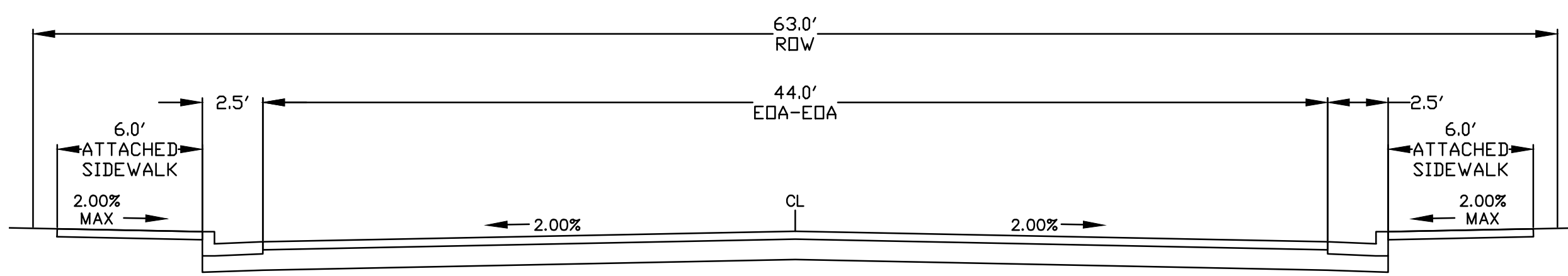
Ⓞ - GUTTER CROSS SLOPES SHALL BE 1/2 IN./FT. WHEN DRAINING AWAY FROM CURB AND 1 IN./FT. WHEN DRAINING TOWARD CURB.

LEGEND FOR RADI	
A	1/8" TO 1/4"
B	1-1/2"
C	1-1/2" TO 2"

LEGEND

- PROPERTY LINE.....
- RIGHT OF WAY.....
- LOT LINE.....
- CENTERLINE ROAD.....
- EASEMENT.....
- EASEMENT (UTILITY).....
- SIDEWALK.....
- POND.....
- TRACT LINE.....
- PROPOSED SLOPE GRADE..... -2.0%
- PROPOSED MAJOR CONTOUR..... 6205
- PROPOSED MINOR CONTOUR..... 6203
- EXISTING MAJOR CONTOUR..... 6205
- EXISTING MINOR CONTOUR..... 6203
- LOT NUMBER..... 7
- EXISTING..... EX
- PROPOSED..... PP
- PEDESTRIAN RAMP (SEE SHT 20 FOR DETAILS)..... Ⓡ
- EPC TYPE A C&G (CATCH)..... Ⓞ
- EPC TYPE A C&G (SPILL)..... Ⓢ
- 8' CROSSSPAN (SEE SHT 20 FOR DETAILS)..... ⓧ
- CURB & GUTTER..... C&G
- TOP FRONT CURB..... TFC
- EDGE OF SIDEWALK..... EOS
- CENTERLINE..... CL
- PLAN & PROFILE..... P&P
- SOUTHWEST RETAIL ROW ST. CUL-DE-SAC..... SW RRS CDS
- SOUTHEAST..... SE
- WEST..... W
- SHEET..... SHT
- SIDEWALK..... SW

NOTE: UTILITY LOCATIONS FOR ALL STREETS ARE TO BE CONFIRMED AT FINAL PLAT AND CONSTRUCTION DOCUMENT STAGE. CONFIGURATION GENERALLY FOLLOWS SANITARY SEWER ALONG CENTERLINE, WATERLINE TO THE NORTH AND EAST AND STORM SEWER TO THE SOUTH AND WEST OF THE ROADWAY



NOTE: PER APPROVED DEVIATION
PCD FILE NO. DEV238

RETAIL ROW ST.
(MODIFIED URBAN NON-RESIDENTIAL COLLECTOR - PUBLIC 63' ROW)
SCALE: 1"=5'

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM
FILE NAME: COMM RD

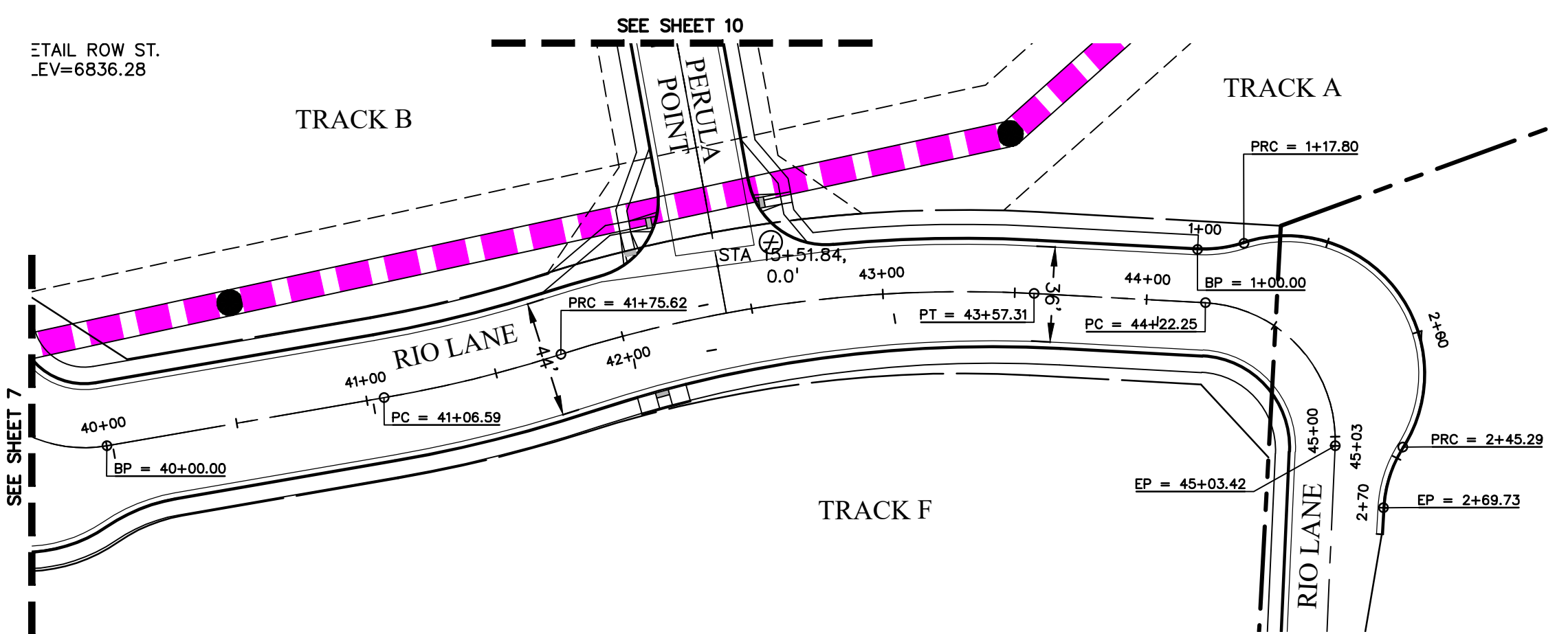
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: 1" = 50'
VERTICAL: 1" = 12.5'

SW RETAIL ROW ST. & CUL-DE-SAC
PLAN & PROFILE

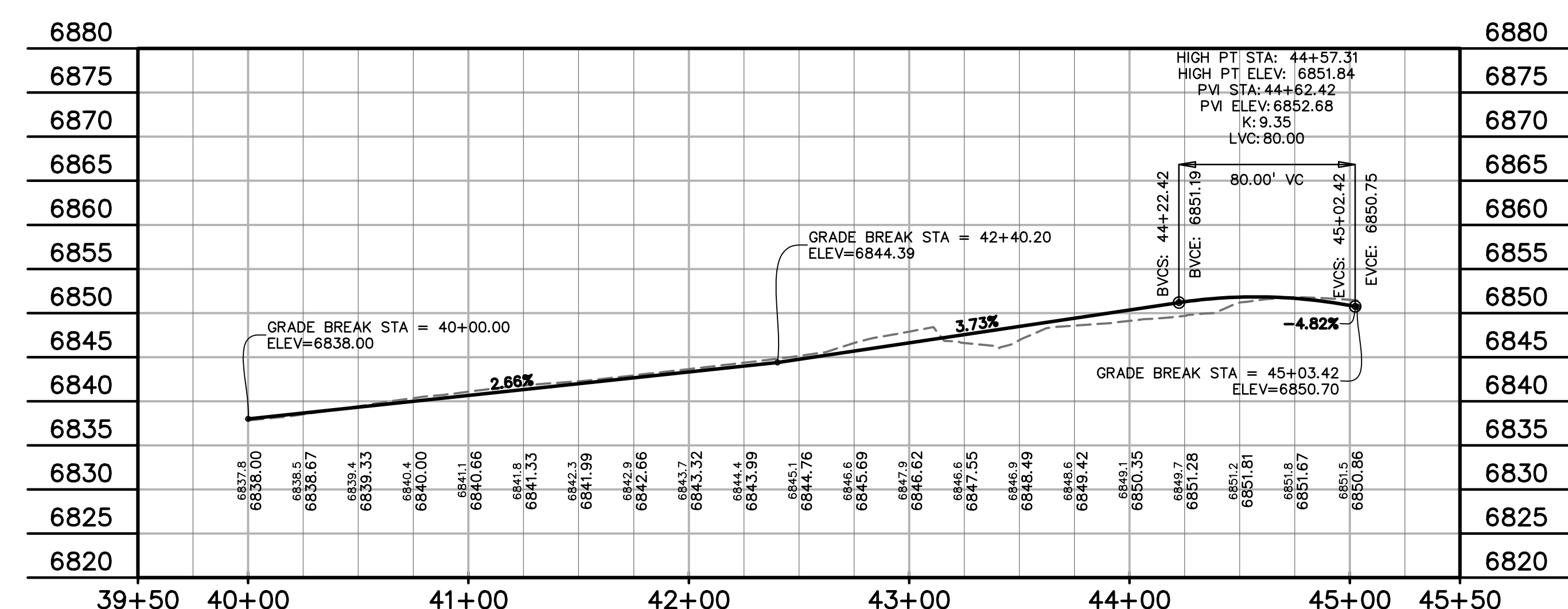
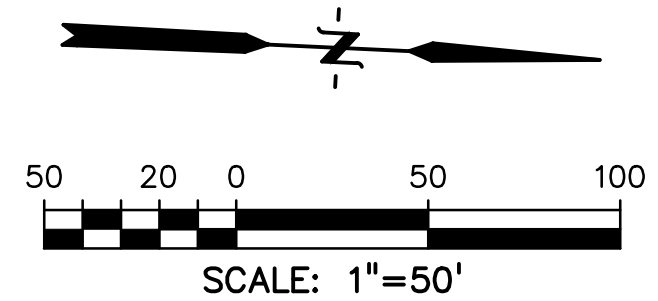
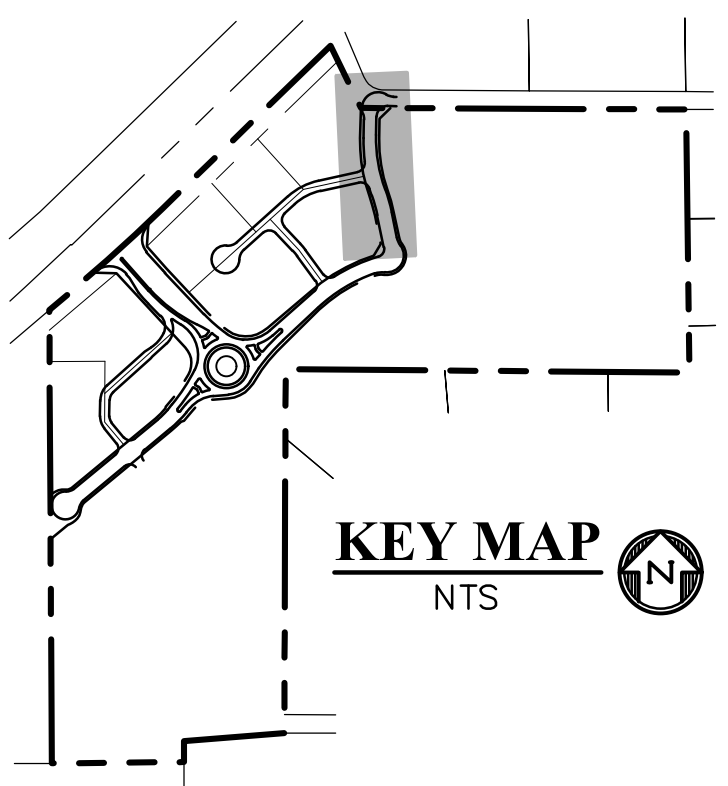
PROJECT NO. 21604-00CSCV
DRAWING NO.





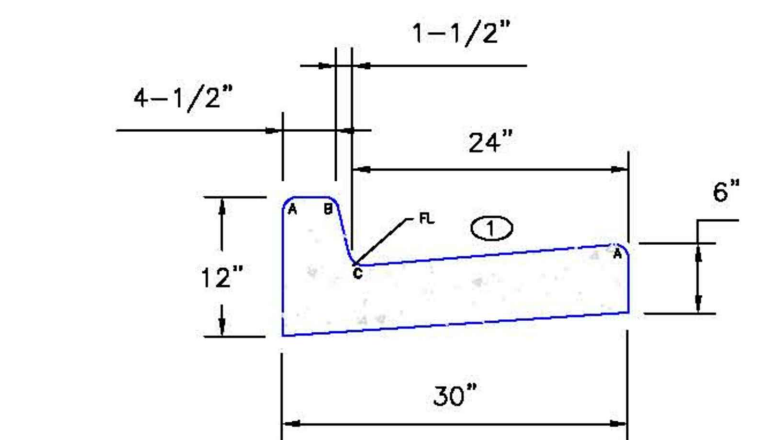
RIO LANE LINE TABLE		
LINE #	LENGTH	DIRECTION

RIO LANE CURVE TABLE			
CURVE #	LENGTH	RADIUS	DELTA



RIO LANE
STA 40+00.00 - 45+03.42
SCALE: 1"=50'

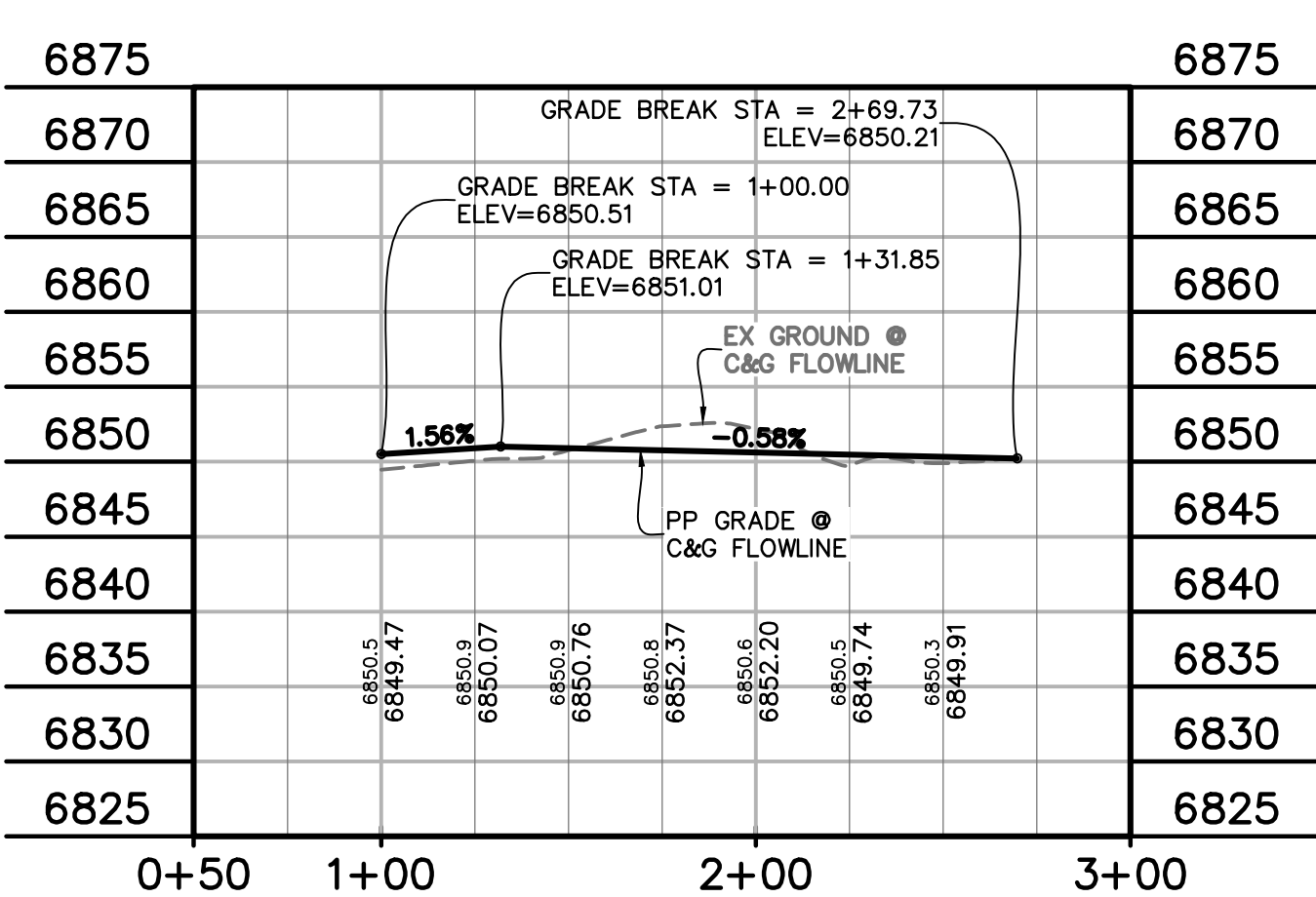
LEGEND FOR RADI	
A	1/8" TO 1/4"
B	1-1/2"
C	1-1/2" TO 2"



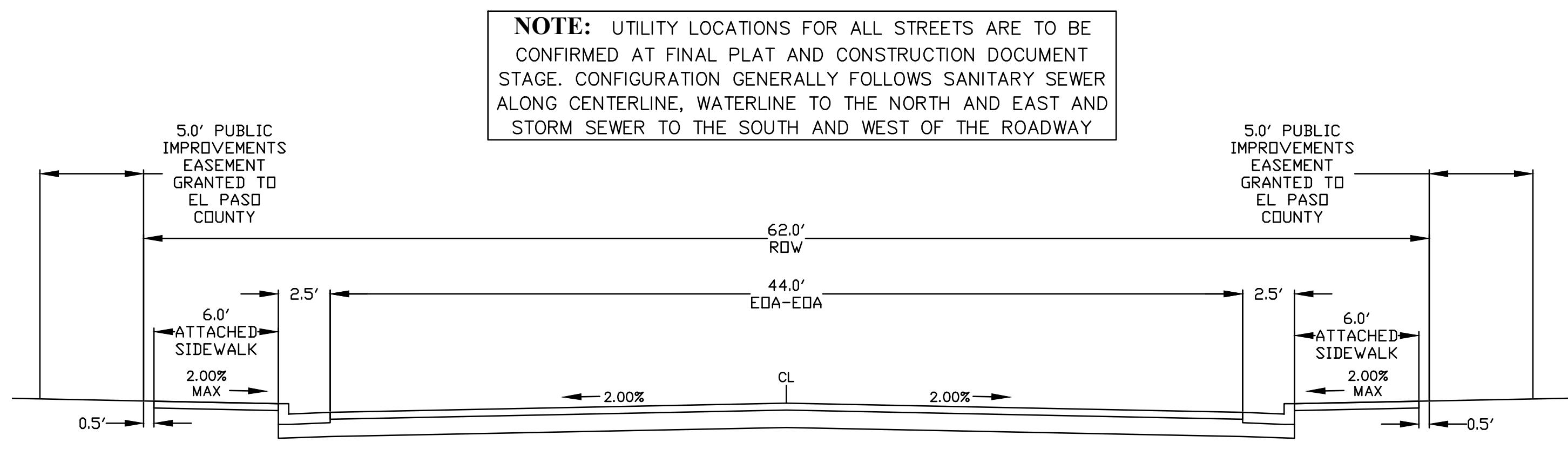
EPC TYPE A
(REVERSE SLOPE OF PAN FOR SPILL CURB)

