

CROSSROADS AT MEADOWBROOK-MIXED USE PRELIMINARY PLAN

LOCATED IN THE SOUTHWEST 1/4 OF SECTION 8, T14S, R65W, OF THE 6th P.M., EL PASO COUNTY, COLORADO

LEGAL DESCRIPTION:

TRACT B, 24th BUSINESS PARK FILING NO. 1, RECORDED APRIL 14TH, 2017 AS RECEIPTION NUMBER 21771930 AND AMENDED BY AFFIDAVIT OF CORRECTION RECEPTION NUMBER 219097386 IN THE RECORDS OF EL PASO COUNTY, COLORADO, LOCATED IN THE SOUTHWEST 1/4 OF SECTION 8, TOWNSHIP 14 SOUTH, RANGE 65 WEST, OF THE 6TH P.M.

PRELIMINARY PLAN NOTES

- THE FOLLOWING REPORTS HAVE BEEN SUBMITTED IN ASSOCIATION WITH THE PRELIMINARY PLAN FOR THIS SUBDIVISION AND ARE ON FILE AT THE COUNTY PLANNING AND COMMUNITY DEVELOPMENT DEPARTMENT: TRANSPORTATION IMPACT STUDY; DRAINAGE REPORT; WATER RESOURCES REPORT; WASTEWATER DISPOSAL REPORT; GEOLOGY AND SOILS REPORT; FIRE PROTECTION REPORT; NATURAL FEATURES REPORT.
- ALL PROPERTY OWNERS ARE RESPONSIBLE FOR MAINTAINING PROPER STORM WATER DRAINAGE IN AND THROUGH THEIR PROPERTY. PUBLIC DRAINAGE EASEMENTS AS SPECIFICALLY NOTED ON THE PLAN SHALL BE MAINTAINED BY THE LOT OWNERS UNLESS OTHERWISE INDICATED. STRUCTURES, FENCES, MATERIALS OR LANDSCAPING THAT COULD IMPED THE FLOW OR RUNOFF SHALL NOT BE PLACED IN DRAINAGE EASEMENTS.
- UNLESS OTHERWISE INDICATED, ALL FRONT, SIDE AND REAR LOT LINES ARE TO BE PLATTED ON EITHER SIDE WITH A 10 FOOT PUBLIC UTILITY AND DRAINAGE EASEMENT. ALL EXTERIOR SUBDIVISION BOUNDARIES ARE HEREBY PLATTED WITH A 10 FOOT PUBLIC UTILITY AND DRAINAGE EASEMENT. THE SOLE RESPONSIBILITY FOR MAINTENANCE OF THESE EASEMENTS IS HEREBY VESTED WITH THE INDIVIDUAL PROPERTY OWNERS.
- DEVELOPER SHALL COMPLY WITH FEDERAL AND STATE LAWS, REGULATIONS, ORDINANCES, REVIEW AND PERMIT REQUIREMENTS, AND OTHER AGENCY REQUIREMENTS, IF ANY, OF APPLICABLE AGENCIES INCLUDING, BUT NOT LIMITED TO, THE COLORADO DIVISION OF WILDLIFE, COLORADO DEPARTMENT OF TRANSPORTATION, U.S. ARMY CORPS OF ENGINEERS, AND THE U.S. FISH AND WILDLIFE SERVICE REGARDING THE ENDANGERED SPECIES ACT, PARTICULARLY AS RELATED TO THE LISTED SPECIES IDENTIFIED IN THE PROJECTS ENVIRONMENTAL ASSESSMENT.
- NO DRIVEWAY SHALL BE ESTABLISHED UNLESS AN ACCESS PERMIT HAS BEEN GRANTED BY EL PASO COUNTY.
- MAILBOXES SHALL BE INSTALLED IN ACCORDANCE WITH EL PASO COUNTY AND US POSTAL SERVICE REGULATIONS.
- EXCEPT AS OTHERWISE NOTED ON THE PRELIMINARY PLAN, INDIVIDUAL LOT PURCHASERS ARE RESPONSIBLE FOR CONSTRUCTING DRIVEWAYS, INCLUDING NECESSARY DRAINAGE CURBSETS PER LAND DEVELOPMENT CODE SECTION 8.3.3.C.2 AND 6.2.2.C.3. DUE TO THEIR LENGTH, SOME OF THE DRIVEWAYS WILL NEED TO BE APPROVED BY THE CIMARRON HILLS FIRE PROTECTION DISTRICT.
- AT THE TIME OF APPROVAL OF THIS PROJECT, THIS PROPERTY IS LOCATED WITHIN THE CIMARRON HILLS FIRE PROTECTION DISTRICT, WHICH HAS ADOPTED A FIRE CODE WITH FIRE MITIGATION REQUIREMENTS DEPENDING UPON THE LEVEL OF FIRE RISK ASSOCIATED WITH THE PROPERTY AND STRUCTURES. THE OWNER OF ANY LOT SHOULD CONTACT THE FIRE DISTRICT TO DETERMINE THE EXACT DEVELOPMENT REQUIREMENTS RELATIVE TO THE ADOPTED FIRE CODE.
- NO-BUILD AREAS ARE AS SHOWN ON THE PRELIMINARY PLAN. NO-BUILD AREAS INCLUDE, BUT ARE NOT LIMITED TO AREAS, WITHIN DESIGNATED DRAINAGE EASEMENTS, LANDSCAPE BUFFERS, SETBACKS SIGHT DISTANCE TRIANGLES, ETC., NO-BUILD AREAS WITHIN INDIVIDUAL LOTS ARE THE RESPONSIBILITY OF THE PROPERTY OWNER. ALL OTHER NO-BUILD AREAS SHALL BE THE RESPONSIBILITY OF MEADOWBROOK CROSSING METROPOLITAN DISTRICT NO. 1.
- THIS PROPERTY MAY BE ADVERSELY IMPACTED BY NOISE, DUST, FUMES, AND LIGHT POLLUTION CAUSED BY ADJACENT INDUSTRIAL PROPERTIES AND ACTIVITIES. THE BUYER SHOULD RESEARCH AND BE AWARE OF THIS POTENTIALITY AND THE RAMIFICATIONS THEREOF.
- SIGNAGE IS NOT APPROVED WITH THIS PLAN. A SEPARATE SIGN PERMIT IS REQUIRED. CONTACT THE EL PASO COUNTY PLANNING AND COMMUNITY DEVELOPMENT DEPARTMENT AT 2880 INTERNATIONAL CIRCLE FOR A SIGN PLAN APPLICATION.
- RETAINING WALLS EXCEEDING 4- FEET WILL NEED TO BE STRUCTURALLY ENGINEERED.
- ALL "STOP SIGNS" AND OTHER TRAFFIC CONTROL SIGNAGE SHALL BE INSTALLED BY THE DEVELOPER AT LOCATIONS SHOWN ON THE SITE DEVELOPMENT PLAN TO MEET MUTCD STANDARDS.
- PRIOR TO BUILDING PERMIT APPROVAL, A FINAL SUBDIVISION PLAT SHALL BE SUBMITTED FOR REVIEW BY THE EL PASO COUNTY PLANNING & COMMUNITY DEVELOPMENT DEPARTMENT, AND APPROVAL BY THE BOARD OF COUNTY COMMISSIONERS, AND THE PLAT MUST BE RECORDED, OR AUTHORIZED DESIGNEE, AND THE PLAT SHALL BE RECORDED.
- FOUR TO SIX FOOT TALL DETENTION FENCE WILL BE CONSTRUCTED ALONG THE PERIMETER OF THE AREA OF DEVELOPMENT, AS NOTED ON THE PLANS. THE MATERIAL AND LOCATION SHALL BE PROVIDED WITH THE SITE DEVELOPMENT PLAN. THIS FENCING MAY ENCRACH INTO DESIGNATED NO-BUILD AREAS AS LONG AS THE ENCRACHING SECTIONS DO NOT IMPED SURFACE WATER RUNOFF.
- ALL STREETS SHALL BE NAMED AND CONSTRUCTED TO EL PASO COUNTY STANDARDS AND/OR ANY APPROVED DEVIATIONS. PRIVATE STREETS SHALL BE OWNED AND MAINTAINED BY THE MEADOWBROOK CROSSING METROPOLITAN DISTRICT NO. 1.
- NOTWITHSTANDING ANY AND/OR ASSOCIATED PUBLIC IMPROVEMENTS OR GRAPHIC REPRESENTATION, ALL DESIGN AND CONSTRUCTION RELATED TO ROADS, STORM DRAINAGE AND EROSION CONTROL SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE MOST RECENT VERSION OF THE RELEVANT ADOPTED EL PASO COUNTY STANDARDS, INCLUDING THE LAND DEVELOPMENT CODE (LDC), THE ENGINEERING CRITERIA MANUAL, THE DRAINAGE CRITERIA MANUAL (DCM), AND DCM VOLUME 2. ANY DEVIATIONS FROM THESE STANDARDS MUST BE SPECIFICALLY REQUESTED AND APPROVED IN WRITING TO BE ACCEPTABLE. THE APPROVAL OF THIS PRELIMINARY PLAN DOES NOT IMPLICITLY ALLOW ANY DEVIATIONS OR WAIVERS THAT HAVE NOT BEEN OTHERWISE APPROVED THROUGH THE DEVIATION APPROVAL PROCESS.
- DEVELOPMENT OF THE PROPERTY WILL BE IN ACCORDANCE WITH THE MOST RECENT VERSION OF THE EL PASO COUNTY LAND DEVELOPMENT CODE, AS AMENDED, FOR CR AND RM-30 ZONES.
- THIS PROPERTY IS NOT LOCATED WITHIN A DESIGNATED FEMA FLOODPLAIN AS DETERMINED BY THE FLOOD INSURANCE RATE MAP, COMMUNITY MAPS NUMBERED '08041C0754C', DATED DECEMBER 7, 2018, THE LIMITS OF WHICH ARE SHOWN ON THE SURVEY. AREAS WITHIN THE FLOODPLAIN ARE ZONE AE, AREAS OUTSIDE THE FLOODPLAIN ARE ZONE X.
- WATER AND WASTEWATER SERVICES FOR THIS SUBDIVISION WILL BE PROVIDED BY THE CHEROKEE METROPOLITAN DISTRICT (CHEROKEE) SUBJECT TO THE DISTRICT'S RULES, REGULATIONS AND SPECIFICATIONS. THE OFFICE OF THE STATE ENGINEER HAS ISSUED AN OPINION OF WATER INADEQUACY BASED ON ITS ANALYSIS AND INTERPRETATION OF A STIPULATED AGREEMENT CONCERNING THE AVAILABILITY OF CERTAIN WATER RIGHTS FOR USE OUTSIDE OF THE UPPER BLACK SQUIRREL CREEK DESIGNATED BASIN, AND THUS FOUND INSUFFICIENCY OF WATER RESOURCES FOR THIS SUBDIVISION BASED ON THAT AGREEMENT. THIS INTERPRETATION DIFFERS FROM CERTAIN OPINIONS ISSUED BY THE OFFICE IN THE PAST. BASED ON ITS OWN REVIEW OF THE STIPULATED AGREEMENT AND ITS HISTORY (AND NOT THE AMOUNT OF WATER ACTUALLY AVAILABLE) THE BOARD OF COUNTY COMMISSIONERS IN AN OPEN AND PUBLIC HEARING DID NOT ACCEPT THE INTERPRETATION OF THE STATE ENGINEER'S OFFICE. THE BOARD OF COUNTY COMMISSIONERS FOUND THAT CHEROKEE HAS COMMITTED TO PROVIDE WATER SERVICE TO THE SUBDIVISION AND ASSERTED THAT ITS LONG TERM WATER SERVICE CAPABILITIES ARE SUFFICIENT. THE BOARD OF COUNTY COMMISSIONERS MADE THIS DETERMINATION IN RELIANCE UPON THE TESTIMONY AND EXPERTISE PROVIDED BY CHEROKEE AT THE PUBLIC HEARING THEREON. AT THE HEARING, CHEROKEE ASSERTED THAT ITS PLANS AND CONTINUED FINANCIAL INVESTMENT IN INFRASTRUCTURE ARE DESIGNED TO ALLOW CHEROKEE TO CONTINUE TO PROVIDE THIS SUBDIVISION AND ITS EXISTING CUSTOMERS WITH WATER AND WASTEWATER SERVICES FOR 300 YEARS OR MORE.
- THE PROPERTY IS LOCATED IN THE AIRPORT OVERLAP ZONE. THIS SERVES AS NOTICE OF POTENTIAL AIRCRAFT OVERFLIGHT AND NOISE IMPACTS ON THIS PROPERTY DUE TO ITS CLOSE PROXIMITY TO AN AIRPORT, WHICH IS BEING DISCLOSED TO ALL PROSPECTIVE PURCHASERS CONSIDERING THE USE OF THIS PROPERTY FOR RESIDENTIAL AND OTHER PURPOSES. THIS PROPERTY IS SUBJECT TO THE OVERFLIGHT AND ASSOCIATED NOISE OF ARRIVING AND DEPARTING AIRCRAFT DURING THE COURSE OF NORMAL AIRPORT OPERATIONS. ALL PROPERTY WITHIN THIS SUBDIVISION IS SUBJECT TO AN AVIGATION EASEMENT AS RECORDED IN BOOK 2478 AT PAGE 304, OF THE RECORDS OF THE EL PASO COUNTY CLERK AND RECORDER.
- A 30 DB(A) INDOOR NOISE REDUCTION SHALL BE ACHIEVED BY APPROVED CONSTRUCTION TECHNIQUES AS EVIDENCED BY A NOISE REDUCTION CERTIFICATE PRIOR TO SITE DEVELOPMENT PLAN APPROVAL.
- ALL PROPERTY WITHIN THIS SUBDIVISION WILL BE SUBJECT TO A RECIPROCAL ACCESS/PARKING AGREEMENT. EASEMENT(S) SHALL BE ESTABLISHED BY SUBSEQUENT FINAL PLAT(S) OR SEPARATE INSTRUMENT(S) ACCOMPANYING THE FINAL PLAT.
- A TITLE 32 SPECIAL DISTRICT ANNUAL REPORT AND DISCLOSURE FORM SATISFACTORY TO THE PLANNING AND COMMUNITY DEVELOPMENT DEPARTMENT SHALL BE RECORDED WITH EACH PLAT.
- LANDSCAPE IMPROVEMENTS AND MAINTENANCE SHALL BE THE RESPONSIBILITY OF THE OWNER OR DESIGNATED ENTITY APPROVED BY THE BOCC.
- LANDSCAPING SHALL CONFORM TO THE REQUIREMENTS OF CHAPTER 6 OF THE COUNTY CODE TOGETHER WITH ALL APPLICABLE CONDITIONS OF APPROVAL IMPOSED BY THE BOARD OF COUNTY COMMISSIONERS.