LEGEND

PROPERTY LINE	-----
RIGHT OF WAY	-----
LOT LINE	-----
CENTERLINE ROAD	-----
EASEMENT	-----
EASEMENT (UTILITY)	-----
SIDEWALK	-----
POND	-----
TRACT LINE	-----
PROPOSED SLOPE GRADE	----- -2.0%
PROPOSED MAJOR CONTOUR	----- 6205
PROPOSED MINOR CONTOUR	----- 6203
EXISTING MAJOR CONTOUR	----- 6205
EXISTING MINOR CONTOUR	----- 6203
LOT NUMBER	7
EXISTING	EX
PROPOSED	PP
PEDESTRIAN RAMP (SEE SHT 20 FOR DETAILS)	Ⓡ
EPC TYPE A C&G (CATCH)	Ⓢ
EPC TYPE A C&G (SPILL)	Ⓣ
8' CROSSSPAN (SEE SHT 20 FOR DETAILS)	Ⓧ
CURB & GUTTER	C&G
TOP FRONT CURB	TFC
EDGE OF SIDEWALK	EOS
CENTERLINE	CL
PLAN & PROFILE	P&P
SOUTHWEST RETAIL ROW ST. CUL-DE-SAC	SW RRS CDS
SOUTHEAST	SE
WEST	W
WOODMEN ROAD	WMR
SHEET	SHT
SIDEWALK	SW



RIO LANE NORTH KNUCKLE
STA 1+00.00 - 2+69.73
SCALE: 1"=50'



NOTE: UTILITY LOCATIONS FOR ALL STREETS ARE TO BE CONFIRMED AT FINAL PLAT AND CONSTRUCTION DOCUMENT STAGE. CONFIGURATION GENERALLY FOLLOWS SANITARY SEWER ALONG CENTERLINE, WATERLINE TO THE NORTH AND EAST AND STORM SEWER TO THE SOUTH AND WEST OF THE ROADWAY

RIO LANE - SOUTH OF PERULA POINT
(URBAN LOCAL - PUBLIC 62' ROW)
SCALE: 1"=5'

PREPARED BY:

DREXEL, BARRELL & CO.
Engineers & Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719) 476-0800
COLORADO SPRINGS • LAFAYETTE

CLIENT:

PROTERRA PROPERTIES
1864 WOODMOOR DRIVE
MONUMENT, CO 80132
(719) 476-0800
CONTACT: STEVE ROSSOLL
rossoll@proterra.com

CONSTRUCTION DOCUMENTS FOR:
THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM
FILE NAME: COMM RD

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

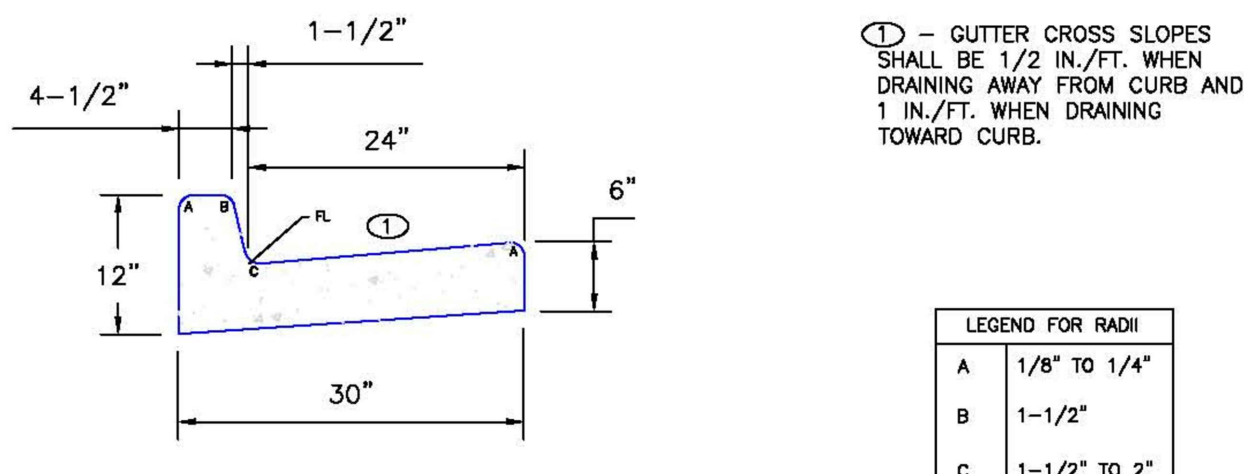
RIO LANE & NORTH KNUCKLE
PLAN & PROFILE

PROJECT NO. 21604-00CSCV
DRAWING NO.

DRAWING SCALE:
HORIZONTAL: 1" = 50'
VERTICAL: 1" = 12.5'

811 Know what's below.
Call before you dig.
CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

C8



⊖ = GUTTER CROSS SLOPES SHALL BE 1/2 IN./FT. WHEN DRAINING AWAY FROM CURB AND 1 IN./FT. WHEN DRAINING TOWARD CURB.

LEGEND FOR RADII

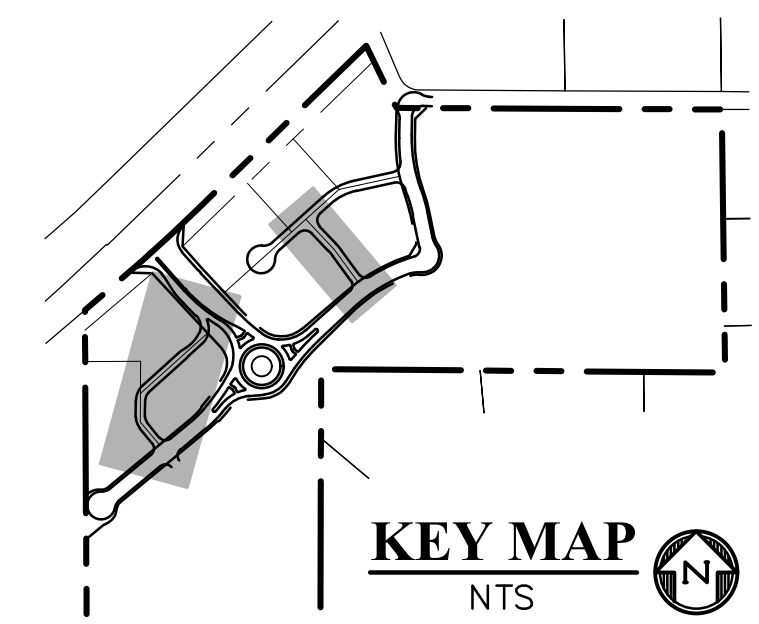
A	1/8" TO 1/4"
B	1-1/2"
C	1-1/2" TO 2"

DUNLIN HEIGHTS & JACKDAW PT. LINE TABLE

LINE #	LENGTH	DIRECTION
--------	--------	-----------

DUNLIN HEIGHTS & JACKDAW PT. CURVE TABLE

CURVE #	LENGTH	RADIUS	DELTA
---------	--------	--------	-------

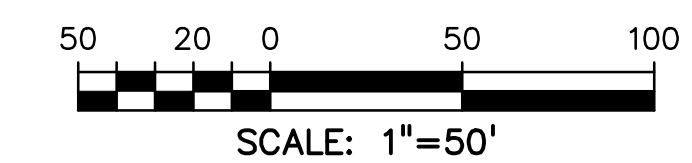
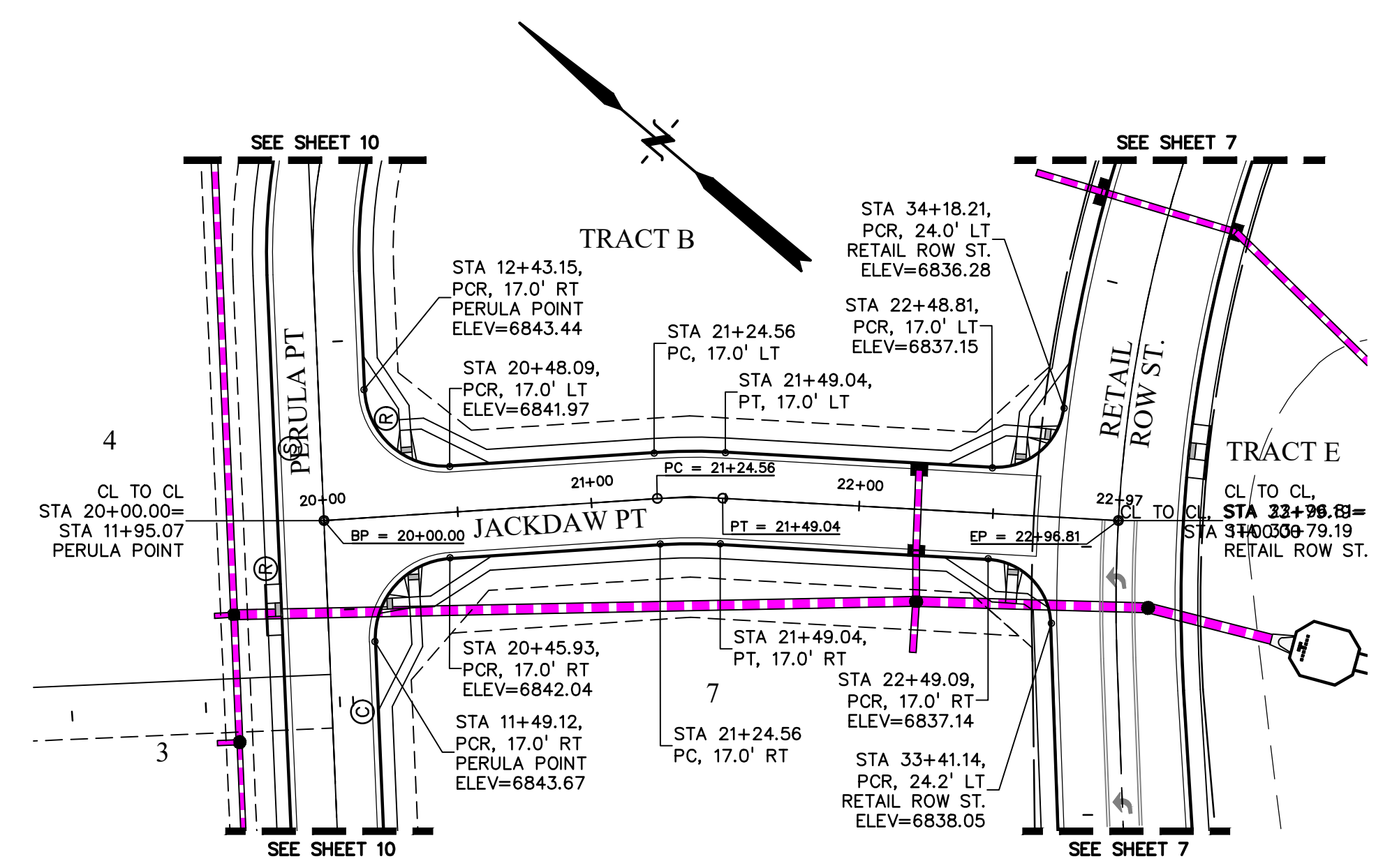
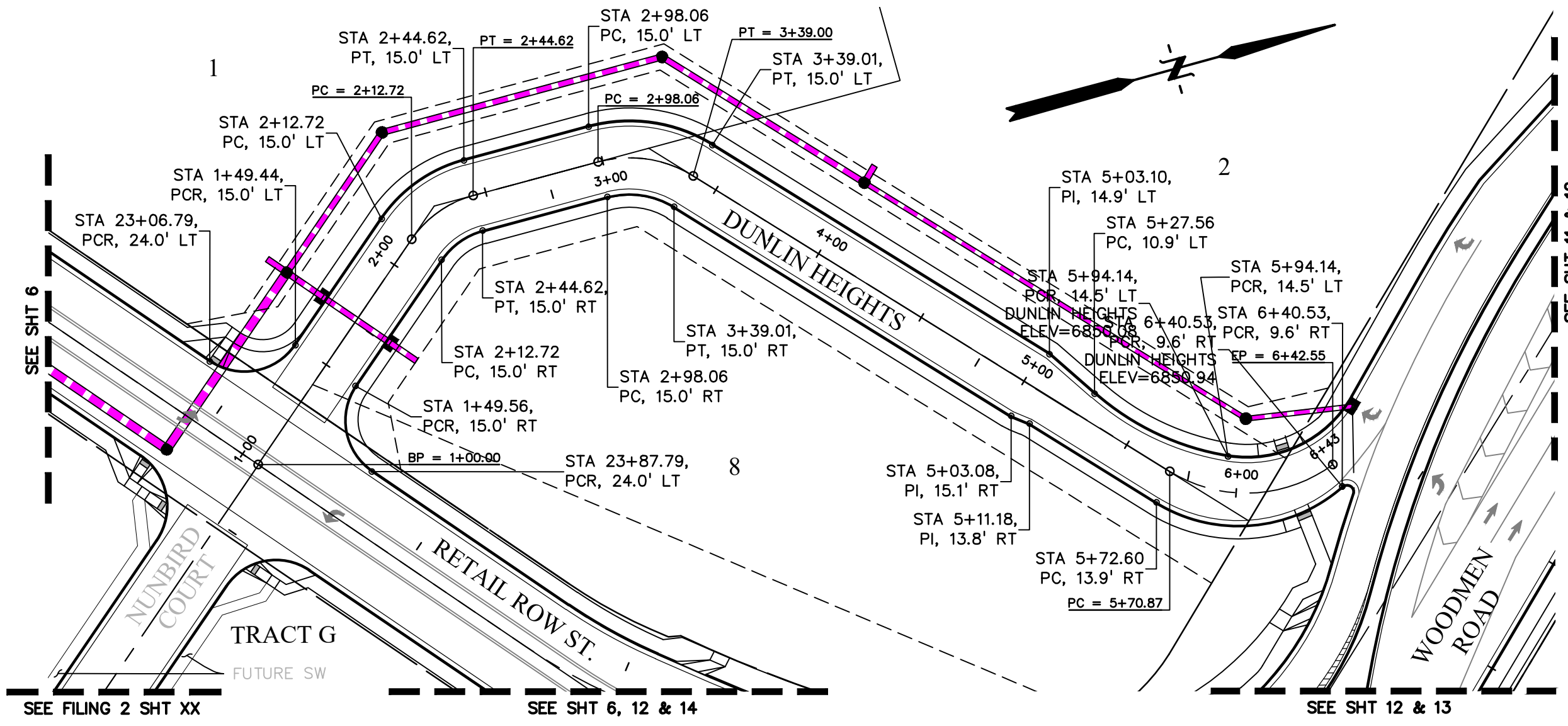


PREPARED BY:

DREXEL, BARRELL & CO.
Engineers & Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719) 260-0887
COLORADO SPRINGS • LAFAYETTE

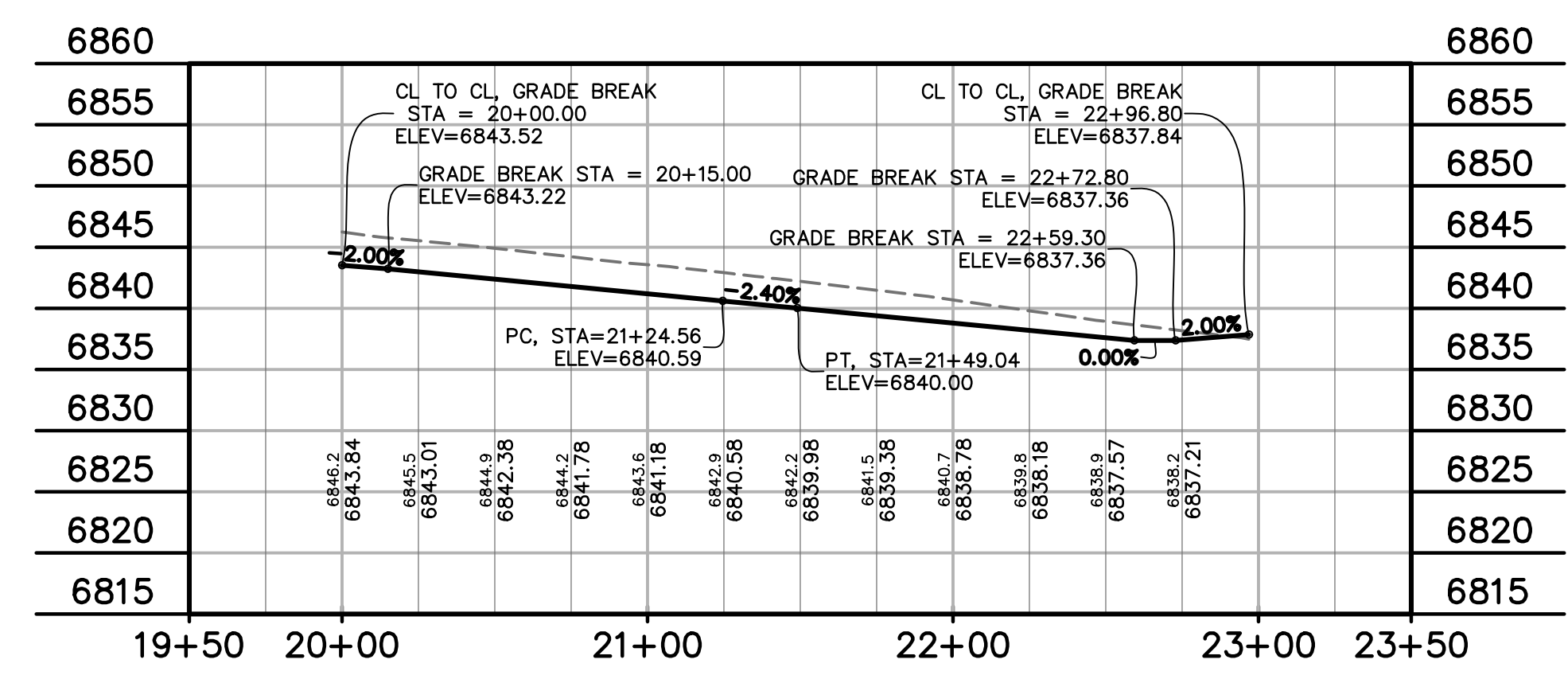
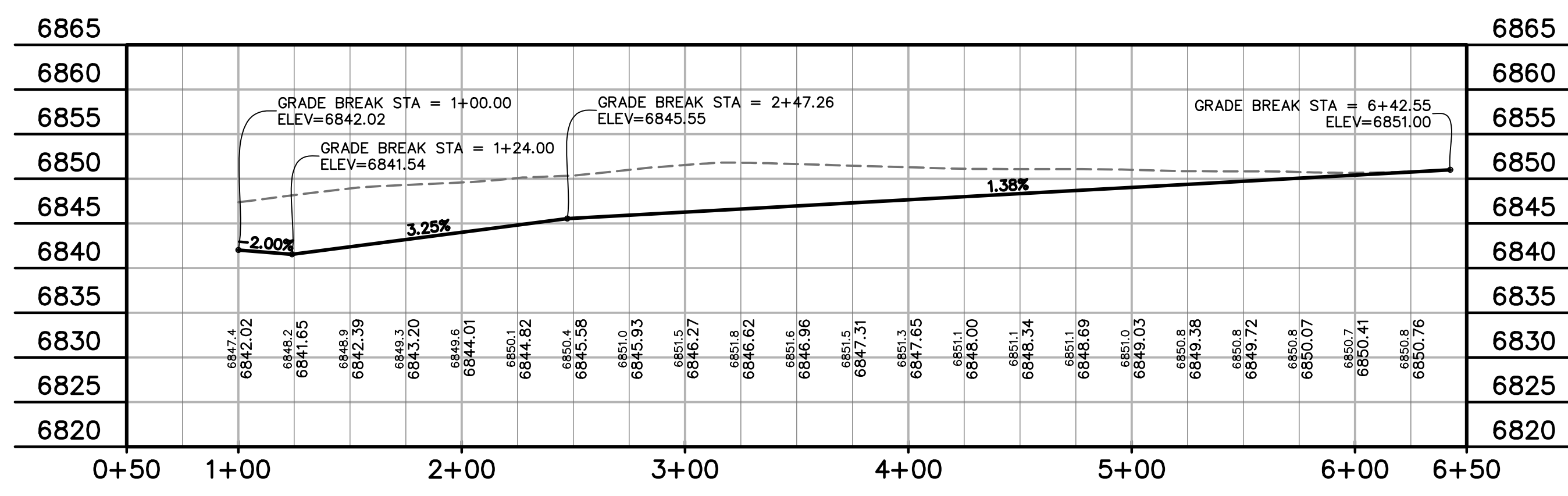
CLIENT:

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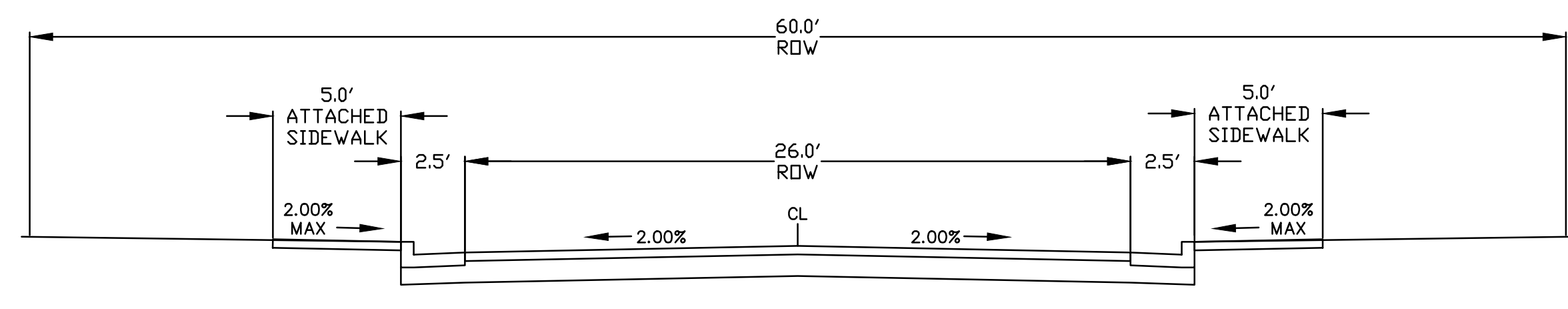
LEGEND

- PROPERTY LINE.....
- RIGHT OF WAY.....
- LOT LINE.....
- CENTERLINE ROAD.....
- EASEMENT.....
- EASEMENT (UTILITY).....
- SIDEWALK.....
- POND.....
- TRACT LINE.....
- PROPOSED MAJOR CONTOUR..... -2.0%
- PROPOSED MINOR CONTOUR..... 6205
- EXISTING MAJOR CONTOUR..... 6203
- EXISTING MINOR CONTOUR..... 6205
- LOT NUMBER..... 7
- EXISTING..... EX
- PROPOSED..... PP
- PEDESTRIAN RAMP (SEE SHT 20 FOR DETAILS)..... (R)
- EPC TYPE A C&G (CATCH)..... (C)
- EPC TYPE A C&G (SPILL)..... (S)
- 8' CROSSPAN (SEE SHT 20 FOR DETAILS)..... (X)
- CURB & GUTTER..... C&G
- TOP FRONT CURB..... TFC
- EDGE OF SIDEWALK..... EOS
- CENTERLINE..... CL
- PLAN & PROFILE..... P&P
- SOUTHWEST RETAIL ROW ST. CUL-DE-SAC..... SW RRS CDS
- SOUTHEAST..... SE
- WEST..... W
- WOODMEN ROAD..... WMR
- SHEET..... SHT
- SIDEWALK..... SW

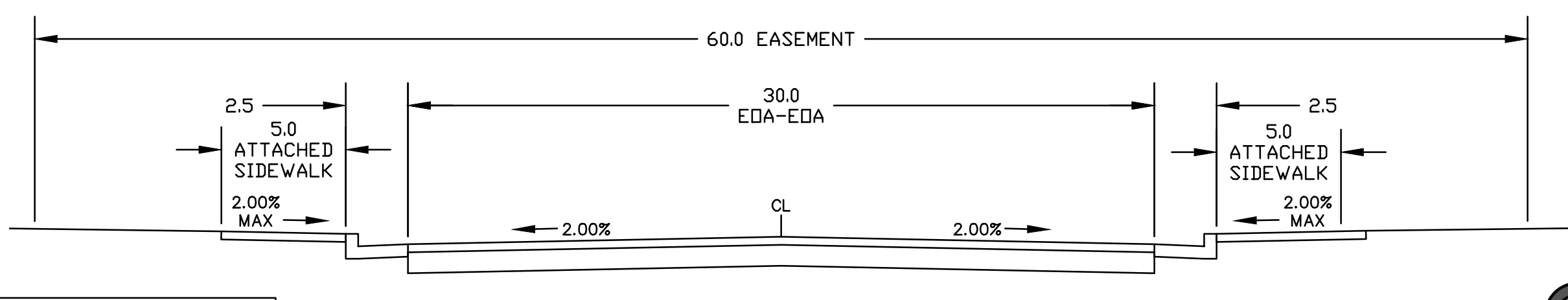


DUNLIN HEIGHTS
STA 1+00.00 - 6+42.55
SCALE: 1"=50'

JACKDAW POINT
STA 20+00.00 - 22+96.80
SCALE: 1"=50'



DUNLIN HEIGHTS
(URBAN LOCAL - PRIVATE DRIVE)
SCALE: 1"=5'



JACKDAW PT
(URBAN LOCAL - PRIVATE RD.)
SCALE: 1"=5'

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CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM
FILE NAME: COMM RD

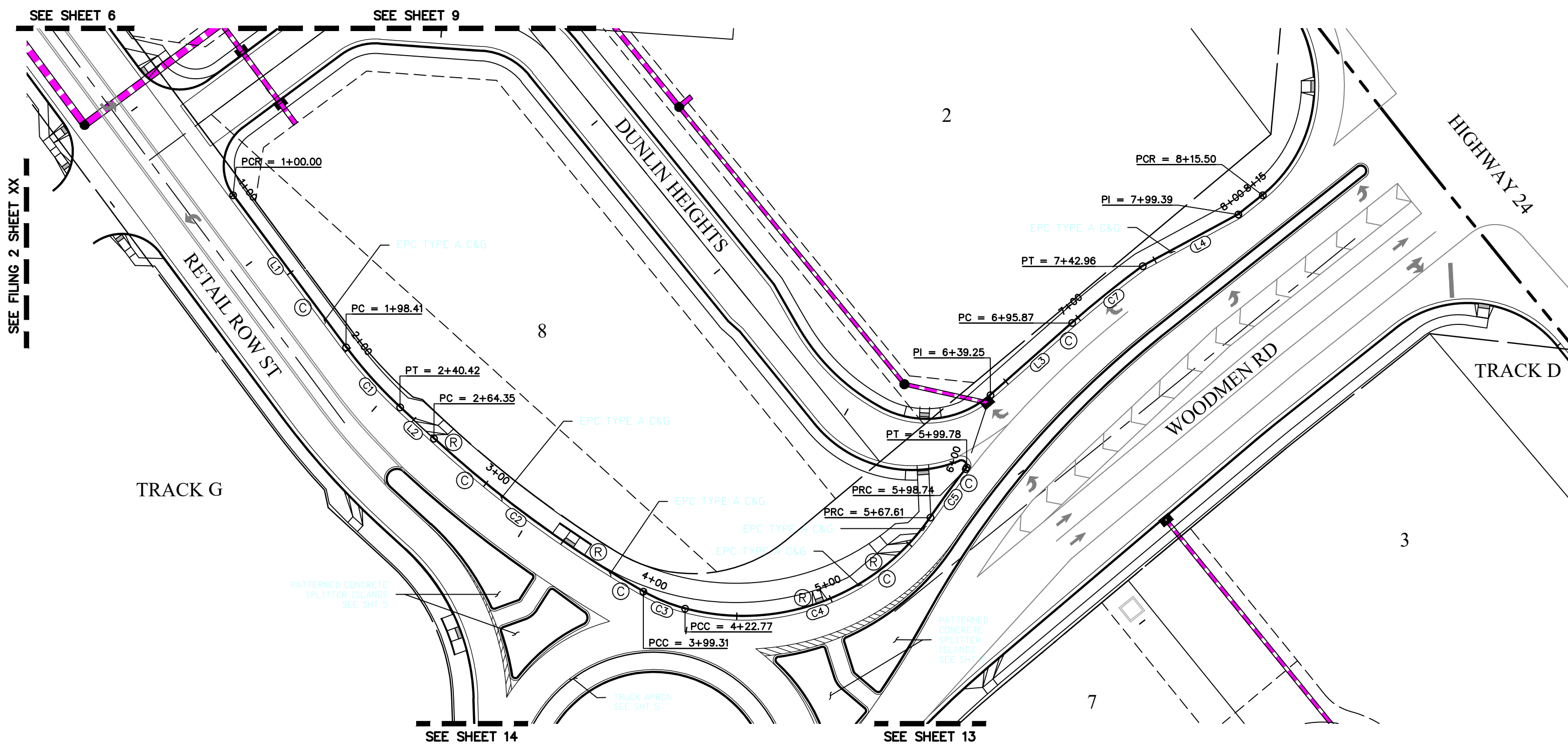
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: 1" = 50'
VERTICAL: 1" = 12.5'

DUNLIN HEIGHTS & JACKDAW POINT
PLAN & PROFILE

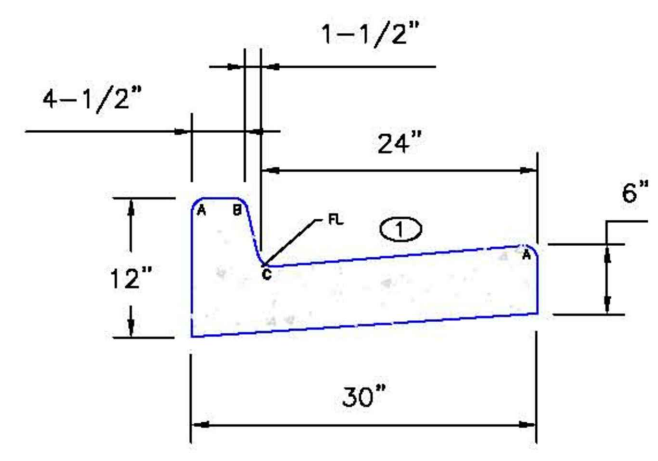
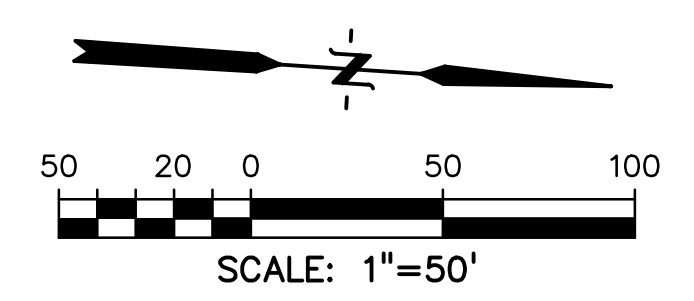
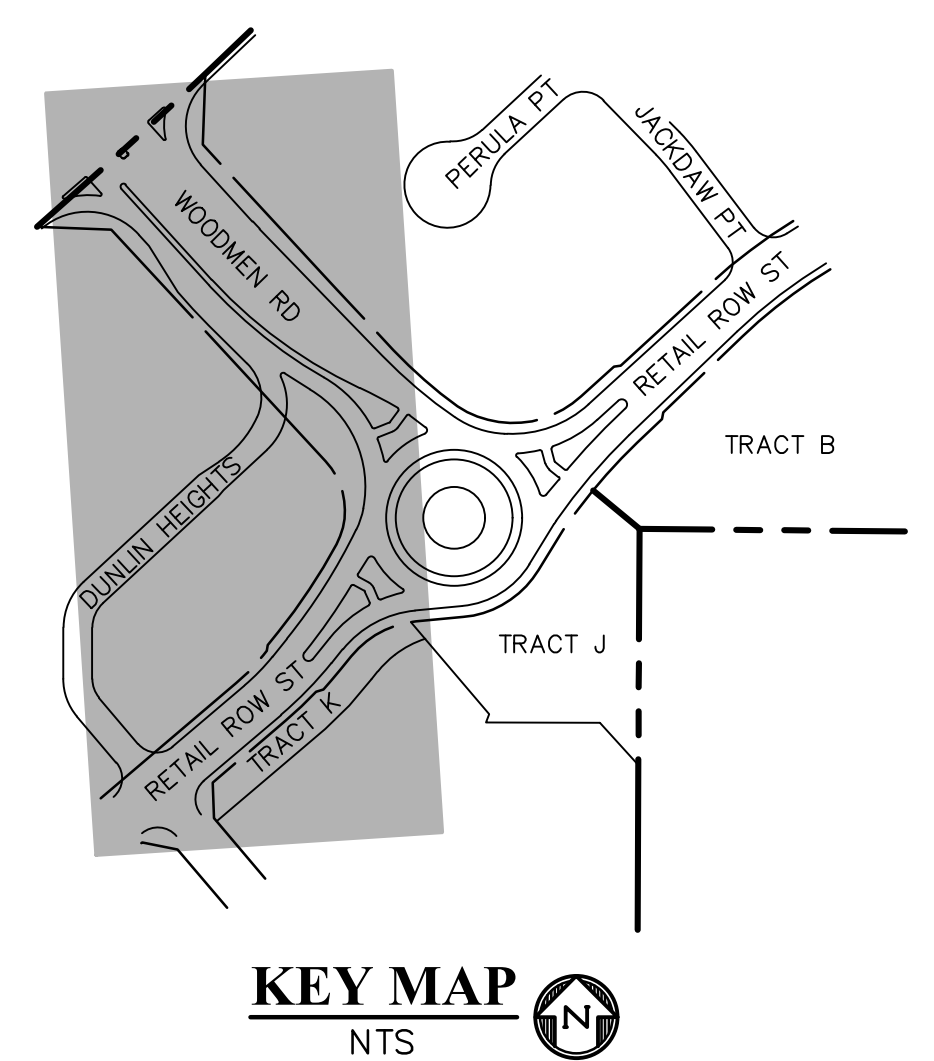
PROJECT NO. 21604-00CSCV
DRAWING NO.

811 Know what's below. Call before you dig.
CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.
C9
SHEET: 9 OF 29



WEST ROUNDABOUT C&G LINE TABLE		
LINE #	LENGTH	DIRECTION

WEST ROUNDABOUT C&G CURVE TABLE			
CURVE #	LENGTH	RADIUS	DELTA



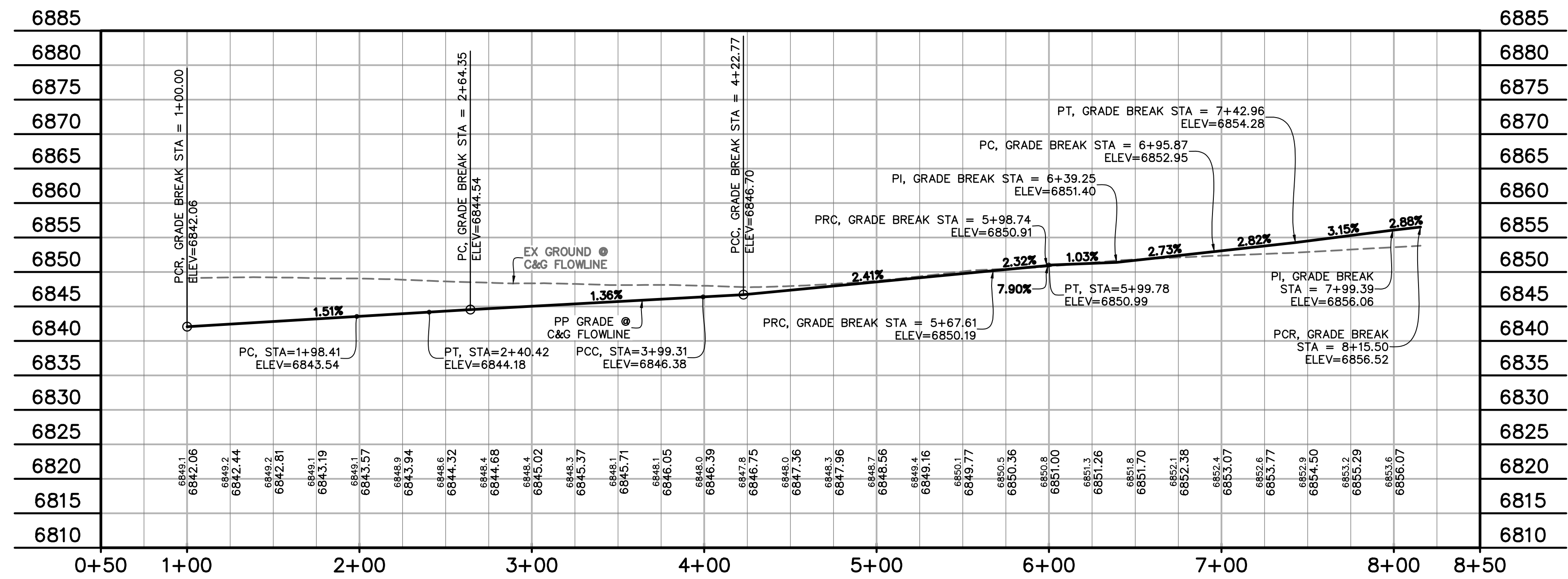
EPC TYPE A
(REVERSE SLOPE OF PAN FOR SPILL CURB)

Ⓞ - GUTTER CROSS SLOPES SHALL BE 1/2 IN./FT. WHEN DRAINING AWAY FROM CURB AND 1 IN./FT. WHEN DRAINING TOWARD CURB.