FLOODPLAIN NOTES:

- PORTIONS OF THIS PROPERTY ARE LOCATED WITHIN A DESIGNATED FEMA FLOODPLAIN AS DETERMINED BY THE FLOOD INSURANCE RATE MAP, COMMUNITY MAP NUMBER '08041C0754C', EFFECTIVE DATE 'DECEMBER 7, 2018'.

PROJECT TEAM:

OWNER/ DEVELOPER:
CROSSROADS METROPOLITAN DISTRICT NO. 1
C/O THE EQUITY GROUP
ATTN: DANNY MIENKA
90 SOUTH CASCADE AVENUE, SUITE 1500
COLORADO SPRINGS, CO 80903

PLANNERS/ LANDSCAPE ARCH.:
KIMLEY-HORN
2 NORTH NEVADA AVENUE
SUITE 300
COLORADO SPRINGS, CO 80903

CIVIL ENGINEER:
MS CIVIL CONSULTANTS, INC.
102 PIKES PEAK AVENUE
5TH FLOOR
COLORADO SPRINGS, CO 80903

SHEET INDEX:

01	COVER SHEET
02	PRELIMINARY PLAN
03	LANDSCAPE SHEET
04	LANDSCAPE STREET DETAIL PLAN

SITE DATA TABLE:

TAX ID NUMBER:	5408007005
CURRENT ZONING:	CR RS-5000 CAD-O
PROPOSED ZONING:	CR CAD-O
PROPOSED LOTS:	11
TOTAL SITE ACREAGE:	29.03 AC
MINIMUM LOT SIZE:	0.72 AC
FRONTAGE SETBACKS:	50' FRONT, 25' SIDE/ REAR, 0' SIDE/REAR SETBACK ADJACENT TO CR ZONE
LOT SETBACKS:	50' FRONT, 25' SIDE/ REAR, 0' SIDE/REAR SETBACK ADJACENT TO CR ZONE
LANDSCAPE SETBACKS:	Expressway, Principal Arterial: 25' SETBACK (1 TREE/25' FRONTAGE) Minor Arterial: 20' SETBACK (1 TREE/25' FRONTAGE) Non-Arterial: 10' SETBACK (1 TREE/30' FRONTAGE)
MAXIMUM LOT COVERAGE:	NONE
MAXIMUM BUILDING HEIGHT:	50' (UNLESS OTHERWISE IMPACTED BY CAD-O RESTRICTIONS)

SOILS & GEOLOGY CONDITIONS, CONSTRAINTS, & HAZARDS NOTE

- A SOILS AND GEOLOGY STUDY FOR COLORADO SPRINGS EQUITIES, LLC, WAS COMPLETED BY RMG ENGINEERS ON AUGUST 6, 2020 AND INCLUDED THE AREA OF DEVELOPMENT PROPOSED WITH THE CROSSROADS APARTMENTS (ALSO KNOWN AS, CROSSROADS MIXED-USE) THE GEOLOGY AND SOILS REPORT FOR THE CROSSROADS MIXED-USE DEVELOPMENT WAS PROVIDED AS PART OF THE CROSSROADS MIXED-USE PRELIMINARY PLAN SUBMITTED TO THE EL PASO BOARD OF COUNTY COMMISSIONERS, PLANNING AND COMMUNITY DEVELOPMENT FILE NUMBER (TBD). DEVELOPERS AND HOME OWNERS SHOULD BECOME FAMILIAR WITH THIS REPORT AND ITS CONTENTS. **
- THERE ARE NO SIGNIFICANT GEOLOGICAL HAZARDS; HOWEVER, THE POTENTIAL FOR GEOLOGIC CONSTRAINTS DO EXIST RELATED TO THE POTENTIAL FOR SHALLOW GROUNDWATER TABLES, EXPANSIVE CLAY OR CLAYEY SANDSTONE, ERODIBLE SANDY SOILS. THESE GEOLOGICAL CONDITIONS ARE CONSIDERED RELATIVELY COMMON TO THE AREA WITH MITIGATION ACCOMPLISHED BY IMPLEMENTING COMMON ENGINEERING AND CONSTRUCTION PRACTICES. IF THE PREVIOUSLY LISTED POTENTIAL GEOLOGIC HAZARDS ARE FOUND TO EXIST, AN EVALUATION SHALL BE PERFORMED AT THE TIME OF FINAL GEOTECHNICAL INVESTIGATION FOR THOSE INDIVIDUAL LOTS. **
- THE PROPOSED DEVELOPMENT IS FEASIBLE. THE GEOLOGIC CONDITIONS IDENTIFIED POTENTIALLY HYDROCOMPACTIVE SOILS, SEISMICITY, RADON, AND EROSION, BUT THESE CONDITIONS ARE CONSIDERED TYPICAL FOR THE FRONT RANGE REGIONS OF COLORADO. MITIGATION OF GEOLOGIC CONDITIONS IS MOST EFFECTIVELY ACCOMPLISHED BY AVOIDANCE. HOWEVER, WHERE AVOIDANCE IS NOT A PRACTICAL OR ACCEPTABLE ALTERNATIVE, GEOLOGIC CONDITIONS SHOULD BE MITIGATED BY IMPLEMENTING APPROPRIATE PLANNING, ENGINEERING AND SUITABLE CONSTRUCTION PRACTICES. **
- SITE-SPECIFIC SOILS STUDIES SHALL BE PERFORMED FOR THE LOTS WITHIN THIS SUBDIVISION PRIOR TO FOUNDATION CONSTRUCTION TO IDENTIFY SUBSURFACE SOIL CONDITIONS ANTICIPATED TO SUPPORT FOUNDATIONS AND PROVIDE PERTINENT GEOTECHNICAL-RELATED PARAMETERS AND RECOMMENDATIONS FOR FOUNDATION DESIGN AND CONSTRUCTION. **

**REFER TO THE SOILS REPORT FOR MORE DETAILED INFORMATION.

These notes are from the recommendations page in report for this project to be added

? IS IT IN THAT DISTRICT?

DETAILS, AND SHOW ON PRELIM PLAN, CONSTRUCTED BY WHOM?

Lots 1-10 ACCESS have NO access to Newt, Hwy 24, Meadowbrook parkway. Lot 11 has an access to Meadowbrook, no access to Hwy 24.