LEGEND FOR RADI	
A	1/8" TO 1/4"
B	1-1/2"
C	1-1/2" TO 2"

LEGEND

- PROPERTY LINE.....
- RIGHT OF WAY.....
- LOT LINE.....
- CENTERLINE ROAD.....
- EASEMENT.....
- EASEMENT (UTILITY).....
- SIDEWALK.....
- POND.....
- TRACT LINE.....
- PROPOSED SLOPE GRADE..... -2.0%
- PROPOSED MAJOR CONTOUR..... 6205
- PROPOSED MINOR CONTOUR..... 6203
- EXISTING MAJOR CONTOUR..... 6205
- EXISTING MINOR CONTOUR..... 6203
- LOT NUMBER..... 7
- EXISTING..... EX
- PROPOSED..... PP
- PEDESTRIAN RAMP (SEE SHT 20 FOR DETAILS)..... Ⓡ
- EPC TYPE A C&G (CATCH)..... Ⓞ
- EPC TYPE A C&G (SPILL)..... Ⓢ
- 8' CROSSSPAN (SEE SHT 20 FOR DETAILS)..... ⓧ
- CURB & GUTTER..... C&G
- TOP FRONT CURB..... TFC
- EDGE OF SIDEWALK..... EOS
- CENTERLINE..... CL
- PLAN & PROFILE..... P&P
- SOUTHWEST RETAIL ROW ST. CUL-DE-SAC..... SW RRS CDS
- SOUTHEAST..... SE
- WEST..... W
- WOODMEN ROAD..... WMR
- SHEET..... SHT
- SIDEWALK..... SW



WEST ROUNDABOUT C&G FLOWLINE
STA 1+00.00 - 8+15.50
SCALE: 1"=50'

PREPARED BY:

DREXEL, BARRELL & CO.
Engineers & Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719) 476-0800
COLORADO SPRINGS • LAFAYETTE

CLIENT:
PROTERRA PROPERTIES
1864 WOODMOOR DRIVE
MONUMENT, CO 80132
(719) 476-0800
CONTACT: STEVE ROSSOLL
rossoll@proterra.com

CONSTRUCTION DOCUMENTS FOR:
THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM

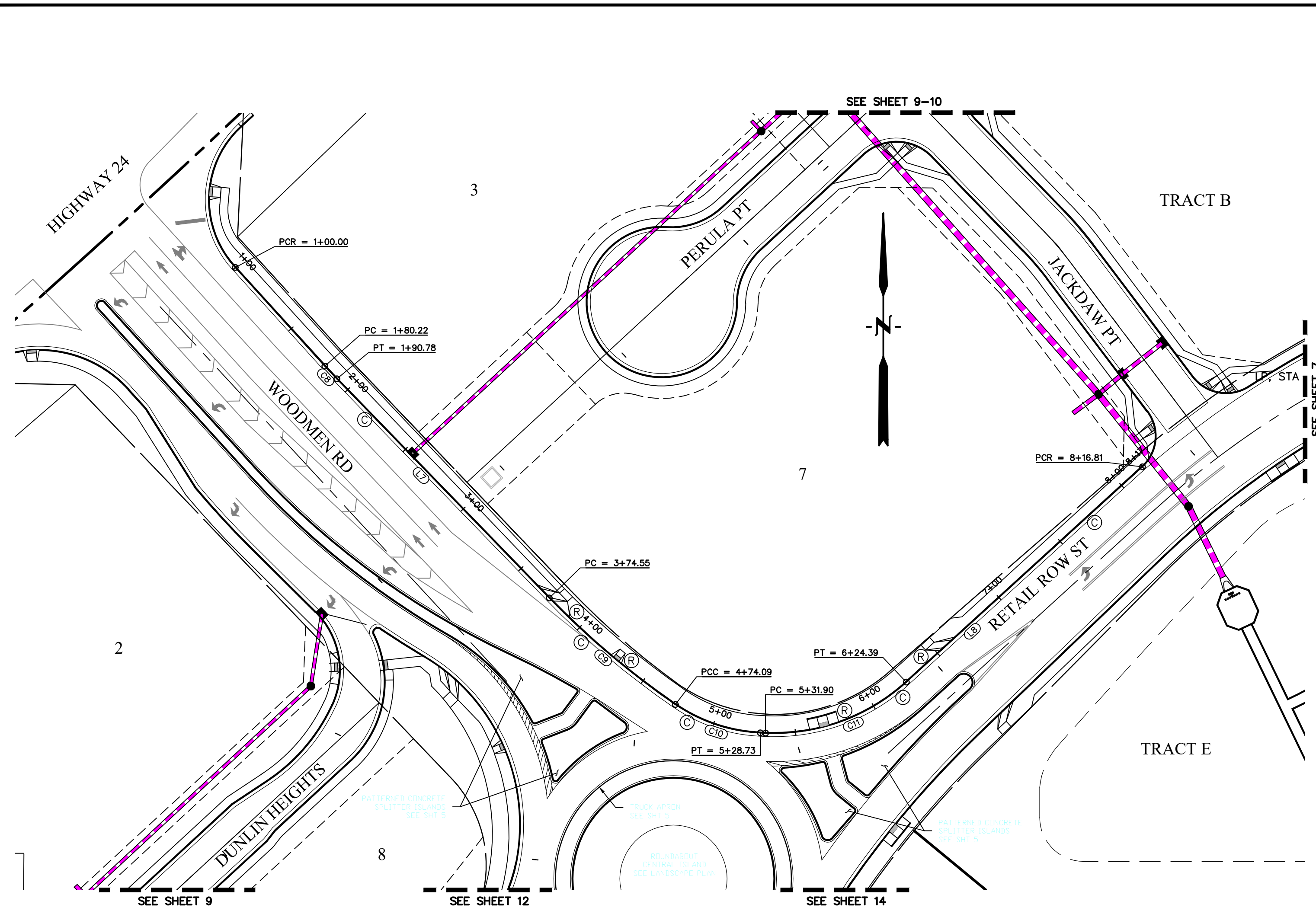
FILE NAME: ROUNDABOUT
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: 1" = 50"
VERTICAL: 1" = 12.5'

W ROUNDABOUT
C&G FLOWLINE
PLAN & PROFILES

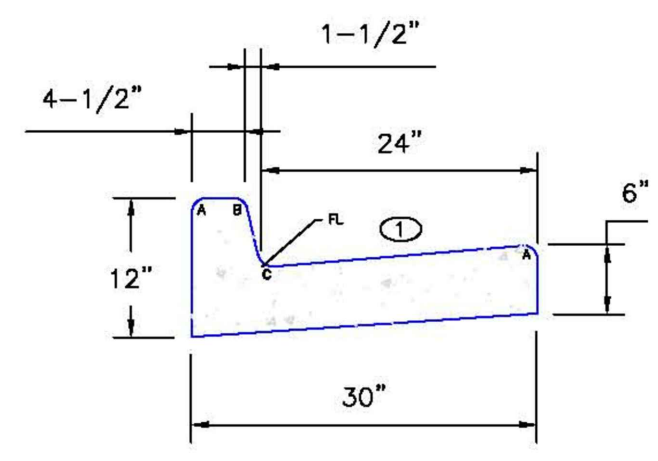
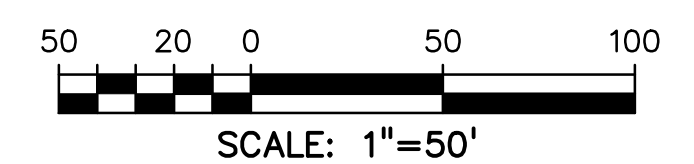
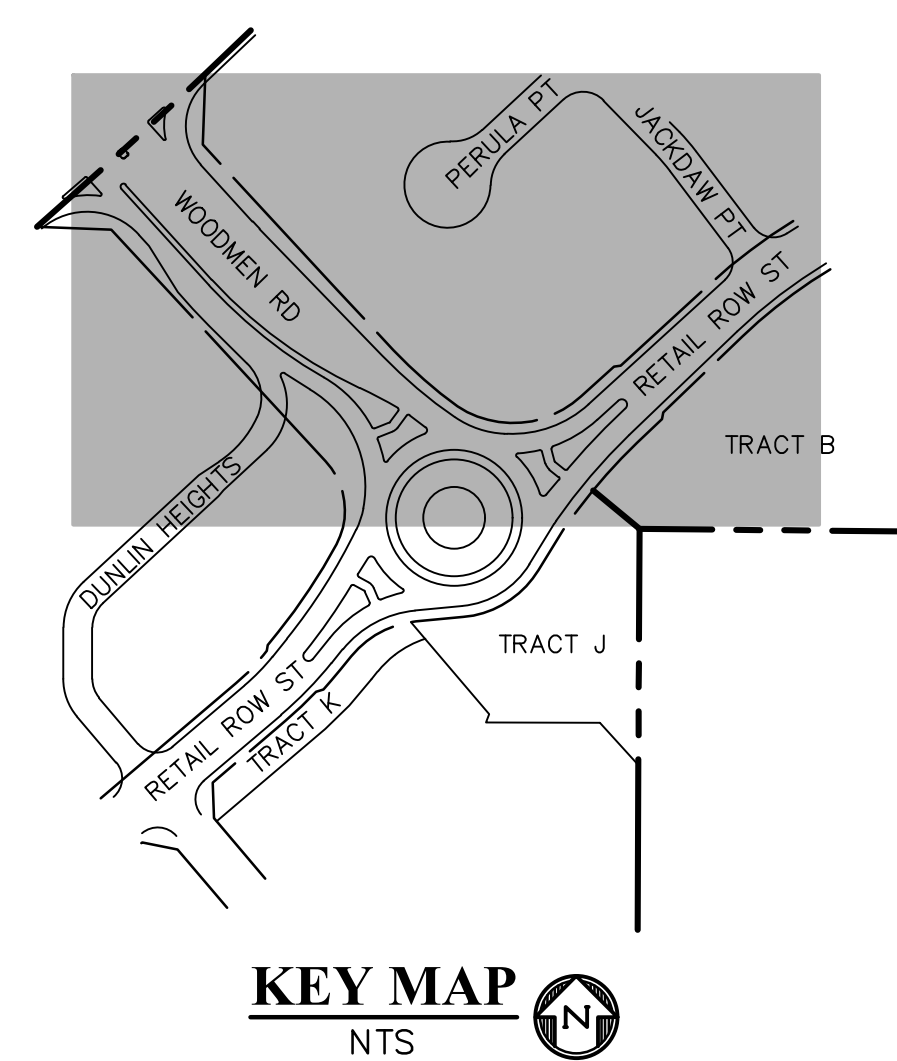
PROJECT NO. 21604-00CSCV
DRAWING NO.

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NORTH ROUNDABOUT C&G LINE TABLE		
LINE #	LENGTH	DIRECTION

NORTH ROUNDABOUT C&G CURVE TABLE			
CURVE #	LENGTH	RADIUS	DELTA



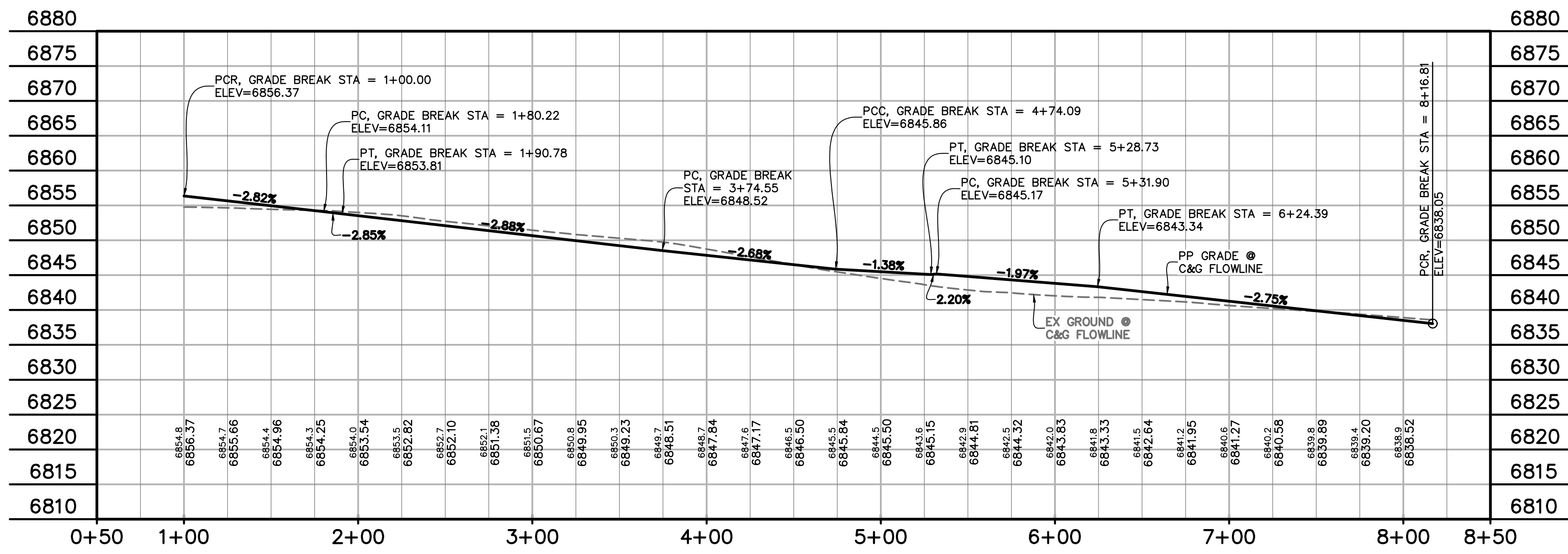
EPC TYPE A
(REVERSE SLOPE OF PAN FOR SPILL CURB)

Ⓞ - GUTTER CROSS SLOPES SHALL BE 1/2 IN./FT. WHEN DRAINING AWAY FROM CURB AND 1 IN./FT. WHEN DRAINING TOWARD CURB.

LEGEND FOR RADI	
A	1/8" TO 1/4"
B	1-1/2"
C	1-1/2" TO 2"

LEGEND

- PROPERTY LINE.....
- RIGHT OF WAY.....
- LOT LINE.....
- CENTERLINE ROAD.....
- EASEMENT.....
- EASEMENT (UTILITY).....
- SIDEWALK.....
- POND.....
- TRACT LINE.....
- PROPOSED SLOPE GRADE..... -2.0%
- PROPOSED MAJOR CONTOUR..... 6205
- PROPOSED MINOR CONTOUR..... 6203
- EXISTING MAJOR CONTOUR..... 6205
- EXISTING MINOR CONTOUR..... 6203
- LOT NUMBER..... 7
- EXISTING..... EX
- PROPOSED..... PP
- PEDESTRIAN RAMP (SEE SHT 20 FOR DETAILS)..... Ⓡ
- EPC TYPE A C&G (CATCH)..... Ⓞ
- EPC TYPE A C&G (SPILL)..... Ⓢ
- 8' CROSSSPAN (SEE SHT 20 FOR DETAILS)..... ⓧ
- CURB & GUTTER..... C&G
- TOP FRONT CURB..... TFC
- EDGE OF SIDEWALK..... EOS
- CENTERLINE..... CL
- PLAN & PROFILE..... P&P
- SOUTHWEST RETAIL ROW ST. CUL-DE-SAC..... SW RRS CDS
- SOUTHEAST..... SE
- WEST..... W
- WOODMEN ROAD..... WMR
- SHEET..... SHT
- SIDEWALK..... SW



NORTH ROUNDABOUT C&G FLOWLINE
STA 1+00.00 - 8+16.81
SCALE: 1"=50'

PREPARED BY:



CLIENT:

PROTERRA PROPERTIES
1864 WOODMOOR DRIVE
MONUMENT, CO 80132
(719) 476-0800
CONTACT: STEVE ROSSOLL
rossoll@proterraco.com

CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM

FILE NAME: ROUNDABOUT
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: 1" = 50'
VERTICAL: 1" = 12.5'

**N ROUNDABOUT
C&G FLOWLINE
PLAN & PROFILES**

PROJECT NO. 21604-00CSCV
DRAWING NO.

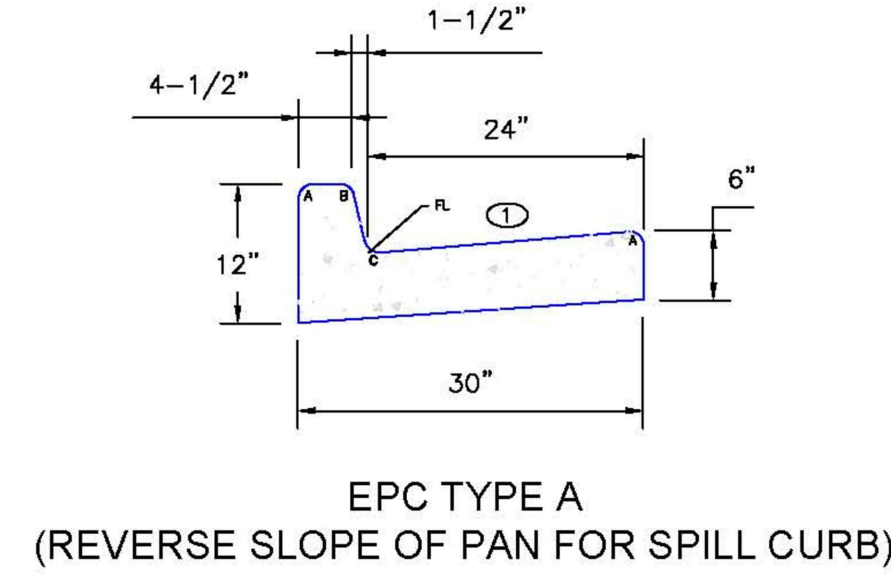
C13

SHEET: 13 OF 29



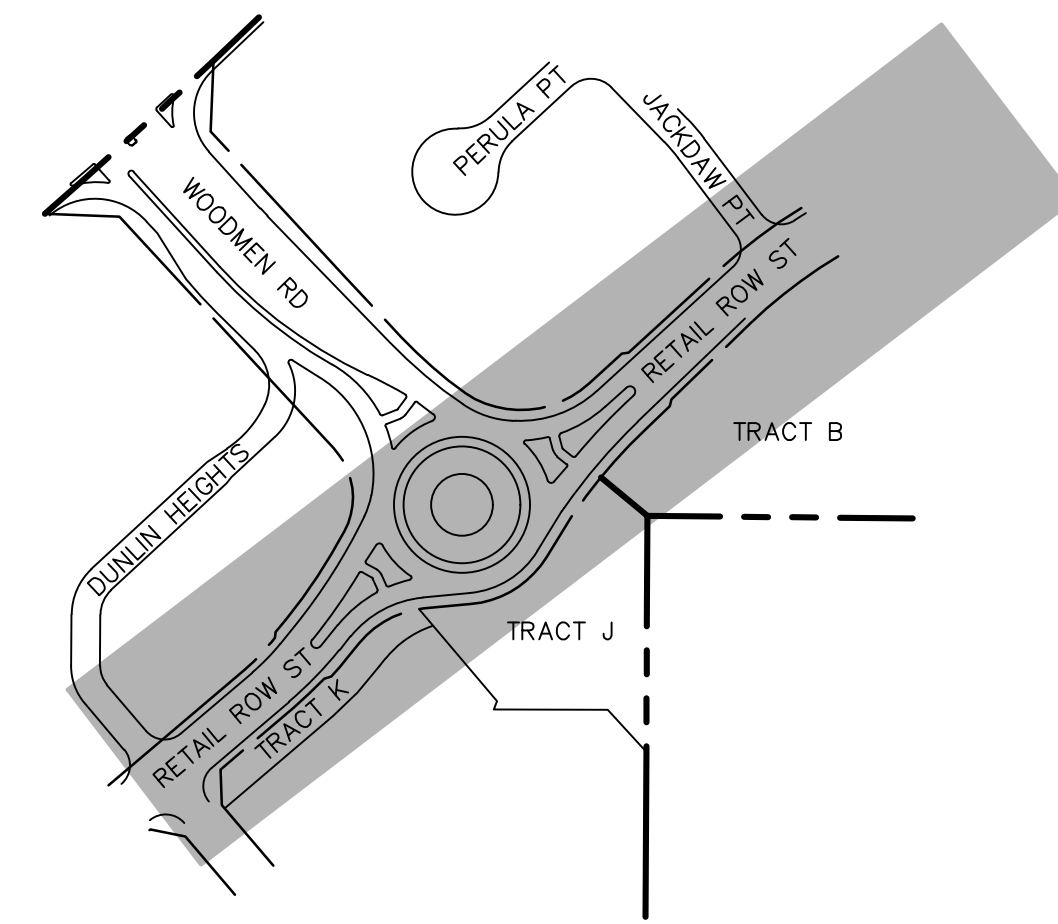
SOUTHEAST ROUNDABOUT C&G CURVE TABLE			
CURVE #	LENGTH	RADIUS	DELTA

SOUTHEAST ROUNDABOUT C&G LINE TABLE		
LINE #	LENGTH	DIRECTION

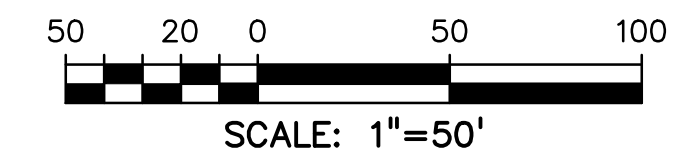


⊖ GUTTER CROSS SLOPES SHALL BE 1/2 IN./FT. WHEN DRAINING AWAY FROM CURB AND 1 IN./FT. WHEN DRAINING TOWARD CURB.