In addition to the previously identified mitigation alternatives, surface and subsurface drainage systems should be considered. Exterior, perimeter foundation drains should be installed around below-grade habitable or storage spaces. Surface water should be efficiently removed from the building area to prevent ponding and infiltration into the subsurface soil.
The foundation systems for the proposed commercial structures, retaining walls greater than 4 feet, and any retention/detention facilities should be designed and constructed based upon recommendations developed in a site-specific subsurface soil investigation.

Add the following preliminary plan notes:
- There shall be no direct lot access to Meadowbrook Parkway, Newt Drive or US Highway 24

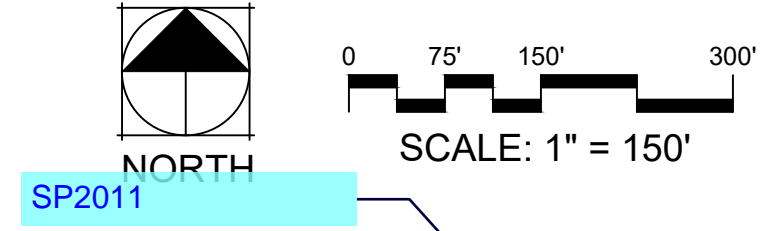
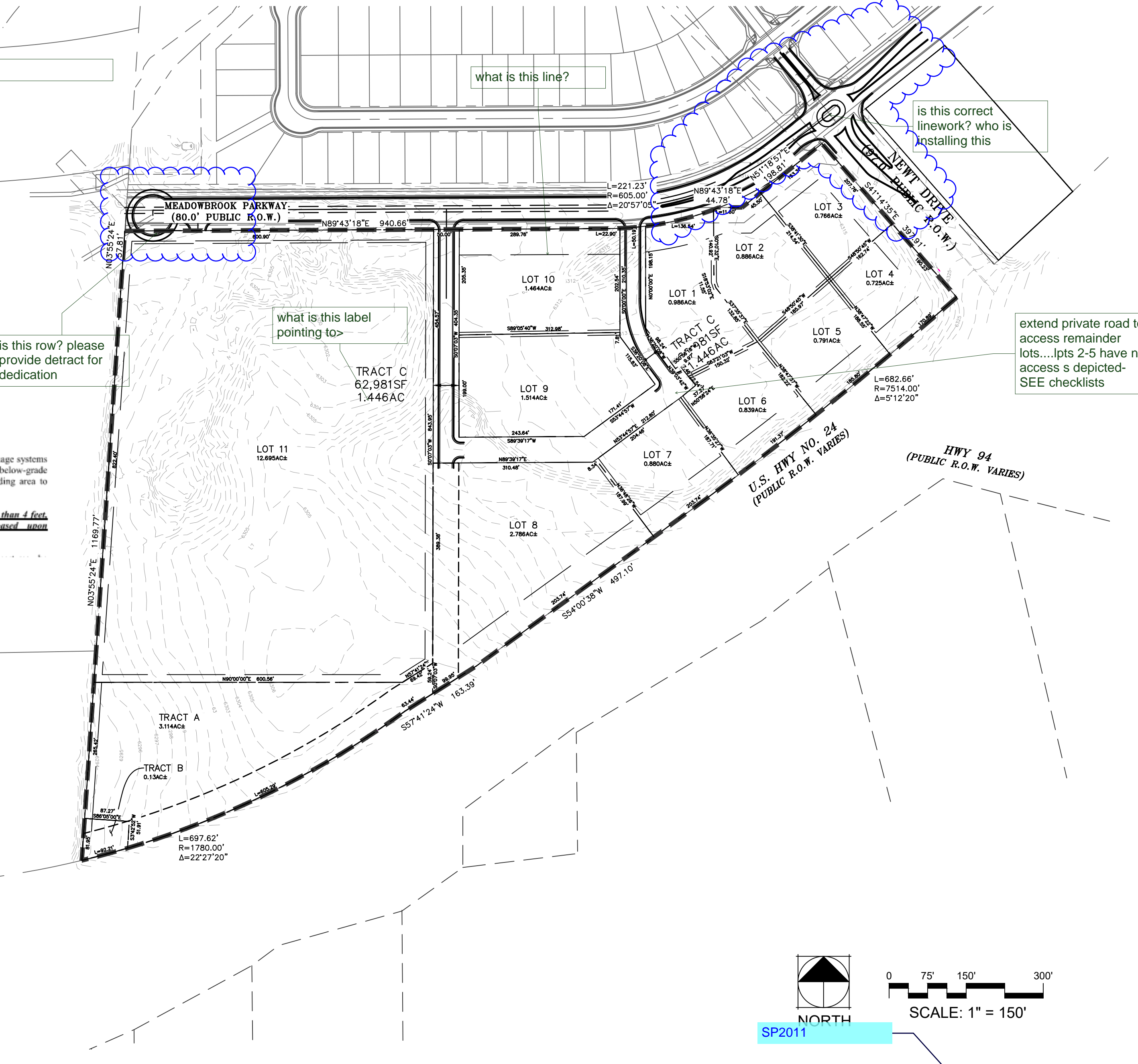
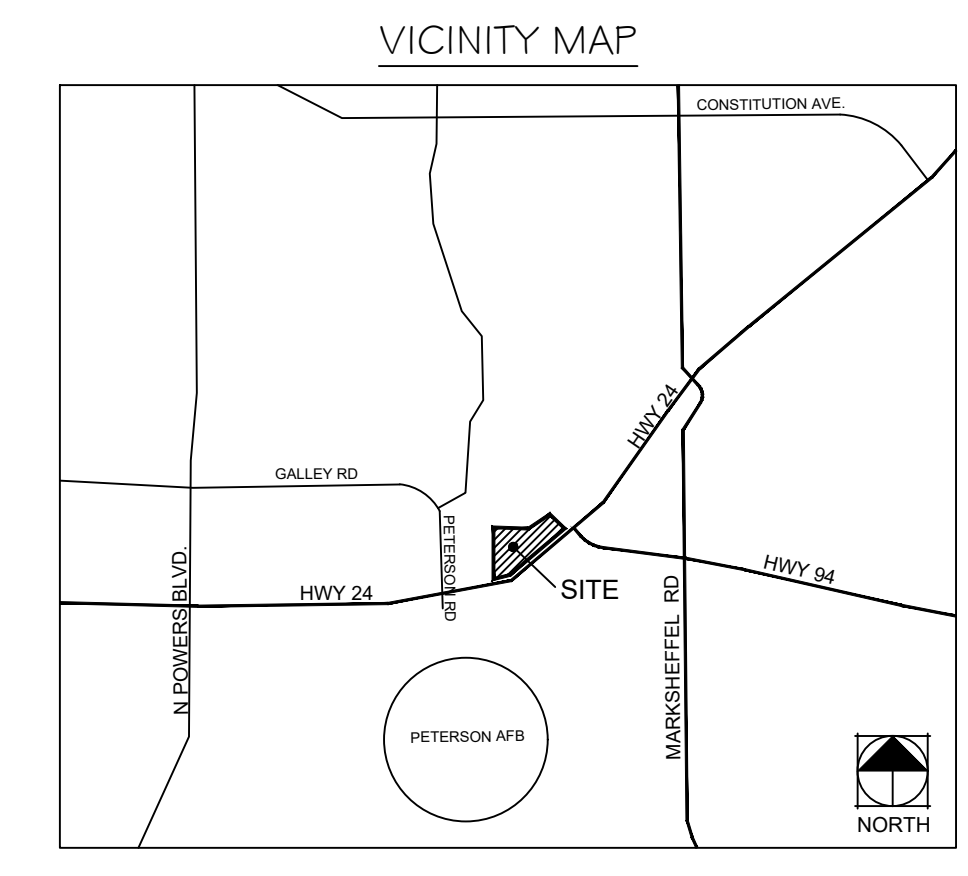
In speaking with owner I thought A PUBLIC OR private road tract to be installed to access all lots... We need to see how lots are being accessed...

provide required mitigation techniques for the residential and com if required...A study addressing noise from HWY 24 is required...Your also in a CAD-O

delete note 25, why are you submitting a landscape plan if developer is not installing perimeter landscape for consistency-

provide preliminary grading plan. Preliminary grading plan to include slope tags along the road centerline and the proposed storm sewer system.

provide preliminary utility layout



PCD FILE NO: XXXXXXXXX

Kimley-Horn

2018 KIMLEY-HORN AND ASSOCIATES, INC.
 2018 NORTH NEVADA AVENUE, SUITE 300
 COLORADO SPRINGS, COLORADO 80903 (719) 453-0180

CROSSROADS AT MEADOWBROOK MIXED USE
 PRELIMINARY PLAN

DESIGNED BY: LMS
 DRAWN BY: LMS
 CHECKED BY: JEH
 DATE: XX/XX/20

PROJECT NO.
096956000
 SHEET
 01 OF 04

BY: DATE: APPR.
 REVISION: NO.

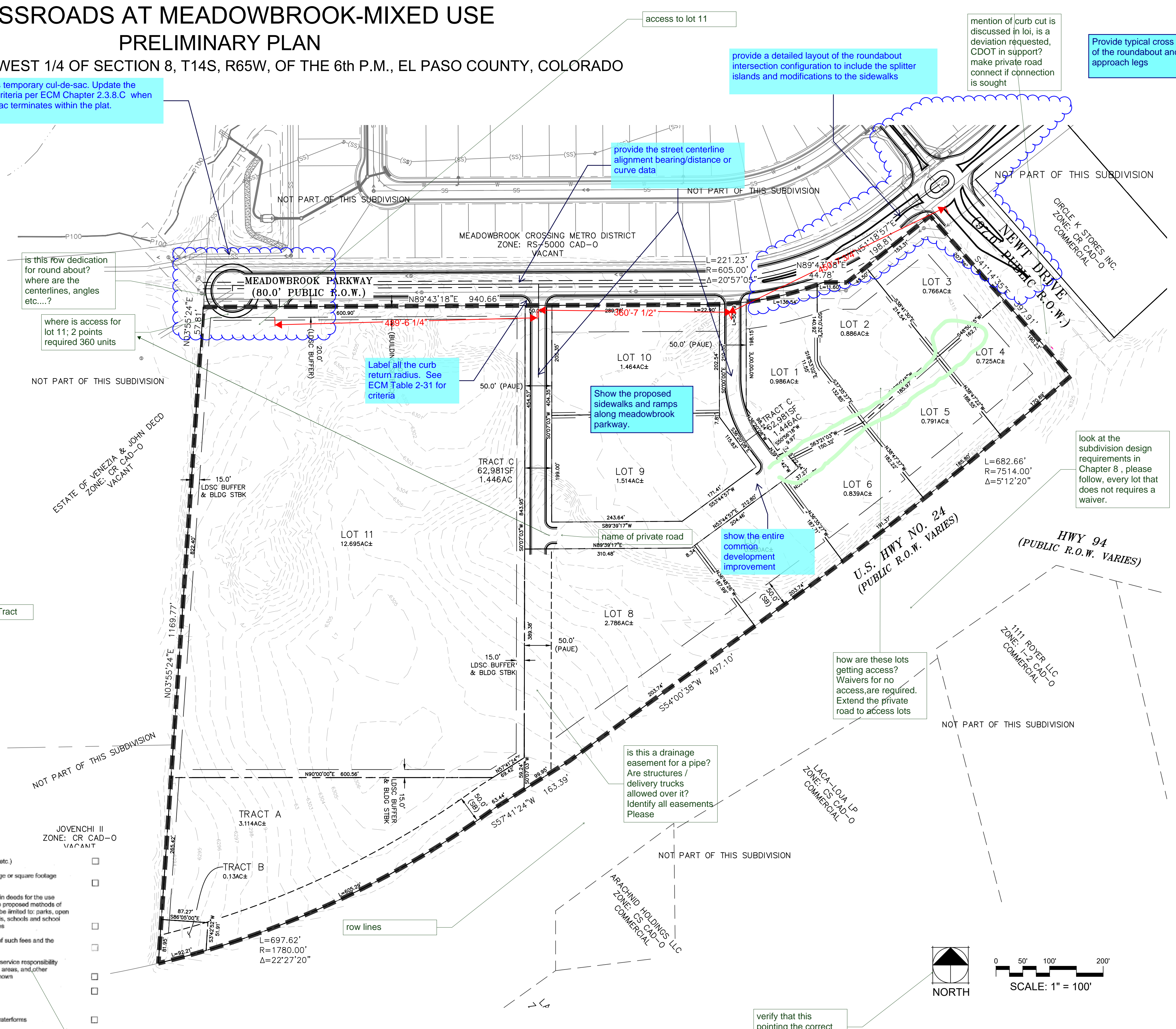
CROSSROADS AT MEADOWBROOK-MIXED USE PRELIMINARY PLAN

LOCATED IN THE SOUTHWEST 1/4 OF SECTION 8, T14S, R65W, OF THE 6th P.M., EL PASO COUNTY, COLORADO

LOT TABLE		
LOT NO.	SF/AC±	USE
1	42,957SF± / 0.986AC±	COMMERCIAL
2	38,580SF± / 0.886AC±	COMMERCIAL
3	33,348SF± / 0.766AC±	COMMERCIAL
4	31,582SF± / 0.725AC±	COMMERCIAL
5	34,445SF± / 0.791AC±	COMMERCIAL
6	36,566SF± / 0.839AC±	COMMERCIAL
7	38,339SF± / 0.880AC±	COMMERCIAL
8	121,349SF± / 2.786AC±	COMMERCIAL
9	65,950SF± / 1.514AC±	COMMERCIAL
10	63,729SF± / 1.464AC±	COMMERCIAL
11	553,005SF± / 12.695AC±	MULTI-FAMILY

TRACT TABLE			
TRACT	SIZE SF/ACRES±	OWNERSHIP & MAINTENANCE	USE
A	135,632SF / 3.114 AC±	MCMD #1	PARK/OPEN SPACE/DRAINAGE/PUBLIC IMPROVEMENT/PUBLIC UTILITY/LANDSCAPING/SIDEWALKS & PEDESTRIAN EASEMENT
B	5,821SF / 0.130 AC±	MCMD #1	OPEN SPACE/DRAINAGE/PUBLIC IMPROVEMENT/PUBLIC UTILITY/LANDSCAPING/SIDEWALKS & PEDESTRIAN EASEMENT
C	62,981SF / 1.450 AC±	MCMD #1	VEHICULAR ACCESS/DRAINAGE/PUBLIC IMPROVEMENT/PUBLIC UTILITY/LANDSCAPING/SIDEWALKS & PEDESTRIAN EASEMENT
TOTAL	204,434SF / 4.694 AC±		

MCMD #1 = MEADOWBROOK CROSSING METROPOLITAN DISTRICT NO. 1



Label as temporary cul-de-sac. Update the design criteria per ECM Chapter 2.3.8.C when cul-de-sac terminates within the plat.

provide a detailed layout of the roundabout intersection configuration to include the splitter islands and modifications to the sidewalks

mention of curb cut is discussed in loi, is a deviation requested, CDOT in support? make private road connect if connection is sought

Provide typical cross section of the roundabout and the approach legs

provide the street centerline alignment bearing/distance or curve data

is this row dedication for round about? where are the centerlines, angles etc....?

where is access for lot 11; 2 points required 360 units

Label all the curb return radius. See ECM Table 2-31 for criteria

Show the proposed sidewalks and ramps along meadowbrook parkway.

show the entire common area development improvement

look at the subdivision design requirements in Chapter 8, please follow, every lot that does not requires a waiver.

is that the correct district? That's not was recently approved.

add all items to each sheet- so much is missing! A 3rd review fee will apply- Quality control submittal please.

is this a drainage easement for a pipe? Are structures / delivery trucks allowed over it? Identify all easements Please

how are these lots getting access? Waivers for no access, are required. Extend the private road to access lots

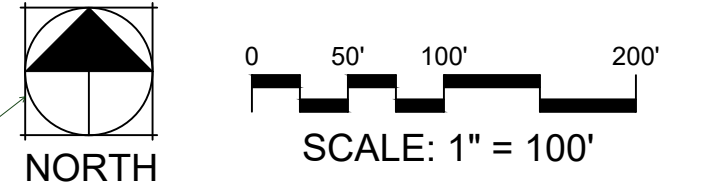
verify that this pointing the correct way

gas electric provided by?

Mr. Lerry 11/19/2020 5:09 PM

- Scale Adequate to represent the information (1"=200' preferred)
- Size Maximum Size forty-two (42') on one side
- Contours One acre or less lot size two foot contours (2')
Larger than one acre five foot contours (5')
Accuracy to be no less than 1/4 contour interval
Mountainous Topography/High Relief twenty foot contours (20')
Contours extended no less than 100' onto adjacent property?
- General Information Required on Plan**
- Subdivision Name
 - General Legal Description by 1/4 Section, Section, Township, Range
 - Legal includes approximate survey tie to an accepted survey monument?
 - Name & address of person/corporation/organization preparing preliminary plan
 - Name & address of subdivider
 - Name & address of property owners, within, surrounded by, and adjacent to the subdivision including identification of all platted subdivisions by name
 - Date of preparation, north point, written and graphic scale
 - Vicinity map to locate tract - map to show relationship to existing major thoroughfares
 - Boundary lines of the subdivision showing approximate length of lines
 - Approximate location, rights of way, width, surfacing, functional classification, and names of existing and proposed maintained and dedicated public and private streets
 - Approximate length of street centerlines, radii of curves, centerline grades, and type of curb, gutter and sidewalk
 - Approximate location, length, width and use of all existing and proposed easements, utility rights of way, major utility facilities, intersections, bridges, culverts, and drainageways. Indicate underground facilities
 - Approximate location, length, width and type of all non-thoroughfare transportation
- 1 links (e.g. paths, bikeways, trails, PRT, guideways, railroads, etc.)
- 1 Approximate layout, dimensions, angles, land use, and acreage or square footage of each lot

- 1 links (e.g. paths, bikeways, trails, PRT, guideways, railroads, etc.)
- 1 Approximate layout, dimensions, angles, land use, and acreage or square footage of each lot
- 1 The approximate location of land to be conveyed or reserved in deeds for the use of all property owners, residents, or the general public and the proposed methods of dedication and maintenance of such lands; to include but not be limited to: parks, open space, public streets and thoroughfares, bikeways, paths, trails, schools and school sites, public utilities, and community and social service facilities
- 1 If fees are to be paid in lieu of land, the approximate amount of such fees and the concurrence of the parties involved are to be shown
- 1 Notes to indicate disposition, maintenance responsibility, and service responsibility (suppliers) for water and sanitation, energy supplies, common areas, and other services and areas which will serve the community must be shown
- Approximate location of existing and proposed structures
- Approximate location of:
 - (1) Watercourses, existing bodies of water, and other waterforms
 - (2) Sites of historic or archaeological significance as inventoried in the El Paso County Historic Preservation Plan
 - (3) Identified and inventoried sites of natural or scenic importance

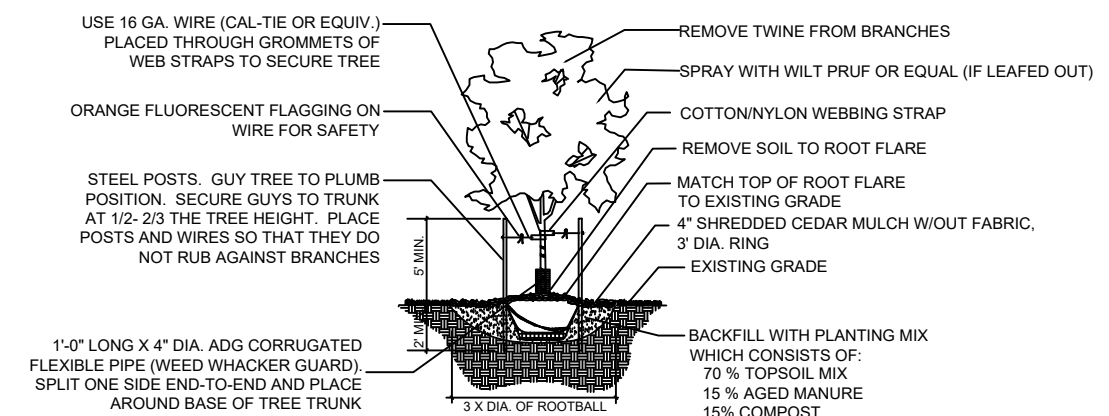


PCD FILE NO: XXXXXXXXX

PROJECT NO. 096956000
 SHEET 02 OF 04
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 DRAWN BY: LMS
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 DATE: XX/XX/20
 Kimley»Horn
 2018 KIMLEY-HORN AND ASSOCIATES, INC.
 300 NORTH NEWHOPE AVE., SUITE 300
 COLORADO SPRINGS, COLORADO 80903 (719) 453-0180
 REVISION NO. BY DATE APPR.