LEGEND FOR RADII	
A	1/8" TO 1/4"
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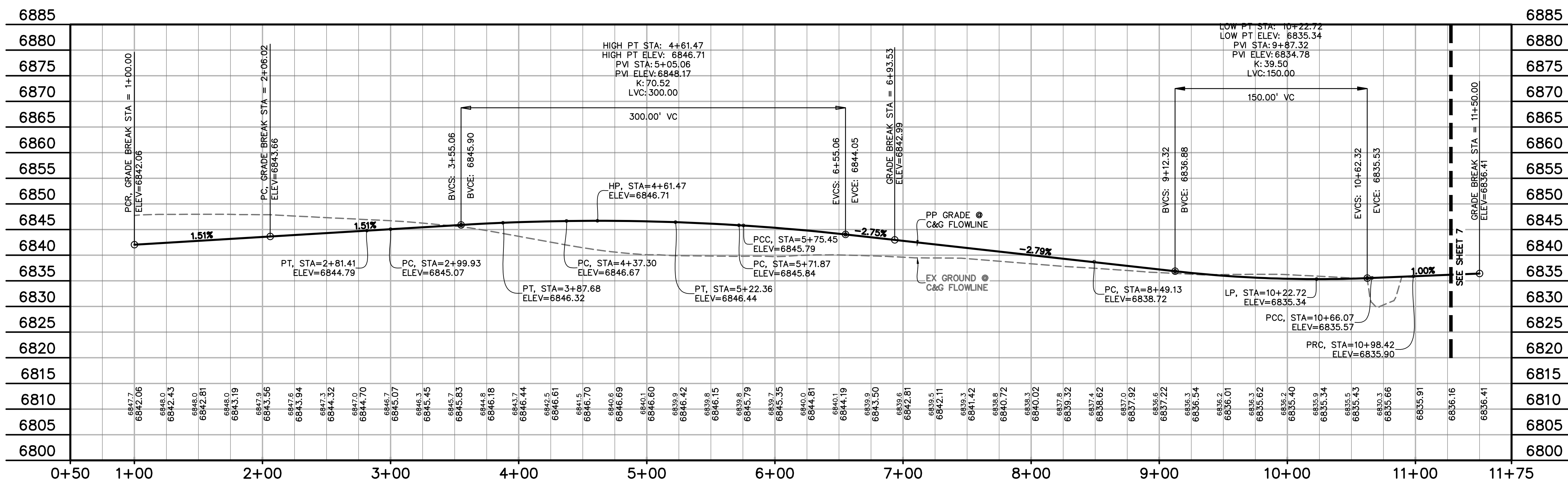
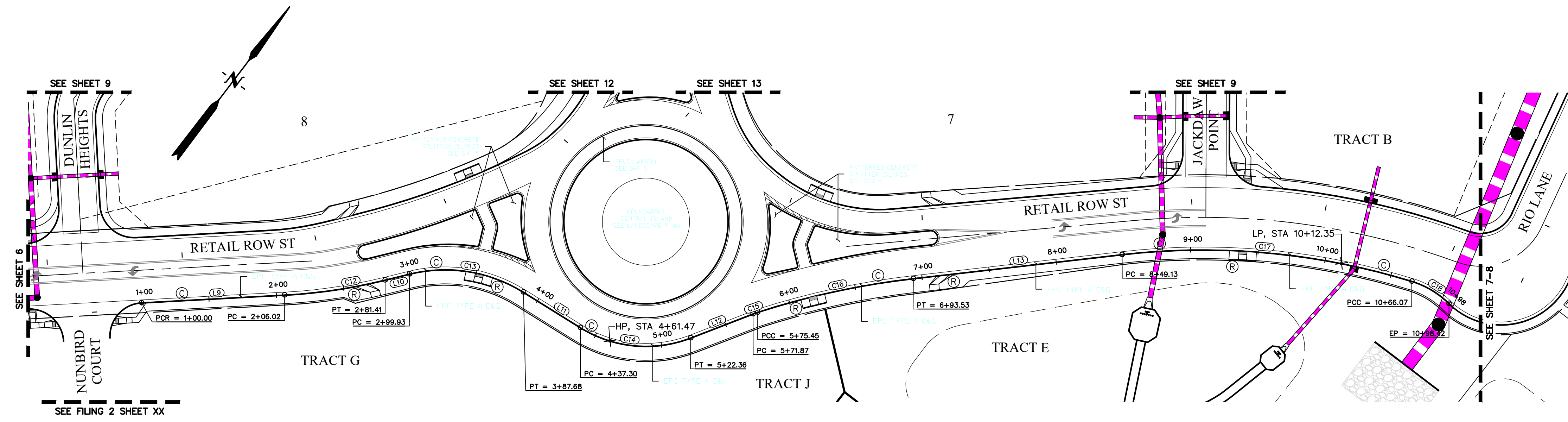


KEY MAP
NTS



LEGEND

- PROPERTY LINE
- RIGHT OF WAY
- LOT LINE
- CENTERLINE ROAD
- EASEMENT
- EASEMENT (UTILITY)
- SIDEWALK
- POND
- TRACT LINE
- PROPOSED GRADE -2.0%
- PROPOSED MAJOR CONTOUR 6205
- PROPOSED MINOR CONTOUR 6203
- EXISTING MAJOR CONTOUR 6205
- EXISTING MINOR CONTOUR 6203



SOUTHEAST ROUNDABOUT C&G FLOWLINE
STA 1+00.00 - 11+50.00
SCALE: 1"=50'

PREPARED BY:

DREXEL, BARRELL & CO.
Engineers-Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
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FILE NAME: ROUNDABOUT

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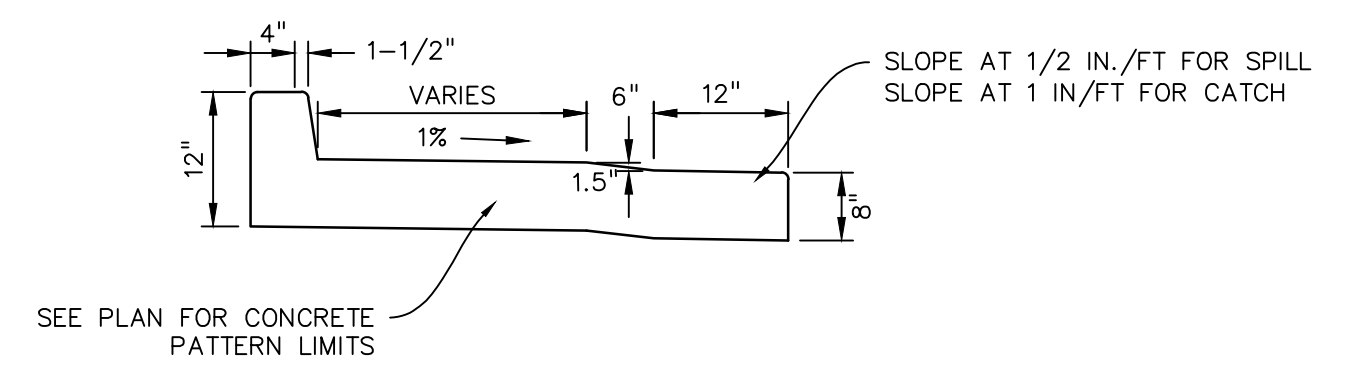
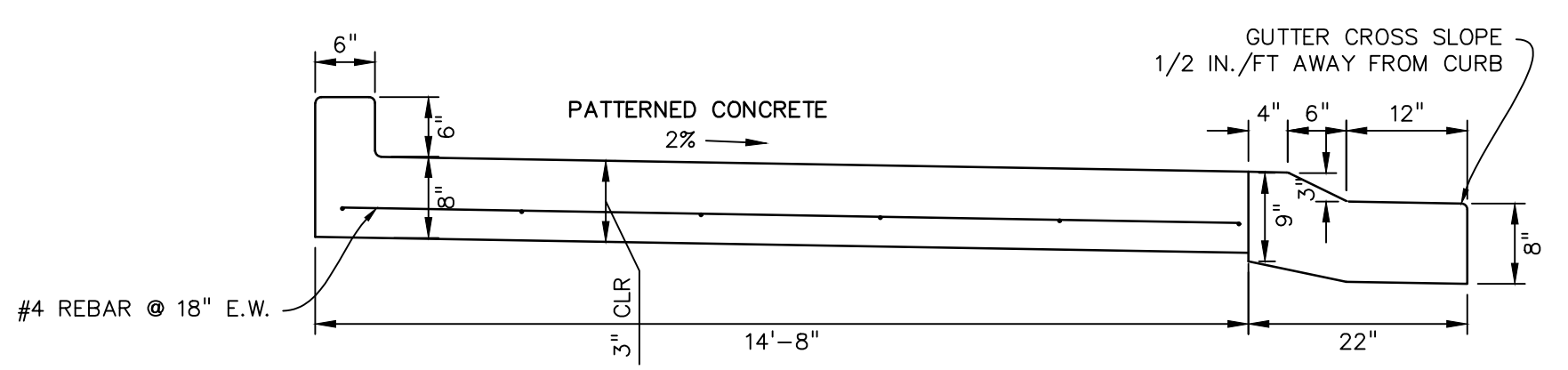
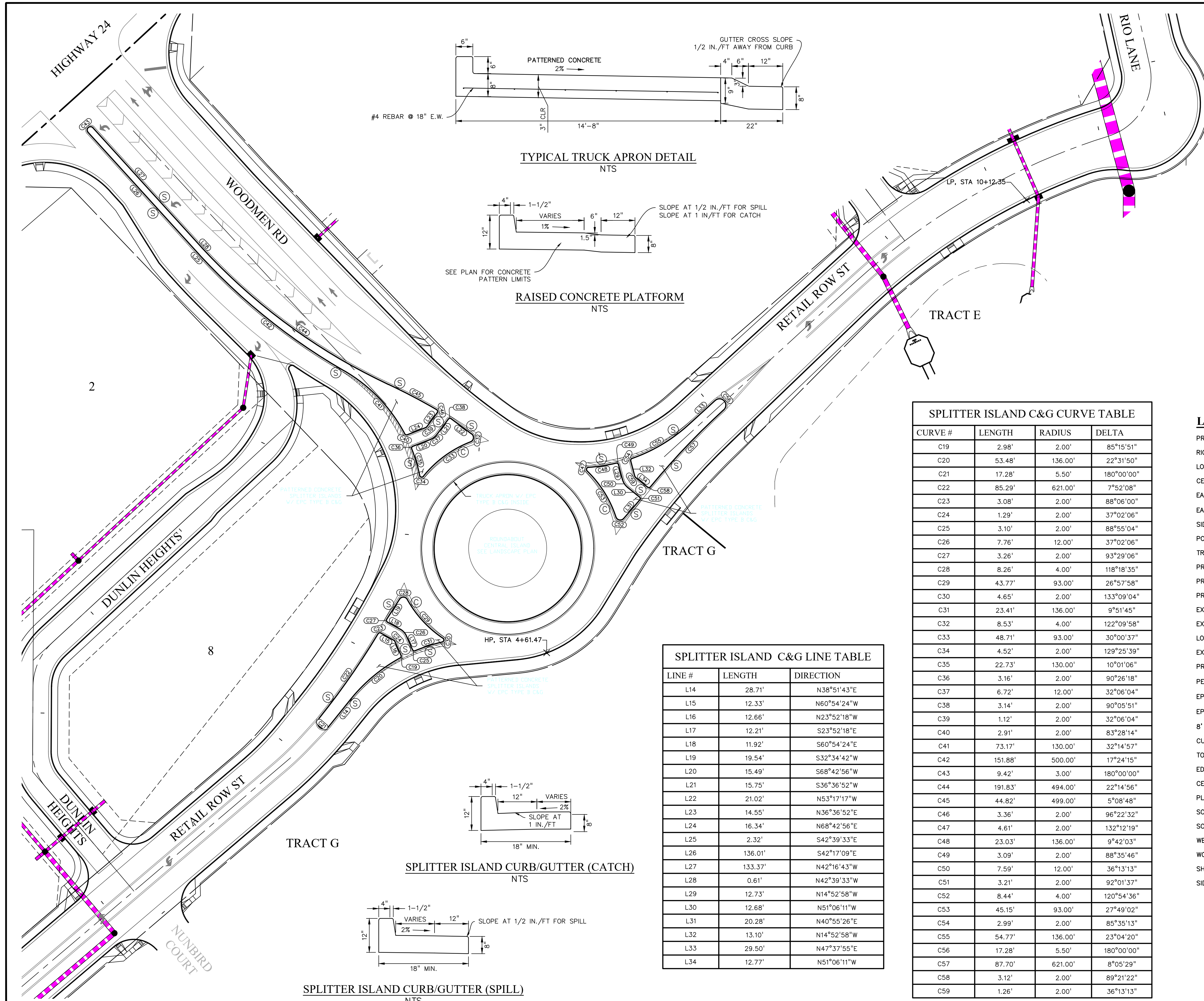
DRAWING SCALE:
HORIZONTAL: 1" = 50"
VERTICAL: 1" = 12.5'

SE ROUNDABOUT
C&G FLOWLINE
PLAN & PROFILES

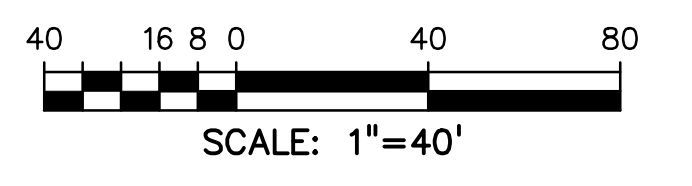
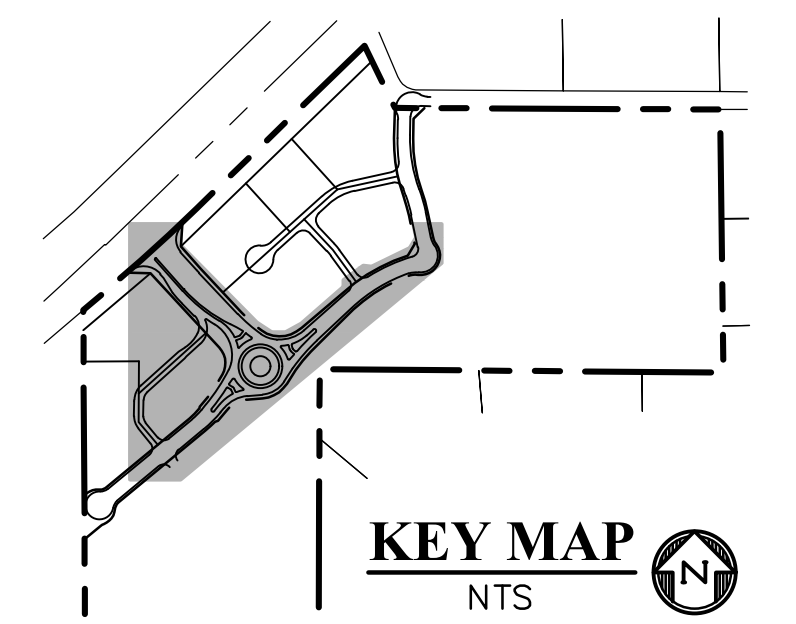
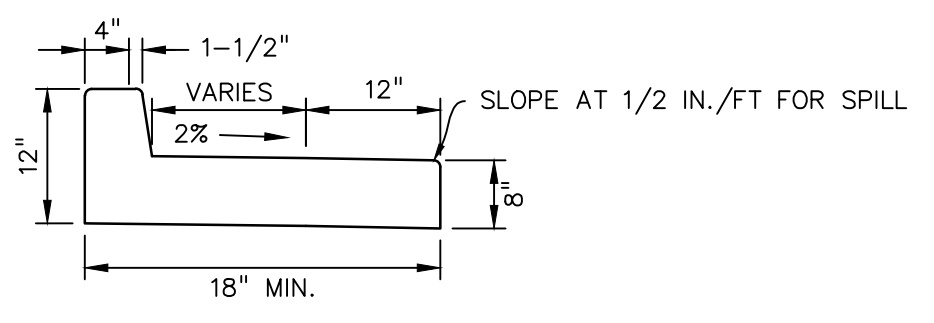
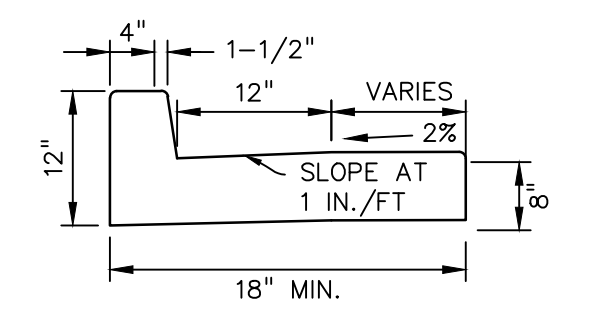
PROJECT NO. 21604-00CSCV
DRAWING NO.