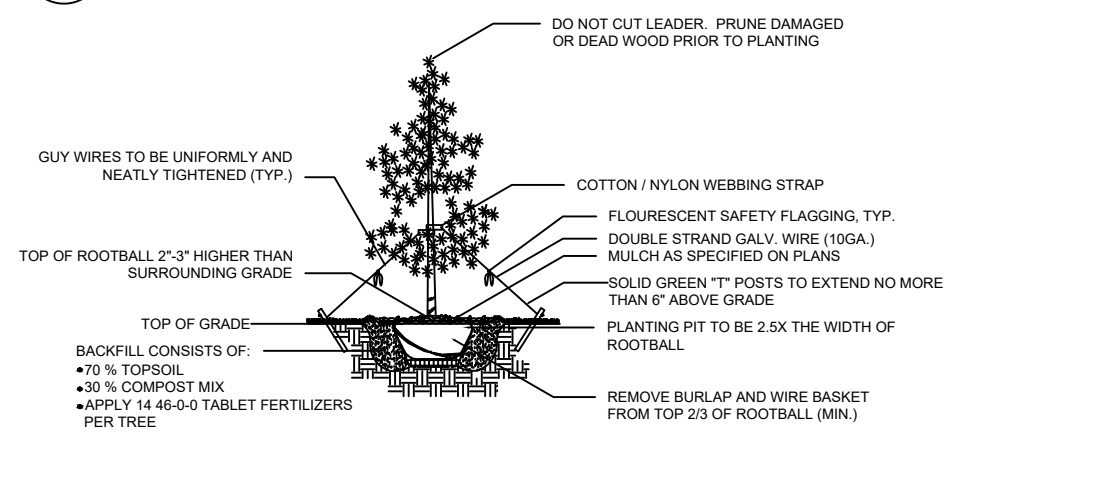
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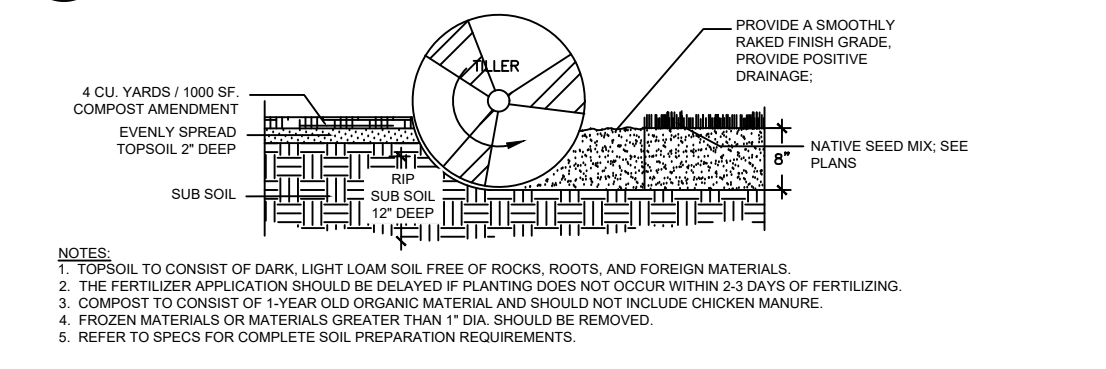


NOTES:
1. DEPTH SO THAT TOP OF THE ROOT FLARE IS LEVEL WITH SURROUNDING GRADE. THE ROOT FLARE SHOULD DETERMINE THE DEPTH OF THE PIT, NOT THE TOP OF THE ROOT BALL.
2. THE BALL OF TREE SHOULD SIT ON UNDISTURBED SOIL.
3. CUT BOTTOM OF BASKET. REMOVE ALL SOIL FROM ROOT BALL TO TOP OF ROOT FLARE. SET TREE AND REMOVE ENTIRE BASKET AFTER CORRECT PLACEMENT. REMOVE BURLAP FROM THE SIDE OF THE ROOT BALL. REMOVE ALL TWINE FROM AROUND THE TRUNK AND BACKFILL.

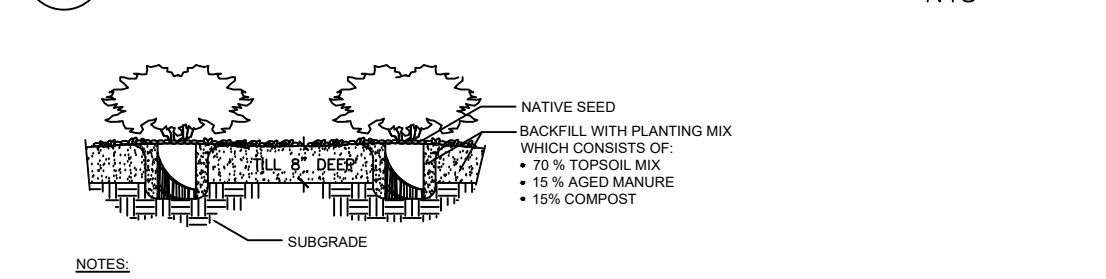
1 DECIDUOUS TREE PLANTING PLANTING DETAIL



2 EVERGREEN TREE PLANTING PLANTING DETAIL



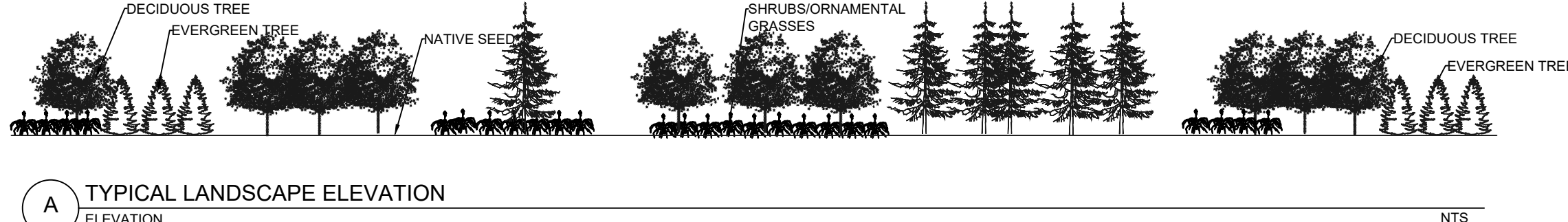
3 SOIL PREP- NATIVE SEED AREAS PLANTING DETAIL



NOTES:
1. CONTRACTOR TO WATER PLANT MATERIAL DAILY UNTIL AN AUTOMATIC IRRIGATION SYSTEM IS OPERATIONAL OR OTHER IRRIGATION PROVIDED.
2. THE FABRIC GRADGES SHALL PROVIDE POSITIVE DRAINAGE AWAY FROM WALLS AND BUILDINGS.
3. THE TYPING OF PLANTING REQS AND PLACEMENT OF BACKFILL IS TO OCCUR JUST PRIOR TO PLANTING. THEREAFTER PROTECTION FROM COMPACTION AND CONSTRUCTION TRAFFIC SHOULD BE PROVIDED.

4 SHRUB DETAIL PLANTING DETAIL

LANDSCAPE MATERIAL SCHEDULE		NOTES
COBBLE MULCH	PER PLAN	COBBLE TO BE 1/2" WHOLE RIVER ROCK INSTALLED TO A DEPTH OF 3" OVER WEED BARRIER FABRIC. SUBMIT SAMPLE FOR OWNER REPRESENTATIVE APPROVAL PRIOR TO INSTALLATION.
NATIVE SEED	PER PLAN	NATIVE SEED



DECIDUOUS TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	HEIGHT	WIDTH	SIZE	NOTES
	PV	TBD	Pinus virginiana	Canada Chokecherry	20-30'	15-20'	2.0' Caliper	B&B
	QM	TBD	Quercus macrocarpa	BUR OAK	30-35'	30-35'	2.0' Caliper	B&B
	AG	TBD	Acer glabral 'Flame'	FLAME AMUR MAPLE	15-20'	15-20'	2.0' Caliper	B&B
	CP	TBD	Crataegus phaenogyium	WASHINGTON HAWTHORN	20-25'	15-25'	2.0' Caliper	B&B
EVERGREEN TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	HEIGHT	WIDTH	SIZE	NOTES
	PE	TBD	Pinus edulis	PINYON PINE	15-20'	10-15'	6" HT	B&B
	JS	TBD	Juniperus scopulorum	ROCKY MOUNTAIN JUNIPER	20-25'	8-12'	6" HT	B&B
	PN	TBD	Pinus Nigra	Ponderosa Pine	25-30'	15-20'	6" HT	B&B
SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	HEIGHT	WIDTH	SIZE	NOTES
	YF	TBD	Yucca filamentosa 'Bright Edge'	BRIGHT EDGE YUCCA	2-3'	2-4'	5 GAL.	CONTAINER
	CN	TBD	Chrysothamnus nauseosus var. nauseosus	Baby Blue Rabbitbrush	1-4'	1-4'	5 GAL.	CONTAINER
	BT	TBD	Berberis thunbergii 'Rose Glow'	Rose Glow Japanese Barberry	3-5'	3-5'	5 GAL.	CONTAINER
	BD	TBD	Buddleja davidii nanhoensis 'Mango'	Petite Indigo Butterfly Bush	4-5'	4-5'	5 GAL.	CONTAINER
	JA	TBD	Jamnesia americana	Waxflower	4-5'	5-8'	5 GAL.	CONTAINER
	PA	TBD	Perovskia atriplicifolia 'Little Spire'	Little Spire Russian Sage	2-3'	2-3'	5 GAL.	CONTAINER

STREET NAME OR ZONE BOUNDARY:	HIGHWAY 24	MARKSCHEFFEL RD	AIR LANE	PRO OMNIBUS HT.	SEMPER SUPRA PT.	NOX ALBUM VW.
STREET CLASSIFICATION:	EXPRESSWAY	MAJOR ARTERIAL	NON ARTERIAL	NON ARTERIAL	NON ARTERIAL	NON ARTERIAL
SETBACK DEPTH REQUIRED/PROVIDED:	25' / 25'	25' / 25'	25' / 25'	107' / 10'	107' / 10'	107' / 10'
LINEAR FOOTAGE:	2542'	1870'	1593'	1476'	328'	535'
TREE/FEET REQUIRED:	1 / 20'	1 / 30'	1 / 30'	1 / 30'	1 / 30'	1 / 30'
NUMBER OF TREES REQUIRED/PROVIDED:	127 / 127	80 / 80	13 / 13	40 / 40	40 / 40	40 / 40
PLANT ABBREVIATION DENOTED ON PLAN:	HB	MB	SB	PB	PB	PB
% GROUND PLANE VEG. REQUIRED/PROVIDED:	75 / 75%	75 / 75%	75 / 75%	75 / 75%	75 / 75%	75 / 75%

- ### GENERAL NOTES
- CONTRACTOR SHALL WARRANTY ALL PLANT MATERIAL FOR ONE YEAR AFTER SUBSTANTIAL COMPLETION.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF ALL PLANT MATERIALS IN A HEALTHY STATE DURING CONSTRUCTION AND THROUGH FINAL COMPLETION AS PART OF PLANTING COST. ANY DAMAGE TO PLANT MATERIAL SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE. AFTER FINAL COMPLETION, OWNER SHALL BE RESPONSIBLE FOR PLANT MAINTENANCE.
 - PLANT QUANTITIES ARE PROVIDED FOR CONTRACTOR'S CONVENIENCE ONLY AND SHALL BE VERIFIED BY CONTRACTOR BY REVIEWING PLANTING PLAN SYMBOLS AND GENERATING SEPARATE PLANT LIST.
 - ALL DECIDUOUS TREES SHALL BE WRAPPED IN THE FALL WITH STRETCHABLE TREE WRAP IN ACCORDANCE WITH ACCEPTED HORTICULTURAL PRACTICE; WRAP SHALL BE REMOVED NO LATER THAN MAY 1 OF THE FOLLOWING SPRING. INSTALL WOOD MULCH UNDER EACH TREE; REFER TO TREE PLANTING DETAIL. WOOD MULCH SHALL BE SHREDDED WOOD MULCH.
 - TREE PLANTING SOIL AMENDMENT AND FERTILIZER REQUIREMENTS: BACKFILL SOIL WITHIN EACH TREE PIT SHALL CONSIST OF 30% COMPOST AND 70% EXISTING SOIL.
 - COBBLE MULCH AREAS: INSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS PRIOR TO PLACING GRAVEL MULCH; INSTALL WEED BARRIER FABRIC UNDER ALL GRAVEL MULCH; ROLL FABRIC WITHOUT STRETCHING OR PULLING; OVERLAP FABRIC EDGES A MINIMUM OF 6". STAKE FABRIC PER MANUFACTURER'S SPECIFICATIONS. COBBLE (2"-4" WHOLE RIVER ROCK) SHOULD BE PLACED IN DESIGNATED AREA AS SHOWN ON PLANS AT A 4" DEPTH; ENSURE WEED BARRIER FABRIC IS NOT VISIBLE. SUBMIT SAMPLE FOR OWNER REPRESENTATIVE APPROVAL PRIOR TO INSTALLATION.
 - ALL PLANT MATERIALS SHALL HAVE BACKFILL CAREFULLY PLACED AROUND BASE AND SIDES OF BALL TO TWO-THIRDS (2/3) DEPTH OF BALL, THEN THOROUGHLY SOAKED WITH WATER TO ALLOW SETTLEMENT. ALL WIRE, BURLAP FASTENERS AND LOOSE BURLAP AROUND BASE OF TRUNK SHALL BE REMOVED AT THIS TIME. REMAINDER OF PIT SHALL THEN BE BACKFILLED, ALLOWING FOR DEPTH OF MULCH, SAUCER AND SETTLEMENT OF BACKFILL. BACKFILL SHALL THEN BE THOROUGHLY WATERED ONCE AGAIN.
 - LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING EXISTING EROSION CONTROL MEASURES DURING HIS DURATION OF WORK ON-SITE.
 - LANDSCAPE CONTRACTOR IS RESPONSIBLE TO PROVIDE EROSION CONTROL BLANKET ON ALL SLOPES.
 - ALL AREAS DISTURBED DURING CONSTRUCTION TO BE RESEED.
 - CONTRACTOR SHALL PROVIDE ALL DEVICES AND LABOR NEEDED TO ESTABLISH NATIVE SEED.
 - ALL TREES SHALL BE IRRIGATED VIA A DRIP SYSTEM. FINAL DESIGN AND P.O.C WILL BE IDENTIFIED FINAL BUILDING PERMIT.
 - LANDSCAPE DETAILS PROVIDED FOR STREETScape LANDSCAPE AND BUFFERING REQUIRED FOR SUBDIVISION IMPACTS. INDIVIDUAL LOTS SHALL BE RESPONSIBLE FOR DEVELOPING AND IMPLEMENTING A LANDSCAPE PLAN TO MEET INDIVIDUAL LOT REQUIREMENTS. MAINTENANCE OF LANDSCAPE ELEMENTS AUTHORIZED BY THIS PRELIMINARY PLAN SHALL BE RE MAINTENANCE RESPONSIBILITY OF THE SPECIAL DISTRICT AND/OR BUSINESS OWNERS ASSOCIATION. INDIVIDUAL LOT LANDSCAPING SHALL BE RHE RESPONSIBILITY OF THE INDIVIDUAL LOT OWNERS.

wrong plan - resubmit for this prelim and a review will occur

RS 5000 buffer required

NO.	REVISION	DATE	APPR.

Kimley»Horn
2018 KIMLEY-HORN AND ASSOCIATES, INC.
100 NORTH NEWARK AVE., SUITE 300
COLORADO SPRINGS, COLORADO 80903 (719) 453-0180

DESIGNED BY: LMS
DRAWN BY: LMS
CHECKED BY: JEH
DATE: XX/XX/20

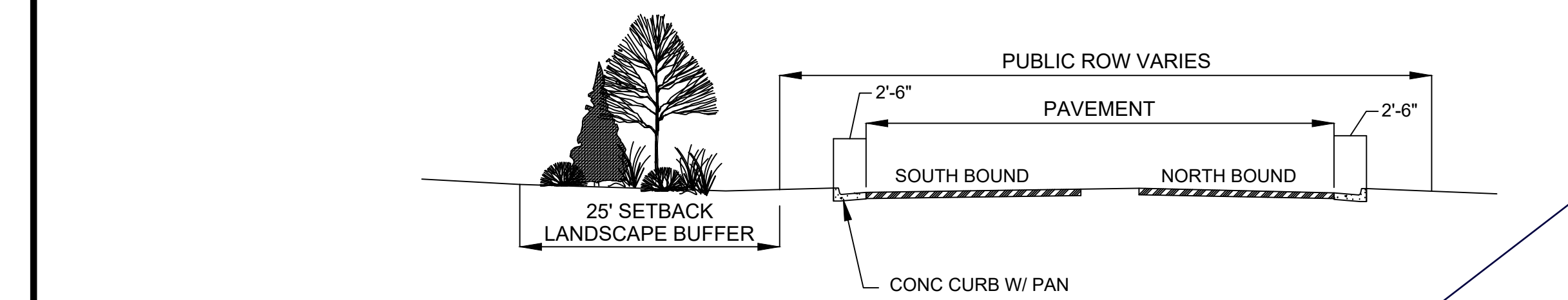
CROSSROADS AT MEADOWBROOK MIXED USE
PRELIMINARY PLAN

PROJECT NO.
096956000
SHEET
03 OF 04

PCD FILE NO: XXXXXXXX

CROSSROADS AT MEADOWBROOK-MIXED USE PRELIMINARY PLAN

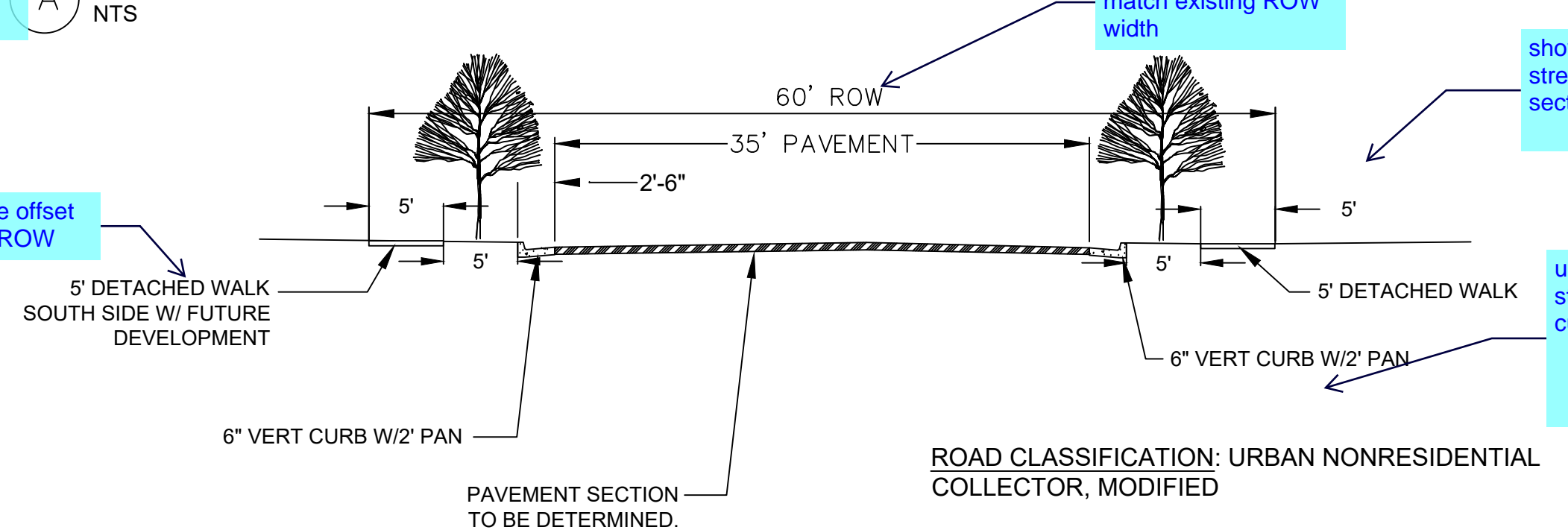
LOCATED IN THE SOUTHWEST 1/4 OF SECTION 8, T14S, R65W, OF THE 6th P.M., EL PASO COUNTY, COLORADO



Update road classification of highway 24. See Exhibit Map 14 in the EPC Major Transportation Corridor Plan (MTCP)

Update to identify as "Street cross section and landscape detail" Or provide a separate street cross section detail.