C14
SHEET: 14 OF 29





SEE PLAN FOR CONCRETE PATTERN LIMITS



SPLITTER ISLAND C&G CURVE TABLE

CURVE #	LENGTH	RADIUS	DELTA
C19	2.98'	2.00'	85°15'51"
C20	53.48'	136.00'	22°31'50"
C21	17.28'	5.50'	180°00'00"
C22	85.29'	621.00'	7°52'08"
C23	3.08'	2.00'	88°06'00"
C24	1.29'	2.00'	37°02'06"
C25	3.10'	2.00'	88°55'04"
C26	7.76'	12.00'	37°02'06"
C27	3.26'	2.00'	93°29'06"
C28	8.26'	4.00'	118°18'35"
C29	43.77'	93.00'	26°57'58"
C30	4.65'	2.00'	133°09'04"
C31	23.41'	136.00'	9°51'45"
C32	8.53'	4.00'	122°09'58"
C33	48.71'	93.00'	30°00'37"
C34	4.52'	2.00'	129°25'39"
C35	22.73'	130.00'	10°01'06"
C36	3.16'	2.00'	90°26'18"
C37	6.72'	12.00'	32°06'04"
C38	3.14'	2.00'	90°05'51"
C39	1.12'	2.00'	32°06'04"
C40	2.91'	2.00'	83°28'14"
C41	73.17'	130.00'	32°14'57"
C42	151.88'	500.00'	17°24'15"
C43	9.42'	3.00'	180°00'00"
C44	191.83'	494.00'	22°14'56"
C45	44.82'	499.00'	5°08'48"
C46	3.36'	2.00'	96°22'32"
C47	4.61'	2.00'	132°12'19"
C48	23.03'	136.00'	9°42'03"
C49	3.09'	2.00'	88°35'46"
C50	7.59'	12.00'	36°13'13"
C51	3.21'	2.00'	92°01'37"
C52	8.44'	4.00'	120°54'36"
C53	45.15'	93.00'	27°49'02"
C54	2.99'	2.00'	85°35'13"
C55	54.77'	136.00'	23°04'20"
C56	17.28'	5.50'	180°00'00"
C57	87.70'	621.00'	8°05'29"
C58	3.12'	2.00'	89°21'22"
C59	1.26'	2.00'	36°13'13"

SPLITTER ISLAND C&G LINE TABLE

LINE #	LENGTH	DIRECTION
L14	28.71'	N38°51'43"E
L15	12.33'	N60°54'24"W
L16	12.66'	N23°52'18"W
L17	12.21'	S23°52'18"E
L18	11.92'	S60°54'24"E
L19	19.54'	S32°34'42"W
L20	15.49'	S68°42'56"W
L21	15.75'	S36°36'52"W
L22	21.02'	N53°17'17"W
L23	14.55'	N36°36'52"E
L24	16.34'	N68°42'56"E
L25	2.32'	S42°39'33"E
L26	136.01'	S42°17'09"E
L27	133.37'	N42°16'43"W
L28	0.61'	N42°39'33"W
L29	12.73'	N14°52'58"W
L30	12.68'	N51°06'11"W
L31	20.28'	N40°55'26"E
L32	13.10'	N14°52'58"W
L33	29.50'	N47°37'55"E
L34	12.77'	N51°06'11"W

LEGEND

- PROPERTY LINE.....
- RIGHT OF WAY.....
- LOT LINE.....
- CENTERLINE ROAD.....
- EASEMENT.....
- EASEMENT (UTILITY).....
- SIDEWALK.....
- POND.....
- TRACT LINE.....
- PROPOSED SLOPE GRADE..... -2.0%
- PROPOSED MAJOR CONTOUR..... 6205
- PROPOSED MINOR CONTOUR..... 6203
- EXISTING MAJOR CONTOUR..... 6205
- EXISTING MINOR CONTOUR..... 6203
- LOT NUMBER..... 7
- EXISTING..... EX
- PROPOSED..... PP
- PEDESTRIAN RAMP (SEE SHT 20 FOR DETAILS)..... R
- EPC TYPE A C&G (CATCH)..... C
- EPC TYPE A C&G (SPILL)..... S
- 8' CROSSSPAN (SEE SHT 20 FOR DETAILS)..... X
- CURB & GUTTER..... C&G
- TOP FRONT CURB..... TFC
- EDGE OF SIDEWALK..... EOS
- CENTERLINE..... CL
- PLAN & PROFILE..... P&P
- SOUTHWEST RETAIL ROW ST. CUL-DE-SAC..... SW RRS CDS
- SOUTHEAST..... SE
- WEST..... W
- WOODMEN ROAD..... WMR
- SHEET..... SHT
- SIDEWALK..... SW

PREPARED BY:

DREXEL, BARRELL & CO.
Engineers • Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719) 476-0800
(719) 260-0887
COLORADO SPRINGS • LAFAYETTE

CLIENT:

PROTERRA PROPERTIES
1864 WOODMOOR DRIVE
MONUMENT, CO 80132
(719) 476-0800
CONTACT: STEVE ROSSOLL
rossoll@proterra.com

CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM

FILE NAME: ROUNDABOUT
PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
HORIZONTAL: 1" = 40"
VERTICAL: N/A

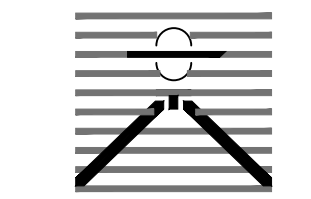
SPLITTER ISLAND ROUNDABOUT PLAN

PROJECT NO. 21604-00CSCV
DRAWING NO.

811 Know what's below. Call before you dig.
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C15
SHEET: 15 OF 29

ALL ROADS UNLESS OTHERWISE NOTED
(URBAN LOCAL)
NTS

PREPARED BY:



DREXEL, BARRELL & CO.
Engineers & Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
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PEYTON, EL PASO COUNTY, COLORADO

ISSUE DATE

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: SDM
CHECKED BY: TDM
FILE NAME: RD SEC&DET

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

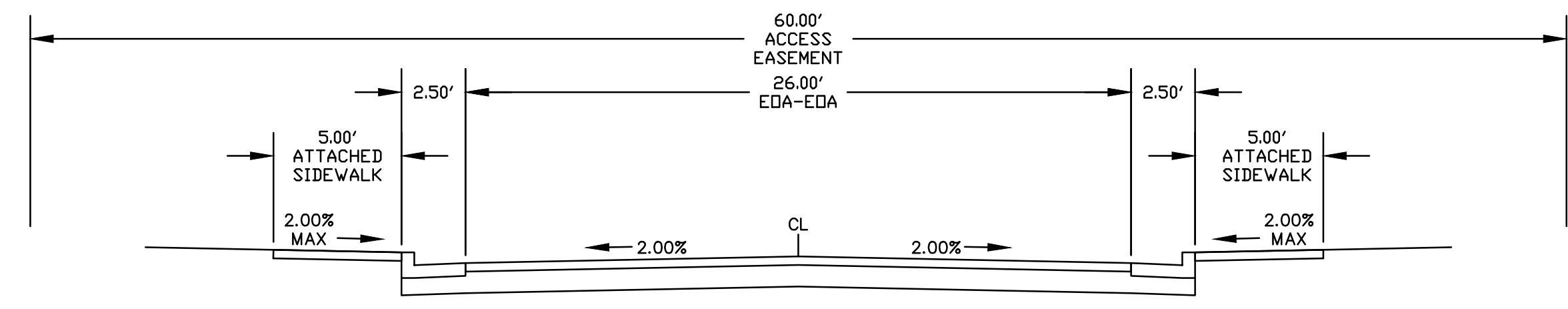
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VERTICAL: N/A

ROAD SECTIONS

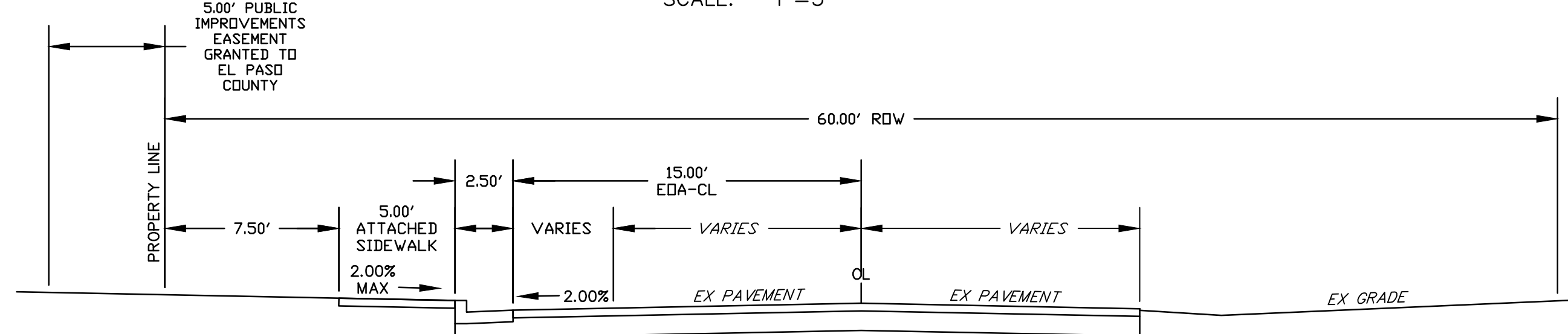
PROJECT NO. 21604-00CSCV
DRAWING NO.

C19

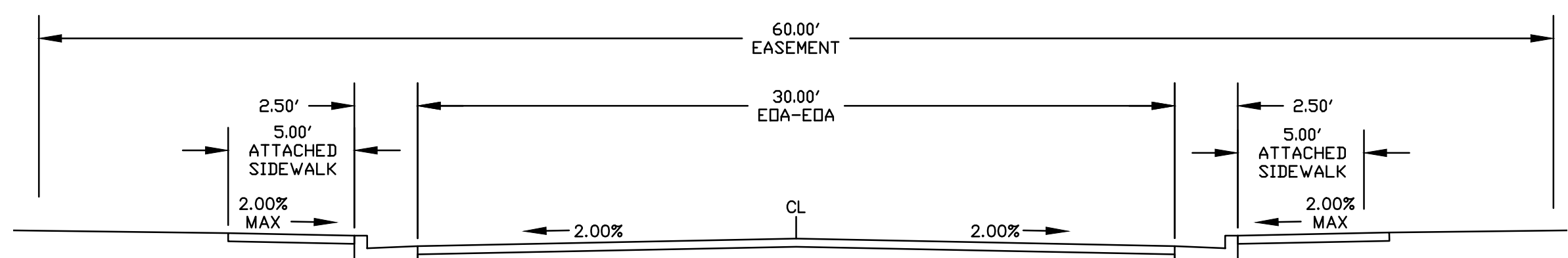
SHEET: 19 OF 29



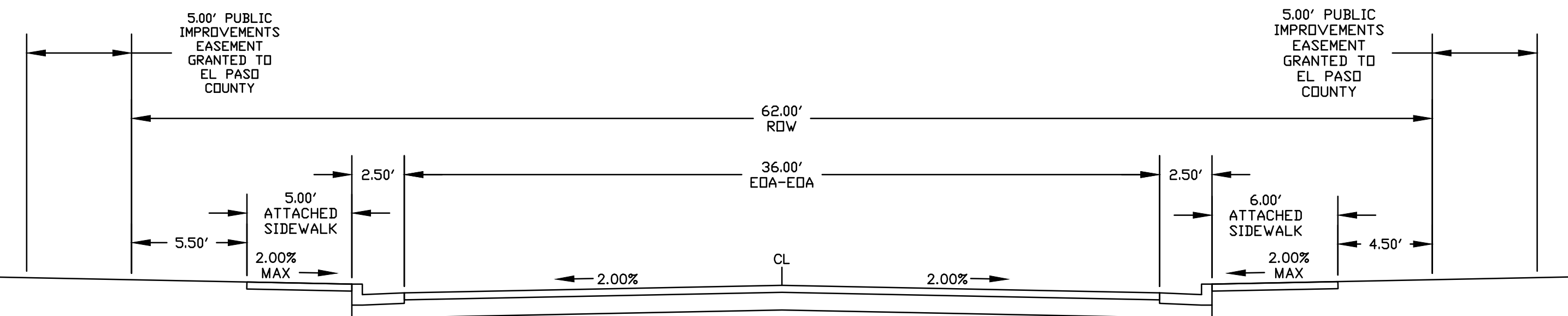
DUNLIN HEIGHTS
(URBAN LOCAL - PRIVATE DRIVE)
SCALE: 1"=5'



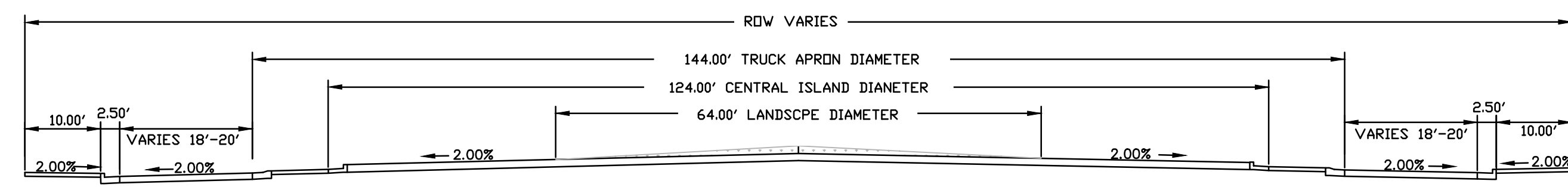
RIO LANE - EXISTING
(RURAL LOCAL - PUBLIC 60' ROW)
SCALE: 1"=5'



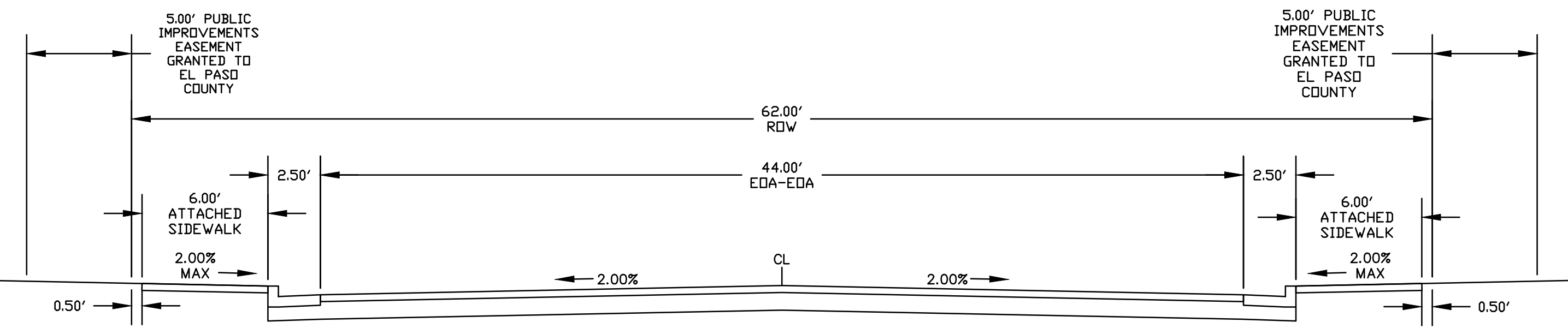
PERULA PT & JACKDAW PT
(URBAN LOCAL - PRIVATE RD.)
SCALE: 1"=5'



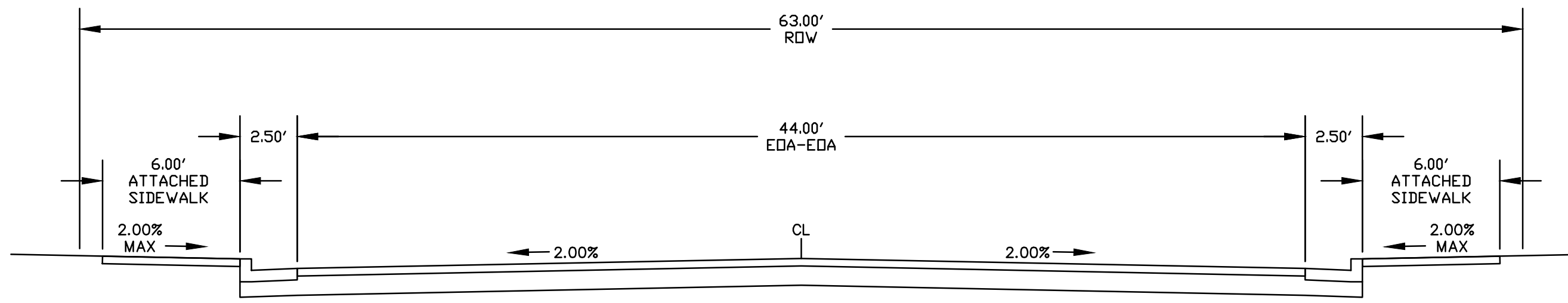
RIO LANE - NORTH OF PERULA POINT
(URBAN LOCAL - PUBLIC 62' ROW)
SCALE: 1"=5'



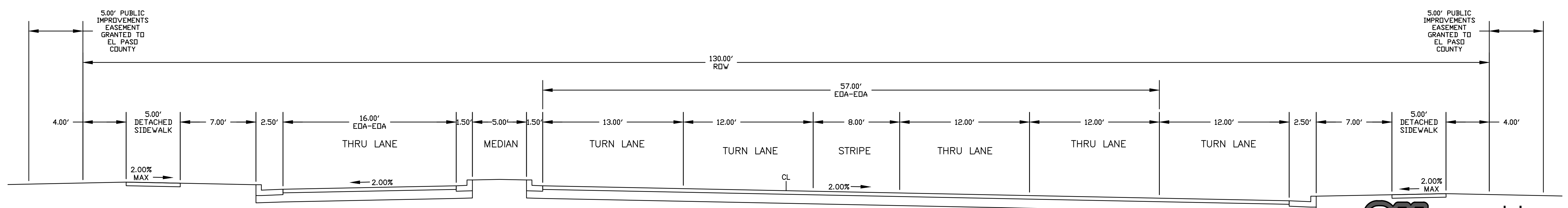
ROUNDBOUNT SECTION
SCALE: NTS



RIO LANE - SOUTH OF PERULA POINT
(URBAN LOCAL - PUBLIC 62' ROW)
SCALE: 1"=5'

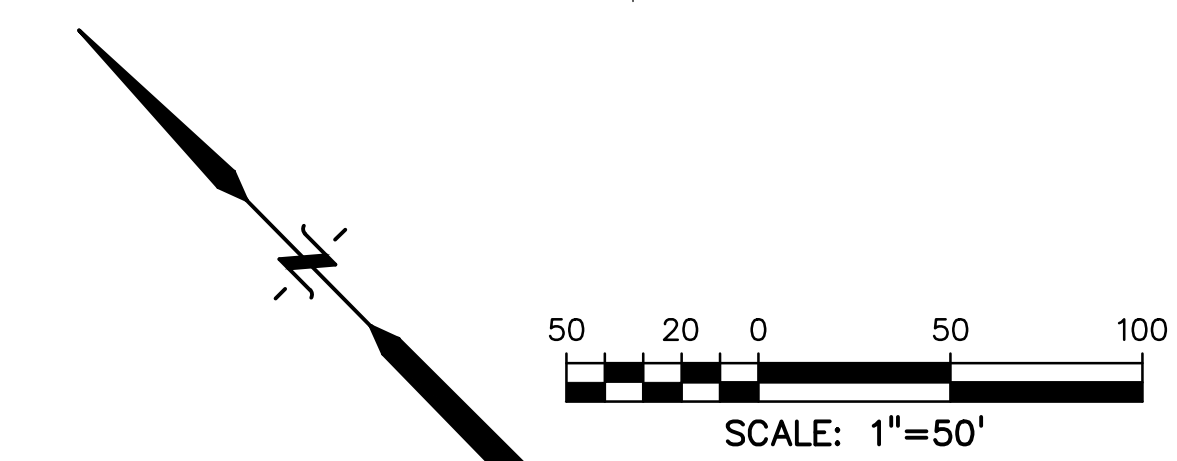
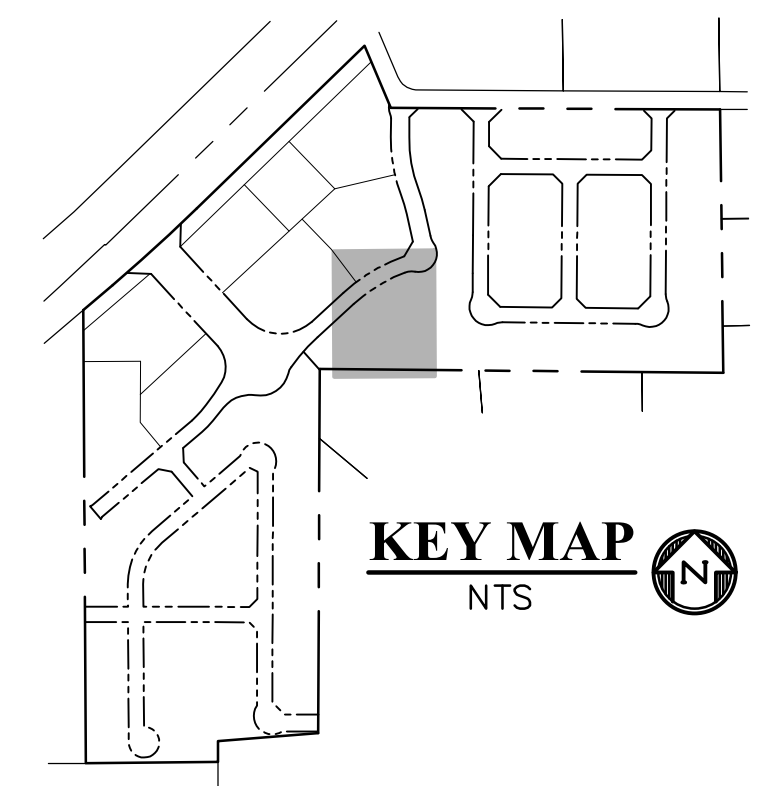
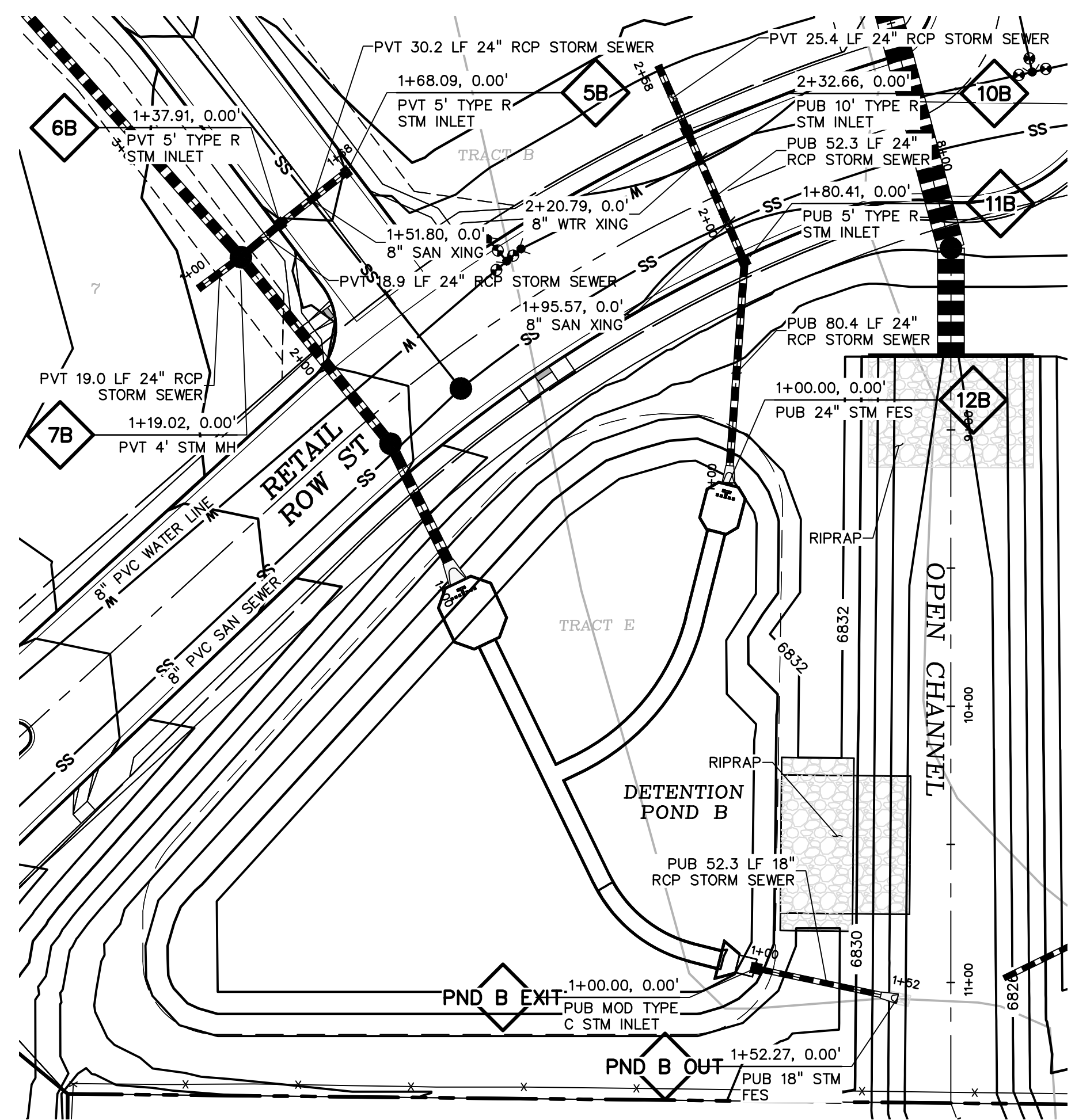


NOTE: PER APPROVED DEVIATION PCD FILE NO. DEV238
RETAIL ROW ST.
(MODIFIED URBAN NON-RESIDENTIAL COLLECTOR - PUBLIC 63' ROW)
SCALE: 1"=5'



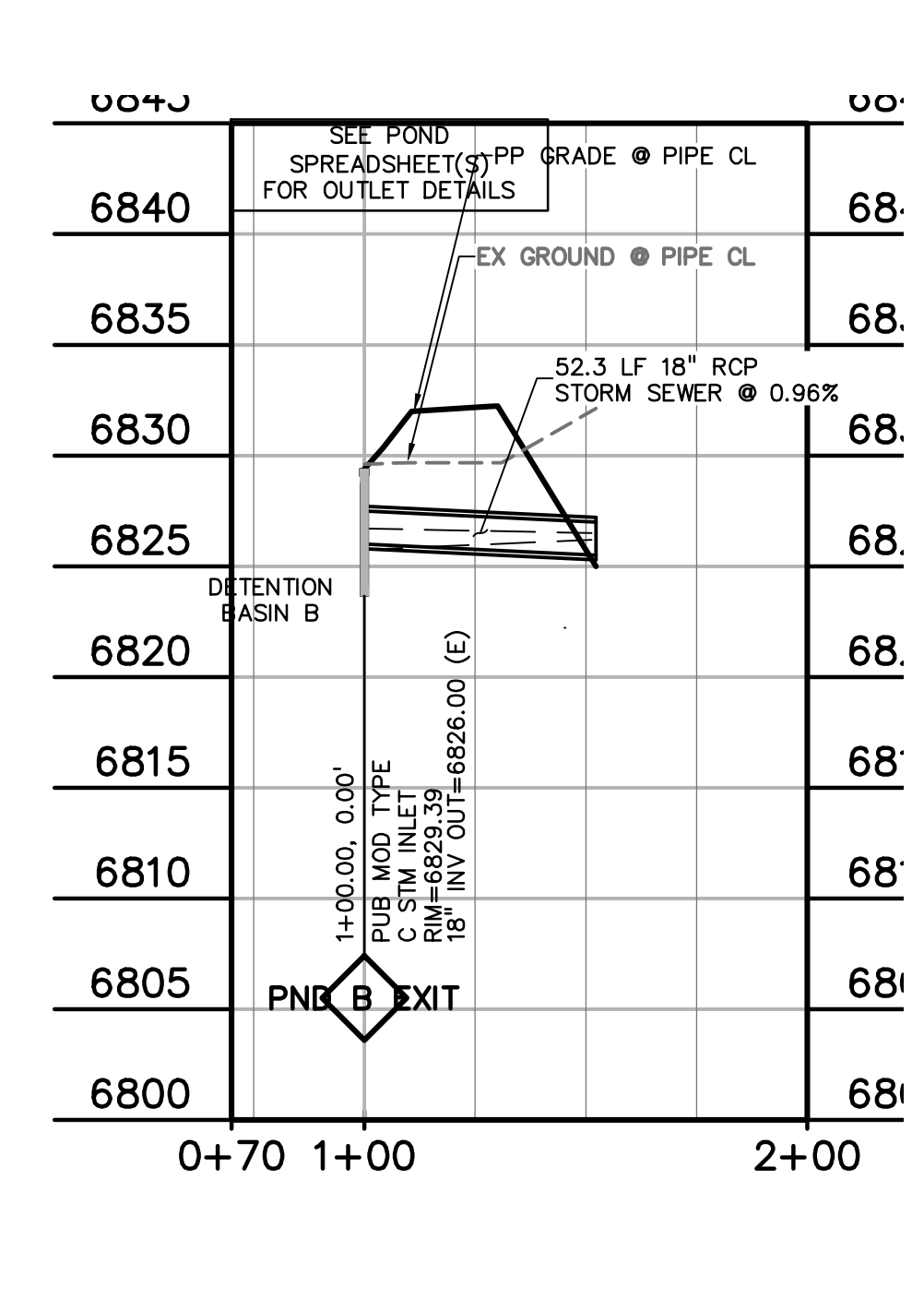
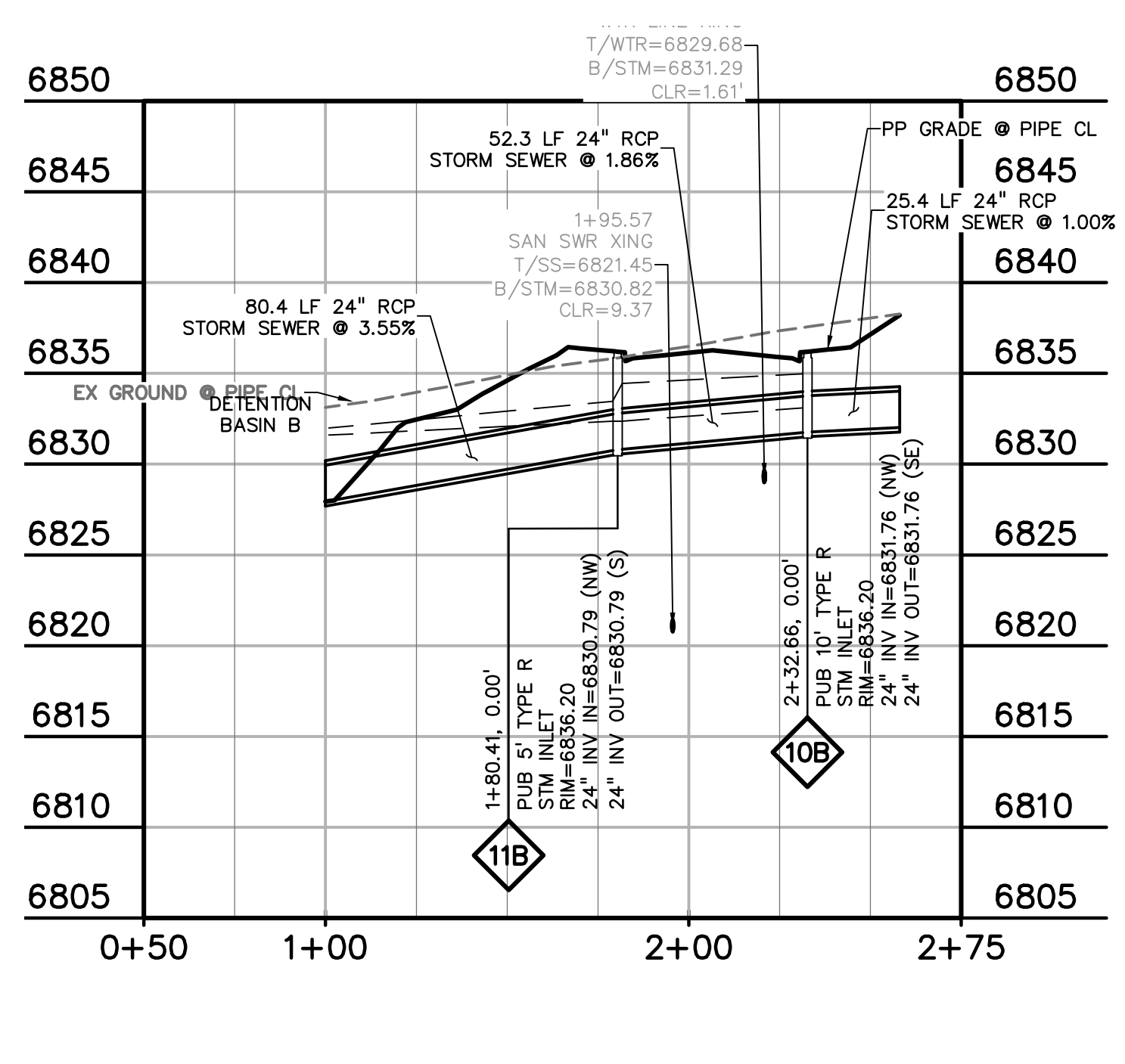
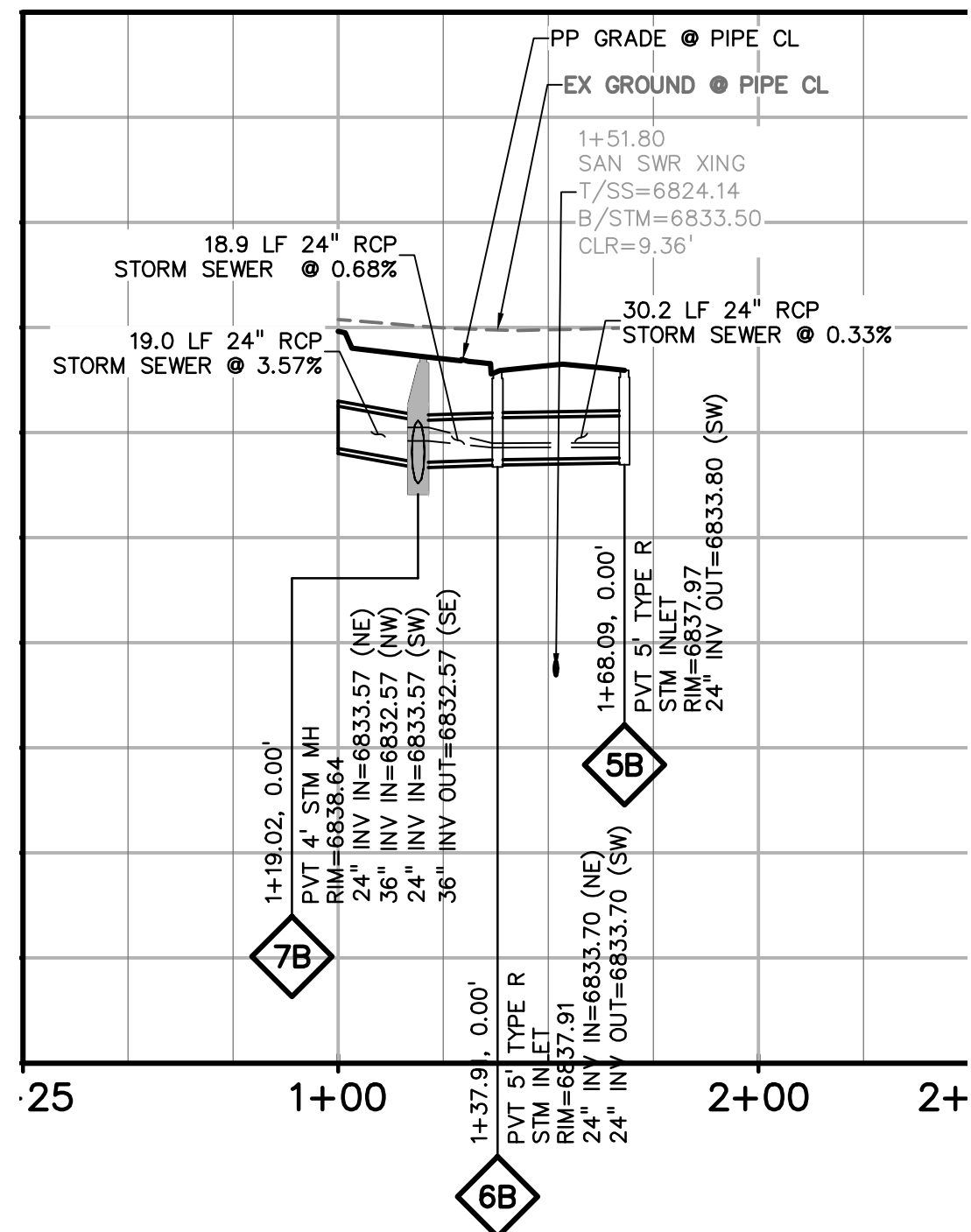
WOODMEN RD.
(URBAN NON-RESIDENTIAL COLLECTOR - PUBLIC 130' ROW)
SCALE: 1"=5'





LEGEND

- PROPERTY LINE
- EASEMENT
- PP RIGHT OF WAY
- PP LOT LINE
- PP CURB & GUTTER
- PP SIDEWALK
- PP STORM SEWER LINE
- HYDRAULIC GRADE LINE
- ENERGY GRADE LINE
- PP STORM INLET
- PP MANHOLE
- PP WATER LINE
- PP FIRE HYDRANT
- PP WATER VALVE
- PP SANITARY SEWER LINE
- PP STORM SEWER MANHOLE DESIGNATOR
- LOT NUMBER
- PP POND
- PP TELEPHONE
- PP TRAIL
- PP GRADE @ ϕ OF PIPE
- EX WATER FIRE HYDRANT
- EX WATER LINE
- EX WT PLUG
- EX SS LINE
- EX SS MH
- EX GROUND @ ϕ OF PIPE
- EX ELECTRIC
- EX 100Y FLOOD
- EX EXISTING
- MH MANHOLE
- PP PROPOSED
- PT POINT
- SS SANITARY SEWER
- ST STORM SEWER
- WT WATER
- XING CROSSING
- C&G CURB AND GUTTER
- PUB PUBLIC



PREPARED BY:

DREXEL, BARRELL & CO.
Engineers & Surveyors
101 SAWATCH ST., SUITE 100
COLORADO SPGS, COLORADO 80903
CONTACT: TIM D. MCCONNELL, P.E.
(719) 260-0887
COLORADO SPRINGS • LAFAYETTE

CLIENT:

PROTERRA PROPERTIES
1864 WOODMOOR DRIVE
MONUMENT, CO 80132
(719) 476-0800
CONTACT: STEVE ROSSOLL
rossoll@proterraco.com

CONSTRUCTION DOCUMENTS FOR:

THE COMMONS AT FALCON FIELD
12445 RIO LANE, AND VACANT LAND
PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
INITIAL ISSUE	12/16/24