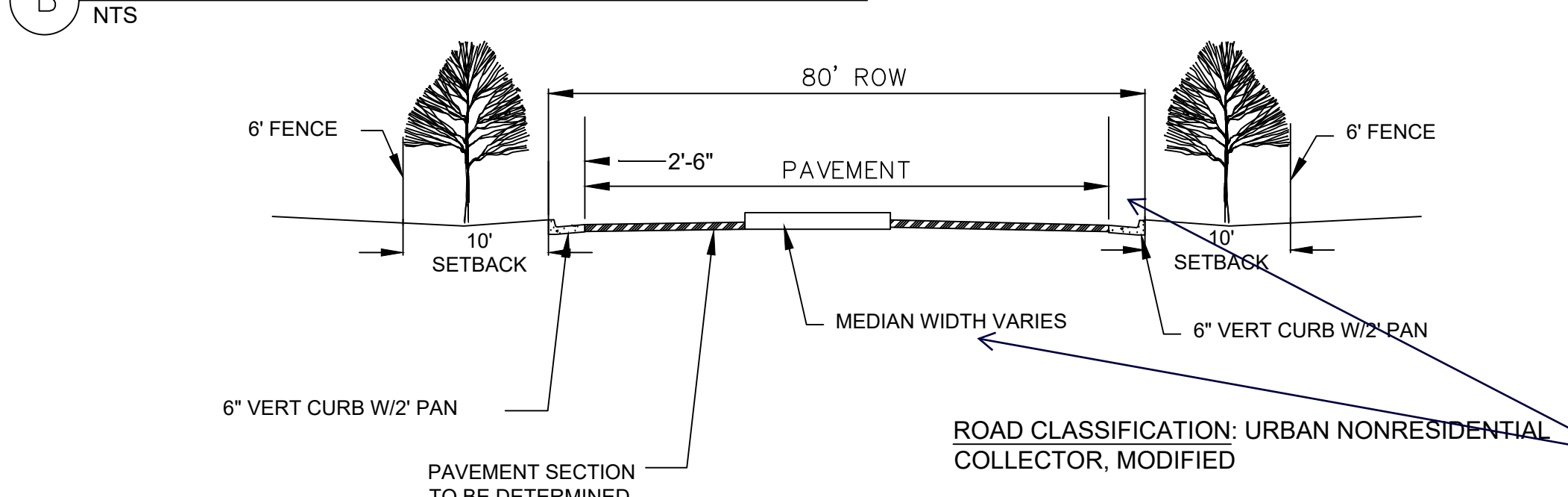
A STREET LANDSCAPE DETAIL "A" - HIGHWAY 24



sidewalk to be offset 0.5' from the ROW

update C&G callout and identify the standard EPC curb type (ex: 6" vert curb w/ 2' pan is a EPC Type A C&G)

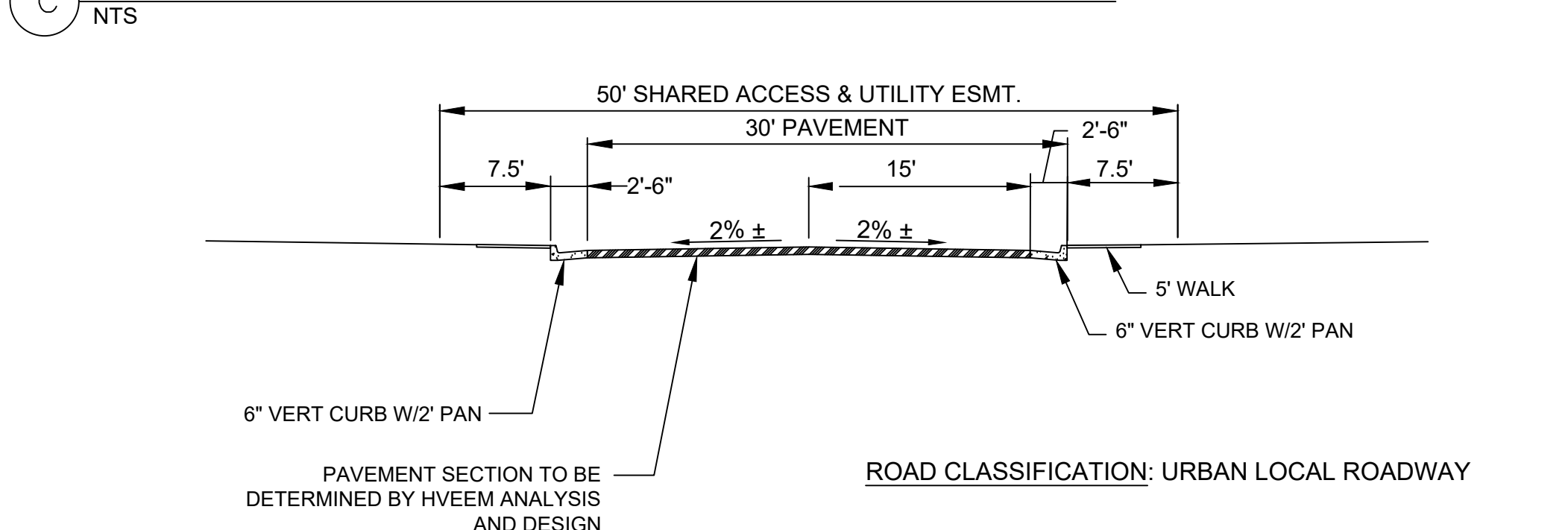
B STREET LANDSCAPE DETAIL "B" - NEWT DRIVE



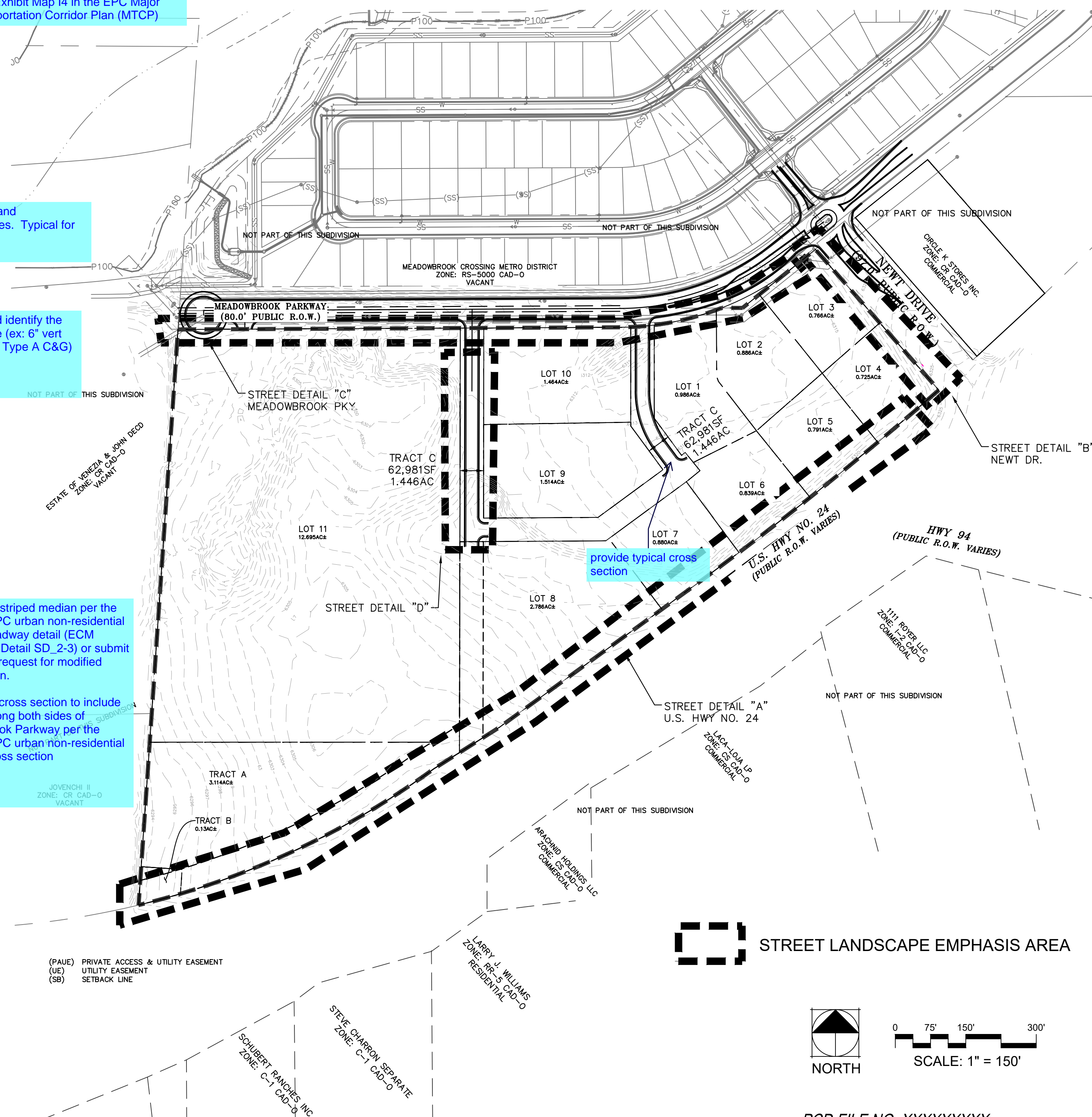
Revise to a striped median per the standard EPC urban non-residential collector roadway detail (ECM Appendix F Detail SD_2-3) or submit a deviation request for modified cross section.

Update the cross section to include sidewalk along both sides of Meadowbrook Parkway per the standard EPC urban non-residential collector cross section

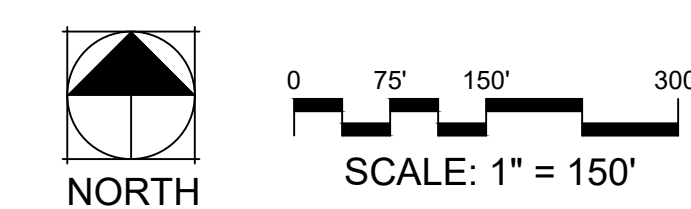
C STREET LANDSCAPE DETAIL "C" - MEADOWBROOK PARKWAY



D TYPICAL STREET LANDSCAPE DETAIL "D" - PRIVATE EASEMENT



STREET LANDSCAPE EMPHASIS AREA



PCD FILE NO: XXXXXXXXX

NO.	REVISION	BY	DATE	APPR.

Kimley»Horn
2018 KIMLEY-HORN AND ASSOCIATES, INC.
900 NORTH NEWLAND AVE.
COLORADO SPRINGS, COLORADO 80903 (719) 453-0180

DESIGNED BY: LMS
DRAWN BY: LMS
CHECKED BY: JEH
DATE: XX/XX/20

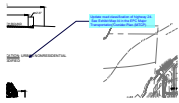
CROSSROADS AT MEADOWBROOK MIXED USE
PRELIMINARY PLAN

PROJECT NO.
096956000
SHEET
04 OF 04

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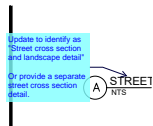
Prelim Plan V_1 engr comments.pdf Markup Summary

dsdlaforce (27)



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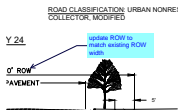
Update road classification of highway 24. See Exhibit Map I4 in the EPC Major Transportation Corridor Plan (MTCP)



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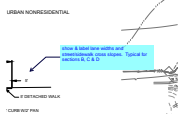
Update to identify as "Street cross section and landscape detail"

Or provide a separate street cross section detail.