DESIGNED BY: KGV
DRAWN BY: CGH
CHECKED BY: TDM
FILE NAME: 21604-STM

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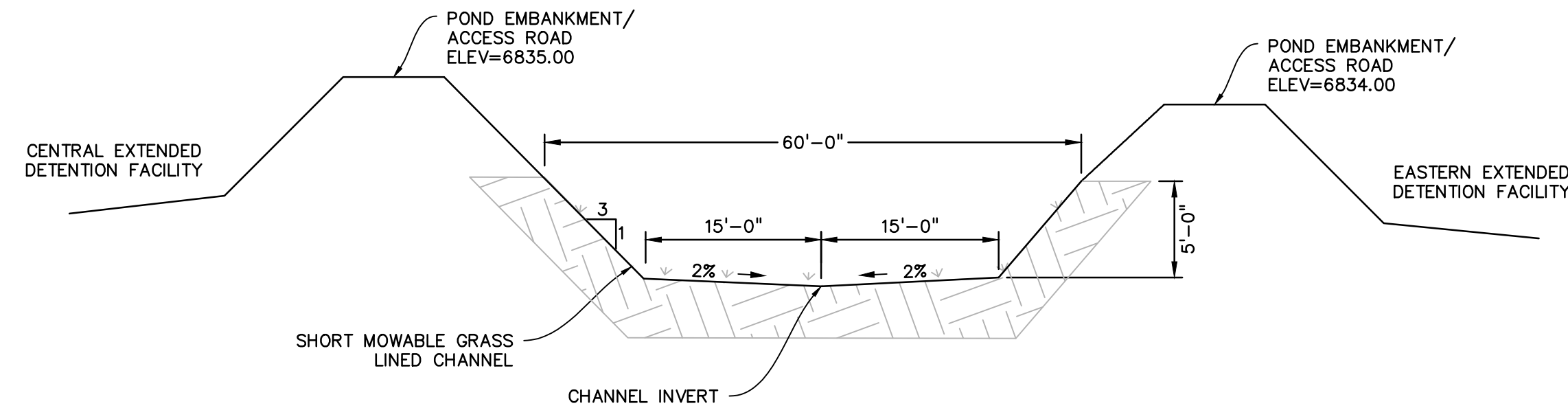
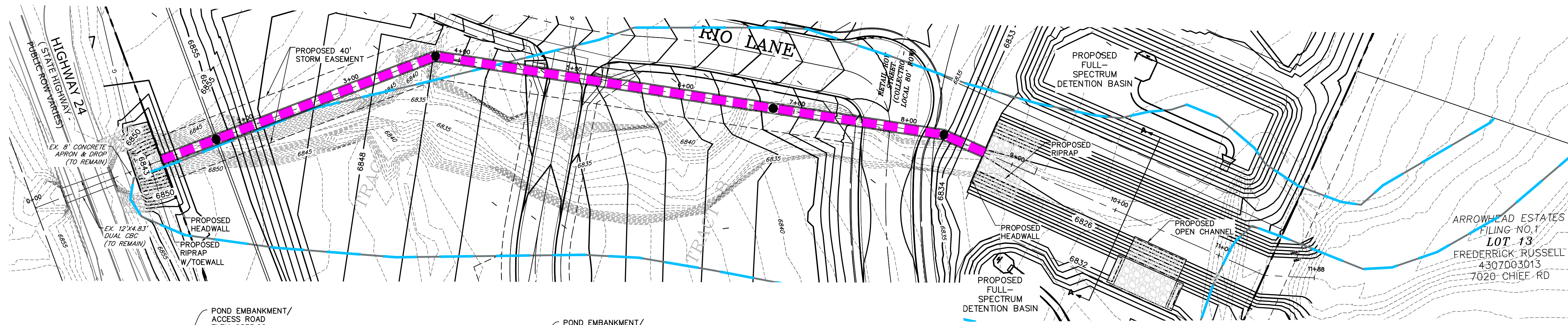
DRAWING SCALE:
HORIZONTAL: 1" = 50'
VERTICAL: 1" = 10'

STORM SEWER CHANNEL PLAN & PROFILE

PROJECT NO. 21604-00CSCV
DRAWING NO.

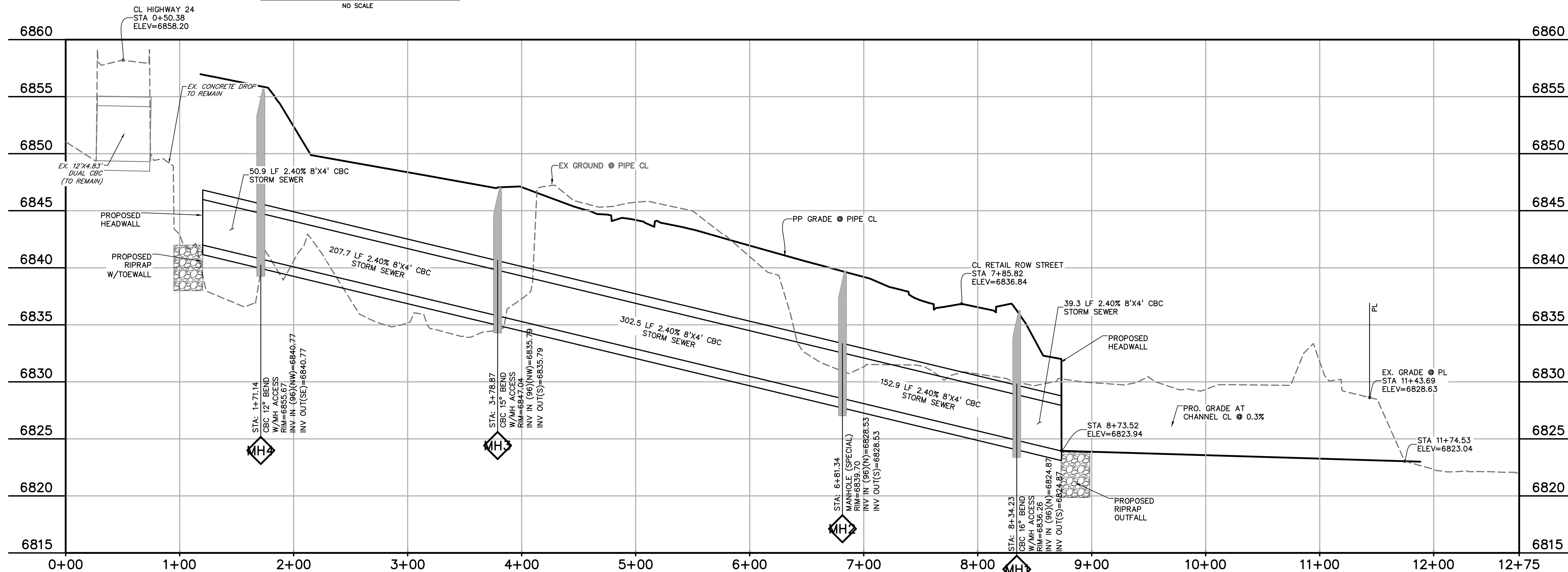
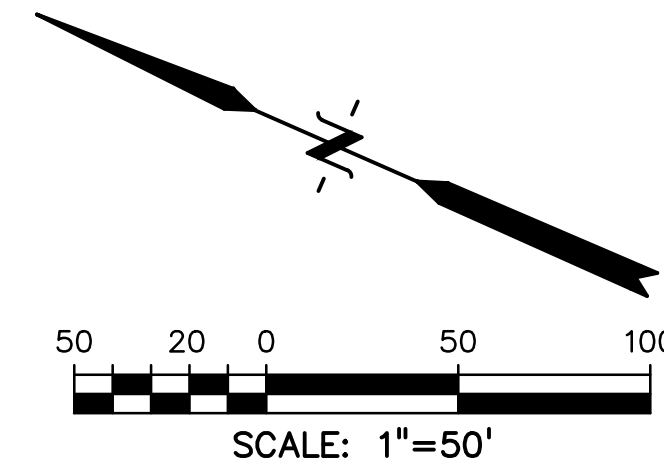
811 Know what's below.
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STM3



LEGEND

- PROPERTY LINE
- LOT LINE
- EASEMENT
- CURB & GUTTER
- SIDEWALK
- PRO. STORM SEWER
- EX. STORM SEWER
- EFFECTIVE 100-YEAR FLOODPLAIN



PREPARED BY:

DREXEL, BARRELL & CO.
 Engineers • Surveyors
 101 SAWATCH ST., SUITE 100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. McCONNELL, P.E.
 (719) 260-0887
 COLORADO SPRINGS • LAFAYETTE

CLIENT:

PROTERRA PROPERTIES
 1864 WOODMOOR DRIVE
 MONUMENT, CO 80132
 (719) 476-0800
 CONTACT: STEVE ROSSOLL
 rossoll@proterra.com

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 PEYTON, EL PASO COUNTY, COLORADO

ISSUE	DATE
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DESIGNED BY: KGV
 DRAWN BY: CGH
 CHECKED BY: TDM
 FILE NAME: 21604-STM-CBC

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: 1" = 50'
 VERTICAL: N/A

PIPE SYSTEM EXHIBIT

PROJECT NO. 21604-00CSCV
 DRAWING NO.

811 Know what's below.
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 CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

EX01

GENERAL NOTES

- CONCRETE SHALL BE CLASS B.
- HEADWALL SHALL BE PERPENDICULAR TO THE PIPE UNLESS OTHERWISE SHOWN ON THE PLANS. TABULATED DIMENSIONS AND QUANTITIES MUST BE ADJUSTED FOR SKEW INSTALLATIONS.
- FOR HEADWALL DETAILS, SEE STANDARD PLAN M-601-10.
- HEADWALLS EXPOSED TO PIPE AND BEING PROTECTED FROM STEEL AND CONCRETE QUANTITIES.
- EXPOSED CONCRETE CORNERS SHALL BE CHAMFERED 3/8 IN.
- ALL REINFORCING BARS SHALL HAVE A 2 IN. MINIMUM CLEARANCE.
- WHEN TWO OR MORE PIPES ARE Laid SIDE BY SIDE, THEY SHALL BE SPACED BY THE SHOWN PIPES AND 3 FT. SPACING BETWEEN HEADWALLS.
- ADD 0.8% OF OR 3 (0.3) WHEN APRON IS REQUIRED.

TYPICAL BAR LAYOUT FOR CONCRETE HEADWALLS

HEADWALL FOR RIGID ROUND PIPE

HEADWALL FOR FLEXIBLE PIPE ARCH

HEADWALL FOR FLEXIBLE ROUND PIPE

HEADWALL FOR STRUCTURAL PLATE ARCH

SKEW FACTOR TABLE

SKEW ANGLE (°)	90	85	80	75	70	65	60	55	50	45	40	35	30
CONCRETE QUANTITY	1.000	1.004	1.015	1.035	1.064	1.103	1.151	1.221	1.305	1.414	1.558	1.743	2.000
STEEL QUANTITY	1.000	1.004	1.015	1.035	1.064	1.103	1.151	1.221	1.305	1.414	1.558	1.743	2.000

Computer File Information

Creation Date: 07/04/12
 Last Modification Date: 07/04/12
 Drawing File Name: 420002000.dgn
 CADD Version: MicroStation V8

Sheet Revisions

Date	Comments

Colorado Department of Transportation
 4201 East Arkansas Avenue
 Denver, Colorado 80222
 Phone: (303) 757-8083
 Fax: (303) 757-8650

HEADWALL FOR PIPES
 STANDARD PLAN NO. M-601-10
 Issued By: Project Development Branch July 4, 2012
 Sheet No. 1 of 1

GENERAL NOTES

- FOR SIZE AND LOCATION OF PIPES, SEE THE PLANS.
- ALL CONCRETE SHALL BE CLASS B.
- FOOTINGS IN ROCK SHALL BE REINFORCED TO ROCK AND SET FIRMED IN ACCORDANCE WITH SUBSECTION 601.03.03.
- EXPOSED CONCRETE CORNERS SHALL BE CHAMFERED 3/8 IN.
- HEADWALL SHALL HAVE REINFORCING STEEL INSTALLED IN A PATTERN SIMILAR TO STANDARD PLAN M-601-10.
- THE COST OF REINFORCING STEEL SHALL BE INCLUDED IN THE WORK UNLESS THE STEEL QUANTITIES ARE LISTED IN THE PLANS AND ARE PAID FOR SEPARATELY.

CONCRETE HEADWALL INSTALLATIONS

PIPE OUTLET PAVING

CONCRETE QUANTITIES FOR ONE CONCRETE HEADWALL (CUBIC YARDS)

PIPE TYPE	PIPE DIAMETER (AND EQUIVALENT DIAMETER) (IN)								
	18	24	30	36	42	48	54	60	
CIRCULAR	ROOD	1.0	1.3	1.5	2.0	2.7	3.6	4.6	6.0
	FLEXIBLE	1.1	1.4	1.6	2.1	2.7	3.0	4.0	5.2
ELLIPTICAL	ROOD	23 x 14	30 x 18	36 x 24	45 x 27	54 x 34	63 x 38	72 x 44	81 x 50
	FLEXIBLE	22 x 13	29 x 18	36 x 22	43 x 27	50 x 31	58 x 36	66 x 41	75 x 47
ARCH	METAL	0.9	1.3	1.4	1.9	1.8	2.4	2.4	3.2
	CONCRETE	0.9	1.3	1.4	1.9	1.8	2.4	2.4	3.2

PIPE OUTLET PAVING (CUBIC YARDS)

THICKNESS	MATERIAL	PIPE DIAMETER (IN)			
		18	24	36	48
4"	CONCRETE	0.4	0.8	1.2	2.6
8"	CONCRETE	0.4	0.8	1.2	2.6
18"	RIPRAP	2.0	3.5	5.4	7.8

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Sheet Revisions

Date	Comments

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 4201 East Arkansas Avenue
 Denver, Colorado 80222
 Phone: (303) 757-8083
 Fax: (303) 757-8650

HEADWALLS AND PIPE OUTLET PAVING
 STANDARD PLAN NO. M-601-12
 Issued By: Project Development Branch July 4, 2012
 Sheet No. 1 of 1

NOTE

TYPE III MANHOLES SHALL BE USED WHEN APPROPRIATE AND TYPICALLY WHEN THE FOLLOWING CONDITIONS ARE MET:

- PIPE IS 48" OR LARGER INSIDE DIAMETER.
- NO CHANGE IN PIPE SIZE.
- NO CHANGE IN PIPE MATERIAL.
- NO CHANGE IN HORIZONTAL ALIGNMENT.
- SLOPE IS FLAT AND CONTINUOUS.

2. TYPE III MANHOLES SHALL BE FABRICATED BY THE MANUFACTURER/SUPPLIER AND DELIVERED TO THE SITE AS A SINGLE UNIT. FIELD FABRICATION SHALL NOT BE PERMITTED.

3. EITHER LADDER OR STEPS SHALL BE INSTALLED. LOWEST STEP SHALL BE A MAXIMUM OF 30" ABOVE THE INVERT OF THE PIPE.

PLAN VIEW

SECTION VIEW

R.C.P. CONNECTION DETAIL

SPECIAL LID FOR USE WITH C.S.P. RISER

SCALE: NOT TO SCALE

Computer File Information

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 Last Modification Date: 07/04/12
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 CADD Version: MicroStation V8

Sheet Revisions

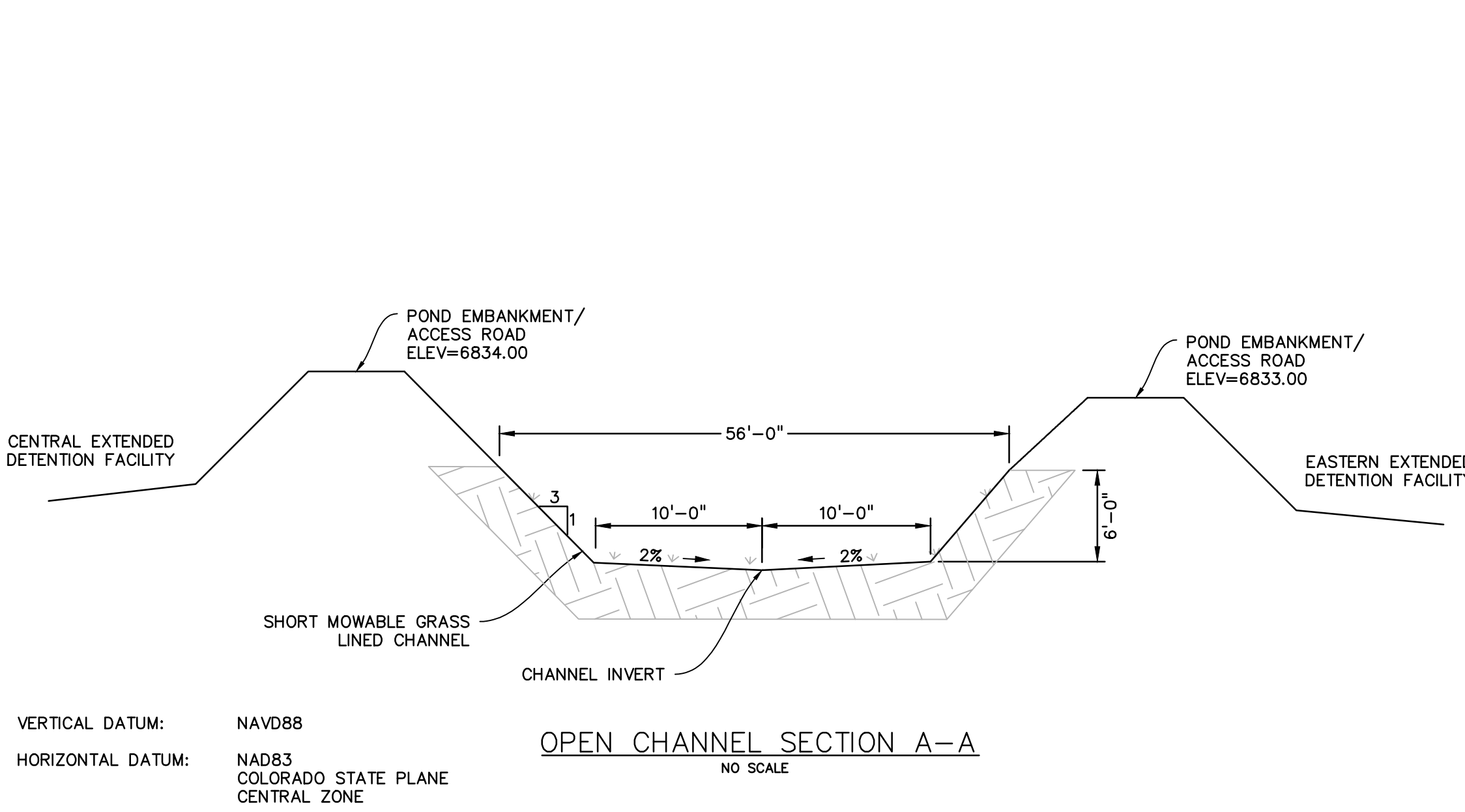
Date	Comments

Colorado Department of Transportation
 4201 East Arkansas Avenue
 Denver, Colorado 80222
 Phone: (303) 757-8083
 Fax: (303) 757-8650

Storm Sewer Manhole Detail Type III
 Standard Drawing
 DATE APPROVED: 8/11/11
 André P. Brackin
 DEPARTMENT OF TRANSPORTATION

REVISION DATE: 11/10/04
 FILE NAME: SD_3-3

EL PASO COUNTY
 DEPARTMENT OF TRANSPORTATION



PREPARED BY:

DREXEL, BARRELL & CO.
 Engineers-Surveyors
 101 SAWATCH ST., SUITE 100
 COLORADO SPGS, COLORADO 80903
 CONTACT: TIM D. MCCONNELL, P.E.
 (719)260-0887
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CLIENT:

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 1864 WOODMOOR DRIVE
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 CONTACT: STEVE ROSSOLL
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 PEYTON, EL PASO COUNTY, COLORADO

ISSUE DATE

INITIAL ISSUE 12/16/24

DESIGNED BY: KGV
 DRAWN BY: CGH
 CHECKED BY: TDM
 FILE NAME: 21604-STM-CBC

PREPARED UNDER MY DIRECT SUPERVISION FOR AND ON BEHALF OF DREXEL, BARRELL & CO.

DRAWING SCALE:
 HORIZONTAL: N/A
 VERTICAL: N/A

DETAILS

PROJECT NO. 21604-00CSV
 DRAWING NO.