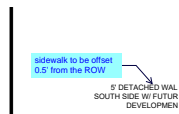
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update ROW to match existing ROW width



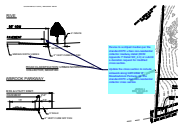
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show & label lane widths and street/sidewalk cross slopes. Typical for sections B, C & D



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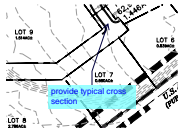
sidewalk to be offset 0.5' from the ROW



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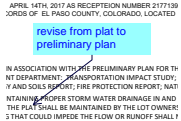
Revise to a striped median per the standard EPC urban non-residential collector roadway detail (ECM Appendix F Detail SD_2-3) or submit a deviation request for modified cross section.

Update the cross section to include sidewalk along both sides of Meadowbrook Parkway per the standard EPC urban non-residential collector cross section



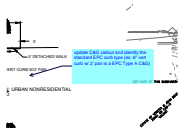
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provide typical cross section



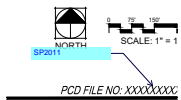
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revise from plat to preliminary plan



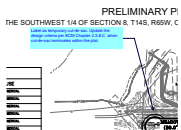
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update C&G callout and identify the standard EPC curb type (ex: 6" vert curb w/ 2' pan is a EPC Type A C&G)



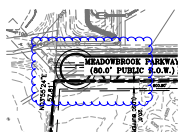
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SP2011

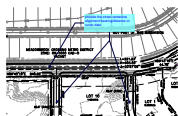


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Label as temporary cul-de-sac. Update the design criteria per ECM Chapter 2.3.8.C when cul-de-sac terminates within the plat.

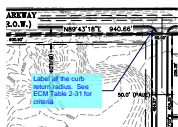


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provide the street centerline alignment bearing/distance or curve data

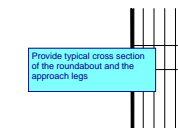


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Label all the curb return radius. See ECM Table 2-31 for criteria

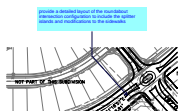


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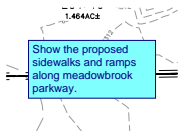
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Provide typical cross section of the roundabout and the approach legs



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Date: 1/22/2021 10:09:27 AM
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provide a detailed layout of the roundabout intersection configuration to include the splitter islands and modifications to the sidewalks



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Date: 1/22/2021 10:10:38 AM
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Show the proposed sidewalks and ramps along meadowbrook parkway.



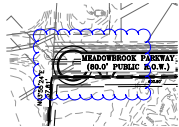
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provide preliminary grading plan. Preliminary grading plan to include slope tags along the road centerline and the proposed storm sewer system.



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provide preliminary utility layout



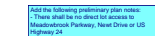
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provide preliminary utility layout



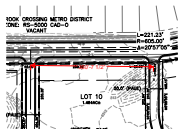
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provide preliminary utility layout



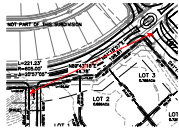
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Add the following preliminary plan notes:
 - There shall be no direct lot access to Meadowbrook Parkway, Newt Drive or US Highway 24



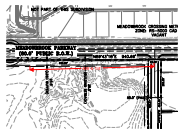
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Date: 1/25/2021 10:17:24 AM
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360'-7 1/2"



Subject: Length Measurement
Page Label: [2] 096956011-PP - PRELIM
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Author: dsdlaforce
Date: 1/25/2021 10:17:31 AM
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Color: ■
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Space:

450'-7 3/4"



Subject: Length Measurement
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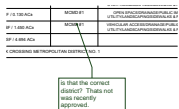
489'-6 1/4"



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show the entire common development improvement

dsdparsons (53)



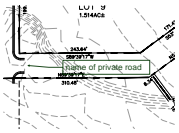
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is that the correct district? That's not was recently approved.



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Private Road Tract



Subject: Callout
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name of private road

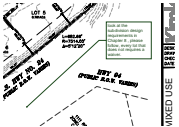


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how are these lots getting access? Waivers for no access, are required. Extend the private road to access lots

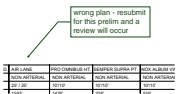


Subject: Image
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:24:32 PM
Status:
Color: ■
Layer:
Space:



Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:25:34 PM
Status:
Color: ■
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look at the subdivision design requirements in Chapter 8 , please follow, every lot that does not requires a waiver.



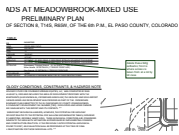
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Page Label: [3] 096956011-PP - NOTES
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:26:54 PM
Status:
Color: ■
Layer:
Space:

wrong plan - resubmit for this prelim and a review will occur



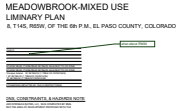
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Page Label: [3] 096956011-PP - NOTES
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:27:12 PM
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Color: ■
Layer:
Space:

RS 5000 buffer required



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:28:56 PM
Status:
Color: ■
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delete these bldg setbacks; front is where access is taken from at a lot by lot case



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:29:09 PM
Status:
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Space:

what about RM30



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:34:56 PM
Status:
Color: ■
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provide required mitigation techniques for the residential and com if required...A study addressing noise from HWY 24 is required...Your also in a CAD-O



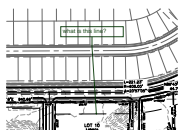
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Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:37:24 PM
Status:
Color: ■
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extend private road to access remainder lots....lpts 2-5 have no access s depicted- SEE checklists



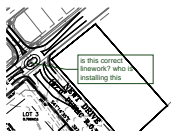
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Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:38:32 PM
Status:
Color: ■
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what is this label pointing to>



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:38:56 PM
Status:
Color: ■
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what is this line?



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:39:26 PM
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Color: ■
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is this correct linework? who is installing this



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:39:53 PM
Status:
Color: ■
Layer:
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note 19 says its not?



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:41:24 PM
Status:
Color: ■
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delete note 25, whya re you submitting a landscape plan if developer is not installing perimeter landscape for consistency-



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:42:09 PM
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In speaking with owner I thought A PUBLIC OR private road tract to be installed to access all lots..., We need to see how lots are being accessed...



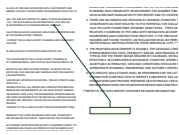
Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:42:38 PM
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Color: ■
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? IS IT IN THAT DISTRICT?



Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:43:08 PM
Status:
Color: ■
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DETAILS, AND SHOW ON PRELIM PLAN, CONSTRUCTED BY WHOM?

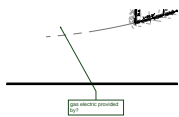


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Date: 1/19/2021 3:43:22 PM
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Color: ■
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Subject: Callout
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:45:24 PM
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Lots 1-10 ACCESS have NO access to Newt, Hwy 24, Meadowbrook parkway. Lot 11 has one access to Meadowbrook, no access to Hwy 24.



Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:46:03 PM
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gas electric provided by?

1

Subject: Text Box
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:46:32 PM
Status:
Color: ■
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1

Subject: Text Box
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Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:46:38 PM
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Color: ■
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1

Subject: Text Box
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Date: 1/19/2021 3:46:40 PM
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1

Subject: Text Box
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Date: 1/19/2021 3:46:41 PM
Status:
Color: ■
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1

Subject: Text Box
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:46:43 PM
Status:
Color: ■
Layer:
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1

Subject: Text Box
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Date: 1/19/2021 3:46:44 PM
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Color: ■
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1

Subject: Text Box
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Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:47:02 PM
Status:
Color: ■
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1

1

Subject: Text Box
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Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:47:05 PM
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Color: ■
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1

1

Subject: Text Box
Page Label: [2] 096956011-PP - PRELIM
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Date: 1/19/2021 3:47:09 PM
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1

1

Subject: Text Box
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Author: dsdparsons
Date: 1/19/2021 3:47:11 PM
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1

1

Subject: Text Box
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Date: 1/19/2021 3:47:16 PM
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1

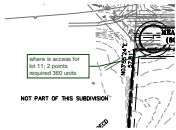


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Status:
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1

Subject: Text Box
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1

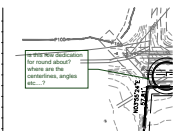


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Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:49:10 PM
Status:
Color: ■
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where is access for lot 11; 2 points required 360 units

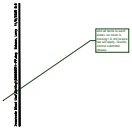


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Page Label: [2] 096956011-PP - PRELIM
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Date: 1/19/2021 3:49:21 PM
Status:
Color: ■
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Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:50:24 PM
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Color: ■
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is this row dedication for round about? where are the centerlines, angles etc....?



Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:51:42 PM
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add all items to each sheet- so much is missing !
 A 3rd review fee will apply- Quality control
 submittal please.

1

Subject: Text Box
Page Label: [2] 096956011-PP - PRELIM
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Author: dsdparsons
Date: 1/19/2021 3:52:17 PM
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1

1

Subject: Text Box
Page Label: [2] 096956011-PP - PRELIM
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1

0:XXXXXXXXXX

Subject: Owner Certification
Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:53:23 PM
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Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:54:25 PM
Status:
Color: ■
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verify that this pointing the correct way



Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/19/2021 3:54:42 PM
Status:
Color: ■
Layer:
Space:

row lines

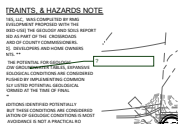


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Date: 1/20/2021 7:20:19 AM
Status:
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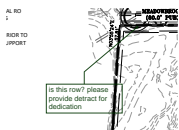
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Page Label: [1] 096956011-PP - COVER
Lock: Unlocked
Author: dsdparsons
Date: 1/20/2021 7:21:10 AM
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Color: ■
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These notes are from the recommendations page in report for this project to be added



Subject: Callout
Page Label: [1] 096956011-PP - COVER
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Author: dsdparsons
Date: 1/20/2021 7:21:17 AM
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?



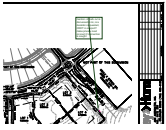
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Date: 1/20/2021 7:21:49 AM
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Color: ■
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is this row? please provide detract for dedication



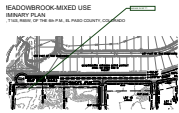
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Lock: Unlocked
Author: dsdparsons
Date: 1/20/2021 7:22:37 AM
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G



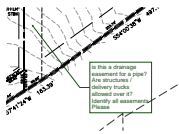
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Lock: Unlocked
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Date: 1/20/2021 7:23:59 AM
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mention of curb cut is discussed in loi, is a deviation requested, CDOT in support? make private road connect if connection is sought



Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/20/2021 7:24:19 AM
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access to lot 11



Subject: Callout
Page Label: [2] 096956011-PP - PRELIM
Lock: Unlocked
Author: dsdparsons
Date: 1/20/2021 7:31:06 AM
Status:
Color: ■
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is this a drainage easement for a pipe? Are structures / delivery trucks allowed over it? Identify all easements Please